



**Agenda
Minnetonka City Council
Regular Meeting
Monday, Jan. 30, 2023
6:30 p.m.
Council Chambers**

1. Call to Order
2. Pledge of Allegiance
3. Roll Call: Schaeppi-Coakley-Kirk-Schack-Wilburn-Calvert-Wiersum
4. Approval of Agenda
5. Approval of Minutes:
 - A. Dec. 12, 2022 study session minutes
 - B. Jan. 9, 2023 regular meeting minutes
6. Special Matters: None.
7. Reports from City Manager & Council Members
8. Citizens Wishing to Discuss Matters Not on the Agenda
9. Bids and Purchases:
 - A. Bids for the Minnetonka Community Center project
Recommendation: Reject all bids (4 votes)
10. Consent Agenda - Items Requiring a Majority Vote:
 - A. Resolution for the Smetana Road Trail project
Recommendation: Adopt the resolution and authorize to execute an agreement (4 votes)
 - B. Gleason Lake Road reconstruction project
Recommendation: Approve the updated intersection concept (4 votes)
 - C. Resolution for the Red Circle Drive turn lane improvements project
Recommendation: Adopt the resolution (4 votes)
 - D. 2023 Strategic Profile action steps

Recommendation: Accept the action steps (4 votes)

- E. 2023 general liability insurance and workers' compensation renewals

Recommendation: Authorize renewal of policies as outlined (4 votes)

- F. Approve 2023 study session work plan

Recommendation: Approve work plan (4 votes)

- G. Appointment of hearing officers for administrative citation hearing program

Recommendation: Approve the appointment (4 votes)

11. Consent Agenda - Items Requiring Five Votes:

- A. Applications for renewed precious metal and secondhand dealer licenses for 2023

Recommendation: Approve the license renewals (5 votes)

- B. Conditional use permit and site plan review, with variance and expansion permit for a drop-off and pick-up loop at Groveland Elementary at 17310 Minnetonka Blvd.

Recommendation: Adopt the resolution (5 votes)

12. Introduction of Ordinances:

- A. Items concerning Greystar at 10701 Bren Road East

Recommendation: Introduce the ordinance and refer it to the planning commission (4 votes)

13. Public Hearings:

- A. On-sale intoxicating and Sunday on-sale intoxicating liquor licenses for Novel Spirits, LLC, dba The Social Kitchen & Libations, located at 12411 Wayzata Boulevard

Recommendation: Grant the licenses (5 votes)

14. Other Business:

- A. Concept plan for Marsh Run II Development at 11816 Wayzata Blvd

Recommendation: Provide feedback, no formal action required

- B. Concept plan for Ridgewood Road Villas located at 18116 Ridgewood Road

Recommendation: Provide feedback, no formal action required

15. Appointments and Reappointments:

A. Boards and commissions appointments and reappointments

Recommendation: Approve the appointments and reappointments (4 votes)

16. Closed session to conduct city manager performance evaluation; pursuant to Minnesota Statute § 13D.05, subd. 3(a)

17. Adjournment

**Minutes
City of Minnetonka
City Council Study Session
Monday, December 12, 2022**

Council Present: Deb Calvert, Kissy Coakley, Brian Kirk, Rebecca Schack, Bradley Schaeppi, Kimberly Wilburn, Mayor Brad Wiersum

Staff: Scott Boerboom, Moranda Dammann, Mike Funk, Corrine Heine, Darin Nelson, Will Manchester, Kelly O'Dea, Susan Thomas, John Vance, Julie Wischnack

1. Call to Order

Wiersum called the meeting to order at 6:30 p.m.

2. Introductions/Roll Call

The city staff and councilmembers in attendance introduced themselves. Councilmember Bradley Schaeppi arrived a few minutes after introductions.

3. Strategic Profile

City Manager Mike Funk introduced the topic.

Finance Director Darin Nelson gave a presentation on the "Financial Strength and Operational Excellence" strategic priority.

Police Chief Scott Boerboom gave a presentation on the "Safe and Healthy Community" strategic priority.

Assistant City Planner Susan Thomas gave a presentation on the "Sustainability and Natural Environment" strategic priority.

Community Development Director Julie Wischnack gave a presentation on the "Livable and Well-Planned Development" strategic priority.

Public Works Director Will Manchester gave a presentation on the "Infrastructure and Asset Management" strategic priority.

Assistant City Manager Moranda Dammann gave a presentation on the "Community Inclusiveness" strategic priority.

Councilmembers asked questions and provided feedback.

4. 2023 City Council Study Session work plan

Funk summarized the tentative schedule for 2023 study sessions and solicited council feedback on the work plan.

Councilmembers discussed what other topics they would like to see included in the work plan that are not already.

**Minutes
City of Minnetonka
City Council Study Session
Monday, December 12, 2022**

Councilmembers agreed to review the revised list of additional topics for consideration and provide feedback to Funk by the end of the week on what their top three choices would be to add to the work plan.

Funk stated the final 2023 study session work plan will be brought back to the council for formal approval at the January 9, 2023 regular meeting.

5. 2023 Legislative Breakfast priorities

Mayor Brad Wiersum introduced the topic and summarized the purpose of the event.

Funk stated the event is scheduled for Friday, Jan. 13, and legislators, the League of Minnesota Cities, and Metro Cities have been invited to attend. He then summarized the city's legislative priorities.

Councilmembers asked questions and provided feedback.

6 Study Session logistics

Funk introduced the topic and asked council for feedback on whether study sessions should be broadcast and livestreamed, and whether the minutes for study sessions should be more detailed.

The council requested that the audio recording of study sessions continue to be posted on the website, and to post the presentation slides on the website. The council also agreed that the minutes do not need to be changed.

7. January Study Session – topics and dates

Funk outlined the topics for the January 23 study session.

8. Closed Session. Section 13D.05, subd. 3(c) of the Open Meeting Law, allows the city council to close a meeting to develop an offer to purchase the properties at 15000 and 15208 Minnetonka Blvd.

Wilburn moved, Schack seconded a motion to enter closed session, pursuant to Section 13D.05, subd. 3(c) of the Open Meeting Law, to develop an offer to purchase the properties at 15000 and 15208 Minnetonka Blvd.

Calvert, Coakley, Schack, Schaeppi, Wilburn and Wiersum voted "yes".

Kirk abstained and recused himself from the closed session.

Motion passed. Council entered closed session at 9:24 p.m.

Council reconvened at 10:30 p.m.

**Minutes
City of Minnetonka
City Council Study Session
Monday, December 12, 2022**

5. Adjournment

Wiersum adjourned the meeting at 10:30 p.m.

Respectfully submitted,

Becky Koosman

City Clerk

**Minutes
Minnetonka City Council
Monday, January 9, 2023**

1. Call to Order

Mayor Brad Wiersum called the meeting to order at 6:31 p.m.

2. Pledge of Allegiance

All joined in the Pledge of Allegiance.

3. Roll Call

Council Members, Deb Calvert, Bradley Schaeppi, Kissy Coakley, Brian Kirk, Rebecca Schack, Kimberly Wilburn and Brad Wiersum were present.

4. Approval of Agenda

Calvert moved, Schack seconded a motion to accept the agenda with addenda to Items 14.A, 14.C, and 14.D. All voted "yes." Motion carried.

5. Approval of Minutes:

A. December 19, 2022 regular meeting minutes

Calvert explained she spoke with staff regarding a change to the minutes on page 2.

Calvert moved, Kirk seconded a motion to approve the minutes as amended. All voted "yes." Motion carried.

6. Special Matters:

A. Strategic Profile quarter four/end of year report

City Manager Mike Funk gave the staff report.

Calvert commented she was excited to learn the city was holding monthly meetings with the communities EMS provider in order to address response times. She thanked staff for all of their efforts on the quarter four/end of the year strategic profile report.

Calvert moved, Kirk seconded a motion to accept the report.

Wiersum stated he appreciated all of the council's and staff's disciplined efforts on the strategic profile.

All voted "yes." Motion carried.

7. Reports from City Manager & Council Members

City Manager Mike Funk reported on upcoming city events and council meetings.

Calvert stated she was walking in Big Willow Park last night and she appreciated the fact the trails have been maintained.

Coakley encouraged residents to clean up after their pets throughout the winter months on the city's trails.

Schaeppi wished everyone a Happy New Year. He thanked the city's public works department for all of their efforts during the recent snow event.

Schaeppi announced that he would not be running for reelection this year. He stated it has been an honor for him to serve and noted it has been a great experience for him. He encouraged the residents of Minnetonka to consider running for local government. He explained he was available for any questions residents may have about running for local government.

Wiersum wished everyone a Happy New Year and thanked all of his fellow councilmembers for their service to the community.

8. Citizens Wishing to Discuss Matters not on the Agenda:

John Mirocha, 5423 Maple Ridge Court, updated the council on the Friends of Minnetonka Parks work for 2022 and discussed the organizations plans for 2023. He reviewed the total number of events and volunteer hours the group worked in 2021 and 2022. He discussed how the group would continue to strengthen communication, coordination and collaboration through their park stewardship monthly meetings.

Wiersum thanked the Friends of Minnetonka Parks for all of their efforts on behalf of the community.

9. Bids and Purchases: None.

10. Consent Agenda – Items Requiring a Majority Vote:

A. Designation of official newspaper for 2023

Schack moved, Wilburn seconded a motion to designate Sun Sailor as the official newspaper. All voted "yes." Motion carried.

B. Resolution amending and replacing the existing conditional use permit for the cemetery use of the properties at 3228 and 3300 Woodlawn Avenue

Schack moved, Wilburn seconded a motion to adopt Resolution 2023-001. All voted "yes." Motion carried.

C. Resolution approving agreement with the Minnesota Department of Transportation

Schack moved, Wilburn seconded a motion to adopt Resolution 2023-002. All voted "yes." Motion carried.

D. Resolution for the 2023 Ridgedale Tower Rehabilitation Project

Schack moved, Wilburn seconded a motion to adopt Resolution 2023-003. All voted "yes." Motion carried.

11. Consent Agenda – Items requiring Five Votes: None.

12. Introduction of Ordinances: None.

13. Public Hearings: None.

14. Other Business:

A. Ordinance establishing licensing requirements for the sale or disposal of edible products containing tetrahydrocannabinol (THC)

Community Development Director Julie Wischnack gave the staff report.

Wilburn requested further information regarding packaging restrictions. Wischnack commented on the requirements the city would have in place regarding THC gummy packaging. City Attorney Corrine Heine discussed how the city would address the flavors of gummies. She reported the council could always revisit the ordinance if issues had to be addressed in the next six or nine months.

Schaeppi stated this ordinance also addresses THC infused beverages. Wischnack reported this was correct.

Schaeppi inquired if THC products could be picked up at Target any other store that offers pick-up services. Heine explained a person to person interaction was required for the sale of THC products.

Schaeppi asked how much the city would be charging and questioned if the amount was adequate. Heine reported the amount charged for these licenses was supposed to represent a reasonable estimate of the cost to administer and enforce the ordinance. She explained an exception could be made due to the nature of this product, but noted she would have to investigate this further.

Schaeppi stated he had young children. He inquired if there would be a prohibition of certain language when advertising these products. Wischnack indicated there were some regulations in the current ordinance that addresses where, but not the terms used when advertising THC products.

Calvert commented she did not have any questions at this time. She stated she anticipated the legislature would be revisiting this topic in the upcoming session.

Wiersum reported Edina, Maple Grove and Plymouth all had moratoriums in place regarding the sale of THC products. He explained he was concerned about the evidence that was piling up on these products. He questioned if the ordinance would not be effective until July 1, 2023, could a moratorium on THC product sales be put in place through June 30, 2023. He indicated he was becoming more and more uncomfortable with the sale of these products. He questioned if restrictions could be implemented that would not allow the sale of these products in Minnetonka. He commented on a discussion he had with a resident in Minnetonka who was concerned about the easy access to THC products in the city and how this was impacting his son. He reported these products were impacting lives negatively. While he understood prohibition did not make sense, but he wanted to ensure the products were not being sold to kids. He explained he supported tying the cost of THC licenses to liquor licenses given the fact it will cost the city a great deal of money to enforce this new ordinance. He stated he would be attending a Tonka Cares meeting tomorrow where intoxicating products would be discussed. He inquired if the city wanted to be this easy to purvey products that hurt kids.

Schack stated she supported the proposed ordinance and she supported the sale of THC products in Minnetonka. She reported the city just approved 80 liquor licenses in the city without a second thought. While she didn't want to diminish the impact THC products could have on kids, she also didn't want to overly restrict the sale of THC products. She stated THC was such a nuance issue and marijuana has been the scapegoat in society for many years. She believed it was the responsibility of parents to keep these products out of the hands of children. She questioned why the community did not look at alcohol sales in the same manner, given its destructive power for some individuals. She

anticipated residents would go outside the community if Minnetonka were to ban sales. She thanked staff for all of their work on the ordinance and noted she would be offering her support.

Kirk explained he had much of the same thoughts as Councilmember Schack. He agreed marijuana got caught up in the war on drugs, which has disproportionately affected one population more than others. He believed this ordinance was moving the city in the right direction. He anticipated if the legislature did take further action to address the sale of THC products then the city would have an ordinance in place that could be amended. He supported the sale of THC products being treated similar to the sale of alcohol.

Calvert explained she strongly relied on data and she hoped that the legislature would clarify numerous things regarding the sale of THC products in the upcoming legislative session. She hoped the legislature was following local, state and national data. She stated she would appreciate the city having an ordinance in place at this time and understood this ordinance could be amended if the state were to pass legislation this year. She agreed it was the responsibility of parents to keep these products out of the hands of children.

Coakley reported she supported the ordinance, but she did have many of the same questions that were raised by Councilmember Schaeppi. She indicated she had a 14 year old daughter and noted she was seeing the THC products in local stores. She wished the city could regulate where THC products were located in stores to keep them off of front counters. She supported THC products being located behind glass to ensure these products were not being stolen by kids. Heine advised the ordinance does include language that addresses the storage of THC items, noting they had to be stored behind a counter or other area that was not freely accessible to the public.

Coakley commented in other cities she has seen THC products sitting right on the checkout counters. Heine reported this ordinance prevents the ready access of THC products to those under the age of 21.

Schaeppi indicated the THC situation was a mess that had been created by the state legislature. He read a quote from an attorney that works to address the sale of THC products. He stated all of the talk that this was a clarification, was a lie. He explained the city was dealing with this issue because a handful of elected leaders put this matter forward in an omnibus bill. He stated he was frustrated with how all cities now had to work through the issues that were created by the state's leaders. He reported he supported small businesses in the community and the rights of adults who choose to use THC products. He commented on how statistics were going up, proving there was a problem with how THC was impacting children. He hoped the state legislature would be able to address this concern, but he feared they did not care. He further discussed how he was

conflicted by this ordinance and hoped the state would properly address this issue in their upcoming session. He recommended people with THC licenses pay a premium given how much time and effort it would take the city to enforce the ordinance and that no drive up sales be allowed.

City Manager Mike Funk reported there may be comments from the public on this topic.

Wiersum opened the meeting for public comments.

Tom Madden, business owner in Minnetonka, reported he has read the ordinance. He explained he supported the proposed ordinance. He indicated he has two children and he has talked to them about these products. He stated these products were not allowed in his home or his stores. He agreed the sale, access and use of THC products was a big parenting issue.

TJ Stalzer, 14525 Minnesota 7, explained he was a small business owner. He reported his business focused on massage therapy and the sale of THC products for therapeutic reasons. He stated he sold very small amounts of THC products and charging an exorbitant fee for the THC license would be a hindrance to his business. He commented further on how Governor Walz would be working to legalize marijuana in the coming year.

Wiersum closed the meeting for public comments.

Wiersum commented he believed the THC ordinance would move the city in the right direction. He explained he would appreciate having a moratorium on the sale of THC products in place until the ordinance was in effect due to how unregulated THC products were at this time. He reported he was struggling with the fact he did not believe society would improve by having more intoxicants for sale in the community. He supported the state having good laws in place to address the sale of THC products.

Schaeppi asked if the city attorney would be willing to enforce this ordinance against drive up sales, or would additional language be required to address this concern. Heine advised the existing language addresses this concern.

Wiersum thanked the council for all of their comments and discussion regarding this ordinance.

Schack moved, Kirk seconded a motion to adopt Ordinance 2023-01. All voted "yes." Motion carried.

B. Resolution designating a new acting mayor and alternate acting mayor

City Manager Mike Funk gave the staff report.

Wiersum recommended Councilmember Coakley serve as the acting mayor and Councilmember Schaeppi serve as the alternate acting mayor for 2023.

Schack moved, Coakley seconded a motion to adopt Resolution 2023-004. All voted "yes." Motion carried.

C. Concept plan for Saville Flats, generally located in the southeast corner of the Excelsior Boulevard/County Road 101 intersection

City Planner Loren Gordon gave the staff report.

Curt Fretham, Lake West Development representative, introduced himself to the council. He provided the council with further information regarding the proposed condominium flats and noted the surrounding uses. He explained the northern portion of the lot would have to be rezoned to medium density to accommodate the condos and the southern portion of the lot would have four single family homes. He commented on the neighborhood meeting that was held and noted the concerns that were voiced centered around tree loss and traffic. He discussed how stormwater would be treated onsite and noted a traffic study would be completed for the project area. He described why he was requesting a change in use, which he believed was necessary to revitalize this corner in Minnetonka. He requested council consider how they see this property and how they would like to see it redeveloped in the future. He provided further comment on the proposed elevations for the Saville Flats and thanked the council for their time.

Wiersum stated he understood single level living housing products were in demand in Minnetonka. He requested further information regarding the design and layout of the condo units.

Rob Hull, architect for Lake West Development, discussed how the units would be laid out noting the first level would have eight units, the second level would have eight units and the third floor would have four units. He explained there would be a variety of one and two bedroom units and all units would be handicap accessible.

Schack stated this area of Minnetonka was transitional and she believed it made sense to have increased density. She understood this was a problem traffic area due to the school. She believed the condo flats were an attractive concept. She explained she would want more information regarding the tree and traffic impacts for the proposed project.

Calvert indicated she read the minutes from the planning commission meeting. She agreed this project made a great deal of sense for these parcels. However, she was concerned with the traffic in the area. She reported she would have to be convinced of the public benefit for this project if it were a PUD. She explained she appreciated the proposed architecture and design of the building. She suggested the developer consider flipping the building and the parking lot in order to reduce the impact on the adjacent neighbors. She encouraged the developer to consider what energy efficiency measures could be incorporated into this project. In addition, she requested the number of units be reconsidered.

Coakley stated her main concern was with traffic. She appreciated the fact the developer would be completing a traffic study. She questioned why the developer had not considered smaller units for families, given the need in the community.

Mr. Fretham discussed how he had considered the traffic patterns in the area. He stated an empty nester product would have differing traffic patterns than those of families with school aged children, which he believed would be a better fit for the area.

Kirk asked which properties would be included in the project. Mr. Fretham reviewed the properties that would be included in the proposed development.

Wilburn questioned if the condos would be age restricted. Mr. Fretham reported the units would be designed for empty nesters, but noted a single parent with a child could also live in these units. He explained the units would only be one or two bedrooms in size, which would not provide enough space for most families.

Wilburn requested further information regarding the affordability levels of the affordable units. Mr. Fretham stated two of the units would be affordable. Gordon explained these two units would be at 60% AMI.

Kirk commented he appreciated the fact a traffic study would be completed in order to better understand how this development would impact Spring Lane. He stated he was not as concerned how this development would impact the intersection at Excelsior Boulevard and County Road 101. He was of the opinion the homes on these properties have reached their useful life and the properties were in need of redevelopment. He commented on how this redevelopment would lead to a change in land use density. He encouraged the planning commission to further consider the level of density within this project, along with the tree loss. He recommended staff look into how tree loss was considered within the Solbakken development. He explained he would like to see a proper tree buffer remain in place on this property. He reported he appreciated the proposed architecture and design for the condos, but suggested the density be further reviewed.

Schaeppi stated he believed the city council should have a deeper conversation regarding medium density and what properties should be zoned medium density. He explained he appreciated the investment the developer was willing to make in the community. He stated he appreciated the step down in density within this project and how the condos would be located on the corner. He encouraged the developer to consider saving more trees or offer more tree plantings in order to break up the massing of the building. He thanked the applicant for their efforts and noted he believed this project would be an improvement over what was currently on the properties.

Wiersum commented on how traffic was always a concern for medium density projects. He stated the challenge for this development was not the traffic that would be added, but rather was with the traffic that was already there. He understood certain times of the day were very uncomfortable traffic-wise. He looked forward to learning from the traffic study what the surrounding intersections rated today and how they would be impacted by the proposed development. He explained he appreciated the architecture of the proposed condo building and understood single-level living was desirable in Minnetonka. He believed this was not a bad corridor for medium density housing. He was of the opinion this project had merit and the product made sense. He indicated the challenges for the project included setbacks, buffers, and how the developer would minimize the project impact on the surrounding properties.

Wiersum opened the meeting for public comments.

David Larson, 5330 Tracy Lynn Terrace, explained he lived just south of the proposed development. He stated he sees this parcel of land part of his neighborhood. He supported this land having single family homes. He reported he was concerned with how the proposed development would add more traffic to the neighborhood. He stated there were lots of children that played outside in his neighborhood and he wanted to see them protected. He recommended the council keep the zoning as is and that additional density not be added to these properties.

Alan Au, 5320 Spring Lane, commented on the traffic in his neighborhood and the number of children that play outside. He explained there were also students that walked down Spring Lane to school. He noted Spring Lane was a rather narrow street, which was a concern for him, if there was additional traffic added. He discussed how a high density project on this corner would add more traffic, crime and parking concerns to the neighborhood.

Wiersum closed the meeting for public comments.

Calvert commented on the parking lot and driveway design. She encouraged the developer to reduce the amount of hardscape. She suggested the driveway be shorter and the parking lot be smaller.

Wiersum thanked the council and public for their comments.

Wiersum recessed the city council meeting.

Wiersum reconvened the city council meeting.

D. Acquisition of The Marsh located at 15000 & 15280 Minnetonka Boulevard

Kirk recused himself from discussing this item.

City Manager Mike Funk, Recreation Services Director Kelly O'Dea and Community Development Director Julie Wischnack gave the staff report.

Schaeppi asked what staff knew about the viability of rehabbing the building. He inquired if the building would last another 10 or 15 years. Wischnack reported a fair amount of searching within the property has been conducted and lists have been created. She understood the roof needed to be replaced, but noted the building was structurally intact. She anticipated the boilers would have to be replaced within the next 10 to 15 years. O'Dea indicated the pool was in very good condition.

Schaeppi questioned if a budget had been created to update and maintain this facility. Funk explained the purchase price for this property was \$4,275,000 and staff was recommending \$2 million be dedicated to operations for the next two years. He commented further on how staff was considering how programming between the Marsh and the community center would align in the future and how this would impact budgets going forward. He stated only soft costs were expected to be spent in the next six months. He commented further on how the \$2 million would assist with addressing the operational costs for up to two years.

Wiersum discussed how the city of Minnetonka could not consider this purchase without having solid finances and a AAA bond rating in place.

Wiersum opened the meeting for public comments.

Leonard Dayton, 4833 Sparrow Road, explained he grew up in Deephaven and returned to Minnetonka to raise his family. He indicated he has been a resident of Minnetonka for the past 60 years. He stated his family has utilized the Marsh for a variety of reasons over the years. He noted the warm pool therapy at the Marsh was one of the only options in the metro area. He appreciated how the Marsh

was senior friendly. He explained he was concerned with how this property would operate going forward and how private development could change the Marsh.

Laura Toper, 5608 Papineau, stated she has been a member of the Marsh longer than she has been a resident of Minnetonka. She explained she moved to Minnetonka because of the Marsh. She thanked the council and staff for all of their hard work on this topic. She reported she joined the Marsh after an injury. She encouraged the city to consider purchasing the Marsh because this property was a pillar in the community and meets the strategic goals and objectives of the city council. She appreciated the community focus within the Marsh and urged the council to move forward with the purchase.

Barbara Sterling, 7333 Gallagher Drive in Edina, explained she has worked in the city of Minnetonka for the past nine years at the spa within the Marsh. She agreed the Marsh was a pillar in the community that brought both people and jobs to Minnetonka. She encouraged the city council to purchase this property, noting she would like to continue serving her clients in the community.

Jane Welch, 4700 Wedgewood Drive, thanked the mayor for meeting with her and for walking through the Marsh property with her. In addition, she thanked City Manager Funk for speaking with her. She explained she would like to keep the Marsh open to the public and noted this property would be able to support itself. She urged the council to support the purchase of this property.

Margaret Meyer, 11460 Fairfield Road West, thanked the council for considering the purchase of this property. She discussed how the Marsh has created a sense of family for her. She believed the YMCA did not properly run this facility and she appreciated the fact the Marsh could be revitalized for the community.

Wiersum closed the meeting for public comments.

Schack stated she has lost sleep about making this decision. She explained she carried animosity towards the YMCA and the position they have put the city in. She understood the city had the opportunity to slow this process down and she appreciated the fact the Marsh was willing to offer a discounted price to the city. She indicated she wanted to see the Marsh a tremendous success in the community that serves all Minnetonka residents. She stated she would be offering her support for the purchase.

Calvert reported when she found out the Marsh was for sale she was quite concerned. She indicated this was not an easy decision for the council to make because of the risk involved. She stated there were so many things for the council to consider with respect to staffing, services, programming and fee structures for the Marsh. However, with the risk of purchasing this property, there was also an opportunity. She discussed how the pool space within the Marsh

would meet a growing need in the community. She commented further on how the Marsh would meet the strategic goals and objectives for the city. She thanked staff for all of their efforts to bring this item before the council and reported she would be supporting the purchase of the Marsh.

Wilburn stated she agreed with much of what had been said. She explained she appreciated the reduced purchase price from the YMCA. She believed this property offered the city a great opportunity and noted she would be supporting the purchase of the Marsh.

Schaeppi commented on the history of this property and noted his family has visited the Y for swimming lessons. He indicated he supported a hurry up process if staff could gather all of the necessary information. He reported he would like to see an estimated proforma for the Marsh going forward, along with an appraisal of the property. He stated he would support approving the purchase of the Marsh with the contingency a closing should occur by February 13, 2023.

Coakley explained the council has been discussing renovations at the community center and how there was a need for more space. She reported the Marsh would meet a great need in the community and she trusted the city to move forward with this purchase.

Wiersum stated he believed the city would be in a better place if the council approved the purchase of the Marsh. He appreciated the sense of health, community and the connections residents were making on this property. He explained he was uncertain what the future of the Marsh would hold, but he believed the membership needed to be broadened. He supported the city council learning as much as they can as quickly as they can with respect to the Marsh. He was of the opinion the community would be better with the Marsh than without it. For this reason, he would be supporting the purchase.

Calvert moved, Coakley seconded a motion to adopt Resolution 2023-005, Resolution 2023-006 and Resolution 2023-007. All voted "yes." Motion carried. (Councilmember Kirk was recused from the vote).

Wilburn moved, Calvert seconded a motion to amend the CIPs including the change memorandum. All voted "yes." Motion carried. (Councilmember Kirk was recused from the vote).

15. Appointments and Reappointments: None.

16. Adjournment

Calvert moved, Schack seconded a motion to adjourn the meeting at 10:53 p.m. All voted "yes." Motion carried.

Respectfully submitted,

Becky Koosman
City Clerk



**City Council Agenda Item 9A
Meeting of Jan. 30, 2023**

Title: Bids for the Minnetonka Community Center project

Report from: Kevin Maas, Facilities Manager

Submitted through: Mike Funk, City Manager
Darin Nelson, Finance Director
Kelly O'Dea, Recreation Services Director
Will Manchester, P.E., Public Works Director

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

On Oct. 24, 2022, council rejected bids from the Oct. 4, 2022 bid opening and authorized a new advertisement for bids for the Minnetonka Community Center, due to bids received being higher than anticipated at that time. The project was updated to a single source project contractor delivery method and bids were again opened on Nov. 29, 2022.

Recommended Action

- 1) Reject all bids, including bid alternates, from the Nov. 29, 2022 Minnetonka Community Center bid opening.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Statement: The Community Center Project supports the sustainable maintenance and replacement of assets as well as provides a public facility for all members of the community.

Financial Consideration

Is there a financial consideration? No Yes \$3,580,000

Financing sources: Budgeted Budget Modification New Revenue Source
Use of Reserves Other Interfund Lending

Statement: The Community Center Project is budgeted in 2022 and 2023 of the proposed 2023-2027 Capital Improvements Program and would be funded through an internal city interfund loan over 15 years.

Background

The project proposed renovation of the community center to upgrade the community, dining and public meeting rooms, increase the capacity of public restrooms and upgrade technology. The project is the final phase to the building to improve access, current building code compliance, safety and Americans with Disabilities Act (ADA) compliance, following upgrades to city hall and construction of the new public safety facility. Office spaces for recreation staff were also included and would meet new standards previously established by these projects. The area was constructed in 1986 and has not been renovated since that time.

Bid Opening

Bids were opened for the project on Nov. 29, 2022. Eleven bids were received in response to the call for bids, and the results are as follows.

Contractor	Base Bid+ Alternates #1-6
Versacon	\$2,152,504.00
Ebert Construction	\$2,170,000.00
Jorgenson Construction	\$2,185,000.00
Parkos Construction	\$2,200,000.00
Construction Results	\$2,240,720.00
Ram General Contracting	\$2,382,540.00
Dering Pierson Group	\$2,430,000.00
Morcon Construction	\$2,466,900.00
Market and Johnson	\$2,477,000.00
American Liberty	\$2,494,700.00
Century Construction	\$2,697,000.00

Following the bid opening, independent of this project, the city was given an unanticipated opportunity to purchase the property known as The Marsh, located at 15000 and 15208 Minnetonka Blvd. On Jan. 9, 2023, the city council approved an agreement to purchase The Marsh. Due to this consideration and the need to reallocate city funds, staff is recommending to reject bids for the project at this time.

Estimated Project Costs and Funding

The total estimated construction cost, including engineering, administration and contingency, is \$2,740,000. The budgeted amount for the project is shown below and is included in the adopted 2022 – 2026 Capital Improvements Program (CIP) and 2023-2027 CIP.

Future Considerations

	CIP Budget Amount	Proposed Funding	Expense
Construction Costs			\$2,160,000
Contingencies			\$250,000
Engineering, Administration, and Indirect Costs			\$330,000
2022 Funding	\$305,000	\$305,000	
2023 Funding	\$3,275,000	\$3,045,000	
Total Budget	\$3,580,000	\$3,350,000	\$2,740,000

While the rebid of the project produced favorable bids and reduced the previous project costs by approximately \$1,160,000, city funds are proposed to be reallocated to the consideration of The Marsh property purchase agreement.

A recreational facility and programming space study will be completed in the future to take a comprehensive review of The Marsh, Williston Fitness Center, Glen Lake Activity Center and the Community Center to determine the various needs and most effective use of these facilities.



**City Council Agenda Item 10A
Meeting of Jan. 30, 2023**

Title: Resolution for the Smetana Road Trail Project

Report From: Mitch Hatcher, P.E., Engineering Project Manager

Submitted through: Mike Funk, City Manager
Corrine Heine, City Attorney
Darin Nelson, Finance Director
Will Manchester, P.E., Public Works Director
Phil Olson, P.E., City Engineer

Action Requested: Motion Informational Public Hearing

Form of Action: Resolution Ordinance Contract/Agreement Other N/A

Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The Smetana Road Trail Project proposes construction of a 0.9 mile trail along the south side of Smetana Road from Sanibel Drive to Westbrooke Way. The proposed trail improvements include the addition of an eight-foot-wide bituminous trail, concrete curb and gutter installation, American Disabilities Act (ADA) improvements, drainage improvements, overhead power burial and other private utility relocation.

Recommended Action

1. Adopt the attached resolution accepting plans and specifications and authorizing the advertisement for bids for the Smetana Road Trail Project, Project No. 23305.
2. Authorize the mayor and city manager to execute an agreement with Xcel Energy, subject to non-material changes as approved by the city engineer and city attorney, in the amount of \$173,833.90 for the Smetana Road Trail Project, Project No. 23305.

Strategic Profile Relatability

- | | |
|---|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input checked="" type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input type="checkbox"/> N/A | |

Statement: The Smetana Road Trail Project includes the construction of a top priority trail, enhancing the trail network connectivity and infrastructure.

Financial Consideration

Is there a financial consideration? No Yes \$2,890,000
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Statement: The Smetana Road Trail Project is budgeted in 2023 and 2024 of the 2023-2027 Capital Improvements Program.

Background

On Oct. 3, 2022, the city council adopted a resolution accepting the feasibility report and authorizing the preparation of plans and specifications for the Smetana Road Trail Project.

This trail segment along Smetana Road, between Sanibel Drive and Westbrooke Way, was initiated through planning of the Opus area in anticipation of the Southwest LRT and is consistent with the goals of the Trail Improvement Plan. The CIP has identified and included funding for this segment since 2013, which predates the creation of the Priority Trail Segments of unfunded trails. Uncertainty with the Southwest LRT project had previously delayed construction.

Proposed Improvements

Trail improvements include the addition of a new eight-foot wide, off-road, multi-use bituminous trail along the south side of Smetana Road, from Sanibel Drive to Westbrooke Way. The trail construction includes grading, concrete curb and gutter, drainage improvements, and pedestrian ramps and crossings. In most areas, the trail is proposed to be installed with a four-foot-wide grass boulevard behind the concrete curb and gutter. In constrained areas, like wetlands and other natural features, the trail will be shifted toward the roadway to minimize impacts. Disturbance to the wetland area will be restored using a native seed mix designed for wet meadows.

Bridge modifications including new concrete parapets and ornamental railing are required to accommodate the trail as it crosses the pedestrian bridge just east of Feltl Court. Additionally, grading, tree removal and impacts to landscaping of adjacent properties are necessary to construct the trail; however, impacts will be minimized as much as possible.

Street Improvements

In coordination with trail construction, a pavement mill and overlay is proposed along the project corridor. Originally, this section of roadway was scheduled for an overlay in 2024, but in 2022 was reprioritized and combined with the trail project. This will allow for pavement removal and patches due to trail construction, concrete curb and gutter installation, and utility crossings to be milled and a new pavement surface installed through the project corridor. This will also take advantage of bidding the two projects together and reduce costs and disruption of the area.

Private Utilities

Most of the project corridor has underground private utilities; however, on the west end of the project, burial of overhead power lines is proposed. Burial is expected to occur in coordination with trail construction. Staff has been coordinating with Xcel Energy and other utility companies and will continue to do so during construction.

Xcel Energy has prepared the attached statement of work, which requires city payment of fifty percent of the estimated costs up-front before work begins. Once the project is complete, Xcel Energy bills the city for the remainder of the actual project costs. These costs are proposed to be paid from the city's Electric Franchise Fee Fund and are already included within the city's adopted CIP. Xcel Energy is requesting execution of their standard agreement for the statement of work, which outlines the conditions for overhead power line burial along the corridor. The city attorney has reviewed this standard agreement.

It is anticipated that CenterPoint Energy will replace and install new gas main ahead of trail construction for select areas of the project corridor.

Easement Acquisition

Permanent easements will be required from approximately two properties to construct and maintain the proposed trail. Staff is actively working with property owners on the acquisition process and anticipate having easements prior to construction.

Public Engagement

An informational meeting was held on Aug. 17, 2022 for neighboring property owners in Minnetonka and Hopkins. A total of 10 residents out of 1,918 invited properties attended the meeting. In addition to mailed invitations, webpage updates and roadside signage were used to notify interested properties.

At the meeting, staff presented the project background, existing conditions, proposed trail improvements, construction impacts and project schedule. Staff discussed how this type of project is very intensive and disruptive to traffic and access. Also, the project will require tree removal, driveway impacts, landscaping impacts and temporary disruptions to utility services. Comments and questions at the meeting were typical to these types of projects including trail priority and usage, pedestrian safety, property impacts, privacy for adjacent properties, scheduling, and construction impacts. Staff took comments and concerns from residents into consideration during design to minimize impacts. Residents were generally supportive of the project.

In-line with other city projects, staff will continue to use various strategies to provide project updates including signage, text alerts, email updates, citizen alerts and newsletters. Staff sent out an update to all project subscribers, currently 237, indicating that council would consider accepting plans and specifications and authorizing the advertisement for bids. The update was also posted to the project webpage.

Estimated Project Costs and Funding

The total estimated construction cost, including engineering, administration and contingency, is \$2,890,000. Trail improvement costs are funded by Minnetonka, and street improvement costs are shared by Minnetonka and Hopkins based on the improvements occurring within each city. A Joint Powers Agreement defining these cost responsibilities was approved by the city council on Oct. 3, 2022. Project funding for trail improvements is included in 2023 within the 2023 – 2027 Capital Improvements Program (CIP). The budgeted amounts for the project are shown below and the fund balances currently can support the estimated project costs.

	Budget Amount	Proposed Funding	Expense
Construction Costs			\$1,550,000
Contingency			\$400,000
Engineering, Administration, and Indirect Costs			\$300,000
Easement Acquisition			\$240,000
Overhead Power Burial			\$400,000
City of Minnetonka			
Trail Expansion Fund	\$1,550,000	\$1,550,000	
Park and Trail Improvement Fund	\$350,000	\$350,000	
Street Improvement Fund	\$300,000	\$300,000	
Electric Franchise Fund	\$800,000	\$400,000	
City of Hopkins	\$290,000	\$290,000	
Total Budget	\$3,290,000	\$2,890,000	\$2,890,000

Schedule

If the recommended actions are approved by council, staff will open bids in March with intentions of council consideration to award the contract following. Construction will begin this spring and be completed this fall. Once the project is bid and a prime contractor is selected, construction information and phasing will be communicated to council and residents.

Resolution No. 2023-

**Resolution accepting plans and specifications and authorizing the advertisement for bids
for the Smetana Road Trail Project, Project No. 23305**

Be It Resolved by the City Council of the City of Minnetonka, Minnesota as follows:

Section 1. Background.

1.01. Pursuant to city council authorization on Oct. 3, 2022, plans and specifications have been prepared by or under the direction of the city engineer, who is a Licensed Professional Engineer in the State of Minnesota, for the Smetana Road Trail Project, Project No. 23305.

1.02. The plans and specifications for the construction of the Smetana Road Trail Project, Project No. 23305, have been presented to the city council for approval.

Section 2. Council Action.

2.01. The plans and specifications, copies of which are on file with the engineering department, are hereby accepted upon the recommendation of the city engineer.

2.02. The city clerk shall prepare and cause to be inserted in the official newspaper and in Finance & Commerce an advertisement for bids for the making of such improvements under such approved plans and specifications. The advertisement shall specify the work to be done, shall state that bids will be opened and read aloud at the Minnetonka City Hall, that all bids must be made online at the QuestCDN bidding site, and that no bids will be considered unless accompanied by bid security in the amount of five (5) percent of the amount of the bid, which security must be submitted as required by the contract documents.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Jan. 30, 2023.

Brad Wiersum, Mayor

Attest:

Becky Koosman, City Clerk

Action on This Resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

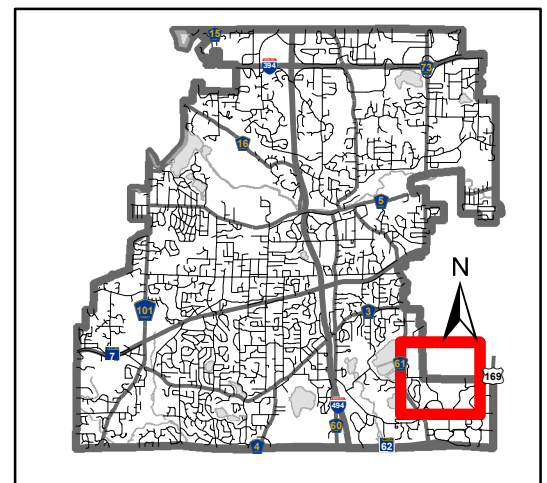
I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on Jan. 30, 2023.

Becky Koosman, City Clerk



Smetana Road Trail Project

- ■ ■ ■ ■ Proposed Trail Location
- — — — — Municipal Boundary Line



This map is for illustrative purposes only.

2023 Funding Summary

Funding Sources		Proposed Funding		Balance
	2023 CIP	Smetana Road	Local Street Preservation	
Trail Expansion Fund	\$ 1,550,000	\$ 1,550,000		\$ -
Park and Trail Improvement Fund	\$ 350,000	\$ 350,000		\$ -
Street Improvement Fund	\$ 5,490,000	\$ 300,000	\$ 5,190,000	\$ -
Electric Franchise Fund	\$ 800,000	\$ 400,000		\$ 400,000
City of Hopkins	\$ 290,000	\$ 290,000		\$ -
Total Project Cost	\$ 8,480,000	\$ 2,890,000	\$ 5,190,000	\$ 400,000



**STATEMENT OF WORK REQUESTED
BY COUNTY, CITY, TOWN OR TOWNSHIP
FOR PROJECTS WITH ESTIMATED
CONSTRUCTION COSTS OVER \$25,000**

DATE: DECEMBER 16TH, 2022
 WORK REQUESTED BY: (Insert name of Municipality) CITY OF MINNETONKA C/O MITCH HATCHER
"Municipality"
 WORK LOCATION: 5185 BEACHSIDE DR, MINNETONKA, MN 55343
 ADDRESS: 14600 MINNETONKA BLVD
MINNETONKA, MN 55435

CONSISTING OF:

The following shall constitute the "Work" to be performed by Xcel Energy:

COVERT 1000' OF EXISTING OVERHEAD FACILITIES TO UNDERGROUND. PROJECT LIMITS FROM SHADY OAK RD TO 250' EAST OF SANIBEL DR ON SMETANA DR

Municipality agrees to pay Xcel Energy for Xcel Energy's actual total cost of the Work, subject to the Municipality's right of cost review in accordance with the terms of this Statement of Work ("Statement"). The current estimate for the Work is \$ 173,833.90 ("Estimate"). The Estimate is comprised of the following major components:

<u>Component</u>	<u>Sub-estimate</u>
2 - PMH-11 SWITCHING CABINETS	
1 - SINGLE PHASE 120/240 50kVA PADMOUNT TRANSFORMER	
1370' OF FEEDER 750AL CABLE & BORING	
185' OF 1 PHASE #2AL CABLE & BORING	

Total: _____

The undersigned hereby requests and authorizes Xcel Energy to perform the Work. In consideration thereof and in lieu of a City Requested Facilities Surcharge, the City agrees to pay Xcel Energy on the date of this Statement an initial payment of (\$ 86,916.95) which is fifty (50) percent of the Estimate ("Down Payment").

All Work shall be performed pursuant to good utility practice (as that term is generally understood in the utility industry) utilizing Xcel Energy's commercially reasonable efforts to complete the Work within the Estimate under Xcel Energy's then current design standards, operating procedures, and safety procedures. The facilities installed or removed by Xcel Energy shall be the property of Xcel Energy and any payment by Municipality shall not entitle Municipality to any ownership interest or right therein. Municipality's and Xcel Energy's rights and obligations with respect to the facilities and services provided through the facilities are subject to the terms of this Statement, as well as the additional terms and conditions provided in the Xcel Energy Electric Rate Book, as now exists or may hereafter be changed, on file with the Minnesota Public Utilities Commission. In advance of the Work, Municipality agrees to inform Xcel Energy of any Municipality-related or other projects that may affect the Work. During the Work, Xcel Energy agrees to provide the Municipality notice of any proposed change orders increasing the cost of the Work. Municipality acknowledges that change orders that result from requests of Municipality with respect to the performance of the Work or the scope of the Work may increase Xcel Energy's actual cost of the Work. Upon completion of the Work, Xcel Energy agrees to provide Municipality with final detail of the actual work performed and the actual costs of such work performed. Xcel Energy will identify any information included in such information that is non-public pursuant to Minn. Stat. Ch. 13. Upon request by



Municipality, Xcel Energy shall provide Municipality the opportunity to review more detailed documentation of the Work performed and related costs. Xcel Energy agrees to keep Municipality reasonably informed with respect to Xcel Energy's performance of the Work, consistent with good utility practice and will, at minimum, apprise Municipality when half of the Estimate has been spent and when ninety percent of the Estimate has been spent. Xcel Energy also agrees to timely notify the Municipality when the Work is substantially complete. Upon receipt of the invoice for the cost balance, the City shall have the right to require that Xcel Energy provide reasonable cost support documentation, including change orders, for its actual total cost of the Work. The Municipality shall pay the balance of cost not subject to reasonable dispute within the timeframe set forth in the Minnesota Municipal Prompt Payment Act, Minn. Stat. 471.425. Xcel Energy and Municipality shall reasonably try to resolve any disputes with respect to costs incurred in performance of the Work in good faith. In the event Xcel Energy and Municipality are unable to resolve any such disputes, the parties may seek redress in a forum with jurisdiction over the dispute.

This Statement of Work is agreed to by Xcel Energy and Municipality and receipt of the above Down Payment of \$86,916.95 is hereby acknowledged on behalf of Xcel Energy.

XCEL ENERGY SIGNATURE	CUSTOMER SIGNATURE
Northern States Power Company	Legal Entity Name (if applicable):
By:	Authorized Signer (see signing options below*): <i>Signing Option 1</i> <input type="checkbox"/> AGREE <i>Signing Option 2</i> (Signature below) By:
Printed Name:	Printed Name:
Title:	Title (if applicable):
Date:	Date:

* **Signing Option 1 (just click to agree):** By clicking the AGREE checkbox above, you acknowledge that you are the customer or an authorized signer for the customer and have read, understand, and agree to the above-stated terms.
Signing Option 2: Add Electronic Signature and return by e-mail **OR** print, sign, scan and return by e-mail **OR** print and sign and return by mail.

XCEL ENERGY WORK ORDER # 13459122

ESTIMATED CONSTRUCTION \$ 153,092.86

ESTIMATED REMOVAL \$ 20,741.04

ESTIMATED TOTAL \$ 173,833.90

FORM 17-7012



**City Council Agenda Item 10B
Meeting of Jan. 30, 2023**

Title: Gleason Lake Road Reconstruction project
Report From: Chris Long, P.E., Assistant City Engineer
Submitted through: Mike Funk, City Manager
Corrine Heine, City Attorney
Darin Nelson, Finance Director
Will Manchester, P.E., Public Works Director
Phil Olson, P.E., City Engineer

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

On Oct. 24, 2022, the City of Plymouth requested approval of an intersection concept for a new mini-roundabout at the intersection of Gleason Lake Road and Vicksburg Lane. Since that time, the City of Plymouth has slightly revised the concept with negligible changes during final design and staff is requesting approval of the revised concept.

Recommended Action

Approve the updated intersection concept for the Gleason Lake Drive Reconstruction, Project No. 23609.

Strategic Profile Relatability

- | | |
|---|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input checked="" type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input type="checkbox"/> N/A | |

Statement: The Gleason Lake Drive Reconstruction Project includes intersection improvements, which will improve safety for all users including vehicles, pedestrians and bicyclists.

Financial Consideration

Is there a financial consideration? No Yes \$500,000
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Statement: Minnetonka's share of the Gleason Lake Drive Reconstruction Project is budgeted in 2023 of the 2023-2027 Capital Improvements Program.

Background

On Oct. 24, 2022, the city council approved a new intersection concept layout at Gleason Lake Road and Vicksburg Lane, as well as a Joint Powers Agreement with the City of Plymouth. At that meeting, council reviewed the benefits of a new intersection layout that improved safety, traffic flow and provided safer options for pedestrian bicyclists.

Since Oct. 24, 2022, the intersection concept design has been updated to balance ongoing public feedback and different project needs and requirements. Below is a list of items updated on the proposed concept.

- The roundabout has been elongated to further reduce vehicle speeds through the mini-roundabout. Most importantly, vehicle speeds from southbound Vicksburg Lane to westbound Gleason Lake Road have been reduced with the geometric updates.
- Adjacent residents were concerned with impacts to their properties and adjacent landscaping on all sides of the project. The project design reduces impacts as much as possible while still ensuring a safe design.
- Rectangular Rapid-Flashing Beacons (RRFB) were requested by city council at the meeting on Oct. 24, 2022, at all crossing locations. Due to changes in the roundabout shape and balancing impacts to adjacent properties, the new concept includes two designated pedestrian crossings with RRFBs instead of three. The project would still allow for one RRFB signaled north/south crossing and one RRFB signaled east/west crossing. It would, however, eliminate a second north/south crossing on the east side of the roundabout. With the elimination of this crossing, measures will be taken to deter pedestrians from creating a cut-through path to the east of Townes Lane.

Staff finds the revised final design concept to be in-line with the previously approved concept, with only negligible revisions, and therefore requests that the council approve the updated concept. The Plymouth city council approved the updated design at its Jan. 24, 2023 meeting.

Public Engagement

The City of Plymouth provided an open house on Oct. 30, 2022, to residents of both Plymouth and Minnetonka, sharing the updated intersection concept design as well as the full project reconstruction scope. A total of 1 Minnetonka resident and 25 Plymouth residents attended the meeting. A total of 299 Minnetonka project subscribers were notified in advance of the meeting. This meeting was in addition to the public engagement as discussed in the Oct. 24, 2022 council report.

At the meeting, City of Plymouth staff discussed the concept updates and answered questions about the project. Comments and questions at the meeting were typical of projects including scheduling, access, property impacts and safety concerns. A copy of the informational meeting comment cards and a list of resident questions and staff answers are attached.

As with other city projects, staff will continue to use various strategies to provide project updates including text alerts, email updates, citizen alerts and newsletters. Staff sent out an update to all project subscribers indicating that council would be reviewing the updated concept.

Proposed Improvements

The intersection of Gleason Lake Road and Vicksburg Lane is proposed to be fully reconstructed and reconfigured, making the flow of traffic more efficient and realigning the roadways to improve safety for all users. Additional details regarding the proposed improvements, need for the project, access points, and pedestrian crossings were included in the Oct. 24, 2022 staff report.

Estimated Project Costs and Funding

The City of Minnetonka's associated cost for the Gleason Lake Road and Vicksburg Lane intersection will be determined following receipt of bids. The city has budgeted \$500,000 for this project and funding is included in 2023 within the 2023 – 2027 Capital Improvements Program (CIP).

Since council has already approved a Joint Powers Agreement for this project, as long as favorable bids are received below the budgeted amount, no additional council approvals are necessary.

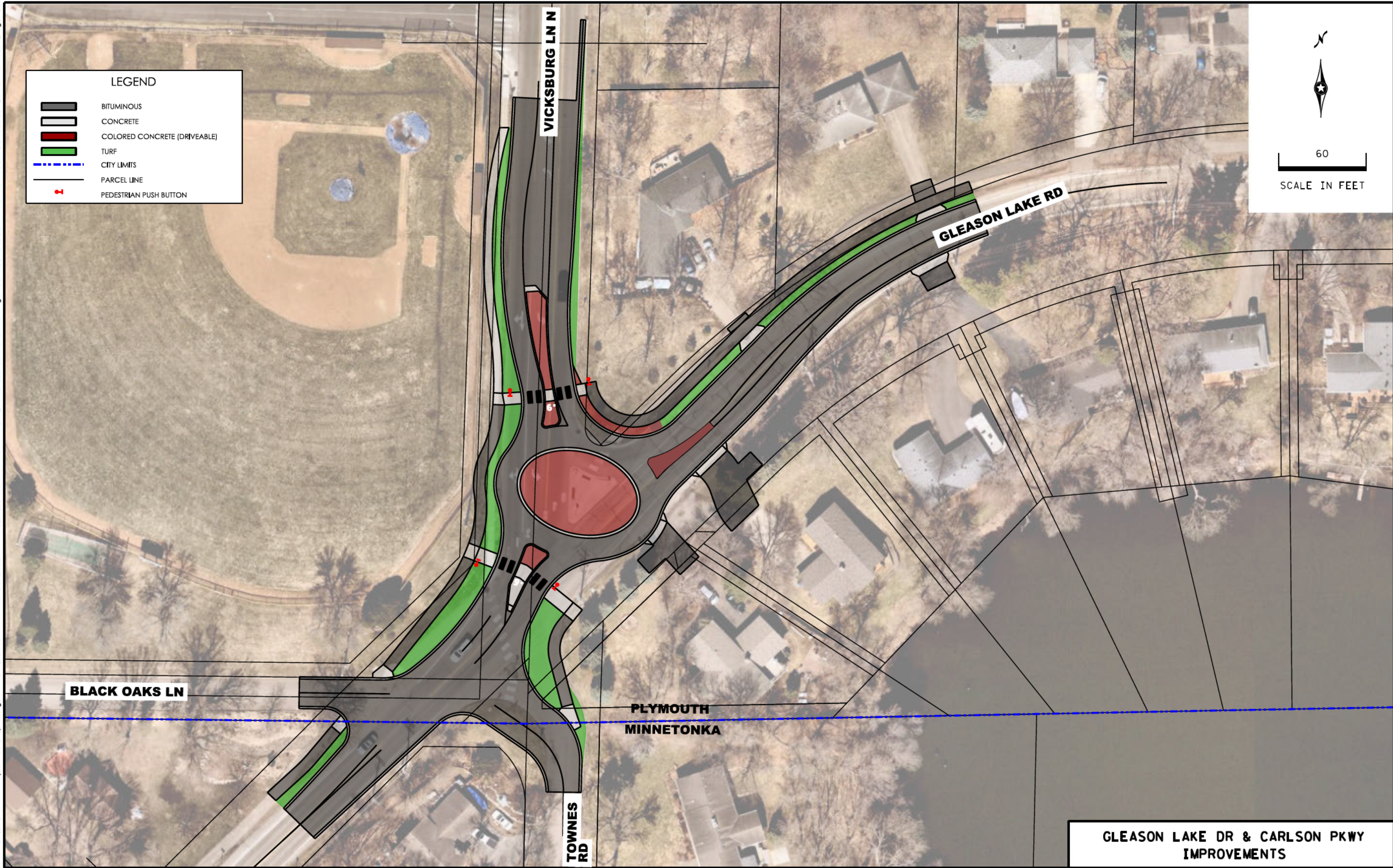
Schedule

If the recommended action is approved by council, staff will coordinate with the City of Plymouth as it bids and potentially awards the project in early spring. Construction of the project would likely begin in the spring of 2023 and be substantially completed in the fall of 2023.

LEGEND

- BITUMINOUS
- CONCRETE
- COLORED CONCRETE (DRIVEABLE)
- TURF
- CITY LIMITS
- PARCEL LINE
- PEDESTRIAN PUSH BUTTON

60
SCALE IN FEET



**GLEASON LAKE DR & CARLSON PKWY
IMPROVEMENTS**

Open House Meeting
November 30, 2022
Sarah and Mark Kipp
17 Niagara Ln N

Sheet C0.04 Existing Conditions

Drawing shows our driveway as bituminous. **It is concrete.**

Will we need to remove and possibly reinstall the existing block retaining wall?

Note, this wall was built following a suggestion from Mr. Faulkner to prevent runoff and to protect our water shutoff.

Sheet C4.09 Water Plan and Profile

How far will Niagara Ln be lowered in front of our driveway?

How much of a change in slope will this cause our driveway?

We are concerned this change will impact the backing our 26' travel trailer into our driveway. **If the change in slope is significant, will the City remove more of our drive to decrease the slope?**

Sheet C6.11

Our driveway shows 4 – Install 3" Bituminous Driveway/Trial. It should be 2 – Install 6" concrete walk/driveway.

Drawing is missing 10 Reinstall mailbox and the new location.

Our mailbox currently meets the USPS guidelines.

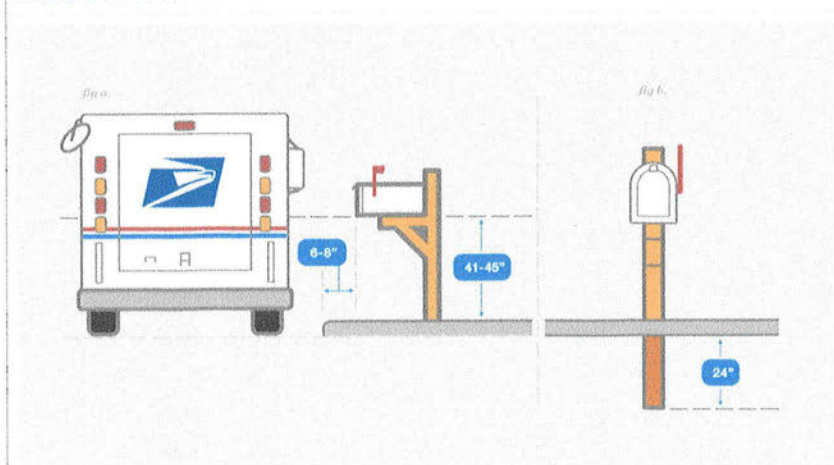
Has the USPS Postmaster approved the plan to have the mailbox relocated to the property side of the new trial and not along the curb?

As our route is a driving route, will the mail carrier drive on the trail to deliver our mail?

From the USPS Web site (<https://www.usps.com/manage/mailboxes.htm>)

Position your mailbox 41" to 45" from the road surface to the bottom of the mailbox or point of mail entry. Place your mailbox 6" to 8" back from the curb. If you do not have a raised curb, contact your local postmaster for guidance. Put your house or apartment number on the mailbox.

Where to Place the Mailbox



Drawings C0.04, C6.11 C9.31and 9.32

93+00, 93+25, 93+50

The new proposed new finished grade just west of the proposed trail appears it will impact the existing and remaining trees outside of the construction limits.

How will the contractor protect the trees identified to remain?

What is the process to replace any damaged trees once the project is completed?

What grass or vegetation will be used on the new grade? It will it be selected based on the amount of sun/shade available?

From section 93+50, our water shutoff appears it will be well above the new finished grade. It is currently at grade. Will it be relocate, lowered, during this project to be at grade?

Temporary Right of Entry

“Right of entry is granted with the understanding that all disturbed areas will be restored following construction.”

Our driveway has been treated has been sealed TK-290, a solvent based sealer.

What sealer will be used on the concrete after our driveway is replaced?

TK-290 is a low viscosity, multi-substrate, salt, and water repellent. Available in standard VOC and a Low VOC option, TK-290 series is engineered to prevent damage from environmental conditions including de-icing agents, salts, freeze damage, efflorescence, scaling, and surface popping. TK-290 is a solvent-based formulation to penetrate up to 1/4 inch (6.35 mm) and chemically bond with the substrate to become a permanent part, prohibiting salt, water, chloride ion, and acid rain intrusion. This protection ensures the longest life-cycle to the substrate by preventing surface deterioration. <https://www.tkproducts.com/product/water-repellent-concrete-sealer/>

New Trail

As the proposed new trail was included to address an identified gap in the City’s trail system between Parkers Lake Road and Gleason Lake Drive, please confirm this trail will be added to the City’s Trail System and will be covered under the City’s snow removal policy.

Also, as the mailboxes on Niagara Lane are being relocated away from the curb, can you confirm the trail will be cleared after any snow fall so the mail carrier will be able to deliver our mail?

Comment Card

City of Plymouth

2023 Gleason Lake Dr Improvements

(Optional Contact Information)

Name: Steve Rutz Telephone: _____

Address: 110 Kingsview Ln Email: _____

Comments / Questions: Nick answered all my questions very thoroughly.

For Additional Information, please contact: Nick Amatuccio at nickam@bolton-menk.com or 612-965-3926

2023 Gleason Lake Dr Improvements
November 30, 2022





**City Council Agenda Item 10C
Meeting of Jan. 30, 2023**

Title: Resolution for the Red Circle Drive turn lane improvements project

Report From: Chris Long, P.E., Assistant City Engineer

Submitted through: Mike Funk, City Manager
Darin Nelson, Finance Director
Will Manchester, P.E., Public Works Director
Corrine Heine, City Attorney
Phil Olson, P.E., City Engineer

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The Red Circle Drive Turn Lane Improvements project proposes the addition of a turn lane and signal improvements at the intersection of Red Circle Drive and Shady Oak Road. The improvements accommodate increased development in the Opus area and are consistent with the Opus AUAR Study.

Recommended Action

Adopt the attached resolution accepting plans and specifications and authorizing the advertisement for bids for the Red Circle Drive Turn Lane Improvements project, Project No. 23607.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Statement: The Red Circle Drive Turn Lane Improvements project enhances infrastructure by adding a turn lane to accommodate increased area density and improve traffic operations.

Financial Consideration

Is there a financial consideration? No Yes \$1,700,000
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Statement: The Red Circle Drive Turn Lane Improvements project is budgeted in 2023 of the 2023 -2027 Capital Improvements Program (CIP).

Background

In early 2021, the city completed the Opus Alternate Urban Area Review (AUAR), a planning study and environmental analysis of the Opus area anticipated development and infrastructure. The purpose of the study was to help guide future planning decisions and produce a plan to identify responsible infrastructure while allowing growth to prevent, minimize or mitigate identified environmental impacts during development.

The Opus AUAR identified the addition of a turn lane on Red Circle Drive at the intersection of Shady Oak Road. The added turn lane will accommodate additional traffic related to commercial and residential development in the Opus area to manage traffic to acceptable levels. The Doran residential development is currently underway and planned for completion in the summer of 2023; constructing the turn lane at this time allows the city to coordinate construction with the residential development.

Proposed Improvements

Plans and specifications have been prepared in coordination with the Doran Development for the construction of an additional turn lane on Red Circle Drive. A figure is attached to illustrate the project location.

Work associated with the roadway turn lane addition includes relocation of existing water main, storm sewer, and also modifications to the traffic signal, medians, and existing pedestrian facilities at Shady Oak Road and Red Circle Drive.

In addition to the proposed turn lane improvements, staff continues to work with 10900 Red Circle Drive on opportunities to construct a missing trail segment along Red Circle Drive, which would connect pedestrians and bicyclists from Shady Oak Road to the SWLRT OPUS Station. If the required easements are obtained soon, by donation, staff may work to add this trail segment into this project as an alternate for council consideration at the bid award. If the easement cannot be obtained soon, staff may consider this as a standalone project in the future and it would be brought back to council as a separate item. The costs for this trail are included in the project costs below.

Estimated Project Costs and Funding

The total estimated construction cost, including engineering, administration, and contingency, is \$1,700,000. The table below shows the costs and funding necessary for the project, which is identified for 2023 in the 2023-2027 CIP.

As the details of the design have been further refined, the cost to construct the turn lane is significantly less than previously budgeted. This is primarily due to the reduction of improvements required by Hennepin County at the traffic signal. This allows for the consideration of the trail extension with this project, if the easement is obtained.

Subject: Resolution for the Red Circle Drive Turn Lane Improvements project

	Budget Amount	Proposed Funding	Expense
Construction Costs			\$1,150,000
Contingency			\$250,000
Engineering and Administration			\$300,000
Opus TIF District Improvements	\$2,100,000	\$1,700,000	
Total Budget	\$2,100,000	\$1,700,000	\$1,700,000

Schedule

If the recommended actions are approved by council, bids would be received in March/April and presented to council for final contract approval in March/April. Construction would begin in the spring of 2023 and is planned for completion in the summer of 2023 in coordination with the Doran Development.

Resolution No. 2023-XXX

Resolution ordering the improvements, accepting plans and specifications, authorizing easement acquisition and authorizing the advertisement for bids for the Red Circle Drive Turn Lane Improvements, Project No. 23607

BE IT RESOLVED by the City Council of the City of Minnetonka, Minnesota as follows:

Section 1. Background.

- 1.01. Plans and specifications have been prepared by or under the direction of the city engineer, who is a Licensed Professional Engineer in the State of Minnesota, for the Red Circle Drive Turn Lane Improvements, Project No. 23607.
- 1.02. The plans and specifications for the construction of the Red Circle Drive Turn Lane Improvements have been presented to the city council for approval.

Section 2. Council Action.

- 2.01. The proposed improvements are hereby ordered as proposed.
- 2.02. The plans and specifications, copies of which are on file with the engineering department, are hereby accepted upon the recommendation of the city engineer.
- 2.03. The city attorney and city engineer are hereby authorized to acquire necessary easements by negotiation or condemnation.
- 2.04. The city clerk shall prepare and cause to be inserted in the official newspaper and in Finance & Commerce an advertisement for bids for the making of such improvements under such approved plans and specifications. The advertisement shall specify the work to be done, shall state that bids will be opened and read aloud at the Minnetonka City Hall, that all bids must be made online at the QuestCDN bidding site, and that no bids will be considered unless accompanied by bid security in the amount of five (5) percent of the amount of the bid, which security must be submitted as required by the contract documents.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Jan. 30, 2023.

Brad Wiersum, Mayor
ATTEST:

Becky Koosman, City Clerk
ACTION ON THIS RESOLUTION:

Motion for adoption:
Seconded by:
Voted in favor of:

Voted against:

Abstained:





Absent:

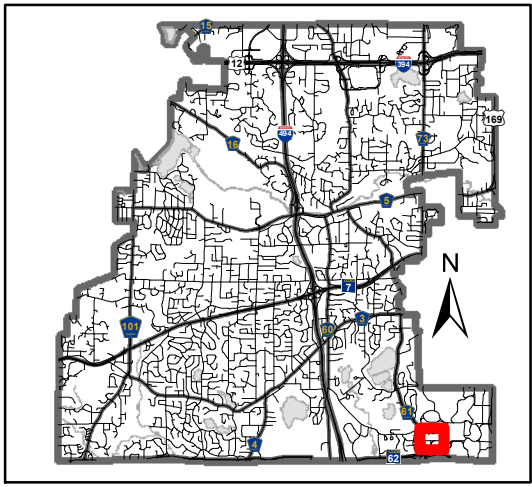
I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on Jan. 30, 2023.

Becky Koosman, City Clerk



Red Circle Drive Turn Lane Improvements - Location Map

-  Construction Limits
-  Add Turn Lane
-  Potential Trail Construction
-  Trail Installation by Doran Apartments



This map is for illustrative purposes only.



**City Council Agenda Item 10D
Meeting of Jan. 30, 2023**

Title: 2023 Strategic Profile action steps
Report from: Moranda Dammann, Assistant City Manager
Submitted through: Mike Funk, City Manager

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The strategic profile is an instrumental and living document that guides the work of the city. At the Dec. 12, 2022 study session, the 2023 action steps were brought forth to council for review and discussion.

Recommended Action

Adopt a motion accepting the 2022 strategic profile action steps.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Statement: The 2023 action steps provides accountability on all six strategic priorities listed above.

Financial Consideration

Is there a financial consideration? No Yes
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other

Background

In late summer and early fall of 2020, the city council met over a series of meetings to draft an updated strategic profile. Staff have implemented the three-level plan and annually develop actionable items for the third level of the plan. The profile is uploaded into the city's strategic profile software, Envisio. This software tracks the progress of the actionable items, holds data for the metrics that align with those items and will generate a public dashboard that will display

overall progress on an ongoing basis.

The [community dashboard](#) component allows the community to view the status. This is another layer of transparency and for the city to demonstrate progress, and it is now fully functional on the city's website.

The top level of the plan includes six priorities. Each priority is assigned a group lead who oversees the development, tracking and progress of the action steps. The team lead pulls in a variety of staff who have expertise in specific areas and are assigned as the owner or contributor to the item. In addition to developing action steps, the groups also identified key metrics that will be included in the strategic profile to help visually support the various action steps and areas of the strategic profile.

Following each quarter, an updated report is brought forth to the city council for review and a motion to accept that quarters report. Once action steps are approved and finalized for 2023, they will be updated into Envisio for reporting and tracking.

Who we are



VISION STATEMENT

Minnetonka is an inclusive community committed to excellence where all residents, workers and visitors are welcome in a beautiful, sustainable place, supported by quality, dependable city services.

MISSION STATEMENT

Provide **quality public services**, while striving to preserve and enhance the distinctive character to make Minnetonka a **special place for everyone**.



OUR GUIDING PRINCIPLES

- We earnestly commit to a **beautiful, sustainable and healthy environment** as a vital part of a stable, prosperous and thriving community.
- We responsibly deliver **excellent public services** and **provide affordable opportunities** to ensure access to all we serve.
- We ethically uphold **community trust** through proactive, inclusive public engagement, transparent communications, and the careful stewardship of our financial, natural, and capital assets.
- We nimbly **lead our city into the future** by anticipating community needs, pursuing service innovation and adoption of new technologies, and forging collaborative partnerships with all sectors of society.

STRATEGIC PRIORITIES	KEY STRATEGIES
<p>Financial Strength and Operational Excellence</p> <p>Maintain a long-term positive financial position by balancing revenues and expenditures for operations, debt management and capital investments. Provide innovative, responsive, quality city services at a level that reflects community values and is supported by available resources.</p>	<ul style="list-style-type: none"> • Maintain the city's AAA bond rating. • Develop an annual budget that meets community needs and is in alignment with the strategic plan and financial policies. • Provide excellent, meaningful programs and amenities to serve and enhance our community.
<p>Safe and Healthy Community</p> <p>Develop programs, policies and procedures that enhance the community's well-being and partner with the community to provide engagement opportunities and build trust. Sustain focus on prevention programs, education, hazard mitigation and rapid emergency response.</p>	<ul style="list-style-type: none"> • Identify and adapt to public safety service models that support evolving changes in service delivery expectations. • Identify safety strategies and practices that promote positive quality of life for all. • Collaboratively review current integrated police and fire policy and training protocols and implement appropriate changes. • Provide a full range of recreational programs, services and amenities.
<p>Sustainability and Natural Environment</p> <p>Support long-term and short-term initiatives that lead to the protection and enhancement of our unique and natural environment while mitigating climate change impacts.</p>	<ul style="list-style-type: none"> • Carefully balance growth and development with preservation efforts that protect the highly valued water and woodland resources of our community. • Develop and implement long-term plans to mitigate threats to water quality, ecosystems, urban forests and the unique natural character of Minnetonka. • Take an active role in promoting energy and water conservation, sustainable operations and infrastructure, recycling and environmental stewardship.
<p>Livable and Well-Planned Development</p> <p>Balance community-wide interests and respect Minnetonka's unique neighborhoods while continuing community reinvestment.</p>	<ul style="list-style-type: none"> • Implement programs and policies to diversify housing and increase affordable housing options. • Encourage and stimulate additional public participation in various development processes • Support evolving needs of business and identify new city initiatives • Continue to support Opus Area transformation as a vital and connected part of Minnetonka
<p>Infrastructure and Asset Management</p> <p>Provide safe, efficient, sustainable, cost-effective and well-maintained infrastructure and transportation systems. Build, maintain and manage capital assets to preserve long-term investment and ensure reliable services.</p>	<ul style="list-style-type: none"> • Provide and preserve a quality local street and trail system. • Ensure connectivity through increased access to local and regional means of transportation (new mobility options). • Develop an annual capital improvement plan that supports the sustainable maintenance and replacement of assets. • Expand and maintain a trail system to improve safe connectivity and walkability throughout the community.
<p>Community Inclusiveness</p> <p>Create a community that is engaged, tolerant and compassionate about everyone. Embrace and respect diversity, and create a community that uses different perspectives and experiences to build an inclusive and equitable city for all.</p>	<ul style="list-style-type: none"> • Develop and implement inclusive recruiting, application, hiring and retention practices to attract excellent, qualified and diverse candidates from all backgrounds. • Actively engage the community by working collaboratively to broaden policy outcomes and respond to community's needs, views and expectations. • Remove identifiable barriers to create equal opportunity for accessing programs and services.



CITY OF
MINNETONKA

Strategic Profile Action Items

Updated: Jan. 2023

Strategic Priority: Financial Strength and Operational Excellence

Maintain a long-term positive financial position by balancing revenues and expenditures for operations, debt management and capital investments. Provide innovative, responsive, quality services at a level that reflects community values and is supported by available resources.

Key Strategies:

1. Maintain the city's AAA bond rating.

Action items:

- a. Maintain a Moody's fund balance rating of "Very Strong", which equates to a fund balance of greater than 30 percent of revenues.
- b. Maintain a Moody's net direct debt rating of "Very Strong", which equates to net direct debt being less than 0.75 percent of the city's taxable market value.
- c. Maintain Water and Sewer Utility fund cash balance of least two times the annual debt service, six months of ongoing operations, and 10 percent of accumulated depreciation.

Performance metrics:

- a. Net Governmental Funds Balance Percentage of Revenues: Greater than 30% = Very Strong
 - b. Year End Governmental Funds Balance
 - c. Net Direct Debt
 - d. Net Direct Debt Revenue - Very Strong
 - e. Utility Fund - Actual Vs Required Cash Reserves
 - f. Governmental Funds Revenue
2. Develop an annual budget that meets community needs and is in alignment with the strategic plan and financial policies.

Action items:

- a. Review annual strategic plan to prioritize city council objectives.
- b. Develop and approve 5-year Capital Improvements Plan (CIP).
- c. Perform long-term levy projections to ensure financial sustainability and responsible stewardship of the public's tax dollars.

Performance metrics:

- a. Estimated 5-year levy forecast.
 - b. Taxable market value
3. Provide excellent, meaningful programs and amenities to serve and enhance our community

Action items:

- a. Hold a Minnetonka Academy in the spring and determine a reoccurring schedule for future academies
- b. Explore agenda management systems, select vendor and develop an implementation plan

- c. Begin working with LOGIS and other resources to implement the ERP and utility billing applications
- d. Create and execute a short term opening plan for the The Marsh and begin the process of evaluating future operating and investment plans for the facility.
- e. Conduct a recreational facility study to evaluate The Marsh, Williston Fitness Center, Community Center and Glen Lake Activity Center to determine community desires, city needs and long term plans for programming and investments.

Strategic Priority: Safe and Healthy Community

Develop programs, policies and procedures that enhance the community's well-being and partner with the community to provide engagement opportunities and build trust. Sustain focus on prevention programs, education, hazard mitigation and rapid emergency response.

Key Strategies:

1. Identify and adapt to public safety service models that support evolving changes in service delivery expectations.

Action items:

- a. Engage a consultant in 2022 to assist the City in the development of a long term staffing strategy with options and service level objectives.
- b. Train the police, along with internal city departments, on preparedness and critical incident plans and procedures in order to improve outcomes. Engage in multi-disciplinary, community team approaches for planning, implementing, and responding to crisis situations.
- c. Collaborate with internal city departments and other stakeholders to develop a public safety plan that will assist in managing growing areas such as Opus and Ridgedale.
- d. Continue commitment to enhance our community mental health response.
- e. Examine key internal processes to identify solutions that improve efficiency and automation.
- f. Based on the consultant report, staff will work towards a future plan for the city's five stations.
- g. Leverage Data Analyst Position to collect and analyze data in order to increase transparency and demonstrate accountability through proactive data sharing initiatives.

Performance metrics:

- a. Average length of fire department member service.
- b. Response time. (Maintain the standard of 10 firefighters on scene of a fire, within 10 minutes - 90% of the time).
- c. Number of officers' crisis intervention trained

2. Identify safety strategies and practices that promote positive quality of life for all.

Action items:

- a. Partner with businesses, schools, and religious groups to develop strategies and identify opportunities for deploying resources that aim to reduce crime by improving relationships, increasing community engagement and fostering cooperation.
- b. Monitor data and analyze the results to identify crime trends. Leverage intelligence and technology to find innovative solutions to those issues. Ensure implementation of technology is designed considering local needs and aligned with national standards.
- c. Ensure that department policies and practices align with community values & expectations, supporting the elements of trust & legitimacy. Reinforce through training and reporting.
- d. Collaborate with communications on utilizing social media and other public awareness platforms to provide awareness and prevention tips on current crime trends and general safety. The goal is to improve relations and transparency between the police department and those we serve.

- e. Identify and encourage opportunities for officers, youth, and other community members to interact in more open and constructive dialogue through non-enforcement activities.
- f. Develop and propose a Standard of Cover to the City Manager and City Council using service level objectives and best practices.

Performance metric:

- a. Crime trend data
 - b. Part 1 and 2 crime statistics
 - c. Community survey results
3. Collaboratively review current integrated police and fire policy and training protocols and implement appropriate changes.

Action items:

- a. Ongoing training for joint standard operating guideline for response to mass casualty/active shooter events.
- b. Implement and train on joint traffic management at emergency and non-emergency scenes.
- c. Develop a policy for joint structure fire response.
- d. Identify weaknesses in current EMS first response delivery system and identify improvement strategies

Performance metric:

- a. Identified deficiencies and strategies
4. Provide a full range of recreational programs, services and amenities.

Action items:

- a. Provide a variety of affordable recreational programs for individuals of all ages.
- b. Enhance recreational facilities to provide quality experiences for the members of our community.
- c. Provide health and wellness services and resources such as senior tax services, blood pressure checks, partnership resources with Senior Community Services, Williston Fitness Center personal training and fitness programming, etc. to enhance the quality of life.
- d. Continue to provide low and no cost special events that build community.

Performance metrics:

- a. Number of free programs/ under \$50
- b. List of facility enhancements
- c. List of free events and attendance

Strategic Priority: Sustainability and Natural Environment

Support long-term and short-term initiatives that lead to the protection and enhancement of our unique and natural environment while mitigating climate change impacts.

Key Strategies:

1. Carefully balance growth and development with preservation efforts that protect the highly valued water and woodland resources of our community.

Action items:

- a. Begin drafting updates to development performance standards to incorporate soil protection along with other natural resources ordinance
- b. Develop handouts/fact sheets on all natural resource-focused ordinances for contractors/developers to enhance compliance
- c. Prepare amendments to zoning district text, cross-referencing natural resources protection ordinances and addressing conflicts where they exist
- d. Initiate an evaluation program to identify successes and gaps related to development compliance with natural resources ordinances
- e. Begin inventorying wetland buffers on public property to enhance wetland protection

Performance metrics:

- a. Total acreage of land held in conservation easements
- b. Number of property visits for technical assistance on sustainable landscaping practices
- c. Number of trees distributed/planted annually

2. Develop and implement long-term plans to mitigate threats to water quality, ecosystems, urban forests and the unique natural character of Minnetonka.

Action items:

- a. Develop park restoration and maintenance plans for high-priority parks identified in the NRMP, including funding strategies
- b. Develop an urban forest management plan, including tree inventory and recommendations for woodland protection and urban tree management
- c. Continue implementation of a city-wide sanitary sewer televising program
- d. Adopt city-wide Atlas 14 flood vulnerability models to aid in planning and implementation
- e. Develop a community-wide Climate Action and Adaptation Plan in conjunction with the Sustainability Commission
- f. Enhance and expand the city's stormwater BMP inventory, assessment, and maintenance program.

Performance metrics:

- a. Total number of storm drains adopted through the city's Adopt-a-Drain program

- b. Number of stormwater treatment practices inspected annually for maintenance
 - c. Total miles of sanitary sewer televised
3. Take an active role in promoting energy and water conservation, sustainable operations and infrastructure, recycling and environmental stewardship.

Action items:

- a. Update the 2014 Water Sustainability Plan
- b. Promote sustainable sourcing and disposal of building materials through outreach, tabling at community events, and/or hosting educational opportunities.
- c. Promote the city's water conservation grant program for indoor and outdoor water use.
- d. Update the Sustainable Minnetonka webpage
- e. Promote sustainable sourcing and disposal of building materials through outreach, tabling at community events, and/or hosting educational opportunities.
- f. Apply to become a SolSmart city

Performance metrics:

- a. Number of home energy squad inspections
- b. Per capita annual gallons of water use
- c. Number of new solar installations
- d. Number of hours EV charging stations are utilized (or Kw hours)
- e. Pounds of plastic materials recycled as part of the NexTrex program

Strategic Priority: Livable and Well-Planned Development

Balance community-wide interests and respect Minnetonka's unique neighborhoods while continuing community reinvestment.

Key Strategies:

1. Implement programs and policies to diversify housing and increase affordable housing options.

Action items:

- a. Identify and partner to support programs that address affordable ownership.
- b. Implement changes to the down payment assistance program that specifically addresses first generation home ownership.
- c. Implement additional housing work plan items including new efforts relating to preserving naturally occurring affordable housing (both rental and ownership).

Performance metric:

- a. Number of new and/or modified policies
- b. Number of actual affordability housing produced/preservation of units

2. Support evolving needs of business and identify new city initiatives

Action items:

- a. Educate, coordinate and collaborate with businesses to address work force issues. This effort would include coordination with educators and employers to create connections for future workers.
- b. Identify and promote cultural business development centers as a resource for existing and new businesses.

Performance metric:

- a. Number of businesses "reached" (Number open to Business, Business visits, Thrive subscribers (including new subscribers).

3. Continue to support Opus Area transformation as a vital and connect part of Minnetonka

Action items:

- a. Ensure current development and planned improvements meet city placemaking goals.
- b. Continue to refine the public space location and broaden connectivity to other areas of the city, which includes continuing to request state bonding support.
- c. Evaluate barriers in zoning ordinance to ensure city vision can be achieved. This includes review of zoning districts requirements, establishment of new zoning districts and refining existing zoning districts.

Performance metric:

- a. Track value of new investment in infrastructure, public spaces, and private development.

4. Encourage and stimulate additional public participation in various development processes

Action items:

- a. Summarize and evaluate existing public input methods.
- b. Identify any barriers for participating in public meetings. Specific attention to cultural and disability barriers.

Performance metric:

- a. Number of items changed

Strategic Priority: Infrastructure and Asset Management

Provide safe, efficient, sustainable, cost-effective and well-maintained infrastructure and transportation systems. Build, maintain and manage capital assets to preserve long-term investment and ensure reliable services.

Key Strategies:

1. Provide and preserve a quality local street and trail system.

Action items:

- a. Coordinate the Tonka-Woodcroft, Minnetonka Boulevard Trail, Smetana Road Trail, Carlson/Cheshire Street and Utility Improvements, Opus Bridges and citywide pavement overlay projects. Preparations for the 2024 Capital Improvements Plan will also be underway.
- b. Coordinate Capital Improvement Program projects and oversee management of local street and trail projects.
- c. Coordinate all regional improvements, including Hennepin County road work, as identified in 2023.
- d. Review and update the city's annual pavement management plan..

Performance metric:

- a. Number of major construction projects in progress/completed
- b. Money spent on asset improvements as percentage of overall system
- c. Total number of regional improvements initiated in 2023
- d. Maintaining an annual pavement rating of 80 or greater

2. Ensure connectivity through increased access to local and regional means of transportation (new mobility options).

Action items:

- a. Staff continues to work with agencies, including Three River's Park District, Hennepin County and MnDOT, on local and regional means of transportation, including future outlooks on MnDOT and Hennepin County's major transportation corridors, as well as upcoming planning efforts.
- b. Work with Metropolitan Transit to meet quarterly to discuss transit route updates to increase connectivity within the city.
- c. Coordinate Capital Improvement Program trail construction along Minnetonka Boulevard, Smetana Road, Hopkins Crossroad utility preparations and in the Opus area.
- d. Prepare citywide trail maintenance management program details..

Performance metric:

- a. Number of new local and regional transportation methods planned
- b. Number of transit improvements under consideration
- c. Number of new trail miles maintained in this area
- d. Number of trail miles planned under maintenance management program

3. Develop an annual capital improvement plan that supports the sustainable maintenance and replacement of assets.

Action items:

- a. Plan for preliminary capital improvements for 2024-2028 including sustainable maintenance and replacement considerations.
- b. Identify utility replacement program needs and future long-term utility replacements.
- c. Review asset management and city infrastructure including fleet operations and other city infrastructure.
- d. Review citywide Geographic Information Systems (GIS) use and plan for future goals.

Performance metric:

- a. Money spent on asset improvements as percentage of overall system
- b. Number/cost of utility improvements identified as compared to overall system
- c. Money spent on asset improvements as percentage of overall system
- d. Number of new uses identified for GIS
- e. Expand and maintain a trail system to improve safe connectivity and walkability throughout the community.

4. Expand and maintain a trail system to improve safe connectivity and walkability throughout the community.

Action items:

- a. Review improved safe connections and walkability improvements for Minnetonka Boulevard and Smetana Road trail projects. Continue to review all city project areas for improvements that can be included in upcoming year's funding.
- b. Collaborate with local school districts for grant funding.
- c. Continue to identify opportunities to connect businesses to the public trail system during development review applications utilizing crime prevention through environmental design (CPTED).
- d. Research opportunities for first/last mile connections to Southwest Light Rail Transit Station Areas.

Performance metric:

- a. Number of new trail miles identified for improvement
- b. Number of grants applications submitted
- c. Number of applications reviewed using CPTED
- d. Total first/last mile connections researched

Strategic Priority: Community Inclusiveness

Create a community that is engaged, tolerant and compassionate about everyone. Embrace and respect diversity, and create a community that uses different perspectives and experiences to build an inclusive and equitable city for all.

Key Strategies:

1. Develop and implement inclusive recruiting, application, hiring and retention practices to attract excellent, qualified and diverse candidates from all backgrounds.

Action items:

- a. Review and audit of marketing materials and accessibility of accessing hiring information and applying for City of Minnetonka open positions
- b. Going into the community to recruit applicants by attending job fairs, advertising through community partnership and attending community events
- c. Strengthen training and processes for interviewers and hiring managers to promote diversity, equity, and inclusion in the selection process
- d. Communicate DEI values widely, including in employee handbook, employee onboarding, other internal communications
- e. Explore the opportunity to partner with surrounding agencies on a workforce development program

Performance metric:

- a. Track number of employees of color.
- b. Track number of applicants of color.

2. Actively engage the community by working collaboratively to broaden policy outcomes and respond to community's needs, views and expectations.

Action items:

- a. Review annual community survey questions
- b. Use data generated through community outreach platforms (Minnetonka matters, community survey, business survey, etc.) to align Minnetonka's brand with community needs, views and expectations
- c. Continue the conversation with the city council on developing a new permanent city commission for council consideration. If approved, take necessary steps to adopt ordinance, recruit, interview and appoint members.
- d. Research and explore ways to improve opportunities for diverse and sustainable vendors to contract with the city

Performance metrics:

- a. Survey results/statistics (Community Survey).
- b. Number of diverse and sustainable vendors contracted
- c. Number of active community partnerships.

3. Remove identifiable barriers to create equal opportunity for accessing programs and services.

Action items:

- a. Award recreation scholarships to 100% of qualified applicants through the Richard Wilson Scholarship Fund for youth program participants.
- b. Provide additional free program offerings/activities to encourage exploration of our parks.
- c. Offer inclusion services for all youth programs and activities
- d. Increase efforts of translation assistance

Performance metrics:

- a. Location map of program offerings
- b. Number of scholarships presented
- c. Recreation program survey response report



**City Council Agenda Item 10E
Meeting of Jan. 30, 2023**

Title: 2023 general liability insurance and workers' compensation renewals

Report From: Moranda Dammann, Assistant City Manager

Submitted through: Mike Funk, City Manager
Darin Nelson, Finance Director

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The city council is being asked to review the proposed insurance package for the city's 2023 policy term, and formally authorize the coverage options for the package policies and workers' compensation policy as outlined by staff.

Recommended Action

Staff recommends that the city council renew the city's insurance policies through the League of Minnesota Cities Insurance Trust (LMCIT) for package policies with the following options:

- \$25,000/\$150,000 deductible for the package policies
- Continuing with an increased coverage for data breach and crime limits
- 100% Open Meeting law coverage
- No waiver of statutory limits

Staff recommends that the council also authorize renewal of the LMCIT workers' compensation policy with a \$10,000 deductible.

Strategic Profile Relatability

- | | |
|---|---|
| <input checked="" type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input type="checkbox"/> N/A | |

Statement: Holding and ensuring adequate insurance for city assets

Financial Consideration

Is there a financial consideration? No Yes \$1,396,388

Subject: 2023 general liability insurance and workers' compensation renewals

Financing sources: Budgeted Budget Modification New
Revenue Source Use of Reserves Other [Enter]

Statement: The premiums fits within the 2023 budget allocation

Background

LMCIT Program

The city has been a member of the LMCIT since the early 1980s. The program continues to offer the broadest coverage for municipal operations at very reasonable rates. LMCIT also offers a program for return of excess premiums based on successful experience ratings, and the city continues to receive dividends for the general liability program. Staff recommends that the city remain in the LMCIT program.

Package Policies

The coverage provided by the package policies include:

General Liability, which provides coverage when the city is liable for incidents such as sewer backups, injuries incurred on city property, employee actions, errors and omissions for elected officials, Open Meeting Law, and Inland Marine (coverage for vehicles not licensed for road use, such as the Zambonies).

Property, which provides coverage for physical losses to city-owned facilities. Coverage is purchased for replacement of structures and contents due to damage by fire or acts of nature.

Automobile, which provides liability and physical damage coverage for all city vehicles.

Premiums and Recommended Coverage

Premiums

The city's general liability premium increased from \$416,745 to \$478,652. The primary factor for the increase was a rise in the city's liability rating. This rating is based on the actual cost of the city's liability claims during a three-year period.

Staff recommends continuing with the city's coverage for data breach and crime limits of \$500,000 for each.

Staff recommends the city stay with the \$25,000 per claim and \$150,000 annual deductibles.

Open Meeting Law

Staff recommends that the city continue with the Open Meeting Law coverage at 100% coverage.

Waiver of Statutory Limits

LMCIT writes its coverage to mirror the liability caps for governmental agencies. Staff continues to recommend that the city not waive those statutory limits.

These premiums are paid from the Insurance Fund, and a sufficient balance is maintained in that fund for these expenditures.

Workers' Compensation

The premium quotation for renewal of the city's worker's compensation for the upcoming insurance year through LMCIT, minus credits for a \$10,000 per occurrence deductible, is \$917,736. The 2022 premium was \$592,431. This increase is due to the annual changes in class code rates and the city's mod factor increasing from 0.55 to 0.67. The mod factor relates to the frequency and severity of an employer's claims over a three-year period, and it is used to calculate the premium. A mod factor of 1.00 is considered average for an employer's particular industry.

This workers' compensation premium fits within the preliminary 2023 budget allocation.



**City Council Agenda Item 10F
Meeting of Jan. 30, 2023**

Title: Approve 2023 study session work plan
Report from: Moranda Dammann, Assistant City Manager
Submitted through: Mike Funk, City Manager

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

At the Dec. 12, 2022, and Jan. 23, 2023 study sessions the Minnetonka City Council reviewed and discussed council priorities and provided staff with direction to create a 2023 study session work plan.

Recommended Action

Approve work plan.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Financial Consideration

Is there a financial consideration? No Yes
Financing sources: Budgeted Budget Modification New Revenue Source

Background

The Minnetonka city council is scheduled to hold twelve (12) study sessions in 2023. In order to maximize these meetings, provide staff direction and focus on council priorities, council was tasked on Jan. 3, 2023 to individually rank topics that have been raised by councilmembers throughout the course of the last year.

At the Jan. 23, 2023 Study Session Council reviewed the rankings and discussed priorities to provide staff with direction to create the 2023 study session work plan. After approval this evening, at each proceeding council meeting the topics for the upcoming study session will be provided. This document can be modified throughout the remainder of the year by a majority of council members.

2023 Council Study Session Work Plan

Quarter 1	
23-Jan	
	Board and Commission Interviews
	Sales and/or Lodging Tax
	Study session work plan
06-Feb	
	Housing updates (pathways and homelessness)
	Review and Update Council Policy & Rules of Procedure (1.3, 2.7)
	2023 Community Survey questions review
20-Mar	
	Council compensation
	Director presentations
	Overview of recreation programming
Quarter 2	
24-Apr	
	Public Safety master plan
	Buckthorn Pilot pick-up program
15-May	
	2024 Kick-Off Budget discussion
	DEI and Human Rights Commission structure
12-Jun	
	2024 CIP/EIP
	Transit Orientated Development
July	Boards & Commissions dinner
24-Jul	
	Homelessness/policy/process
	Small business programs
Quarter 3	
21-Aug	
	2024 budget discussion
31-Aug	Annual joint Planning Commission, EDAC and City Council tour
06-Sep	Annual Park Board and City Council joint meeting with Tour
	Dog Park/Leash policy (start w/Park Board)
11-Sep	
	Storm water management fees
	Zoning - density (community development project)
23-Oct	
	Storage of garbage/recycling containers
	On-street parking regulations/enforcement
Quarter 4	
01-Nov	<i>Annual Park Board and City Council joint meeting (combine w/ tour) Potential Cancel</i>
20-Nov	
	2024 Enterprise budget discussion
	2024 budget discussion
11-Dec	
	2024 Strategic Profile Action Steps
	2024 Study Session Work plan
	2024 Legislative Breakfast - confirm priorities

* No Mow May- Sustainability Commission to regular council meeting



**City Council Agenda Item 10G
Meeting of Jan. 30, 2023**

Title: Appointment of hearing officers for administrative citation hearing program

Report from: Becky Koosman, City Clerk

Submitted through: Mike Funk, City Manager
Moranda Dammann, Assistant City Manager

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Appointment of Jon Morpew and Fabian Hoffner as hearing officers for the administrative citation hearing program.

Recommended Action

Approve the appointment.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Statement: The appointment of hearing officers is aligned with operational excellence and safe and healthy community.

Financial Consideration

Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other

Statement: The administrative citation hearing program is included in the 2023 operational budget.

Background

In 1995, the city council approved an ordinance establishing an administrative citation hearing program. The administrative hearing process offers an alternative to the regular court system to review alleged violations of city ordinances. The process is intended to be informal and less intimidating to alleged violators. Section 1310.035 of the city code requires the council to approve periodically a list of attorneys to be used as hearing officers. This list was last updated in 2016. Due to retirements, the current list is down to two hearing officers.

The city also uses the appointed hearing officers to act as independent hearing officers in proceedings other than administrative penalty matters. For example, the city uses the hearing officers in dangerous dog cases. In total, the hearing officers typically conduct five to 10 hearings annually.

Last fall, both Mr. Morphew and Mr. Hoffner expressed interest in serving as hearing officers for the city. Both attorneys work out of Minneapolis, MN.

Staff is recommending Jon Morphew and Fabian Hoffner be appointed as hearing officers and added to the list of existing hearing officers.



**City Council Agenda Item 11A
Meeting of Jan. 30, 2023**

Title: Applications for renewed precious metal and secondhand dealer licenses for 2023

Report From: Fiona Golden, Community Development Coordinator

Submitted through: Mike Funk, City Manager
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The city has received three applications for renewed precious metal and secondhand dealer licenses for 2023.

Recommended Action

Motion to approve the precious metal and secondhand license renewals for the 2023 calendar year.

Strategic Profile Relatability

- | | |
|--|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input checked="" type="checkbox"/> N/A | |

Statement: N/A

Financial Consideration

Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Statement: N/A

Background

Section 610.005 of the city code states the city council finds that pawnbrokers, precious metal dealers, and certain secondhand dealers potentially provide an opportunity for the commission and concealment of crimes. The purpose of this section is to prevent these businesses from assisting in the commission of crimes. To identify criminal activities through timely collection and sharing of certain transaction information, and to ensure that such businesses comply with basic consumer protection standards, thereby protecting the city's public health, safety, and general welfare.

The city has received applications for renewed precious metal and secondhand dealer licenses for the following establishments:

Best Buy #4	13513 Ridgedale Drive
Minnesota Jewelry Buyers	11900 Wayzata Blvd., #116K
Shane Co	11300 Wayzata Blvd., Suite A

Since the licenses were approved last year, there have been no changes to the ownership structure or day-to-day operations.

No contacts reported at the establishments warrant denial or postponement of renewed licenses, in staff's opinion. The police department has no concerns for any entity listed above.

All applicants meet all the requirements of the precious metal dealer/secondhand dealer ordinance. All assessments and other city claims against these establishments, as well as property taxes, are current.

City Council Agenda Item 11B
Meeting of Jan. 30, 2023

Title: Conditional use permit and site plan review, with variance and expansion permit for a drop-off and pick-up loop at Groveland Elementary at 17310 Minnetonka Blvd

Report From: Ashley Cauley, Senior Planner

Submitted through: Mike Funk, City Manager
Julie Wischnack, AICP, Community Development Director
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Cliff Buhman, on behalf of the Minnetonka Public Schools, is proposing to expand the drop-off and pick-up lane. To accomplish this, an existing path around the ballfields in the northeast corner would be widened to accommodate vehicular circulation.

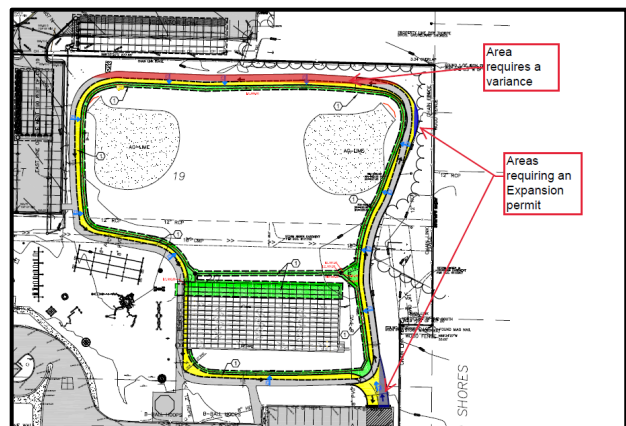


Figure 1: Proposed site plan

Recommended Action

Adopt the resolution approving the request.

Strategic Profile Relatability

- | | |
|--|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input checked="" type="checkbox"/> N/A | |

Statement: N/A

Financial Consideration

Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Planning Commission Recommendation

The planning commission considered the request on Jan. 19, 2023. The commission report, associated plans, and meeting minutes are attached. Staff recommended approval, finding:

- But for the setback variance, the use would continue to meet the conditional use permit standards for educational uses in a residential district.
- The variance and expansion permit required to reduce the drive aisle setback is the result of the lot configuration and existing site improvements. These areas of encroachment would be reasonably screened with existing vegetation and topography or be adjacent to an existing parking lot shared between the school and an adjacent religious institution.
- Widening the existing path would minimize overall site impacts and mitigate queuing issues currently extended onto Minnetonka Blvd.
- The transportation study commissioned by the city found that the currently proposed plan is reasonable and would mitigate queuing issues but encourages that Alternative B, which requires significant site reconstruction, be considered a future plan if budgeting allows.

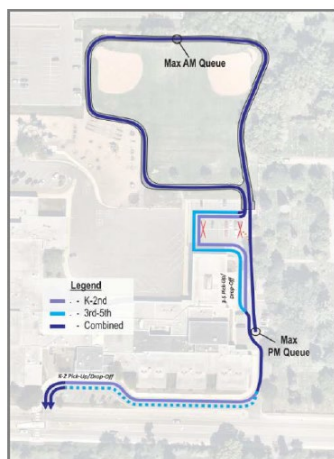


Figure 2: Alternative A
(Currently Proposed)

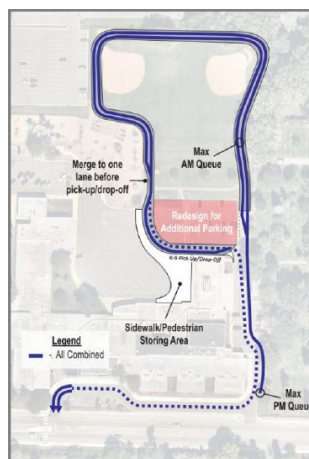


Figure 3: Alternative B

The planning commission concurred with staff's recommendation to approve a conditional use permit, and no one appeared to speak on the item. On a 6-0 vote, the commission recommended the city council approve the request.

MINNETONKA PLANNING COMMISSION
Jan. 19, 2023

Brief Description	Conditional use permit and site plan review, with variance and expansion permit for a drop-off and pick-up loop at Groveland Elementary at 17310 Minnetonka Blvd.
Recommendation	Recommend the city council approve the requests

Background

The Groveland Elementary school building was constructed in 1958. Aerial photography suggests that the eastern access and drive aisle was constructed in 1966 with a setback of 3.5 feet. Aerial photography also suggests that the east parking lot, which has a non-conforming setback of six feet, was constructed prior to 1971. Currently:

- **Enrollment:** Enrollment is expected to fluctuate between 860 and 900 students, with current enrollment at 881 students in grades K-5. No significant change in enrollment is expected at this time.

Open enrollment: Five-year averages suggest that roughly 40 percent of students are open-enrolled. Of those students, 66 percent are city of Minnetonka residents.

- **Staff:** There are 131 staff currently employed at the school. This total includes teachers, paraprofessionals, food services, maintenance, cleaning, and before and after school staff. This number is expected to fluctuate slightly in response to enrollment patterns.
- **Bussing:** Approximately 65 percent of students utilize bus services.
- **Traffic patterns:** Drop-off and pick-up traffic enters from Minnetonka Blvd and is directed north and through the parking lot north of the gymnasium. Traffic is then directed to the pick-up and drop-off areas. Grades K-2 utilize the front entry and grades 3-5 utilize the east entry near the gymnasium.

Proposal

Cliff Buhman, on behalf of the Minnetonka Public Schools, is proposing to expand the drop-off and pick-up lane for Groveland Elementary. To accomplish this, an existing path around the ballfields in the northeast corner would be widened from 6.5 feet to 12 feet. Stormwater would be collected and directed to an expanded underground stormwater facility (shown in green).

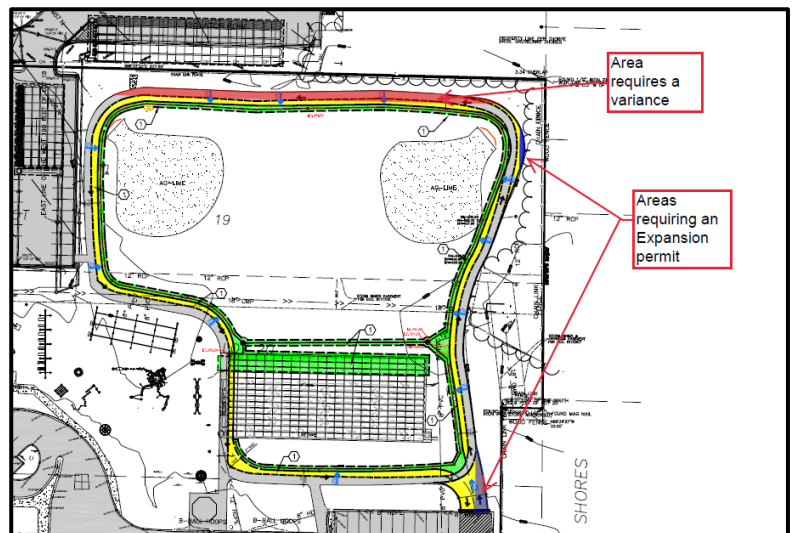


Figure 1: Proposed site plan

Traffic would enter from Minnetonka Blvd, be directed north and around the newly constructed loop, and then through the existing loop within the parking lot and to the existing pick-up and drop-off locations. The proposal requires:

- conditional use permit,
- site plan review,
- variance to reduce the northerly drive-aisle setback from 20 feet to 10 feet;¹ and
- expansion permit for the easterly drive-aisle setback.²

Neighborhood Meeting

On Wednesday, Nov. 30, 2022, Groveland Elementary and Groveland Cemetery held a joint open house meeting for their respective projects. Nine area residents attended the meeting. Relative to the elementary school project, residents:

- Confirmed the proposed stacking queue would replace the amount of existing backup/queueing area occurring on Minnetonka Blvd.
- Confirmed that the expanded path area would be asphalt like the existing path.
- Confirmed that the trees along the northern property line would remain.
- Confirmed the project is anticipated to start next summer.
- Inquired about existing and predicted bussing patterns.
- Generally supported the request and the desire to improve traffic congestion in the area during pick-up and drop-off times.

Staff Analysis

A land use proposal is comprised of many details. These details are reviewed by the members of the city's economic development, engineering, fire, legal, natural resources, planning, and public works departments and divisions. These details are then aggregated into a few primary questions or issues. The analysis and recommendations outlined in the following sections are based on the collaborative efforts of this larger review team.

- **Is the proposed conditional use permit appropriate?**

Yes. But for the setback variance, the use would continue to meet the conditional use permit standards for educational uses in a residential district. A full list of standards and the staff's findings can be found in the "Supporting Information" section of this report.

- **Is the requested setback variance and expansion permit reasonable?**

Yes. The variance and expansion permit required to reduce the drive aisle setback is the result of the lot configuration and existing site improvements. The areas of encroachment would either be reasonably screened with existing vegetation and topography or be adjacent to an existing parking lot shared between the school and an

¹ A variance is required when an expansion of a use will intrude further into a setback area beyond the distance of an existing structure.

² An expansion permit is required when the expansion of a non-conforming structure maintains, or does not encroach further into, a required setback.

adjacent religious institution (St Luke Presbyterian Church). A full list of standards and the staff's findings can be found in the "Supporting Information" section of this report.

- **Are the proposed site impacts reasonable?**

Yes. The proposal would expand an existing path to minimize overall site impacts and would mitigate queuing issues currently occurring on Minnetonka Blvd. The city secured SRF Consulting Group, Inc. to review existing vehicular and pedestrian patterns in the neighborhood in order to evaluate the proposal. Ultimately, the SRF study found:

Queuing during peak hours: Peak morning trips occurring during drop-off time (7:55 to 8:45 a.m.) extend onto Minnetonka Blvd for short periods of time. Pick-up, however, generally occurs for approximately 60 minutes (beginning at 2:45 p.m.) and extends onto Minnetonka Blvd for up to 25 minutes. Existing onsite queuing areas accommodate approximately 67 vehicles, but storage for approximately 100 vehicles is required to mitigate queuing issues during the afternoon peak hour.

Intersection capacity: Operation of intersections are evaluated utilizing a Level of Service (LOS) grading system. LOS A indicates the best traffic operations, whereas LOS F indicates an intersection demand exceeds capacity. The study found that for short periods of time, the LOS operates at a D and F levels.

Table 5. Existing Intersection Capacity Analysis (Peak 15-Minute Analysis)

Intersection	School Arrival Peak Hour ⁽²⁾		School Departure Peak Hour ⁽²⁾	
	LOS	Delay	LOS	Delay
Groveland School Road / Overflow (Church) Access ⁽¹⁾	A/A	4 sec.	A/A	4 sec.
Groveland School Road / Elementary North Access ⁽¹⁾	A/A	5 sec.	A/A	4 sec.
Groveland School Road / Elementary South Access ⁽¹⁾	A/A	1 sec.	A/A	1 sec.
Minnetonka Boulevard / Groveland School Road ⁽¹⁾	A/A	10 sec.	A/A	10 sec.
Minnetonka Boulevard / Elementary West Access ⁽¹⁾	A/A	10 sec.	A/A	9 sec.
Minnetonka Boulevard / Elementary East Access ⁽¹⁾	C/D	31 sec.	D/F	51 sec.
Minnetonka Boulevard / Woodlawn Avenue ⁽¹⁾	A/A	6 sec.	C/E	48 sec.

(1) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst side-street approach LOS. The delay shown represents the worst side-street approach delay.

(2) The school arrival and departure peak hours are defined as 7:50 to 8:50 a.m. and 3:00 to 4:00 p.m., respectively.

Design Review: SRF also evaluated two alternatives to evaluate if they would mitigate queuing and capacity issues: ³

³ Neither alternative considers encouraging traffic onto Groveland School Road to the west as this road is not designed to accommodate the additional trips generated during drop-off and pick-up times.

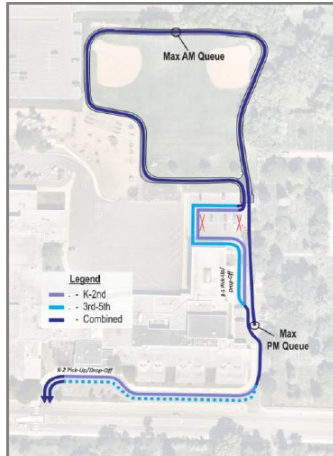


Figure 2: Alternative A (Currently Proposed)

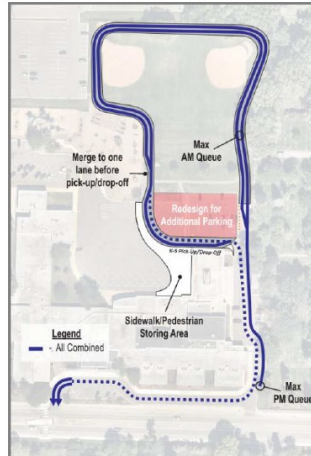


Figure 3: Alternative B

Alternative A (Currently Proposed): Alternative A would provide adequate on-site vehicle storage and would eliminate the queuing and capacity issues on Minnetonka Blvd.

The plan does not improve site inefficiencies and conflict areas (such as two drop-off and pick-up locations) and would separate the playground from adjacent greenspace.

Alternative B: The alternative was developed as part of the transportation study to improve both on-site storage and internal conflict areas. This alternative would increase the width of path to accommodate two lanes and have all traffic exit via the western Minnetonka access. This plan would reduce driver confusion and increase site efficiency. However, this plan would require major reconstruction of the site and on-site bus storage.

Ultimately, the study found that the currently proposed plan is reasonable and would mitigate queuing issues, but encourages that Alternative B be considered a future phase if budgeting allows. The full transportation study is attached.

Staff Recommendation

Recommend that the city council adopt the attached resolution approving a conditional use permit and site plan review, with variance and expansion permit, for Groveland Elementary at 17310 Minnetonka Blvd.

Originator: Ashley Cauley, Senior Planner
Through: Loren Gordon, AICP, City Planner

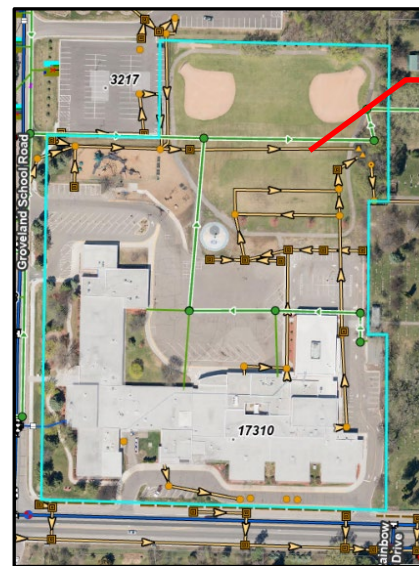
Supporting Information

Land Uses

SURROUNDING LAND USES			
	Land Use	Zoning	Guided by the 2030 comp plan
North	St. Luke Presbyterian Church	R-1	Institutional
East	Groveland Cemetery Single family homes	R-1	Low Density
South	Single family homes	R-1	Low density
West	Commercial The Sanctuary (attached townhomes) Single family homes	PUD & B-2 PUD Single Family	Mixed use Medium Density Low density
SUBJECT PROPERTY			
	Elementary School	R-1	Institutional

Storm Sewer Pipe

The project presents an opportunity for the city for the city to replace an aging stormwater pipe. This pipe was identified as one needing replacement as part of city replacement projects adjacent to the property. The school district has agreed to coordinate site work, if possible. The city is currently gathering timeline, permitting and cost information related to the pipe's replacement.



Stormwater pipe

Standards and Staff's Findings

Conditional Use Permit for educational institutions and facilities in a residential district	
The proposal would meet the general conditional use permit standards as outlined in City Code §300.16, Subd. 2	
Standard	Finding
The use is consistent with the intent of this ordinance;	Educational facilities are conditionally permitted uses within residential districts.
The use is consistent with the goals, policies, and objectives of the comprehensive plan;	The site improvement is consistent with the goals, policies, and objectives of the comprehensive plan, which recognizes the importance of educational facilities, and their

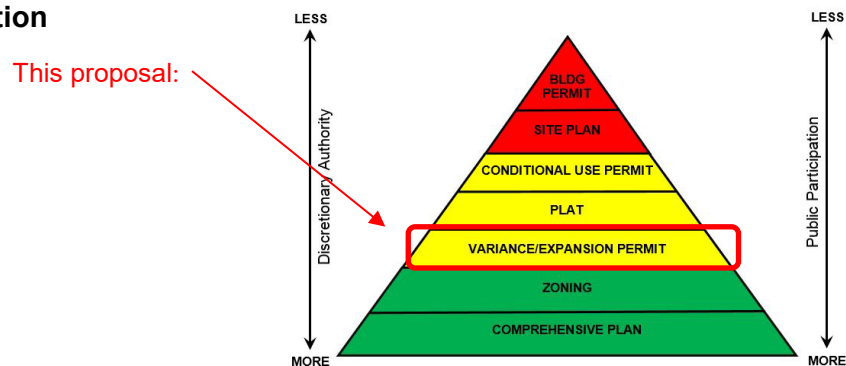
	unique transportation needs, in neighborhoods.
The use does not have an undue adverse impact on governmental facilities, utilities, services, or existing or proposed improvements;	The proposed use would not have an undue adverse impact on governmental facilities, utilities, services, or proposed improvements.
The use does not have an undue adverse impact on public health, safety, or welfare.	The use would not have an undue adverse impact on health, safety, and welfare.
The proposal would meet the specific conditional use permit standards as outlined in §300.16 Subd. 3(a):	
Standard	Finding
Direct access is limited to a collector or arterial roadway as identified in the comprehensive plan or otherwise located so that access can be provided without conducting significant traffic on local residential streets; the use is not permitted on property that has access only by way of a private road or driveway that is used by more than one lot;	Minnetonka Blvd is classified as an A Minor Expander. The school has access onto Minnetonka Blvd onto Groveland School Road. The proposal would direct traffic onto Minnetonka Blvd.
Buildings set back 50 feet from all property lines and parking setbacks are subject to section 300.28 of this ordinance;	The proposal is for site improvements and would not alter building setbacks. By ordinance, drive aisles and parking lots must have a 20-foot setback. As such, the proposal would require: (1) a variance to reduce the setback from 20 feet to 12 feet from the northern property line; and (2) an expansion permit from the eastern property line.
School bus pick-up and drop-off areas are located outside of the public right-of-way and designed to enhance vehicular and pedestrian safety;	The proposal would not alter the bus circulation through the site but is intended to improve vehicular circulation through the site.
Recreational areas designed for outdoor group activities set back 25 feet from residential property, suitable buffering provided to protect neighboring properties from noise and adverse visual impacts, and lighted playing fields permitted only upon demonstration that off-site impacts can be mitigated;	No changes are proposed to the recreational ball fields. However, the walking path would be increased to accommodate vehicular traffic. Existing vegetation would screen the path for adjacent properties and would only be utilized during drop-off and pick-up times.
Parking spaces and parking setbacks are subject to section 300.28 of this ordinance;	No changes are proposed to the existing parking lot. However, by ordinance, drive aisles have similar setback requirements and a variance and an expansion permit are required.
No more than 70 percent of the site is to be covered with impervious surface and the remainder to be suitably landscaped; and	The proposal would not result in the increase the amount of impervious surface to more than 70 percent.

site and building plan subject to review pursuant to section 300.27 of this ordinance.	The standards and findings are listed below.
The proposal would comply with all site and building standards as outlined in City Code 300.27 Subd.5	
Standard	Finding
Consistency with the elements and objectives of the city's development guides, including the comprehensive plan and water resources management plan;	The project has been reviewed by the city's planning, building, engineering, natural resources, fire, and public works staff. Staff finds it generally consistent with the city's development guides.
Consistency with this ordinance;	But for the requested variance and expansion permit, the proposal is consistent with the ordinance.
Preservation of the site in its natural state to the extent practicable by minimizing tree and soil removal and designing grade changes to be in keeping with the general appearance of neighboring developed or developing areas;	The property is developed, and the drop-off and pick-up drive lane would be created by expanding an existing pedestrian path in the northeast corner of the site. While disturbance would occur to prepare the site for the expansion of the path, the proposal would not significantly impact the natural state of the site.
Creation of a harmonious relationship of buildings and open spaces with natural site features and with existing and future buildings having a visual relationship to the development;	The proposal would not have a significant impact on the relationship between buildings and open space as the drive aisle is an expansion of an existing walking path.
<p>Creation of a functional and harmonious design for structures and site features, with special attention to the following:</p> <ul style="list-style-type: none"> a) an internal sense of order for the buildings and uses on the site and provision of a desirable environment for occupants, visitors, and the general community; b) the amount and location of open space and landscaping; c) materials, textures, colors, and details of construction as an expression of the design concept and the compatibility of the same with the adjacent and neighboring structures and uses; and d) vehicular and pedestrian circulation, including walkways, interior drives, and parking in terms of location and number of access points to the public streets, the width of interior drives and access points, general interior circulation, separation of 	<p>The proposal would expand an existing walking path to improve vehicular circulation through the site and increase the amount of stacking available on-site for drop-off and pick-up. The path would not be available for walking during drop-off and pick-up times. The path would then be closed for vehicular traffic outside of these times.</p>

<p>pedestrian and vehicular traffic, and arrangement and amount of parking.</p>	
<p>Promotion of energy conservation through design, location, orientation, and elevation of structures, the use and location of the glass in structures, and the use of landscape materials and site grading; and</p>	<p>The proposal expands an existing path to minimize the amount of grading and site disturbance.</p>
<p>Protection of adjacent and neighboring properties through reasonable provision for surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design not adequately covered by other regulations which may have substantial effects on neighboring land uses.</p>	<p>Vegetation would screen the path for adjacent properties. The runoff would be collected and directed to the underground storm chamber.</p>
<p>By City Code §300.07, Subd. 1, a variance may be granted from the requirements of this ordinance, including those placed on non-conformities.</p>	
<p style="text-align: center;">Standard</p>	<p style="text-align: center;">Finding</p>
<p>A variance is only permitted when it is in harmony with the general purposes and intent of this ordinance and when the variance is consistent with the comprehensive plan.</p>	<p>The variance would be in general harmony with the intent of the zoning ordinance and consistent with the comprehensive plan. The intent of the parking lot setback is to ensure adequate separation from adjacent properties. But for 100 feet, the path would be adjacent to Groveland Cemetery and Saint Luke's. The path would encroach four feet into the non-conforming setback adjacent to the residential property. This area would be screened by existing vegetation.</p>
<p>A variance may be granted when the applicant establishes that there are practical difficulties in complying with this ordinance. Practical difficulties means that the property owner proposes to use the property in a reasonable manner not permitted by this ordinance, the plight of the landowner is due to circumstances unique to the property not created by the landowner, and the variance, if granted, would not alter the essential character of the locality. Economic considerations alone do not constitute practical difficulties.</p>	<p>Reasonableness: The proposal is reasonable, as it would address queuing issues on the adjacent roadway during drop-off and pick-up times. To do this, the path would be widened by six feet at the widest point.</p> <p>Circumstances unique to the property: The variance is the result of the unique lot configuration and existing site improvements.</p> <p>Character of the locality: The ten-foot encroachment into the northerly setback is adjacent to an existing non-conforming parking lot on the St. Luke's property. The school district has a shared parking agreement to use this parking lot. The remaining encroachment would be screened by existing vegetation and topography.</p>
<p>By city code, an expansion permit for a non-conforming use may be granted but is not mandated, when an applicant meets the burden of proving that:</p>	

Standard	Finding
<p>The proposed expansion is a reasonable use of the property, considering such things as:</p> <ul style="list-style-type: none"> • Functional and aesthetic justifications for the expansions; • Adequacy of off-street parking for the expansion; • Absence of adverse off-site impacts from such things as traffic, noise, dust odors, and parking; • Improvement to the appearance and stability of the property and neighborhood. 	<p>The expansion is proposed to accommodate stacking that is currently extending off of the property onto Minnetonka Blvd.</p>
<p>The circumstances justifying the expansion are unique to the property, are not caused by the landowner, are not solely for the landowner's convenience, and are not solely because of economic considerations; and</p>	<p>The proposal would expand an existing walking path to minimize site impacts and impervious surface. The existing parking lot has a 3.5 feet setback at the closest point and is the result of the lot configuration and existing site improvements.</p>
<p>The expansion would not adversely affect or alter the essential character of the neighborhood.</p>	<p>The expansion would not adversely impact or affect the character of the locality, as it is for two smaller point intrusions.</p>

Pyramid of Discretion



Voting Requirement

The planning commission will make a recommendation to the city council. A recommendation for approval requires an affirmative vote of a simple majority. The city council's approval requires an affirmative vote of five members.

Motion Options

The planning commission has three options:

1. Concur with the staff recommendation. In this case, a motion should be made recommending the city council adopt the resolution approving the request.

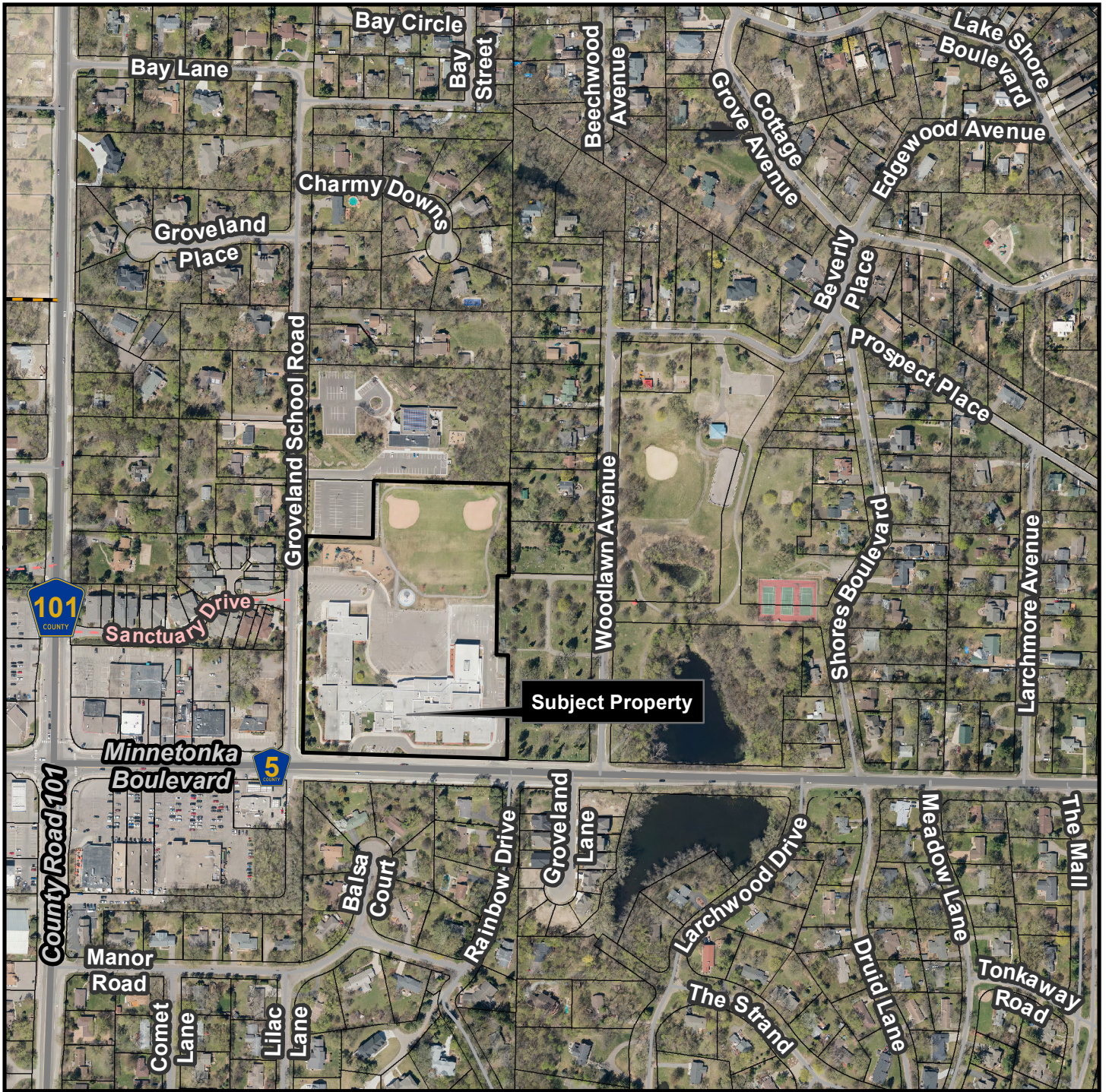
2. Disagree with the staff's recommendation. In this case, a motion should be made recommending the city council deny the request. This motion must include a statement as to why denial is recommended.
3. Table the requests. In this case, a motion should be made to table the item. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant, or both.

**Neighborhood
Comments**

The city sent notices to 92 area property owners and received no comments.

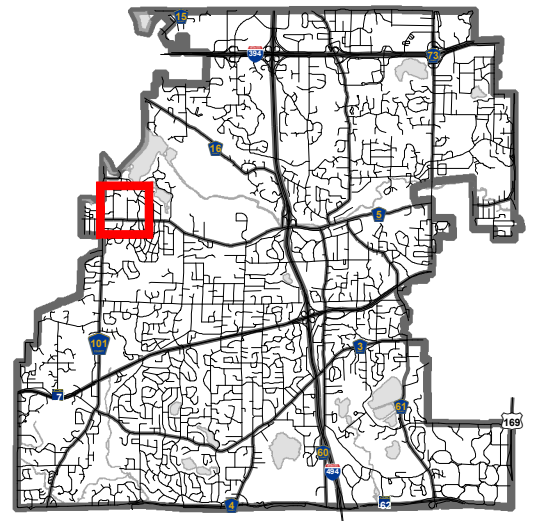
**Deadline for
Decision**

March 10, 2023



Location Map

Project: Groveland Elementary
 Address: 17310 Minnetonka Blvd.

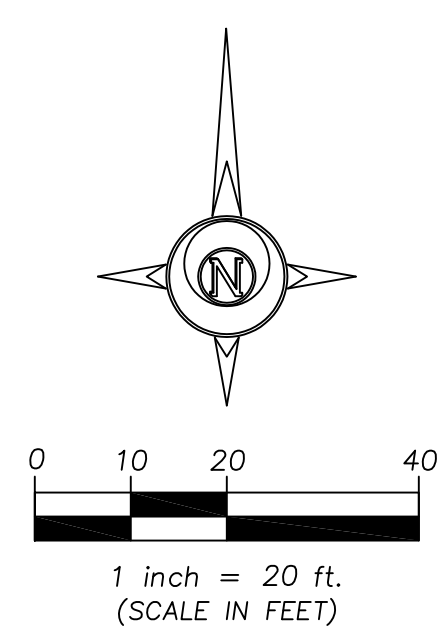


CERTIFICATION:

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision, and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Craig E. Johnson, LS, CFedS
Minnesota License No. 44530

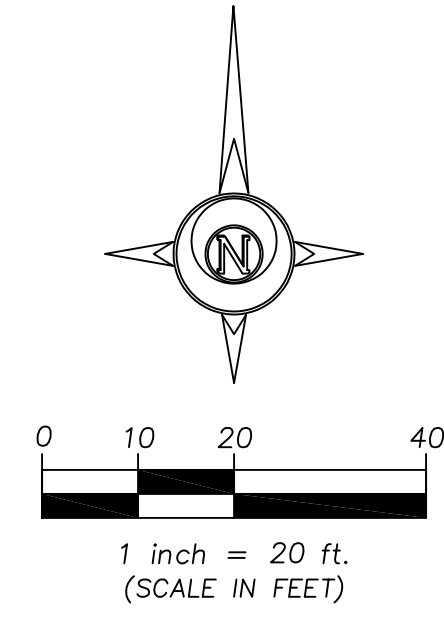
09/19/2016
Date



1 inch = 20 ft.
(SCALE IN FEET)



REVISIONS	DATE	INFO	DRAWN BY
	05/25/2016		Cody J.
			CHECKED BY
			Craig J.
			DATE
			09/19/2016
			PROJECT NO.
			CI6504
			SHEET NO.
			2

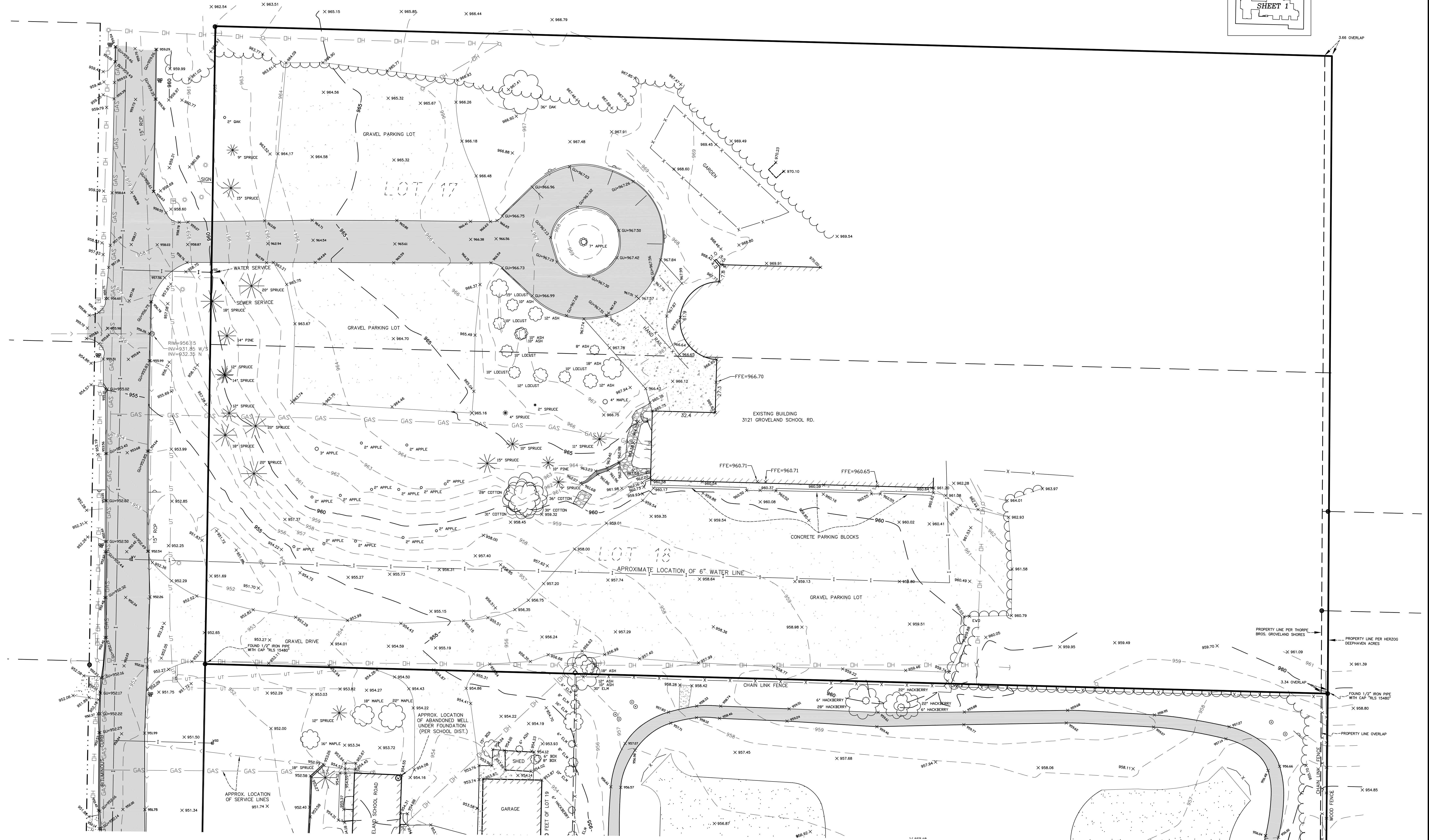
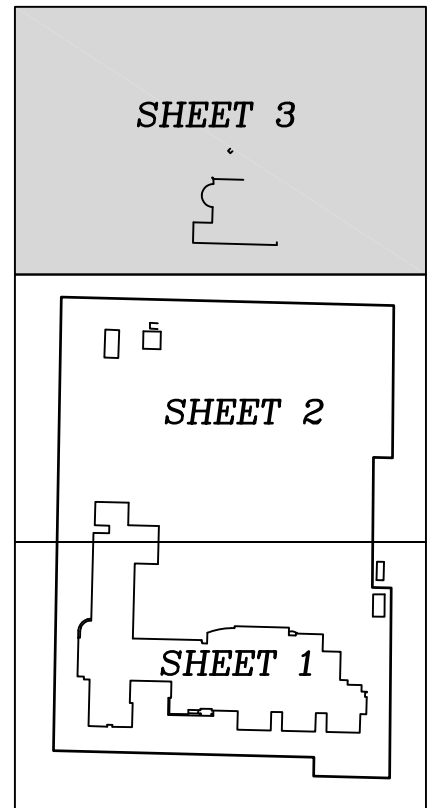


CERTIFICATION:
 I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision, and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Craig Johnson
 Craig E. Johnson, L.S., C.Fed.S.
 Minnesota License No. 44530

09/19/2016
 Date

SHEET KEY



REVISIONS	DATE	INFO	DRAWN BY
	05/25/2016		Cody J.
			CHECKED BY
			Craig J.
			DATE
			09/19/2016
			PROJECT NO.
			C16504
			SHEET NO.
			3

KEYED NOTES:

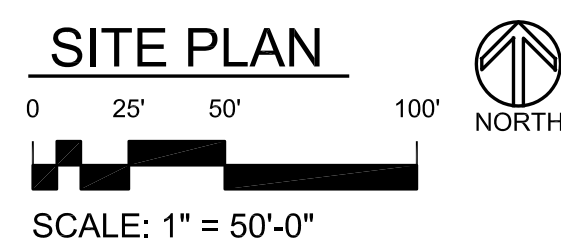
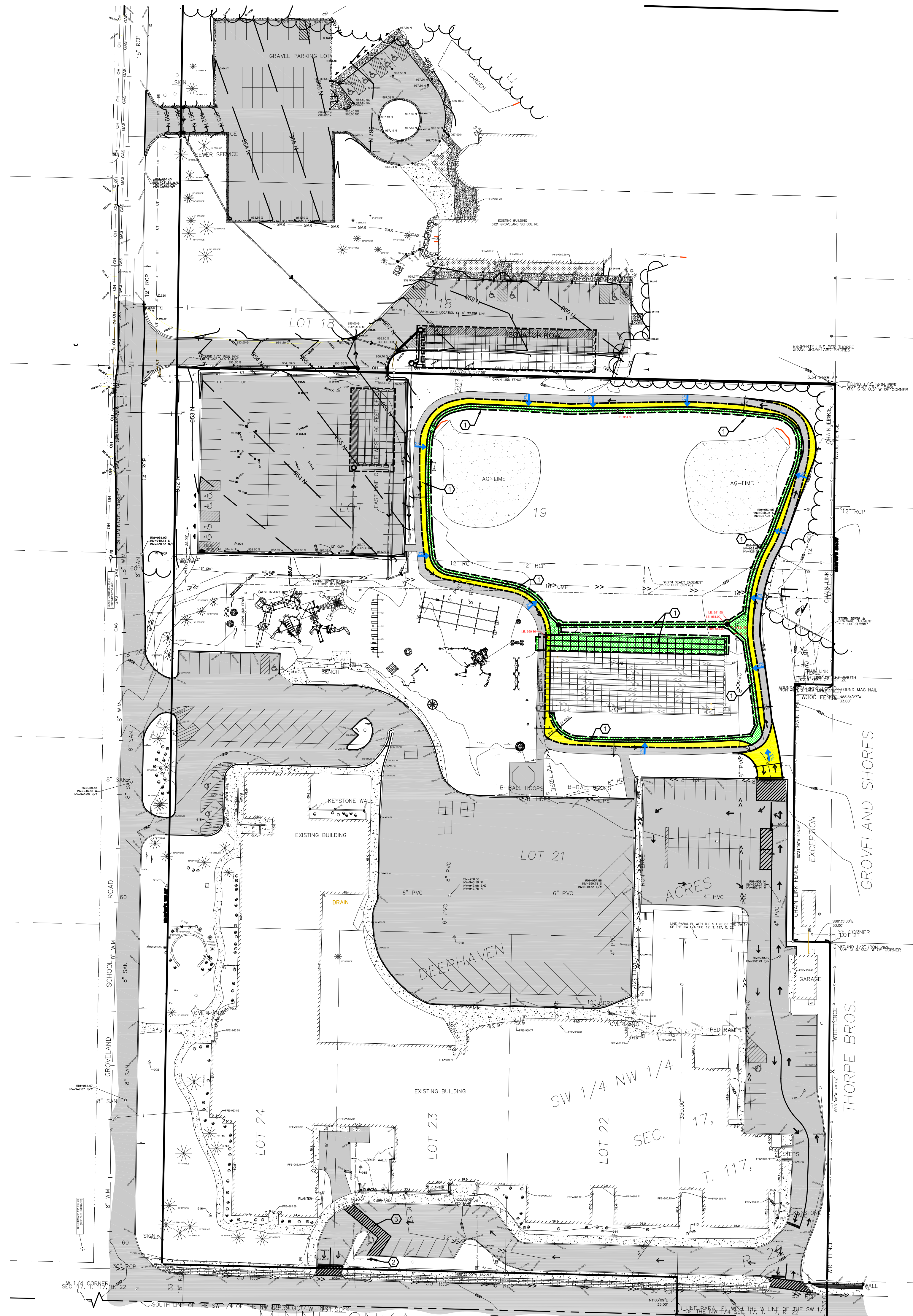
- ① 2023 PROPOSED PATH WIDENING FOR NEW DRIVE WITH 3 ADDED ROWS OF UNDERGROUND STORM CHAMBERS
- ② NEW "NO PARKING" SIGN AND POST.
- ③ PARKING LOTS AND DRIVES TO BE RESTRIPEDED AS SHOWN.

GROVELAND ELEMENTARY SCHOOL - PARKING SPACES

REGULAR	151
ADA	9
BUSES	12

ST. LUKE'S CHURCH - PARKING SPACES

REGULAR	77
ADA	8



**PRELIMINARY
 NOT FOR CONSTRUCTION**

Signature:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION,
 OR REPORT WAS PREPARED BY ME OR UNDER MY
 DIRECT SUPERVISION AND THAT I AM A
 DULY LICENSED PROFESSIONAL ENGINEER UNDER THE
 LAWS OF THE STATE OF MINNESOTA.

SIGNATURE: *Clifford W. Burman*
 TYPED OR PRINTED NAME: CLIFFORD W. BURMAN
 DATE: 11/02/22 REGISTRATION NO.: 17294

Issues and revisions:

ISSUE LEVEL / REVISION:	DATE:	No.:
LEVEL	DATE	

THIS LINE SCALES 1" ON FULL SIZE SHEETS

Client:

**MINNETONKA
 PUBLIC
 SCHOOLS**
 5621 COUNTY ROAD 101
 MINNETONKA, MINNESOTA 55345

**GROVELAND ELEMENTARY
 AND
 ST. LUKE'S CHURCH**
 17310 MINNETONKA BLVD
 MINNETONKA, MINNESOTA 55345

Project title:
**2023
 PATH WIDENING
 FOR DRIVE**
 17310 MINNETONKA BLVD
 MINNETONKA, MINNESOTA 55345

Sheet content:

SITE PLAN

DATE:	11-10-22
CLIENT PROJECT No.:	
INSPEC PROJECT No.:	215476
PROJECT MGR:	CB
DRAWN BY:	BT
CHECKED BY:	BB

Sheet No.:

SHEET SIZE: 24x36
 11/02/2022 1:18 PM BURMAN.TW/HT
 H:\Pavement\School Districts\DesignM - Z\Minnetonka\2023\Groveland ES Car Stacking Running Path cont'd from 2022\Drawings\SITE UTILITIES DRAWINGS\Drawings (AutoCAD Arch)\C-0-Inspeco-Curent Working Drawings\SITE UTILITY 9-22-22.dwg

GENERAL NOTES:

1. LOCATIONS AND SIZES OF ALL UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE ONLY. VERIFY ALL UTILITIES. CONTRACTOR RESPONSIBLE FOR REPAIR TO ANY DAMAGED UNDERGROUND UTILITIES, INCLUDING BUT NOT LIMITED TO, LAWN IRRIGATION SYSTEMS AND DRAIN TILE.
2. CONTRACTOR RESPONSIBLE FOR LOCATING AND PROTECTING ALL SITE UTILITIES. CONTACT GOPHER STATE ONE CALL AND PRIVATE LOCATOR PRIOR TO ANY DEMOLITION/EXCAVATION.
3. ALL CONSTRUCTION MUST COMPLY WITH APPLICABLE ORDINANCES.
4. CONTRACTOR SHALL OBTAIN AND PAY FOR ALL CONSTRUCTION PERMITS.
5. PROTECT EXISTING FACILITIES AND VEGETATION WHICH ARE TO REMAIN. RESTORE ALL DISTURBED AREAS, INCLUDING, BUT NOT LIMITED TO UTILITIES, IRRIGATION SYSTEMS, PAVEMENT, TREES LANDSCAPING, AND GRASSLAND/LAWN AREAS. GRASSLAND/LAWN AREAS TO BE DECOMPACTED AND RESTORED WITH 6" TOPSOIL, FERTILIZER AND STAKED SOD.
6. CONTRACTOR TO SWEEP SITE PAVEMENTS AND ADJACENT STREETS AT CONSTRUCTION VEHICLE ACCESS POINTS EACH WORK DAY WITH PICK UP SWEEPER OR EQUAL TO REMOVE ANY DEBRIS.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR EROSION CONTROL THROUGHOUT PROJECT, INCLUDING, BUT NOT LIMITED TO, STORM WATER STRUCTURES INLET PROTECTION.
8. ALL DIMENSIONS AND OR QUANTITIES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING.
9. CONTRACTOR TO PROVIDE ALL CONSTRUCTION STAKING.

KEYED NOTES:

- 1 NEW BITUMINOUS PAVEMENT, APPROX. 670 SQ. YDS. SEE DETAIL 3/C4.
- 2 4" BITUMINOUS PAVEMENT RECLAIM/RECONSTRUCT. PAVE FINAL LIFT WITH NEW PAVEMENT, APPROX. 910 SQ. YDS. SEE DETAIL 4/C4.
- 3 MATCH NEW BITUMINOUS PAVEMENT TO EXISTING.
- 4 INSTALL NEW 23" VBG6 (2 PANEL) ALUMINUM BARRIER GATE, ITEM #02324. SEE DETAIL 8/C4.
- 5 NEW UNDERGROUND STORM CHAMBERS, 3 ROWS WITH 22 CHAMBERS PER ROW, 66 TOTAL. SEE PLAN SHEET C5.
- 6 NEW 48" CONCRETE MONOLITHIC CATCH BASIN/MANHOLE #1, SEE CB/MH SCHEDULE AND DETAIL 5/C4.
- 7 NEW 48" CONCRETE MONOLITHIC CATCH BASIN/MANHOLE #2, SEE CB/MH SCHEDULE AND DETAIL 5/C4.
- 8 NEW 24" DIA. NYLOPLAST MANHOLE #3, SEE CB/MH SCHEDULE AND DETAIL 10/C4.
- 9 NEW 24" DIA. NYLOPLAST MANHOLE #4, SEE CB/MH SCHEDULE AND DETAIL 10/C4.
- 10 NEW 24" DIA. NYLOPLAST MANHOLE #5, SEE CB/MH SCHEDULE AND DETAIL 10/C4.
- 11 NEW 8" DIA. HDPE PIPE, APPROX. 7 L.F., SEE DETAIL 9/C4.
- 12 NEW 4" DIA. SLOTTED HDPE PIPE, APPROX. 525 L.F., SEE DETAIL 9/C4.
- 13 NEW 4" DIA. SLOTTED HDPE PIPE, APPROX. 330 L.F., SEE DETAIL 9/C4.
- 14 NEW 4" DIA. SLOTTED HDPE PIPE, APPROX. 180 L.F., SEE DETAIL 9/C4.
- 15 NEW 8" DIA. HDPE PIPE, APPROX. 145 L.F., SEE DETAIL 6/C4.
- 16 NEW 4" DIA. CLEANOUT, SEE DETAIL 9/C4.
- 17 INSTALL NEW 30' CANTILEVERED GATE, SEE DETAIL 11/C4.
- 18 STRIPE 2 NEW PARKING SPACES AND CROSSHATCHED AREA.
- 19 TOP OF NEW PAVEMENT ELEVATION TO MATCH TOP OF EXISTING PAVEMENT ELEVATION AT THIS LOCATION +/-0.1'.
- 20 RELOCATED TREE.

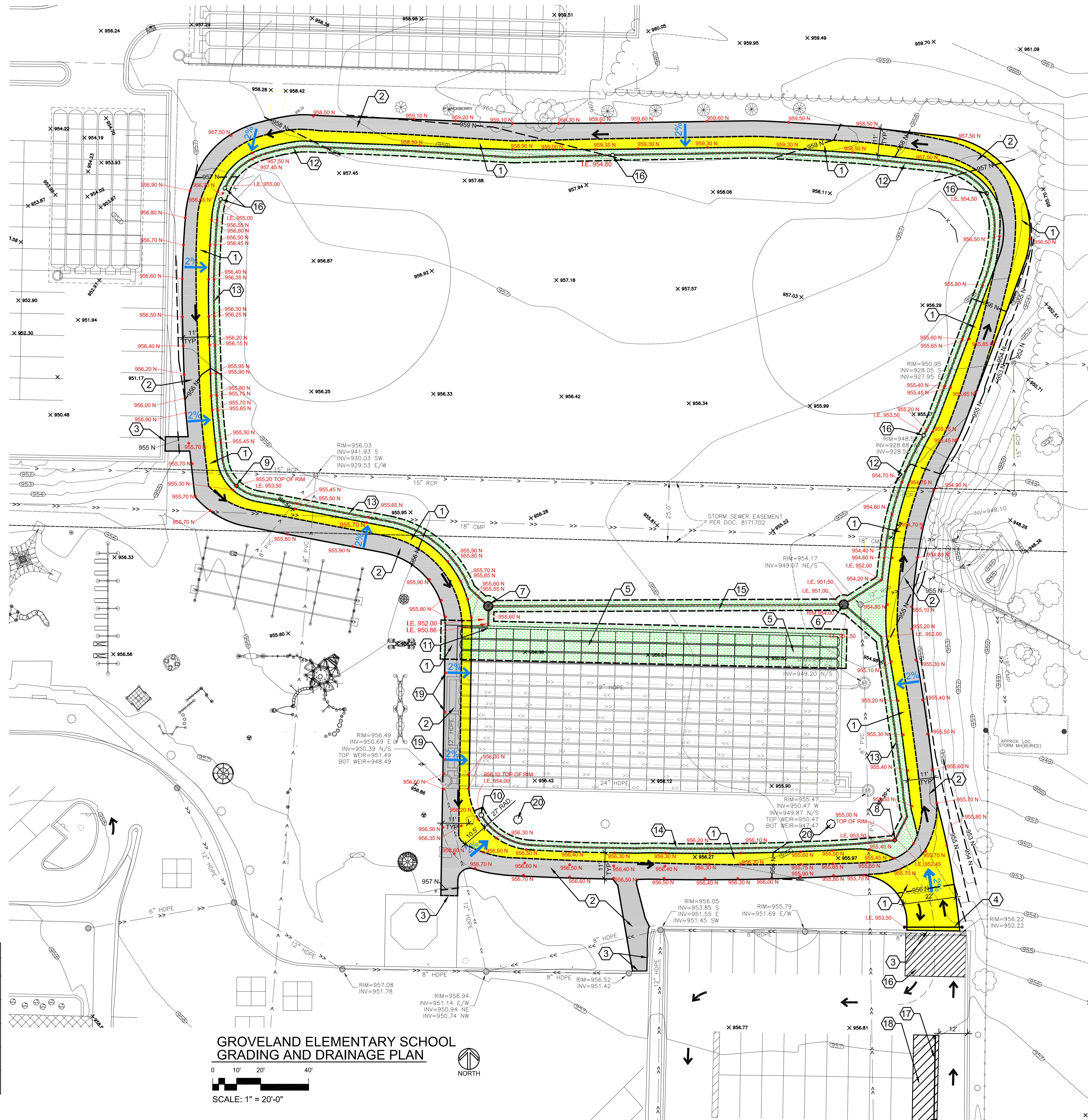
CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL QUANTITIES

- NEW BITUMINOUS PAVEMENT
- EXISTING BITUMINOUS PAVEMENT TO RECLAIM/RECONSTRUCT
- APPROXIMATE DISTURBED GRASS AREA
- DRAINAGE DIRECTION AND SURFACE SLOPE
- VEHICLE TRAFFIC DIRECTION ARROW STRIPING (WHITE)
- 955.70 N SPOT ELEVATION AND SYMBOL

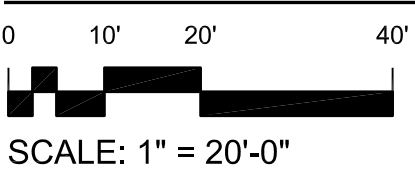
"N" DENOTES NEW TOP OF GRADE ELEVATION
ALL OTHER ELEVATIONS ARE EXISTING UNLESS NOTED OTHERWISE

CATCH BASIN/MANHOLE SCHEDULE

MH #	TYPE	INLET FRAME/GRATE	RIM ELEV.	INV. ELEV. IN	INV. ELEV. OUT	INV. ELEV. BOTTOM
MH #1	48" DIA. CONC. MONOLITHIC	NEENAH R-2270 WITH R-2550A FRAME AND TYPE C RADIAL GRATE AND ADS FLEXSTORM PURE PERMANENT INLET PROTECTION	954.10	951.50	951.00	948.00
MH #2	48" DIA. CONC. MONOLITHIC	NEENAH R-2270 WITH R-2550A FRAME AND TYPE C RADIAL GRATE AND ADS FLEXSTORM PURE PERMANENT INLET PROTECTION	956.00 TOP OF EXISTING GRADE	952.00	950.86	947.86
MH #3	NEW 24" DIA. NYLOPLAST	HEAVY DUTY INLET FRAME/GRATE	955.50	953.20	953.20	952.70
MH #4	NEW 24" DIA. NYLOPLAST	HEAVY DUTY INLET FRAME/GRATE	955.20	953.20	953.20	952.70
MH #5	NEW 24" DIA. NYLOPLAST	HEAVY DUTY INLET FRAME/GRATE	956.10	954.00	954.00	953.50



GROVELAND ELEMENTARY SCHOOL GRADING AND DRAINAGE PLAN



Consultants:

**PRELIMINARY
NOT FOR CONSTRUCTION**

Signature:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM AN ONLY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature: *Clifford W. Bodman*
Typed or Printed Name: CLIFFORD W. BODMAN
Date: 10/13/22 Registration No.: 17594

Issues and revisions:

ISSUE LEVEL / REVISION:	DATE:	No.:
LEVEL	DATE	

THIS LINE SCALES 1" ON FULL SIZE SHEETS

Client:

**MINNETONKA
PUBLIC
SCHOOLS**

5621 COUNTY ROAD 101
MINNETONKA, MINNESOTA 55345

**GROVELAND
ELEMENTARY**

17310 MINNETONKA BLVD
MINNETONKA, MINNESOTA 55345

Project title:

**2023
PATH WIDENING
FOR DRIVE**

17310 MINNETONKA, BLVD
MINNETONKA, MINNESOTA 55345

Sheet content:

**GROVELAND ELEMENTARY SCHOOL
GRADING AND DRAINAGE PLAN**

DATE: 7-20-22
CLIENT PROJECT No.:
INSPEC PROJECT No.: 215476
PROJECT MGR: CB
DRAWN BY: BT
CHECKED BY: BB

Sheet No.:

C3



Memorandum

SRF No. 16267

To: Ashley Cauley, Senior Planner
City of Minnetonka

From: Brent Clark, PE, Traffic Studies Lead
Ashley Sherry, EIT, Traffic Engineer I

Date: January 10, 2023

Subject: Groveland Elementary School Transportation Study

Introduction

SRF has completed a transportation study for Groveland Elementary School in the City of Minnetonka, MN. The school is located in the northeast quadrant of the Minnetonka Boulevard and Groveland School Road intersection (see Figure 1: Project Location). The current parent pick-up/drop-off configuration at the elementary school results in queues that extend onto Minnetonka Boulevard. To improve vehicle storage and reduce queueing issues, the school district has developed potential improvement alternatives. Therefore, the main objectives of the transportation study were to review the existing operations within the study area, evaluate the potential improvement alternatives developed by the school district, and recommend any additional improvement alternatives and/or modifications to mitigate the existing queueing issues and provide safe and efficient operations for all modes of transportation. The following information provides the assumptions, analysis, and recommendations offered for consideration.

Existing Conditions

Existing conditions were reviewed to identify any existing operational issues within the study area. The evaluation of existing conditions includes various data collection efforts, including traffic data, roadway characteristics, school/pedestrian operations, and an intersection capacity analysis, which are summarized in the following sections.

Study Intersections

The following study intersections represent the primary focus of the transportation study. These intersections were identified through discussion with City staff as they relate to potential school impacts, as well as future area infrastructure needs.

- Minnetonka Boulevard and Groveland School Road
- Minnetonka Boulevard and Groveland Elementary West Access/Pedestrian Crossing
- Minnetonka Boulevard and Groveland Elementary East Access
- Minnetonka Boulevard and Woodlawn Avenue
- Groveland School Road and St. Luke Presbyterian Church Access
- Groveland School Road and Groveland Elementary North/South Access

www.srfconsulting.com

3701 Wayzata Boulevard, Suite 100 | Minneapolis, MN 55416-3791 | 763.475.0010 Fax: 1.866.440.6364

An Equal Opportunity Employer



Project Location

Groveland Elementary School Transportation Study
City of Minnetonka

Traffic Data

School arrival and departure peak hour vehicular and pedestrian/bicyclist turning movement counts were collected at the study intersections by SRF during the week of November 28, 2022. Note the Groveland Elementary School hours are currently between 8:40 a.m. and 3:20 p.m. The peak hours of the study area were determined to be:

- **School Arrival Peak Hour:** 7:50 a.m. to 8:50 a.m.
- **School Departure Peak Hour:** 3:00 p.m. to 4:00 p.m.

Roadway Characteristics

A field assessment was completed to identify various roadway characteristics within the transportation system study area, such as functional classification, general configuration, and posted speed limit. A summary of these roadway characteristics is shown in Table 1. Note that these are general characteristics and that there are some deviations within the area or segments of the roadways. For example, school zones are present along Minnetonka Boulevard, in which the speed limit changes from 35 miles per hour (mph) to 25 mph during the school arrival and departure times.

Table 1. Existing Roadway Characteristics

Roadway	Functional Classification ⁽¹⁾	General Configuration	Posted Speed Limit (mph)
Minnetonka Boulevard	A-Minor Arterial	3-lane undivided ⁽²⁾	35/25 ⁽³⁾
Groveland School Road	Local Street	2-lane undivided	25
Woodlawn Avenue	Local Street	2-lane undivided	Unposted

(1) Functional classification based on the *City of Minnetonka's 2040 Comprehensive Plan*.

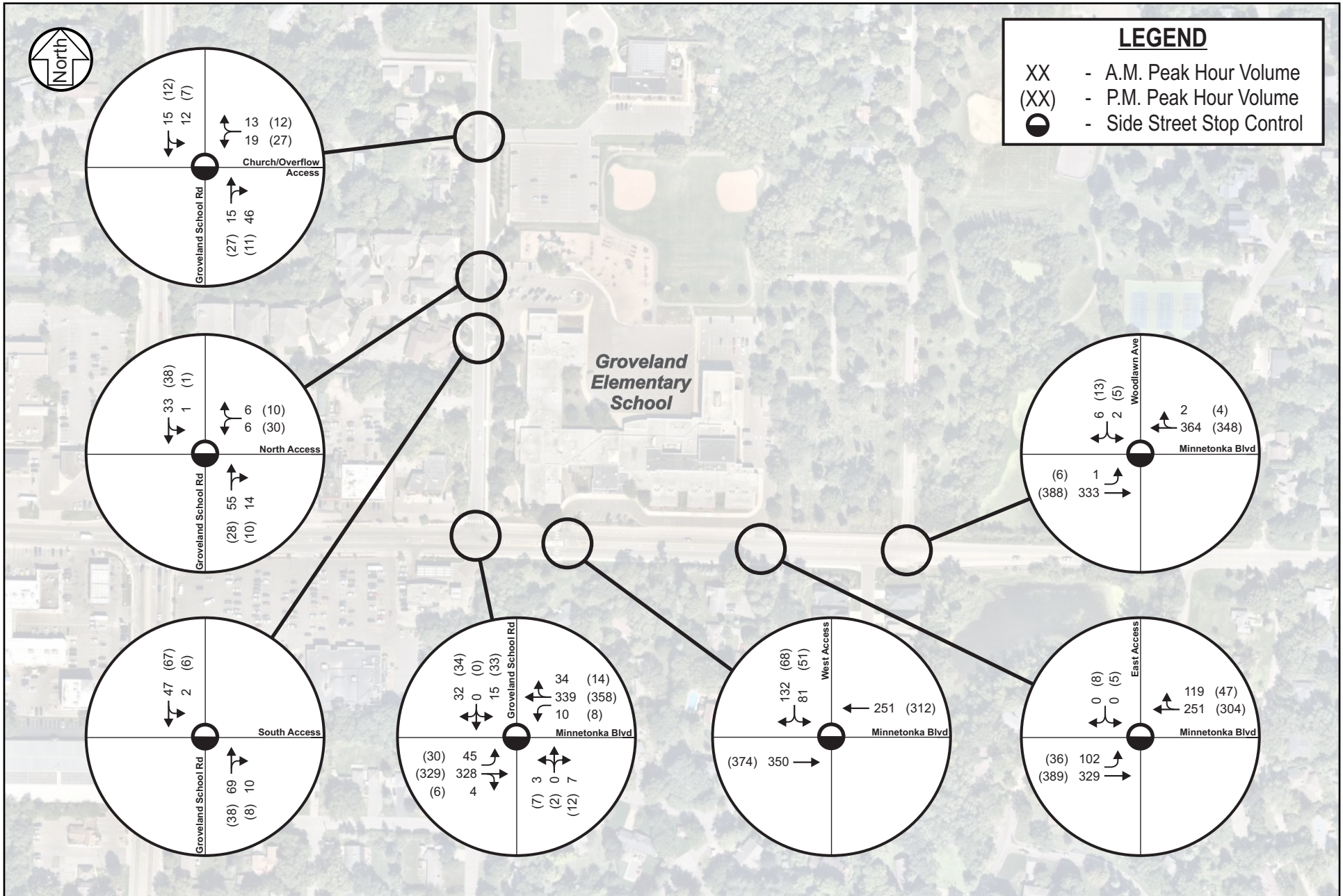
(2) Minnetonka Boulevard transitions to a 2-lane undivided roadway east of Woodlawn Avenue.

(3) A school zone begins approximately 35 feet east of the school's east access and ends approximately 150 feet west of the school's west access. The speed limit of the school zone is 25 mph and is generally limited to school start and end times.

From a traffic control perspective, all study intersections are unsignalized with side-street stop control. No crossing guards or traffic control officers were observed at any study intersections. Existing geometrics, traffic controls, and traffic volumes in the study area are shown in Figure 2.

Groveland Elementary School Operations

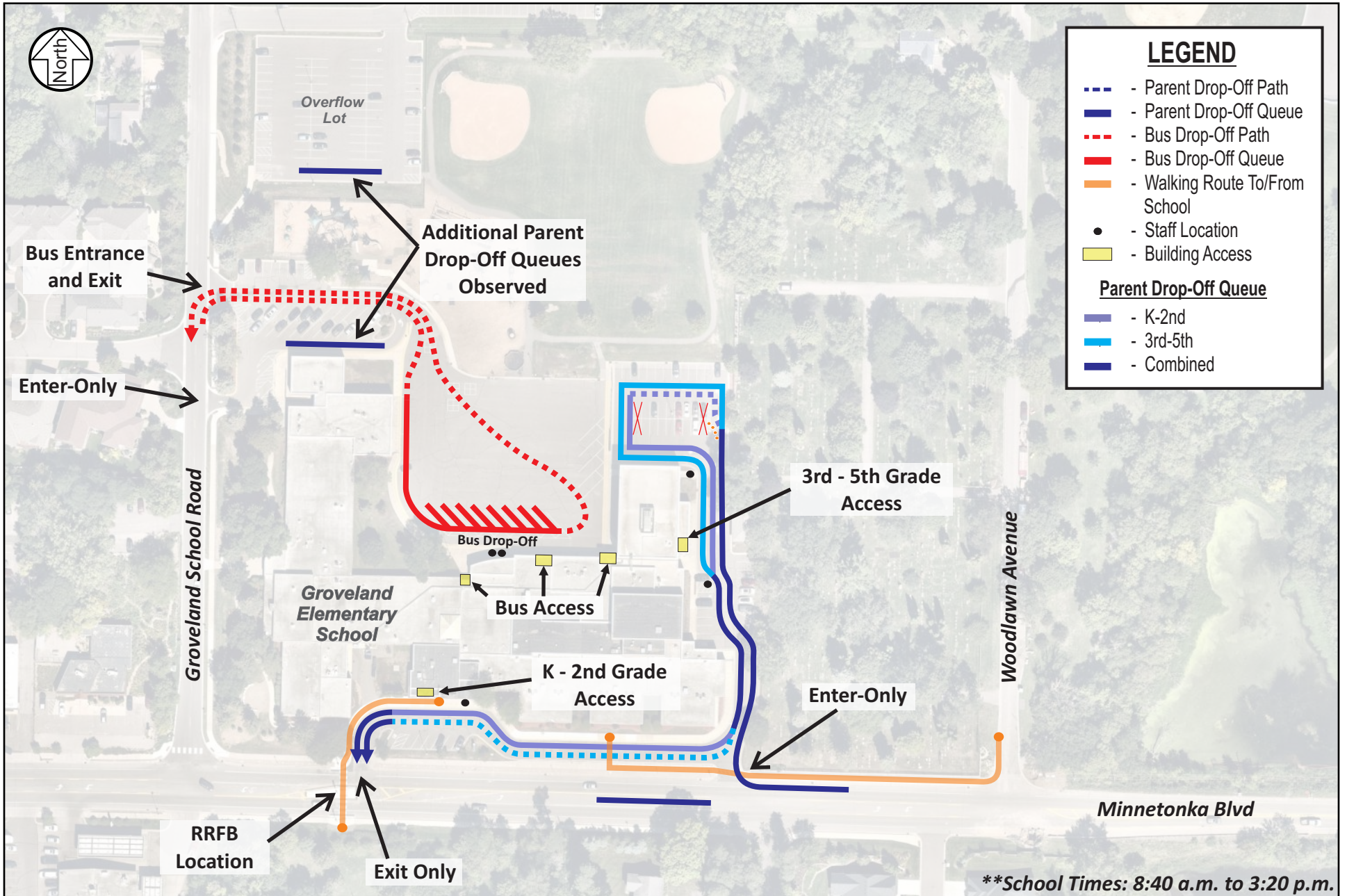
As stated previously, the Groveland Elementary School hours of operations are currently between 8:40 a.m. and 3:20 p.m. The elementary school, which serves grades K-5, currently has an enrollment of approximately 880 students. In order to identify current travel patterns and any access, circulation, pick-up/drop-off, parking, and/or pedestrian crossing issues, field observations were performed at the school. A summary of the elementary school operations during the arrival and departure peak periods are shown in Figures 3 and 4, respectively, and are summarized in the following sections.

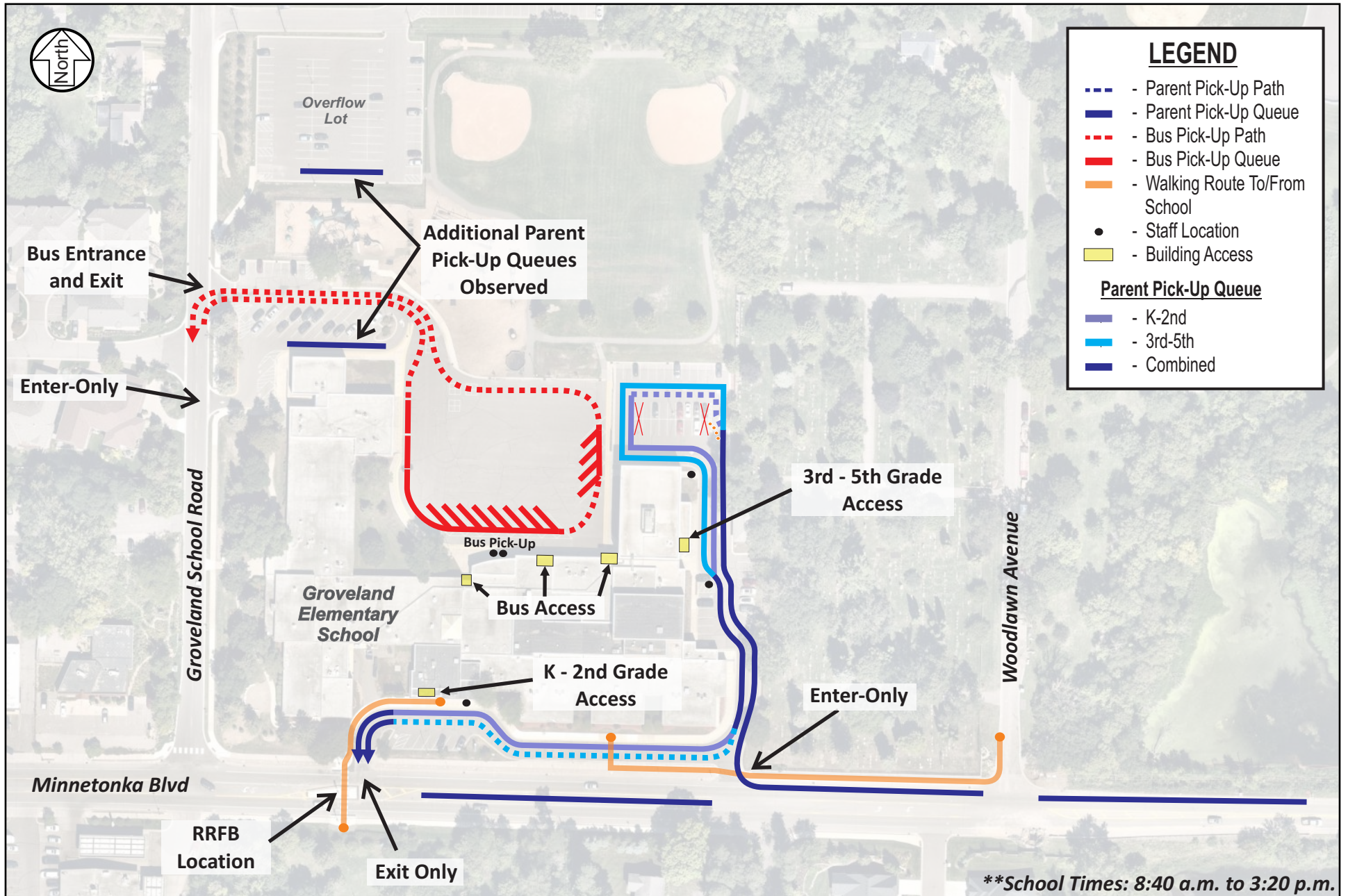


Existing Conditions

Groveland Elementary School Transportation Study
City of Minnetonka

Figure 2





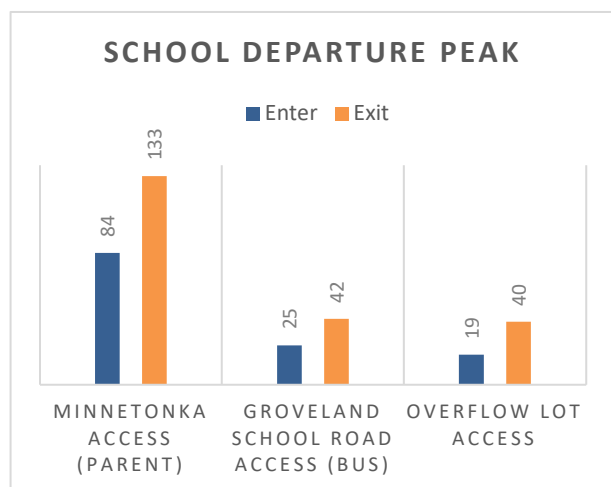
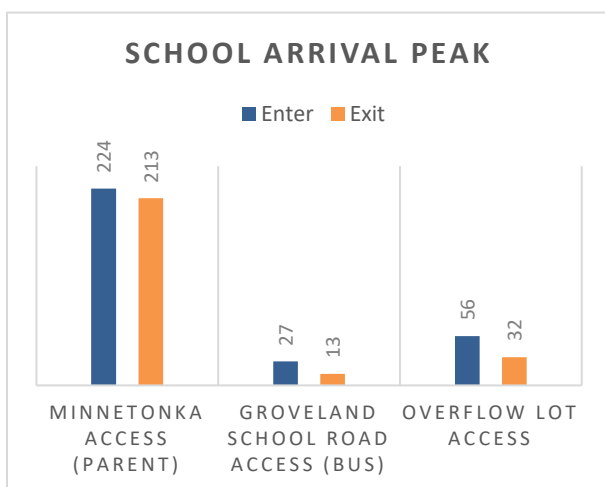
Existing PM School Pick Up Operations
 Groveland Elementary School Transportation Study
 City of Minnetonka

Figure 4

Site Observations

The elementary school has three primary parking lot/vehicular access locations, each serving a different purpose for the school. Driveway counts for trips entering/exiting the school during peak hours are summarized in the insets below.

- The **Minnetonka Boulevard Access** is designated for staff parking and parent pick-up/drop-off operations. There are two access locations (i.e. east and west) along Minnetonka Boulevard. During peak school arrival and departure periods, the east access operates as an enter-only and the west access operates as an exit-only. Parents enter the east access and queue in a counterclockwise direction around the northeast parking lot of the school. Building access is separated by grades; 3rd through 5th have access on the east side of the building, whereas Kindergarten through 2nd have access on the south side of the building, which also serves as the main building entrance. Multiple staff members are positioned in the area to help facilitate the process and direct students to/from the appropriate access. Note site circulation is discussed further in the following section. There are approximately 40 parking spaces east and northeast of the school that are utilized by staff and seven (7) parking spaces in front of the main building access that are designated for visitors.
- The **Groveland School Road Access** is designated for staff parking and bus pick-up/drop-off operations. Similar to Minnetonka Boulevard, there are two access locations along Groveland School Road. The south access is an enter-only and operates as a one-way, and the north access operates as a two-way full access. Busses utilize the north access in order to access the bus pick-up/drop-off area north of the school. It should be noted that some parents have permission from the school to use this access for parent pick-up/drop-offs.
- The **Overflow Access** is located near St. Luke’s Presbyterian church and is used for additional staff parking. The access is approximately 300 feet north of the Groveland School Road north access. In addition, some parents were observed to use the lot for pick-up/drop-offs.



During morning drop-off's, students are not allowed to enter the school until 8:20 a.m., with school starting at 8:40 a.m. Parents start arriving to the school between 7:55 and 8:00 a.m. In general, the school area was clear by 8:45 a.m. Internal queuing was observed to extend onto Minnetonka Boulevard for short periods of time (i.e. approximately five (5) minutes) and queues were generally only three (3) to five (5) vehicles.

During afternoon pick-ups, travel patterns were the same as the morning drop-off. Students were released at 3:20 p.m. and in general all students were picked up and the school area was cleared within 20 minutes. Parents were observed to arrive at the school early, with some parents arriving as early as 2:45 p.m. On-site storage would fill up and queues would extend onto Minnetonka Boulevard for 10 to 25 minutes. Westbound queues were observed to extend between Woodlawn Avenue and Shores Boulevard. The on-site storage provided is estimated to be approximately 1,670 feet (~67 vehicles), whereas approximately 2,500 feet (~100 vehicles) is needed during the afternoon peak.

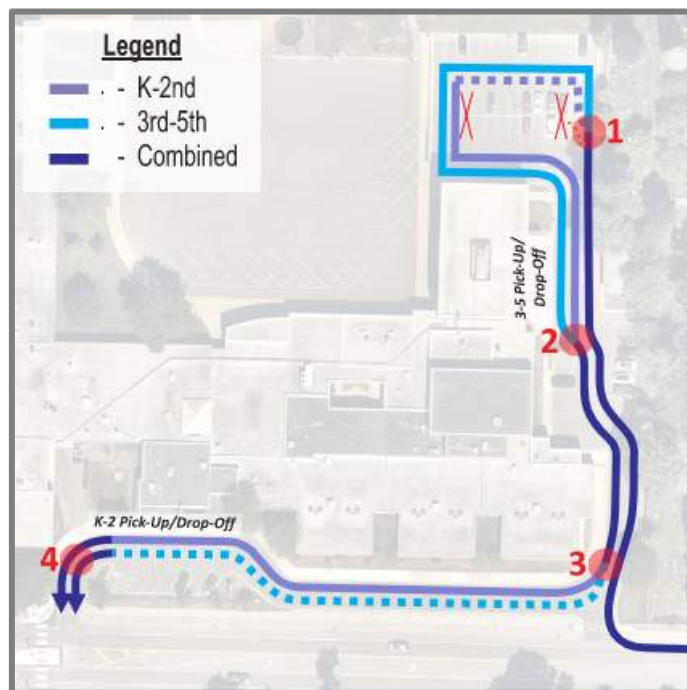
Site Circulation

The elementary school property has several site constraints given the existing access/parking lot, building footprint, and property lines. These constraints result in conflict areas that reduce the parent pick-up/drop-off efficiency and can cause driver confusion. However, the school is operating the site as efficiently as possible, given the constraints. A summary of the internal conflict areas is summarized below. As future improvements are evaluated, it will be important to see if any opportunities are available to improve site circulation at the school.

1) The 3rd/5th queuing blocks K/2nd vehicles from entering their designated storage lane, leaving available storage space underutilized.

2) A merge occurs after the 3rd/5th pick-up/drop-off area. The merge is necessary due to the width of the roadway (i.e. 26 feet) and to transition K/2nd into the inner lane. The merge is facilitated by a staff member.

3) The K/2nd queuing blocks 3rd/5th vehicles from exiting the site. Some 3rd/5th vehicles were observed to drive into the entering lane to get around the queue.



4) The southbound approach operates as a southbound left- and right-turn lane. When K/2nd vehicles are done picking-up/dropping-off it can be difficult for vehicles to position themselves in their desired turn lane (i.e. K/2nd making a southbound-left or 3rd/5th making a southbound-right).

School Pedestrians Routes/Crossing

In addition to vehicular observations, the existing pedestrian routes to/from school were observed to determine if there are any existing issues. In general, the elementary school is facilitated with nearby pedestrian and bicycle facilities. There is a sidewalk on both sides of Minnetonka Boulevard that ends at Woodlawn Avenue, and there is a sidewalk on the east side of Groveland School Road that ends near the St. Luke's Presbyterian Church. During observations, it was unclear if pedestrians were walking to/from neighborhoods, or if parents were parking in locations such as the BP gas station/Lakewinds Food Co-op or Woodlawn Avenue and walking students to/from school to avoid the designated parent pick-up/drop-off.

The primary crossing was observed to be at the rectangular rapid flashing beacon (RRFB) located along Minnetonka Boulevard, just west of the west school access. During the school arrival peak hour there were a total of 22 pedestrian crossings at the RRFB, seven (7) of which were children. During the school departure peak hour there were a total of 30 pedestrian crossings at the RRFB, 14 of which were children. All 21 children that crossed the intersection during school peak hours were accompanied by an adult. The RRFB was observed to be activated at every crossing. In general, no vehicle compliance issues were observed.

Parking Demand

SRF conducted parking utilization surveys at the elementary school on Thursday, December 1, 2022, during one time period (i.e., mid-morning when school is in session) to understand the typical weekday parking demand, which is summarized in Table 2. Results of the data indicate that the parking immediately adjacent to the school is effectively "full" shortly after school starts, with a utilization between 73 to 100 percent. In general, it is desirable to provide enough parking to serve the demand plus an additional five (5) percent, to reduce unnecessary circulation and the perception of inadequate parking. However, there is available parking in the overflow lot, if parking spaces are full near the school.

Table 2. Parking Utilization – Thursday, December 1, 2022

Parking Lot	Parking Supply	9:00 AM	
		Occupied Spaces	Percent Utilized
Overflow	75	26	35%
Northwest	34	34	100%
Northeast	26 ⁽¹⁾	19	73%
East	14	14	100%
South	7	6	86%
Total	156	99	63%

(1) Note there is a total of 34 parking spaces in the northeast lot, however, 8 parking spaces are restricted for parent pick-up/drop-off operations

Intersection Capacity Analysis

An intersection capacity analysis was completed to identify any existing operational issues within the study area. The capacity analysis was completed for the school a.m. and p.m. peak periods of the adjacent roadway. All intersections were analyzed using Synchro/SimTraffic software.

Capacity analysis results identify a Level of Service (LOS) which indicates how well an intersection is operating. Intersections are graded from LOS A through LOS F. The LOS results are based on average delay per vehicle results from SimTraffic, which correspond to the delay threshold values shown in Table 3. LOS A indicates the best traffic operation and LOS F indicates an intersection where demand exceeds capacity. Overall intersection LOS A through D is generally considered acceptable by drivers in the Twin Cities Metropolitan Area.

Table 3. Level of Service Criteria for Signalized and Unsignalized Intersections

LOS Designation	Signalized Intersection Average Delay/Vehicle (seconds)	Unsignalized Intersection Average Delay/Vehicle (seconds)
A	≤ 10	≤ 10
B	> 10 - 20	> 10 - 15
C	> 20 - 35	> 15 - 25
D	> 35 - 55	> 25 - 35
E	> 55 - 80	> 35 - 50
F	> 80	> 50

For side-street stop-controlled intersections, special emphasis is given to providing an estimate for the level of service of the side-street approach. Traffic operations at an unsignalized intersection with side-street stop control can be described in two ways. First, consideration is given to the overall intersection level of service. This takes into account the total number of vehicles entering the intersection and the capability of the intersection to support these volumes.

Second, it is important to consider the delay on the minor approach. Since the mainline does not have to stop, the majority of delay is attributed to the side-street approaches. It is typical of intersections with higher mainline traffic volumes to experience high-levels of delay (i.e., poor levels of service) on the side-street approaches, but an acceptable overall intersection level of service during peak hour conditions.

Schools generally have higher peaking characteristics as compared to typical roadway network during the a.m. and p.m. peak hours (i.e., traffic on/near school grounds tend to be busy for a short period of time close to school start and end times). Therefore, traffic operations were reviewed over two different time intervals to better understand peak conditions. First, the entire peak hour (i.e., 60-minute interval) was analyzed, which is the traffic industry standard. Second, the peak 15-minute interval was reviewed to give a better understanding of operational issues expected in the immediate school area during the periods before and after school start and end times.

Results of the existing intersection capacity analysis (60-minute interval), shown in Table 4, indicate that all study intersections currently operate at an acceptable overall LOS B or better during the school arrival and departure peak hours. In addition, the school access locations were reviewed further during the peak 15-minute arrival/departure period, which is shown in Table 5. Results of the analysis indicate that the east access becomes overloaded during the peak 15-minute interval near school start and end times.

Table 4. Existing Intersection Capacity Analysis (60-Minute Analysis)

Intersection	School Arrival Peak Hour ⁽²⁾		School Departure Peak Hour ⁽²⁾	
	LOS	Delay	LOS	Delay
Groveland School Road / Overflow (Church) Access ⁽¹⁾	A/A	4 sec.	A/A	4 sec.
Groveland School Road / Elementary North Access ⁽¹⁾	A/A	4 sec.	A/A	4 sec.
Groveland School Road / Elementary South Access ⁽¹⁾	A/A	1 sec.	A/A	1 sec.
Minnetonka Boulevard / Groveland School Road ⁽¹⁾	A/A	8 sec.	A/A	9 sec.
Minnetonka Boulevard / Elementary West Access ⁽¹⁾	A/A	8 sec.	A/A	6 sec.
Minnetonka Boulevard / Elementary East Access ⁽¹⁾	A/B	12 sec.	B/C	18 sec.
Minnetonka Boulevard / Woodlawn Avenue ⁽¹⁾	A/A	6 sec.	A/B	12 sec.

(1) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst side-street approach LOS. The delay shown represents the worst side-street approach delay.

(2) The school arrival and departure peak hours are defined as 7:50 to 8:50 a.m. and 3:00 to 4:00 p.m., respectively.

Table 5. Existing Intersection Capacity Analysis (Peak 15-Minute Analysis)

Intersection	School Arrival Peak Hour ⁽²⁾		School Departure Peak Hour ⁽²⁾	
	LOS	Delay	LOS	Delay
Groveland School Road / Overflow (Church) Access ⁽¹⁾	A/A	4 sec.	A/A	4 sec.
Groveland School Road / Elementary North Access ⁽¹⁾	A/A	5 sec.	A/A	4 sec.
Groveland School Road / Elementary South Access ⁽¹⁾	A/A	1 sec.	A/A	1 sec.
Minnetonka Boulevard / Groveland School Road ⁽¹⁾	A/A	10 sec.	A/A	10 sec.
Minnetonka Boulevard / Elementary West Access ⁽¹⁾	A/A	10 sec.	A/A	9 sec.
Minnetonka Boulevard / Elementary East Access ⁽¹⁾	C/D	31 sec.	D/F	51 sec.
Minnetonka Boulevard / Woodlawn Avenue ⁽¹⁾	A/A	6 sec.	C/E	48 sec.

(1) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst side-street approach LOS. The delay shown represents the worst side-street approach delay.

(2) The school arrival and departure peak hours are defined as 7:50 to 8:50 a.m. and 3:00 to 4:00 p.m., respectively.

The following information summarizes the operational and/or queuing issues identified as part of the existing conditions analysis:

- Minnetonka Blvd/East School Access

- As mentioned previously, the east access operates as an enter-only during peak school periods. When the on-site storage is full, queues extend onto Minnetonka Boulevard. This was observed to occur approximately 5 minutes during the morning arrival peak hour, and approximately 10 to 25 minutes during the afternoon departure peak hour.
 - As illustrated in Figures 3 and 4, the queuing on Minnetonka Boulevard is only approximately three (3) to five (5) vehicles during the morning, whereas during the afternoon westbound queues can extend between Woodlawn Avenue and Shores Boulevard (approximately 575 feet).
 - When queues extended onto Minnetonka Boulevard, westbound vehicles destined to the school would position themselves in the shoulder and generally did not block westbound through vehicles. Westbound right-turn vehicles were observed to let eastbound left-turn vehicles into the site.
 - Although eastbound left-turns were generally let into the queue during maximum peak times, it can be difficult to find gaps given the other conflict points at the intersection, such as on-site queuing spacing, westbound thru- vehicles, and pedestrian crossings. This resulted in eastbound left-turn vehicles accepting smaller gaps, which may present a safety issue.

Improvement Consideration: Provide more on-site vehicular storage to reduce queuing impacts to Minnetonka Boulevard. Note the current on-site storage is estimated to be approximately 1,670 feet, whereas approximately 2,500 feet of storage is needed during the afternoon peak hour.

- Minnetonka Blvd/West School Access

- The southbound approach was observed to operate as a southbound right- and southbound left-turn configuration. During busy pick-up/drop-off times, vehicles were observed to have difficulty positioning themselves in the desired travel lane. This resulted in multiple vehicles making a southbound right-turn in the southbound left-turn lane.
- Some vehicles were observed to utilize the pedestrian refuge island area as a two-stage crossing for left-turn movements.

Improvement Consideration: Consider relocating the K-2 pick-up/drop-off area or providing only one pick-up/drop-off area to improve internal site circulation and reduce conflict areas.

Future Enrollment

The current enrollment at Groveland Elementary School is approximately 880 students and 130 staff members. Based on discussions with the Minnetonka School District, no enrollment increases, beyond normal yearly fluctuations, are expected at the school. Therefore, the current vehicular travel patterns and storage needs were used as the basis for the alternative evaluation.

Improvement Alternatives

There is inadequate on-site storage and multiple internal conflict areas at Groveland Elementary School during parent pick-up/drop-offs. As mentioned previously, there are several constraints on-site (such as building footprints, property lines, and travel lane widths) that limit the ability to improve pick-up/drop-off efficiencies. While the site is currently being managed as well as possible given these constraints, it doesn't have the necessary on-site storage to accommodate the vehicular demand, which results in queuing on Minnetonka Boulevard during the school arrival and departure peak periods. Therefore, two (2) improvement alternatives were identified and evaluated. Both Alternatives would provide benefits to the school and study area. The benefits and potential issues associated with each alternative are summarized in the following sections.

It should be noted that the estimated queuing for each alternative is based on the existing queues at the east access, along with an additional 200 feet (approximately 8 to 10 vehicles) to account for parents that may currently be avoiding the pick-up/drop-off area, and picking up/dropping off students elsewhere near campus. While these parents may continue to utilize other locations, the additional vehicles help provide a conservative estimate for future queuing with each alternative.

Alternative A

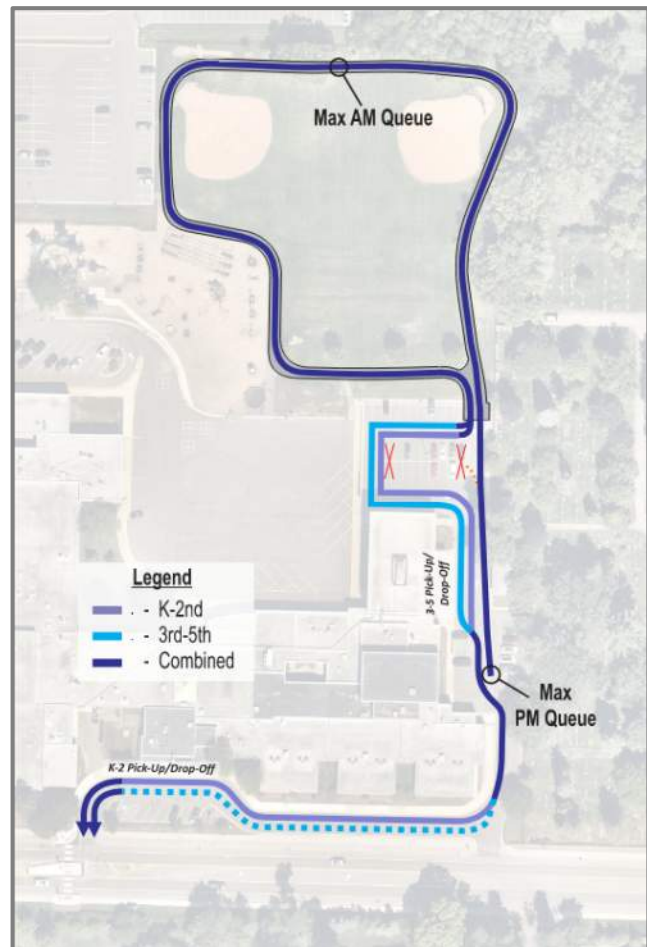
Alternative A was developed by the Minnetonka School District and is illustrated in Figure 5, and the anticipated travel patterns and maximum expected queues are summarized in the inset below. The alternative provides an additional 1,270 feet of vehicular storage by repurposing the existing loop trail around the green space/baseball fields on site. Travel patterns are expected to stay the same as existing, where parents enter the site via the east access and queue in the counterclockwise one-way until they arrive back to the northeast parking lot. While the existing internal conflict areas (#2-4) adjacent to the school are expected to continue, the alternative provides enough on-site storage to mitigate queuing impacts along Minnetonka Boulevard.

Benefits

- Provides on-site storage for vehicle queuing which would eliminate queuing issues on Minnetonka Blvd
- Maintains travel patterns that parents are familiar with

Potential Issues

- Doesn't improve internal conflict areas that cause inefficiencies and driver confusion; two (2) pick-up/drop-off locations anticipated to continue
- Separates the playground from the green space/baseball fields
- Longer loop may cause more K-2 vehicles to try to "cut" and/or make aggressive movements; may requiring coning at the east entrance



GENERAL NOTES:

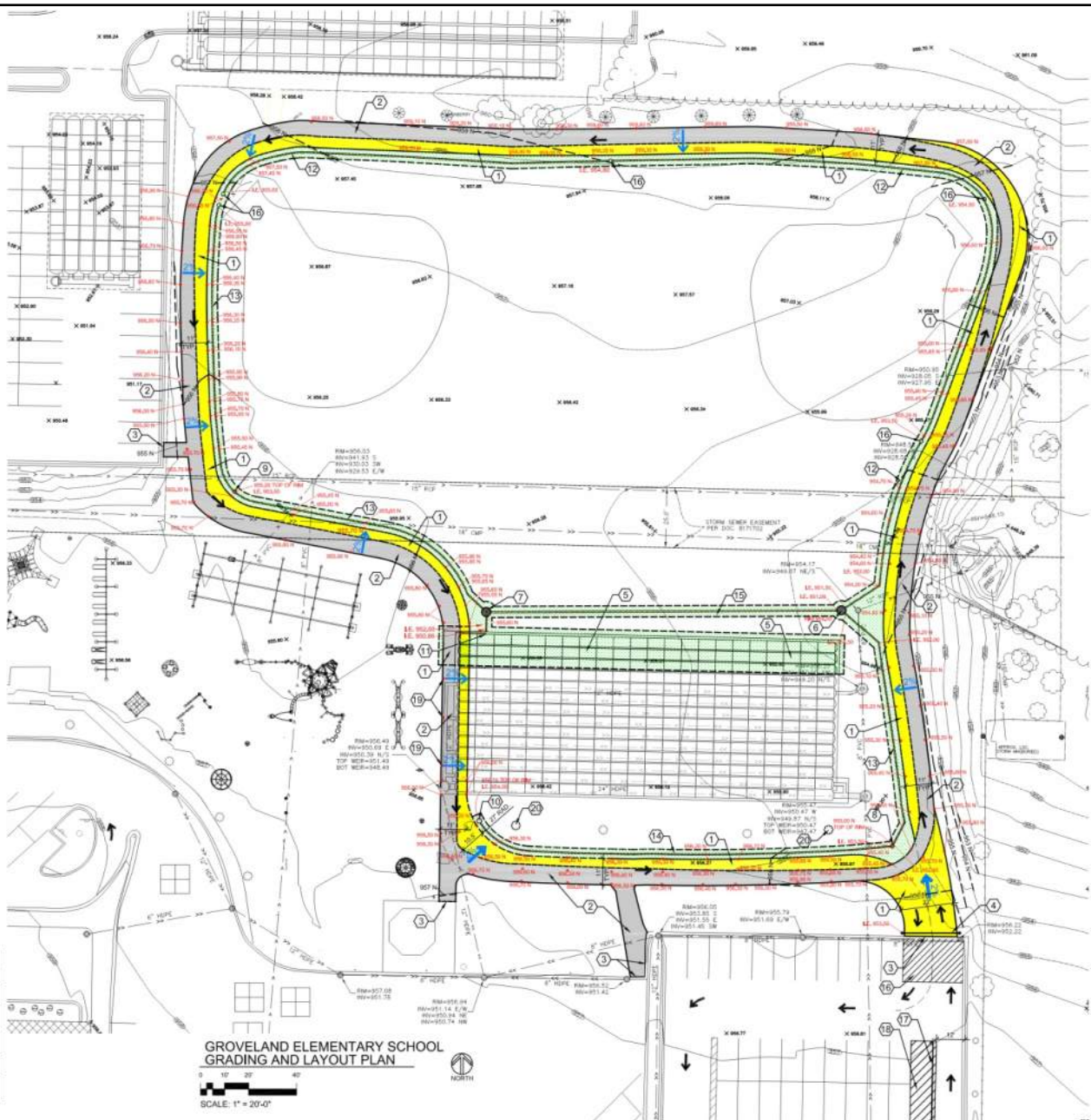
1. LOCATIONS AND SIZES OF ALL UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE ONLY. VERIFY ALL UTILITIES. CONTRACTOR RESPONSIBLE FOR REPAIR TO ANY DAMAGED UNDERGROUND UTILITIES, INCLUDING BUT NOT LIMITED TO, LAWN IRRIGATION SYSTEMS AND DRAIN TILE.
2. CONTRACTOR RESPONSIBLE FOR LOCATING AND PROTECTING ALL SITE UTILITIES. CONTACT Gopher State ONE CALL AND PRIVATE LOCATOR PRIOR TO ANY DEMOLITION/EXCAVATION.
3. ALL CONSTRUCTION MUST COMPLY WITH APPLICABLE ORDINANCES.
4. CONTRACTOR SHALL OBTAIN AND PAY FOR ALL CONSTRUCTION PERMITS.
5. PROTECT EXISTING FACILITIES AND VEGETATION WHICH ARE TO REMAIN. RESTORE ALL DISTURBED AREAS, INCLUDING, BUT NOT LIMITED TO UTILITIES, IRRIGATION SYSTEMS, PAVEMENT, TREES, LANDSCAPING, AND GRASSLAND/LAWN AREAS, GRASSLAND/LAWN AREAS TO BE DECOMPACTED AND RESTORED WITH 6" TOPSOIL, FERTILIZER AND STAKED SOIL.
6. CONTRACTOR TO SWEEP SITE PAVEMENTS AND ADJACENT STREETS AT CONSTRUCTION VEHICLE ACCESS POINTS EACH WORK DAY WITH PICK UP SWEEPER OR EQUAL TO REMOVE ANY DEBRIS.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR EROSION CONTROL THROUGHOUT PROJECT, INCLUDING BUT NOT LIMITED TO, STORM WATER STRUCTURES INLET PROTECTION.
8. ALL DIMENSIONS AND OR QUANTITIES ARE APPROXIMATE. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING.
9. CONTRACTOR TO PROVIDE ALL CONSTRUCTION STAKING.

KEYED NOTES:

1. NEW BITUMINOUS PAVEMENT, APPROX. 670 SQ. YDS, SEE DETAIL 3/C4.
2. 4" BITUMINOUS PAVEMENT RECLAIM/RECONSTRUCT. PAVE FINAL LIFT WITH NEW PAVEMENT, APPROX. 1910 SQ. YDS, SEE DETAIL 4/C4.
3. MATCH NEW BITUMINOUS PAVEMENT TO EXISTING.
4. INSTALL NEW 22' VB05 (2 PANEL) ALUMINUM BARRIER GATE, ITEM #0204. SEE DETAIL 8/C4.
5. NEW UNDERGROUND STORM CHAMBERS, 3 ROWS WITH 22 CHAMBERS PER ROW, 66 TOTAL. SEE PLAN SHEET C5.
6. NEW 48" CONCRETE MONOLITHIC CATCH BASIN/MANHOLE #1, SEE CBMH SCHEDULE AND DETAIL 5/C4.
7. NEW 48" CONCRETE MONOLITHIC CATCH BASIN/MANHOLE #2, SEE CBMH SCHEDULE AND DETAIL 5/C4.
8. NEW 24" DIA. NYLON/PLAST MANHOLE #3, SEE CBMH SCHEDULE AND DETAIL 10/C4.
9. NEW 24" DIA. NYLON/PLAST MANHOLE #4, SEE CBMH SCHEDULE AND DETAIL 10/C4.
10. NEW 24" DIA. NYLON/PLAST MANHOLE #5, SEE CBMH SCHEDULE AND DETAIL 10/C4.
11. NEW 8" DIA. HOPE PIPE, APPROX. 7 L.F., SEE DETAIL 9/C4.
12. NEW 4" DIA. SLOTTED HOPE PIPE, APPROX. 125 L.F., SEE DETAIL 9/C4.
13. NEW 4" DIA. SLOTTED HOPE PIPE, APPROX. 330 L.F., SEE DETAIL 9/C4.
14. NEW 4" DIA. SLOTTED HOPE PIPE, APPROX. 180 L.F., SEE DETAIL 9/C4.
15. NEW 8" DIA. HOPE PIPE, APPROX. 145 L.F., SEE DETAIL 9/C4.
16. NEW 4" DIA. CLEANOUT, SEE DETAIL 9/C4.
17. INSTALL NEW 30' CANTILEVERED GATE, SEE DETAIL 11/C4.
18. STRIPE 3 NEW PARKING SPACES AND CROSSHATCHED AREA.
19. TOP OF NEW PAVEMENT ELEVATION TO MATCH TOP OF EXISTING PAVEMENT ELEVATION AT THIS LOCATION -H/LT.
20. RELOCATED TREE.

- CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL QUANTITIES
- NEW BITUMINOUS PAVEMENT
 - EXISTING BITUMINOUS PAVEMENT TO RECLAIM/RECONSTRUCT
 - APPROXIMATE DISTURBED GRASS AREA
 - DRAINAGE DIRECTION AND SURFACE SLOPE
 - VEHICLE TRAFFIC DIRECTION (ARROW STRIPING (WHITE))
 - SPOT ELEVATION AND SYMBOLS
 - "N" DENOTES NEW TOP OF GRADE ELEVATION ALL OTHER ELEVATIONS ARE EXISTING UNLESS NOTED OTHERWISE

CATCH BASIN/MANHOLE SCHEDULE						
NO.	TYPE	INLET FRAMES/SIZE	FR. SIZES	REV. LEVEL	REL. LEVEL	REL. LEVEL BOTTOM
00101	48" CON. MONOLITHIC	HEAVY DUTY 48" FRAMES	48" x 48"	951.20	951.20	949.00
00102	48" CON. MONOLITHIC	HEAVY DUTY 48" FRAMES	48" x 48"	952.00	952.00	949.00
00103	NEW 24" DIA. NYLON/PLAST	HEAVY DUTY 24" FRAMES	24" x 24"	951.00	951.00	947.75
00104	NEW 24" DIA. NYLON/PLAST	HEAVY DUTY 24" FRAMES	24" x 24"	951.00	951.00	947.75
00105	NEW 24" DIA. NYLON/PLAST	HEAVY DUTY 24" FRAMES	24" x 24"	951.00	951.00	947.75



GROVELAND ELEMENTARY SCHOOL GRADING AND LAYOUT PLAN
SCALE: 1" = 20'-0"



MINNETONKA | WABECKER | MELBY | CHAMBERLAIN

PRELIMINARY
NOT FOR CONSTRUCTION

Signature: *[Handwritten Signature]*

ISSUE LEVEL / REVISION	DATE	NO.

THIS LINE SCALES 1" ON FULL SIZE SHEETS

Client:
MINNETONKA PUBLIC SCHOOLS
5621 COUNTY ROAD 101
MINNETONKA, MINNESOTA 55345

GROVELAND ELEMENTARY
17310 MINNETONKA BLVD
MINNETONKA, MINNESOTA 55345

Project No:
2023 PATH WIDENING FOR DRIVE
17310 MINNETONKA BLVD
MINNETONKA, MINNESOTA 55345

Drawn by:
GROVELAND ELEMENTARY SCHOOL
GRADING AND LAYOUT PLAN

DATE: 7-20-22
CLIENT PROJECT No.: 20476
INSPEC PROJECT No.: CB
PROJECT MGR: BT
DRAWN BY: BB
CHECKED BY: BB

C3



Alternative A
Groveland Elementary School Transportation Study
City of Minnetonka

Figure 5

Alternative B

Alternative B was developed as part of the transportation study as an iteration of Alternative A to improve both the on-site storage and internal conflict areas. A high-level rendering of the alternative is illustrated in the inset below, along with the anticipated travel patterns/maximum expected queues. In order to improve the internal conflict areas, the pick-up/drop-off was combined into one area for all grades (K-5). The combined pick-up/drop-off area is proposed on the north side of the school, where students can access the school using the same access as bus users. Further discussion will need to occur with the school, to determine if the pedestrian storage area and combined pick-up/drop-off would be feasible for their operations.

The alternative would require the reconstruction and repurposing of the north bus lot/playground area. Bus storage and bus turning movements would need to be further reviewed. The new loop would provide additional space that could be utilized to redesign the northeast parking lot to provide additional parking adjacent to the school. In order to provide enough on-site storage, the counterclockwise loop would need to have two travel lanes, which may cause more impacts to the green space/baseball fields than Alternative A. The two lanes would then merge down to one-lane right before the pick-up/drop-off lane to provide a bypass lane. Note signage could be installed to help facilitate this merge. To reduce operational/queueing issues, vehicles would still be instructed to exit at the west access.

Benefits

- Provides on-site storage for vehicle queuing which would eliminate queuing issues on Minnetonka Blvd
- Maintains travel patterns that parents are familiar with
- Provides ability/space to redesign the northeast parking lot and provide additional on-site parking
- Improves internal site circulation and reduces conflict areas
- Could be designed/constructed as a future phase of Alternative A

Potential Issues

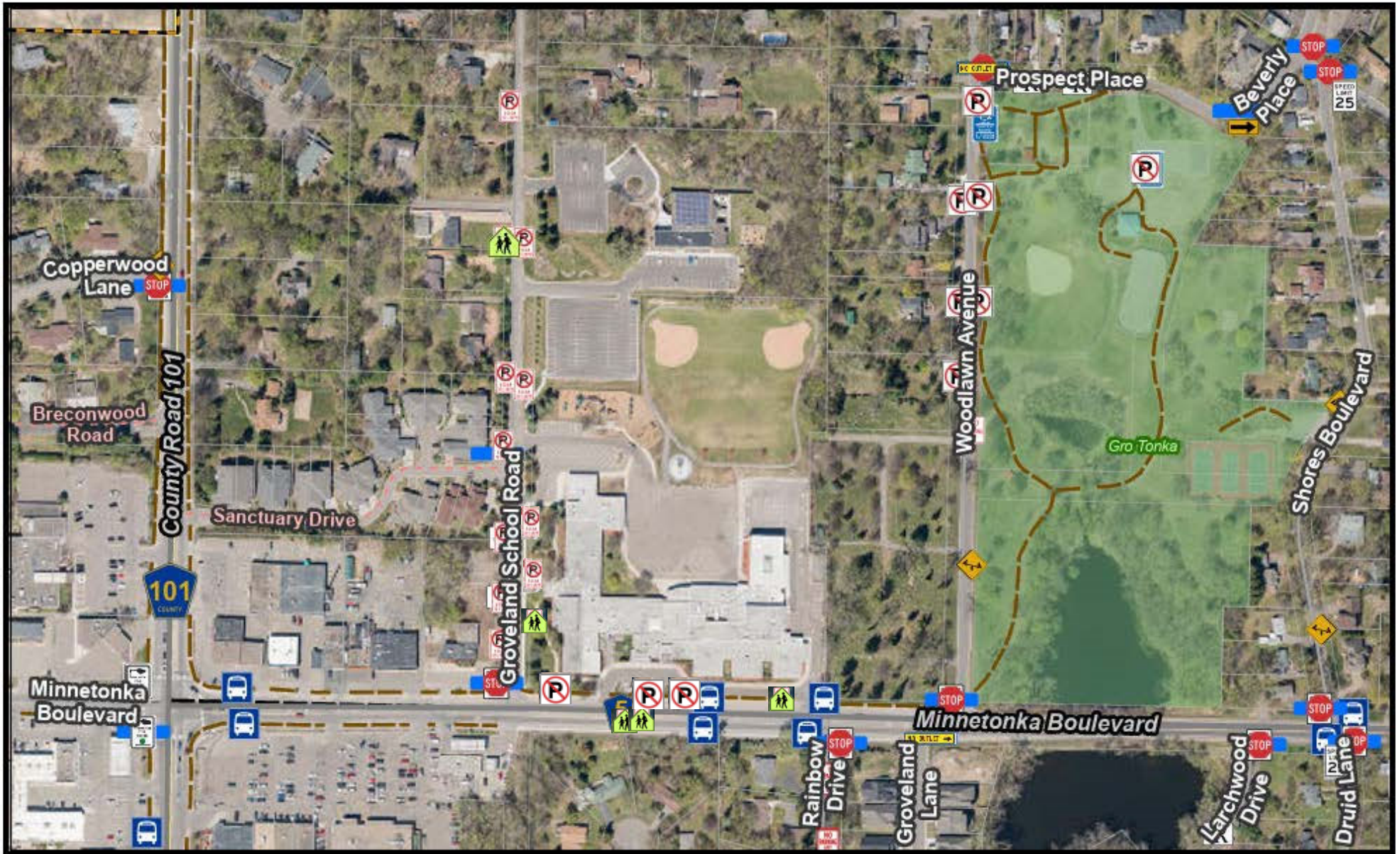
- Would require major reconstruction north of the school
- Impacts the current bus storage and turnaround, which is also used for recess
- Feasibility would need to be further discussed with the school (i.e. combining all grades into one pick-up/drop-off area)
- Separates the playground from the open space/baseball fields



Conclusions and Recommendations

The following information provides a summary of the key conclusions and recommendations offered as part of the study:

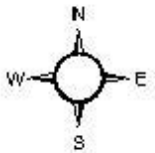
- The following operational issues were observed at the Groveland Elementary School:
 - Queues from the parent pick-up/drop-off were observed to extend onto Minnetonka Boulevard during both the school arrival and departure peak hours. The on-site storage provided is estimated to be approximately 1,670 feet (~67 vehicles), whereas approximately 2,500 feet (~100 vehicles) is needed during the afternoon peak.
 - There are multiple internal conflict areas that reduce the parent pick-up/drop-off efficiency and cause driver confusion.
 - There are several constraints on-site (such as building footprints, property lines, and parking lot lane widths) that limit the ability to improve pick-up/drop-off efficiencies. The site is currently being managed as well as possible given these constraints.
- The current enrollment at Groveland Elementary School is approximately 880 students and 130 staff members. Based on discussions with the Minnetonka School District, no enrollment changes are expected at the school.
- To improve the issues identified, the following two (2) improvement alternatives were identified/evaluated and are discussed within the report.
 - **Alternative A:** Mitigates queuing issues on Minnetonka Boulevard, but does not address internal conflict areas and inefficiencies.
 - **Alternative B:** Mitigates queuing issues on Minnetonka Boulevard, improves pick-up/drop-off efficiency, and reduces internal conflict areas. The alternative, however, is impactful to the overall site and would require major reconstruction on the north side of the school.
- Both Alternatives A and B are recommended for consideration as the school moves forward in their site design process. Both Alternatives would provide the necessary on-site storage to mitigate queuing issues on Minnetonka Boulevard, which would result in safety benefits to the entire study area. Alternative B also reduces internal conflict areas, which would reduce driver confusion and increase efficiency on-site.
 - Alternative B, however, is impactful to the overall school site and would require major reconstruction north of the school. A project of this magnitude may not be financially feasible at this time.
 - Given the similar paths around the open green space/baseball fields, Alternative B could be designed as a future long-term improvement phase to Alternative A and be implemented if/when budget is available.



1/12/2023, 12:14:58 PM

City of Minnetonka

DISCLAIMER:
 This drawing is not a legally recorded plat or an accurate survey.
 It is intended to be only an approximate representation of information from various government offices and other sources.
 It should not be used for a purpose that requires exact measurement or precision.
 People who use this drawing do so at their own risk.
 The City of Minnetonka is not responsible for any inaccuracies contained in this drawing.
 The City of Minnetonka provides no warranty, express or implied, about the correctness of the information.



CITY OF MINNETONKA

- Reviewed the concept plan for Saville Flats located in the southeast corner of the Excelsior Blvd. and Co. Rd. 101 intersection.

6. Report from Planning Commission Members

Powers appreciated the great job public works staff did plowing the streets after the heavy snow falls.

7. Public Hearings: Consent Agenda

No item was removed from the consent agenda for discussion.

Waterman moved, second by Maxwell, to approve the item listed on the consent agenda as recommended in the staff report as follows:

A. Items concerning Groveland Elementary School at 17310 Minnetonka Blvd.

Recommend that the city council adopt the attached resolution approving a conditional use permit and site plan review with a variance and expansion permit for Groveland Elementary at 17310 Minnetonka Blvd.

Henry, Maxwell, Powers, Waterman, Hanson and Sewall voted yes. Banks was absent. Motion carried and the item on the consent agenda was approved as submitted.

8. Other Business

A. Concept plan review for Ridgewood Road Villas at 18116 Ridgewood Road.

Chair Sewall introduced the proposal and called for the staff report.

Cauley reported. Staff recommends commissioners provide comments and feedback to assist the applicant with future direction that may lead to the preparation of more detailed development plans.

Mike Waldo, Ron Clark Construction, representing the applicant, stated that:

- He attended a meeting with neighbors and heard their concerns. It opened up a good dialogue that will be continued.
- The style of home would be similar to Legacy Oaks. It would be a high-quality product. The ranch-style residences with walk outs or look outs would be designed for empty nesters, but there would be no age restriction.
- A wetland delineation, tree survey and preliminary grading plan have already been completed to make sure the proper buffers to the wetland would be maintained and the site would work with the concept plan.

Resolution No. 2023-

Resolution approving a conditional use permit and a site plan review, with variance and expansion permit, for a drop-off and pick-up loop at Groveland Elementary at 17310 Minnetonka Blvd

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

- 1.01 Cliff Buhman, on behalf of the Minnetonka Public Schools, is proposing to widen an existing walking path in the northeast corner of the site to accommodate vehicular circulation during drop-off and pick-up times. The proposal requires (1) conditional use permit; (2) a site plan review; (3) a variance to reduce the drive-aisle setback from 20 feet to 10 (92032.22a).
- 1.02 The property is located at 17310 Minnetonka Blvd. It is legally described in Exhibit A.
- 1.03 By City Code Sec. 300.16, educational institutions and facilities are conditionally permitted uses within low-density residential districts.
- 1.04 City Code Section 300.27 requires site plan review when a proposal will result in a different intensity of use.
- 1.05 City Code Section 300.280, Subdivision 12(b) requires a minimum parking setback and drive aisle setback of 20 feet. The existing parking lot has a 3.5 setback from the eastern property line and was constructed in 1966, prior to the adoption of the setback requirement. The proposed drive-aisle requires an expansion permit for encroachments into the setback not beyond the existing non-conforming setback from the eastern property line. Conversely, variance is required to reduce the setback from 20 feet to 10 feet from the northerly property line.
- 1.06 Minnesota Statute §462.357 Subd. 1(e)(b) allows a municipality, by ordinance, to permit an expansion of non-conformities.
- 1.07 City Code §300.29 Subd. 3(g) allows expansion of a non-conformity only by variance or expansion permit.
- 1.08 City Code §300.29 Subd. 7(c) authorizes the city to grant expansion permits.

- 1.09 On Jan. 19, 2023, the planning commission held a hearing on the proposal. The applicant was provided the opportunity to present information to the commission. The commission considered all of the comments received and the staff report, which are incorporated by reference into this resolution. The commission recommended that the city council approve the permit.
- Section 2. Standards.
- 2.01 City Code §300.16 Subd. 2 outlines the general standards that must be met for granting a conditional use permit. These standards are incorporated into this resolution by reference.
- 2.02 City Code §300.16 Subd. 3(a) outlines the following specific standards that must be met for granting a conditional use permit for such facilities:
1. Direct access is limited to a collector or arterial roadway as identified in the comprehensive plan or otherwise located so that access can be provided without conducting significant traffic on local residential streets; the use is not permitted on property that has access only by way of a private road or driveway that is used by more than one lot;
 2. Buildings set back 50 feet from all property lines and parking setbacks are subject to section 300.28 of this ordinance;
 3. School bus pick-up and drop-off areas located outside of the public right-of-way and designed to enhance vehicular and pedestrian safety;
 4. Recreational areas designed for outdoor group activities set back 25 feet from residential property, suitable buffering provided to protect neighboring properties from noise and adverse visual impacts, and lighted playing fields permitted only upon demonstration that off-site impacts can be mitigated substantially;
 5. No more than 60 percent of the site is to be covered with impervious surface, and the remainder is to be suitably landscaped;
 6. Site and building plan subject to review pursuant to section 300.27 of this ordinance; and
 7. Not connected to, or part of, any residential dwelling.
- 2.03 City Code Section 300.27, Subdivision 5, lists the following standards that must be met for site and building plan review:
1. Consistency with the elements and objectives of the city's development guides, including the comprehensive plan and water resources management plan;
 2. Consistency with this ordinance;

3. Preservation of the site in its natural state to the extent practicable by minimizing tree and soil removal and designing grade changes to be in keeping with the general appearance of neighboring developed or developing areas;
4. Creation of a harmonious relationship of buildings and open spaces with natural site features and with existing and future buildings having a visual relationship to the development;
5. Creation of a functional and harmonious design for structures and site features, with special attention to the following:
 - a) An internal sense of order for the buildings and uses on the site and provision of a desirable environment for occupants, visitors, and the general community;
 - b) The amount and location of open space and landscaping;
 - c) Materials, textures, colors, and details of construction as an expression of the design concept and the compatibility of the same with the adjacent and neighboring structures and uses; and
 - d) Vehicular and pedestrian circulation, including walkways, interior drives, and parking in terms of location and number of access points to the public streets, width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, and arrangement and amount of parking.
6. Promotion of energy conservation through design, location, orientation, and elevation of structures, the use and location of the glass in structures and the use of landscape materials and site grading; and
7. Protection of adjacent and neighboring properties through reasonable provision for surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design not adequately covered by other regulations which may have substantial effects on neighboring land uses.

2.04

By City Code §300.07 Subd. 1, a variance may be granted from the requirements of the zoning ordinance when: (1) the variance is in harmony with the general purposes and intent of this ordinance; (2) when the variance is consistent with the comprehensive plan; and (3) when the applicant establishes that there are practical difficulties in complying with the ordinance. Practical difficulties mean: (1) The proposed use is reasonable; (2) the need for a variance is caused by circumstances unique to the property, not created by the property owner, and not solely based on economic considerations; and (3) the proposed use would not alter the essential character of the surrounding area.

- 2.05 City Code Section 300.29, Subdivision 7(c) states that an expansion permit may be granted but is not mandated when an applicant meets the burden of proving that:
1. The proposed expansion is reasonable use of the property, considering such things as functional and aesthetic justifications for the expansions; adequacy of off-street parking for the expansion; absence of adverse off-site impacts from such things as traffic, noise, dust odors, and parking; and Improvement to the appearance and stability of the property and neighborhood.
 2. The circumstances justifying the expansion are unique to the property, are not caused by the landowner, are not solely for the landowner's convenience, and are not solely because of economic considerations; and
 3. The expansion would not adversely affect or alter the essential character of the neighborhood.

Section 3. Findings.

- 3.01 The proposal meets the general conditional use permit standards outlined in City Code §300.16 Subd.2:
1. Educational facilities are conditionally-permitted uses within residential districts.
 2. The site improvement is consistent with the goals, policies, and objectives of the comprehensive plan, which recognizes and supports educational facilities, and their unique transportation needs, in residential neighborhoods.
 3. The proposed use would not have an undue adverse impact on governmental facilities, utilities, services, and proposed improvements. In fact, the project presents an opportunity to coordinate site work to replace an aging stormwater pipe that bisects the property.
 4. The use would not have an undue adverse impact on health, safety, and welfare.
- 3.02 The proposal meets the specific conditional use permit standards outlined in City Code 300.16 Subd.3(a).
1. Minnetonka Blvd is classified as an A Minor Expander. The school has access onto Minnetonka Blvd onto Groveland School Road. The proposal would direct traffic onto Minnetonka Blvd.
 2. The proposal is for site improvements and would not alter building setbacks. By ordinance, drive aisles and parking lots must have a 20-foot

setback. As such, the proposal would require (1) a variance to reduce the setback from 20 feet to 12 feet from the northern property line; and (2) an expansion permit from the eastern property line.

3. The proposal would not alter the bus circulation through the site but is intended to improve vehicular circulation through the site.
4. No changes are proposed to the recreational ball fields. However, the walking path would be increased to accommodate vehicular traffic. Existing vegetation will screen the path for adjacent properties and would only be utilized during drop-off and pick-up times.
5. No changes are proposed to the existing parking lot. However, by ordinance, drive aisles have similar setback requirements, and a variance and expansion permit is required.
6. The proposal would not increase the amount of impervious surface to more than 70 percent.
7. The proposal would meet site and building plan standards.

3.03 The proposal meets the specific conditional use permit standards outlined in City Code 300.16 Subd.3(a).

1. The project has been reviewed by the city's planning, building, engineering, natural resources, fire, and public works staff. Staff finds it generally consistent with the city's development guides.
2. But for the requested variance and expansion permit, the proposal is consistent with the ordinance.
3. The property is developed, and the drop-off and pick-up drive lane would be created by expanding an existing pedestrian path in the northeast corner of the site. While disturbance would occur to prepare the site for the expansion of the path, the proposal would not significantly impact the natural state of the site.
4. The proposal would not have a significant impact on the relationship between buildings and open space as the drive aisle is an expansion of an existing walking path.
5. The proposal would expand an existing walking path to improve vehicular circulation through the site and increase the amount of stacking available on-site for drop-off and pick-up. The path would not be available for walking during drop-off and pick-up times. The path would then be closed for vehicular traffic outside of these times.
6. The proposal expands an existing path to minimize the amount of grading and site disturbance.

7. Vegetation would screen the path for adjacent properties. The runoff would be collected and directed to the underground storm chamber.

3.04 The proposal meets the variance standard outlined in City Code §300.07 Subd. 1(a):

1. Purpose and Intent of the Zoning Ordinance: The variance would be in general harmony with the intent of the zoning ordinance and consistent with the comprehensive plan. The intent of the parking lot setback is to ensure adequate separation from adjacent properties. But for 100 feet, the path would be adjacent to Groveland Cemetery and Saint Luke's. The path would encroach four feet into the non-conforming setback adjacent to the residential property. This area would be screened by existing vegetation.
2. Consistent with Comprehensive Plan: The comprehensive guide plan recognizes schools and the unique transportation needed to support them and generally supports them.
3. Practical Difficulties: There are practical difficulties in complying with the ordinance:
 - a) Reasonableness: The proposal is reasonable as it would mitigate queuing issues on adjacent roadways during drop-off and pick-up times. To do this, an existing path is widened to accommodate the vehicular traffic. This is less intrusive on the neighborhood than adding additional infrastructure to the property.
 - b) Unique Circumstance: The variance is the result of the unique lot configuration and existing site improvements.
 - c) Character Of Locality: The ten-foot encroachment into the northerly setback is adjacent to an existing non-conforming parking lot on the St. Luke's property. The school district has a shared parking agreement to use this parking lot. The remaining encroachment would be screened by existing vegetation and topography.

3.05 The application for the expansion permit is reasonable and would meet the required standards outlined in City Code §300.29 Subd. 7(c):

1. Reasonableness and Neighborhood Character: The proposed expansion is proposed to mitigate queuing issues on Minnetonka Blvd. The area of encroachment is roughly 417 square feet over the course of a 350-foot stretch.
2. Unique Circumstance: The proposal would expand an existing walking path to minimize site impacts and impervious surfaces. The existing parking lot has a 3.5 setback at the closest point and is the result of the

lot configuration and existing site improvements.

Section 4. City Council Action.

4.01 The above-described proposal is approved, subject to the following conditions:

1. This resolution must be recorded with Hennepin County.
2. A grading permit is required. This permit must include the following:
 - a) An updated site plan including storm sewer easement per doc. 9974407 to show if the additional storm chambers are located within the easement.
 - b) A final stormwater management plan for the disturbed area. This plan must demonstrate conformance with the following criteria:
 - 1) Rate: limit peak runoff flow rates to that of existing conditions from the 2-, 10, and 100-year events at all points where stormwater leaves the site.
 - 2) Volume: provide for on-site retention of 1.1-inch of runoff.
 - 3) Quality: provide the runoff to be treated to at least 60-percent total phosphorus annual removal efficiency and 90-percent total suspended solid annual removal efficiency.
 - c) A stormwater maintenance agreement in the city's approved format for review and approval of city staff.
 - d) A tree mitigation plan. The plan must meet mitigation requirements as outlined in the ordinance. However, at the sole discretion of staff, mitigation may be decreased. Based on the submitted plans, the mitigation requirements would be none based upon submitted plans.
3. Prior to issuance of the permit, a temporary rock driveway, erosion control, tree and wetland protection fencing, and any other measured as identified as the SWPPP must be installed for staff inspection. These items must be maintained throughout the course of construction.
4. Contact MCES for permitting requirements and manhole casting adjustments for work within their easement.
5. As-builts of the stormwater facilities must be submitted.
6. Cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and

signed by the builder and property owner. Through this document, the builder and property owner will acknowledge the following:

- The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.
7. The city council may reasonably add or revise conditions to address any future unforeseen problems.
 8. Any change to the approved use that results in a significant increase in a significant change in character would require a revised conditional use permit.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Jan. 30, 2023.

Brad Wiersum, Mayor

Attest:

Becky Koosman, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a meeting held on Jan. 30, 2023.

Becky Koosman, City Clerk

Exhibit A

Lots 22, 23, and 24, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota;

The West 190 feet of Lot 19, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota;

That part of Lot 19 lying East of the West 190 feet thereof, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota;

Lot 20, except the easterly 33 feet of the southerly 62.9 feet thereof, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota;

Lot 21, except the easterly 33 feet thereof, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota;

That part of the Southwest Quarter of the Northwest Quarter of Section 17, Township 117, Range 22, Hennepin County, Minnesota, described as follows:

Beginning at a point on the South line of said Southwest Quarter of the Northwest Quarter, 1161 feet easterly of the southwest corner thereof; thence northerly, parallel with the West line of said Southwest Quarter of the Northwest Quarter, a distance of 330.00 feet; thence easterly, parallel with the South line of the Southwest Quarter of the Northwest Quarter, to the southeast corner of Lot 21, THE HERZOG DEEPHAVEN ACRES, Hennepin County, Minnesota; thence southerly along the southerly extension of the East line of said recorded plat of THE HERZOG DEEPHAVEN ACRES, to the South line of said Southwest Quarter of the Northwest Quarter; thence westerly along said South line to the point of beginning and there terminating.



**City Council Agenda Item 12A
Meeting of Jan. 30, 2023**

Title: Items concerning Greystar at 10701 Bren Road East

Report From: Ashley Cauley, Senior Planner

Submitted through: Mike Funk, City Manager
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Greystar Development Central, LLC has submitted applications to remove the existing building and construct a new, six-story, 275-unit apartment building. The subject property is 3.16 acres in size and is located at 10701 Bren Road East. The proposal requires the following;

- Rezoning from I-1, industrial, to PUD, planned unit development;
- Master development plan; and
- Site and building plan

Recommended Action

Introduce the rezoning ordinance and refer it to the planning commission.

Strategic Profile Relatability

- | | |
|--|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input checked="" type="checkbox"/> N/A | |

Statement: N/A

Financial Consideration

- Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
- Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Statement: N/A

Background

In Sept. 2022, the council reviewed a [concept plan](#) submitted by Greystar. The plan contemplated removing the existing building and constructing a six-story, market-rate apartment building with 275 units. The plan also contemplated 28 affordable units (14 units at 40% AMI and 14 units at 80% AMI). Access to the surface and parking garage would be from Red Circle Dr. The council generally commented that the density, affordability mix and type, and overall design was appropriate for the site. See [meeting minutes](#).



Figure 1: Concept Plan

Formal Application

Based on comments received during the concept plan review process, Greystar has submitted formal applications for the development of the site. As proposed, the existing building would be removed, and a new six-story, a 275-unit apartment building would be constructed. The building would contain a mix of studio, one-, two-, and three-bedroom units. As proposed, 28 units would be affordable (14 units at 40% AMI and 14 units at 80% AMI). The building would include indoor parking for 395 vehicles and a surface parking lot accommodating seven vehicles.

Issue Identification

The purpose of introducing an ordinance is to give the city council the opportunity to review a new application before sending it to the planning commission for a recommendation. Introducing an ordinance does not constitute an approval. The tentative planning commission date is Feb. 16, 2023. Based on a preliminary review of the proposal, staff has identified the following issues for council comment and further staff analysis:

- **Utilities and easements:** The site is encumbered by a number of easements and utilities. These easements and utilities need to be investigated and verified for ownership and use.
- **Trail system:** The proposed trails will be reviewed to ensure they meet the goals of the Opus Area Placemaking and Urban Design guidelines.
- **Stormwater management:** Review of the stormwater management plan to meet requirements for the site and conveyance of public drainage will be reviewed. The stormwater is collected and directed to a stormwater basin extending around the building.
- **Affordability:** The proposed project includes a six-story building with 275 units. As proposed, the developer will provide 28 affordable units (5% at 40% AMI and 5% at 80% AMI) with a range of bedroom sizes, as indicated in the chart. The mix provides deeper affordability at the 40% AMI range than required by the [Affordable Housing Policy](#). At 40% AMI, a 4-person household could earn up to \$30,000 and would pay approximately

\$750 in rent for a 2-bedroom unit. This project is the first in Opus with 40% rent and income limits. The remaining affordable units are comprised of units serving households earning 80% of the area's median income, and the remaining 246 units will be at market rate. The units will meet the requirements outlined in the [Affordable Housing Policy](#) and follow affordable housing practices included in the city's contracts for affordable housing. A declaration of a restrictive covenant will be recorded to ensure the affordability components remain in place for 30 years.

Description	Units	Unit Mix
Studio	40	14%
1 BR	116	42%
2 BR	78	29%
3 BR	12	4%
Affordable		
Affordable Studio - 40% AMI	2	1%
Affordable 1 BR - 40% AMI	7	3%
Affordable 2 BR - 40% AMI	4	1%
Affordable 3 BR - 40% AMI	1	0%
Affordable Studio - 80% AMI	2	1%
Affordable 1 BR - 80% AMI	7	3%
Affordable 2 BR - 80% AMI	4	1%
Affordable 3 BR - 80% AMI	1	0%

- **Notification area:** Staff has prepared the mailing area using the southwest quadrant mailing list for Opus.

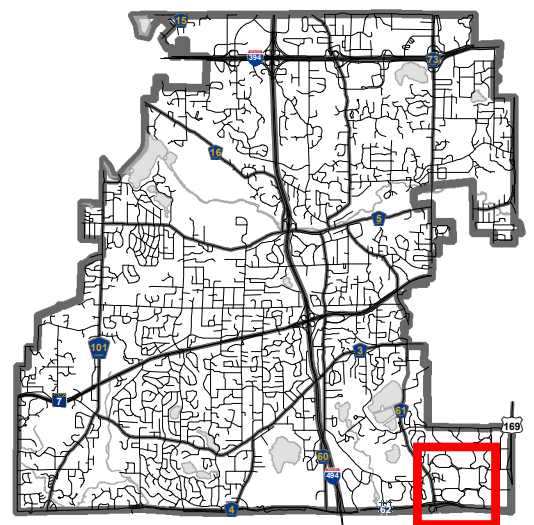


As part of the formal introduction, staff requests the council provides general feedback, note any additional information it would like to see before considering the proposal, and suggest any items it would like the planning commission to specifically consider during its deliberations.



Location Map

Project: Greystar Development
Address: 10701 Bren Road



DEVELOPMENT TEAM EXPERIENCE



ABOUT GREYSTAR

Greystar is a leading, fully integrated real estate company offering expertise in investment management, development, and management of rental housing properties globally. Headquartered in Charleston, South Carolina, Greystar manages and operates over \$221 billion of real estate in 224 markets globally including offices throughout North America, Europe, South America, and the Asia -Pacific region. Greystar is the largest operator of apartments in the United States, managing over 768,000 units globally, and has a robust institutional investment management platform with more than \$58.2 billion of assets under management, including over \$24 billion of development assets.



ABOUT ESG

ESG is a national leader in the planning, design and development of award-winning residences and communities throughout the US. Our architects and designers base their work on timeless design principles. These principles include the integration of pedestrian-friendly streetscapes and landscaping, proximity to mass transit, generating density, and the incorporation of sustainable-design strategies and mixed-use commercial enterprises.

PROJECT NARRATIVE

VISION STATEMENT

Greystar desires to create a market rate rental community to serve residents in an area that will soon benefit from enhanced transit opportunities and an extensive network of recreational trails. The new residents will enhance the Opus Park community and foster the ongoing development of a new neighborhood centered around the light rail station.

PROJECT GOALS

- Support a mixed-use community at the Opus campus by growing the resident population.
- Create a Transit-Oriented Development community that will provide ridership on the new light rail line.
- Enhance and support the district's natural features and integrate the site into the existing landscape of trails.
- Provide density to support the area's existing businesses and provide additional residents to serve as a catalyst for more retail/commercial development within the Opus Park.
- Connect the Red Circle Trail from Shady Oak Road to Bren Road as per the Opus Area Placemaking + Urban Design Implementation Guide.
- Develop a concept for connecting the project to the existing trail network with seating areas, landscaping and an updated stormwater management approach.

PLACEMAKING AND DESIGN CONCEPT

The project's vision and design concept will align with the goals of the Opus Area Placemaking + Urban Design Implementation Guide.

- Located within the "Red Loop", the proposed development is an important part of the broader Opus Area Placemaking and Design trail system. The area is characterized by natural open spaces stitched together with an exceptional network of pedestrian trails and a one-way road system.
- The proposed residential building will integrate into this well-established environment by giving precedence to landscape design and upgraded pedestrian connections.
- New sidewalks will be integrated around the site to the east where it will connect to the proposed LRT station. Along the south side of the building, paths connect to both the Shady Oak Rd. trail system as well as the Orange Loop. New connections within the site, and around the north will further solidify pedestrian connections for residents and visitors of OPUS park.
- With the new Southwest Light Rail station located adjacent to the site, residents will have direct access to a transit option that will reduce dependency on automobile use.
- The grounds immediately adjacent to the buildings will be highly landscaped since there is no structured parking below ground. The landscape program will include full overstory canopy trees.
- The open space around the perimeter of the buildings will incorporate a carefully designed stormwater feature and native landscaping.
- Native vegetation will take precedence based on historic species located in this area.
- Walk-up style residential units with sidewalks, front porches, and entry doors will ground the building and facilitate an active streetscape environment.

PROJECT NARRATIVE

PROJECT DESCRIPTION

- An office building and surface parking exist on the site today. The proposed project consists of a five and six-story market rate apartment building with approximately 275 units. The building will also incorporate approximately 28 affordable residential units (10%: 14 units at 40% AMI and 14 units at 80% AMI). The multifamily building wraps a large interior courtyard and a six-story parking structure that will contain about 395 parking spaces.
- The residential building will be fully amenitized and include a lobby and common areas at ground level. A vehicular drop-off court is accessed from Red Circle Drive. This arrival court will include several guest parking stalls for prospective tenants and visitors. The Level one amenity spaces will overlook the amenity courtyard as well as a double-height resident clubroom located at Level P1 below. A level six “sky lounge” will provide indoor and outdoor spaces with views toward the north and east.
- The landscaped courtyard is inspired by the naturally occurring landscapes of Minnesota. Frozen lakes and ice bubbles, dense aspen and birch forests, wetlands, and bogs, as well as the oak savannah ecosystem inspire forms and program throughout the site. Informal seating and grilling areas are scattered throughout the courtyard. A seating berm and tree bosc provide spaces for relaxation as well.
- At the perimeter of the building, the project will use the existing topography of the site to manage stormwater in a series of cascading raingardens that will connect to the larger stormwater management system.
- Service functions like move-ins and trash pickup will be located on the north side of the building, accessed via an existing private drive and existing shared access easement. Architectural screening and landscaping will visually screen this area from the pedestrian sidewalks along Bren Road.
- Ample bike parking will be provided in the building to support and encourage bicycling.

BUILDING DESIGN AND EXTERIOR MATERIALS

- A key building design element is in response to the street curve at the intersection of Bren Road and Red Circle Drive. The building face along this frontage consists of a series of angled planes that mediate between the rectilinear courtyard and the property line condition. These planes provide opportunity to nestle balconies into the facades and create an interesting fish-scale like expression. While the exterior skin of the building is lighter in color, where this primary massing is carved away, a darker color becomes predominant.
- The exterior materials will include a mix of brick, true three-coat cementitious stucco, painted cementitious panels, metal panel, glass and accent materials to creating an integrated palette that will complement the intensive landscaping of the site.

DESIGN UPDATES SUBSEQUENT TO CONCEPT PLANNING COMMISSION AND CITY COUNCIL MEETINGS

- The Planning Commission and City Council provided thoughtful feedback at the meetings of August 18, 2022, and September 12, 2022.
- City Staff has also provided meaningful feedback during the past several months.
- Changes that have been made based on this feedback include:
 - o Further developed the stormwater conveyance system in the front yards of Red Circle Drive and Bren Road as a resilient landscape feature of the development.
 - o The Bren Road East Elevation has been updated to show a major massing break. This break is placed at the location of the passageway that provides a direct and secure link to the courtyard from the Bren Road pedestrian sidewalk.
 - o Setback along west property line increased from 10’ to 15’.
 - o Added brick to the base of the building at all public facades.
 - o The sky deck and sky lounge geometry has been simplified.
 - o West garage façade is now fully enclosed and concealed and thoughtfully integrated into the overall building design.
 - o Adjusted the secondary color per feedback to a more ochre hue.
 - o Interior façade design of courtyard enhanced with a two-tone wall panel patterning.
 - o Solar access studies have influenced elements of the courtyard to maximize use by residents all year around.
 - o Greystar has committed to providing twenty EV spaces in the parking garage.

EXISTING CONDITIONS: AREA



WALK

- ON SITE TRAIL ACCESS
- 4 MIN. TO LAKE
- 5 MIN. TO OPUS LIGHTRAIL STATION
- 6 MIN. BETWEEN RED CIR DR TO SHADY OAK BUS STOP

DRIVE

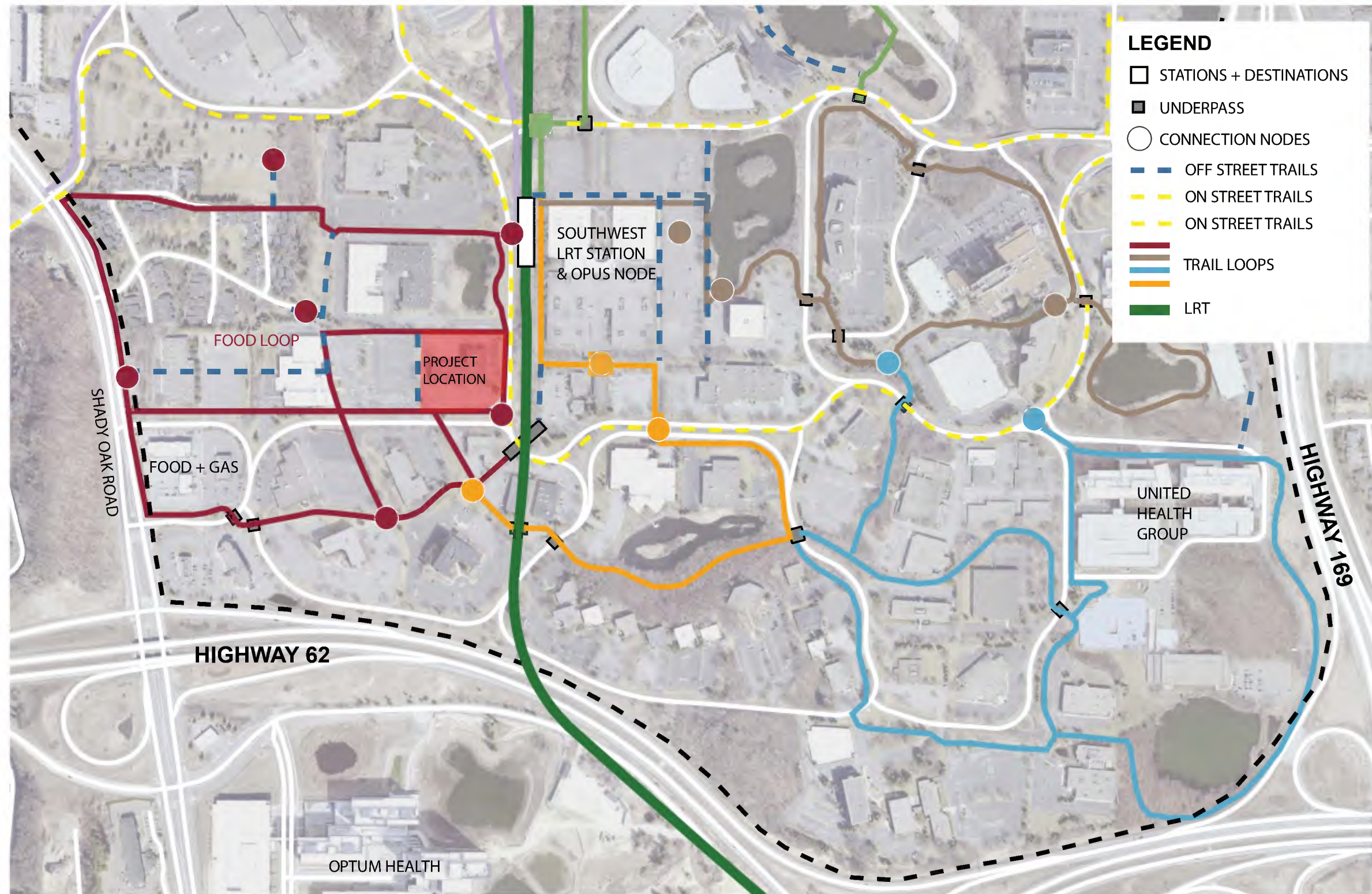
- 10 MIN. TO MEADOWBROOK GOLF COURSE
- 20 MIN. TO DOWNTOWN MINNEAPOLIS
- 22 MIN. TO MSP INTERNATIONAL AIRPORT
- 30 MIN. TO ST. PAUL

ENGAGE

- 17 NEARBY RESTAURANTS
- 3 HOTELS NEARBY
- 3 PARKS W/TRAILS THROUGH THE SITE

- MAIN ROADWAYS
- MAIN APPROACH
- SW LIGHTRAIL - GREEN LINE
- TRAIL SYSTEM + PARKS
- RETAIL/COMMERCIAL/HOTEL
- CORPORATE & EDUCATION
- RESIDENTIAL

EXISTING CONDITIONS: CONNECTIONS



EXISTING CONDITIONS: SITE





500 Washington Avenue South, Suite 1080
Minneapolis, MN 55415
p 612.339.5508 f 612.339.5382
www.esgarch.com



12800 Whitewater Drive, Suite 300
Minnetonka, MN 55343
763.476.6010 telephone

Engineering | Surveying | Planning | Environmental

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.

Brian W. Frank
Registration No. 52728 Date: MM/DD/YYYY
If applicable, contact us for a wet signed copy of this plan which is available upon request at Sambatek's, Minnetonka, MN office.

NOT FOR CONSTRUCTION

LAND USE
APPLICATION
SUBMITTAL
12/06/2022

ORIGINAL ISSUE:
09/19/22

REVISIONS:
No. Description Date

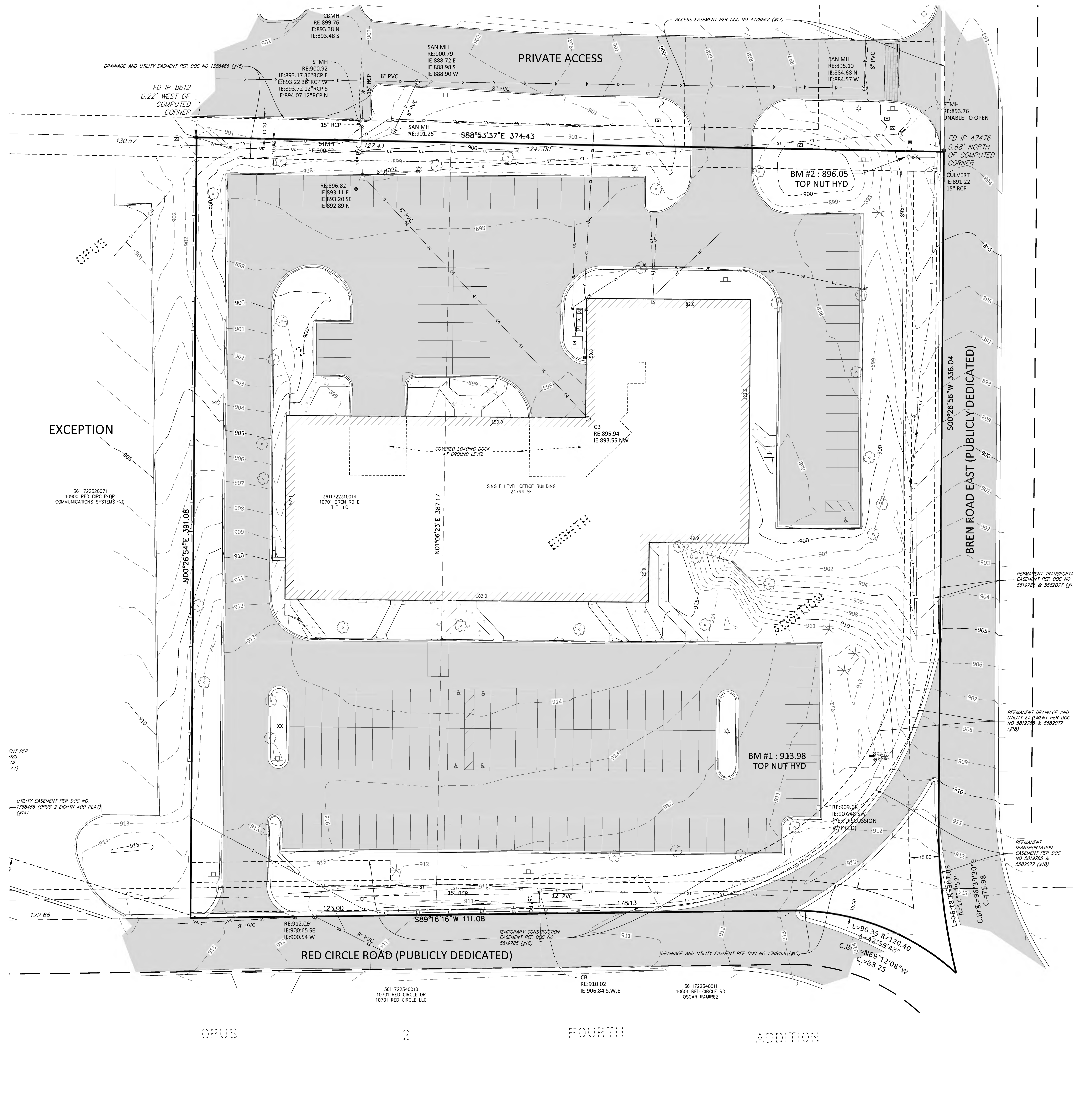
51166
PROJECT NUMBER

TLL BWF
DRAWN BY CHECKED BY
KEY PLAN

BREN ROAD
MULTIFAMILY

EXISTING CONDITIONS

C2.01



LEGEND		
● FOUND MONUMENT	— WATERMAIN	- - - EASEMENT LINE
○ SET MONUMENT MARKED	— SANITARY SEWER	- - - SETBACK LINE
⊗ ELECTRIC METER	— FORCEMAIN (SAN.)	- - - RESTRICTED ACCESS
★ LIGHT	— STORM SEWER	- - - CONCRETE CURB
⊞ AIR CONDITIONER	— FLARED END SECTION	- - - BUILDING LINE
⊞ GUY ANCHOR	— TELEPHONE PEDESTAL	- - - BUILDING CANOPY
⊞ HANDICAP STALL	— TELEPHONE PEDESTAL	□ STANDARD DUTY ASPHALT PAVING
⊞ UTILITY POLE	— ELECTRIC TRANSFORMER	□ BITUMINOUS SURFACE
⊞ POST	— GAS METER	□ CONCRETE SURFACE
⊞ SIGN	— OVERHEAD WIRE	□ LANDSCAPE SURFACE
	— CHAIN LINK FENCE	
	— IRON FENCE	
	— WIRE FENCE	
	— WOOD FENCE	

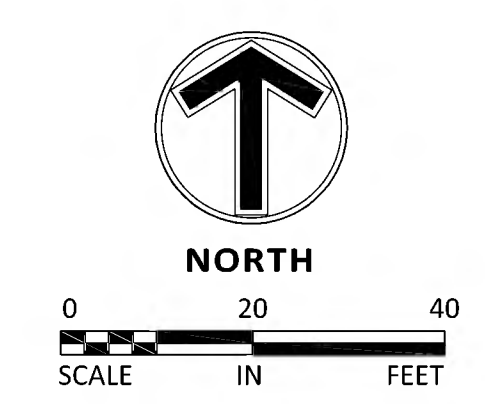
DESCRIPTION
Parcel 1:
Lot 5 and that part of Lot 6, which lies Easterly of the following described line: Beginning at a point on the South line of said Lot 6 distant 123.00 feet Westerly from the Southeast corner of said Lot 6; thence North 1 degree, 09 minutes, 15 seconds East, a distance of 391.08 feet to a point on the North line of said Lot 6, distant 127.43 feet Westerly from the Northeast corner of said Lot 6 and said line there terminating;
All in Block 1, Opus 2 Eighth Addition, Hennepin County, Minnesota
Torrrens Property Certificate of Title No. 1211616

Parcel 2:
Permanent easement for pedestrian and vehicular access and ingress and egress for the benefit of Parcel 1 as set forth in that certain Easement Agreement dated September 21, 2007, recorded September 21, 2007, as Document No. 4428662 (T).

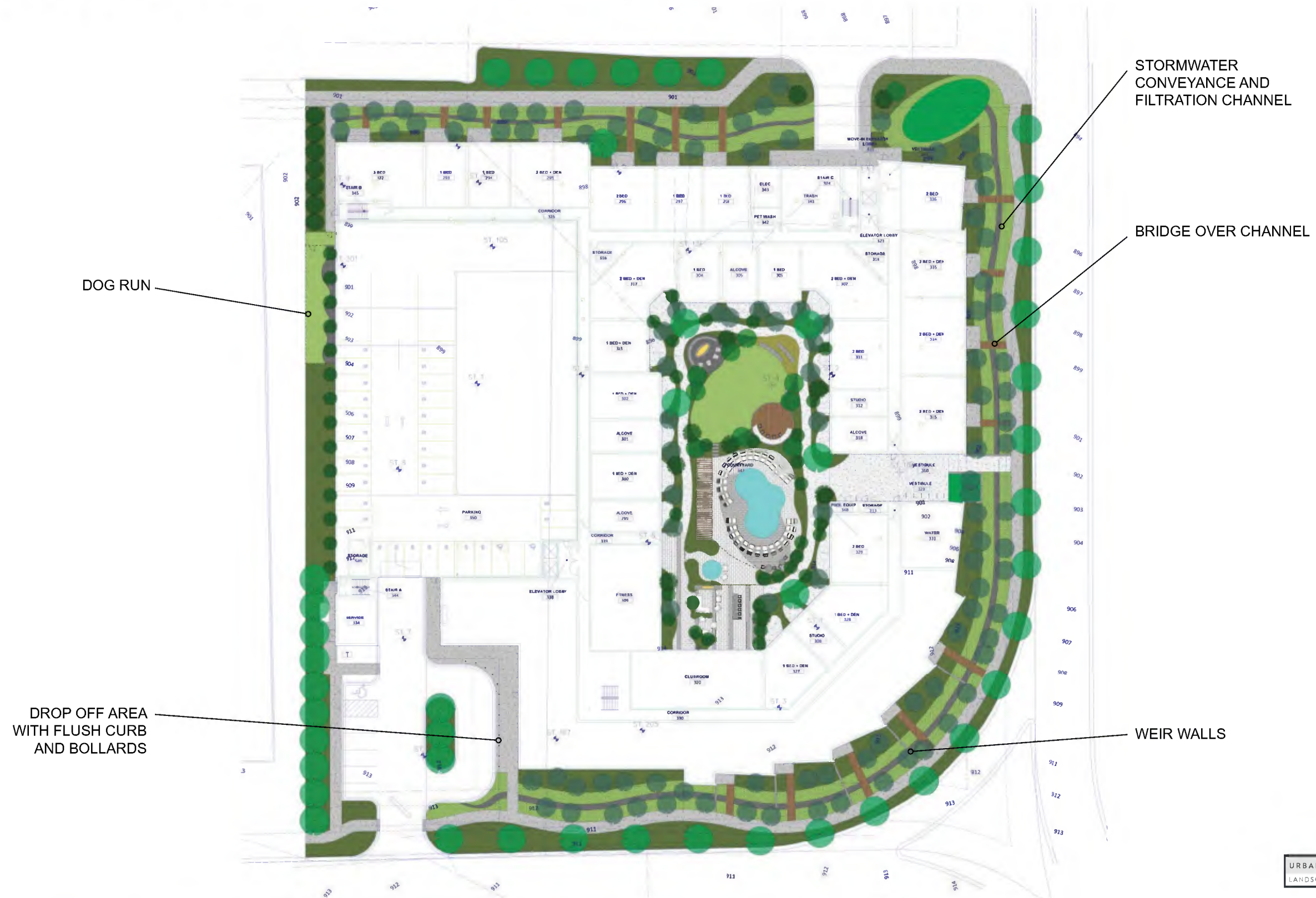
PROPERTY SUMMARY
1. The subject property address is 10701 Bren Road East, the property identification number is 3611722310014.
2. The gross area of the subject property is 3.328 acres or 144,986 square feet.
3. The subject property is zoned xxx, per xxx.
4. The building(s) and exterior dimensions of the outside wall at ground level are shown on the survey. It may not be the foundation wall.

BENCHMARKS
3611722310014 METROPOLITAN COUNTY
The vertical datum is based on NAVD88.
Benchmark #1
Top Nut Hydrant, Southeast corner of site elev = 913.98
Benchmark #2
Top Nut Hydrant, Northeast corner of site elev = 896.05

SURVEY NOTES
2. The bearing system is based on the Hennepin County coordinate system, NAD83 (1985 Adjust), with an assumed bearing of South 00 degrees 26 minutes 56 seconds West for the east line of Lot 5, Block 1, Opus 2 Eighth Addition, Section 36, Township 117, Range 22.
3. Field work was completed on 05/24/2022.



SITE PLAN



LANDSCAPE SITE PLAN

PROPOSED LANDSCAPING IMPROVEMENTS ON NEIGHBORING PROPERTY TO BE APPROVED BY PROPERTY OWNER

PROPERTY LINE

DOG RUN

DROP OFF AREA WITH FLUSH CURB AND BOLLARDS



SIDEWALK PROPOSED BY CITY OF MINNETONKA TO BE APPROVED AND COORDINATED BY BOTH PROPERTY OWNERS

STORMWATER FILTRATION AREA TO BE APPROVED AND COORDINATED BY BOTH PROPERTY OWNERS

BRIDGE OVER CHANNEL CONNECTING TO RESIDENTIAL UNITS

WALKWAY AND TUNNEL CONNECTING SIDEWALK TO GREEN SPACE

STORMWATER CONVEYANCE AND FILTRATION CHANNEL

WEIR WALLS FOR STORMWATER CONVEYANCE

URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS



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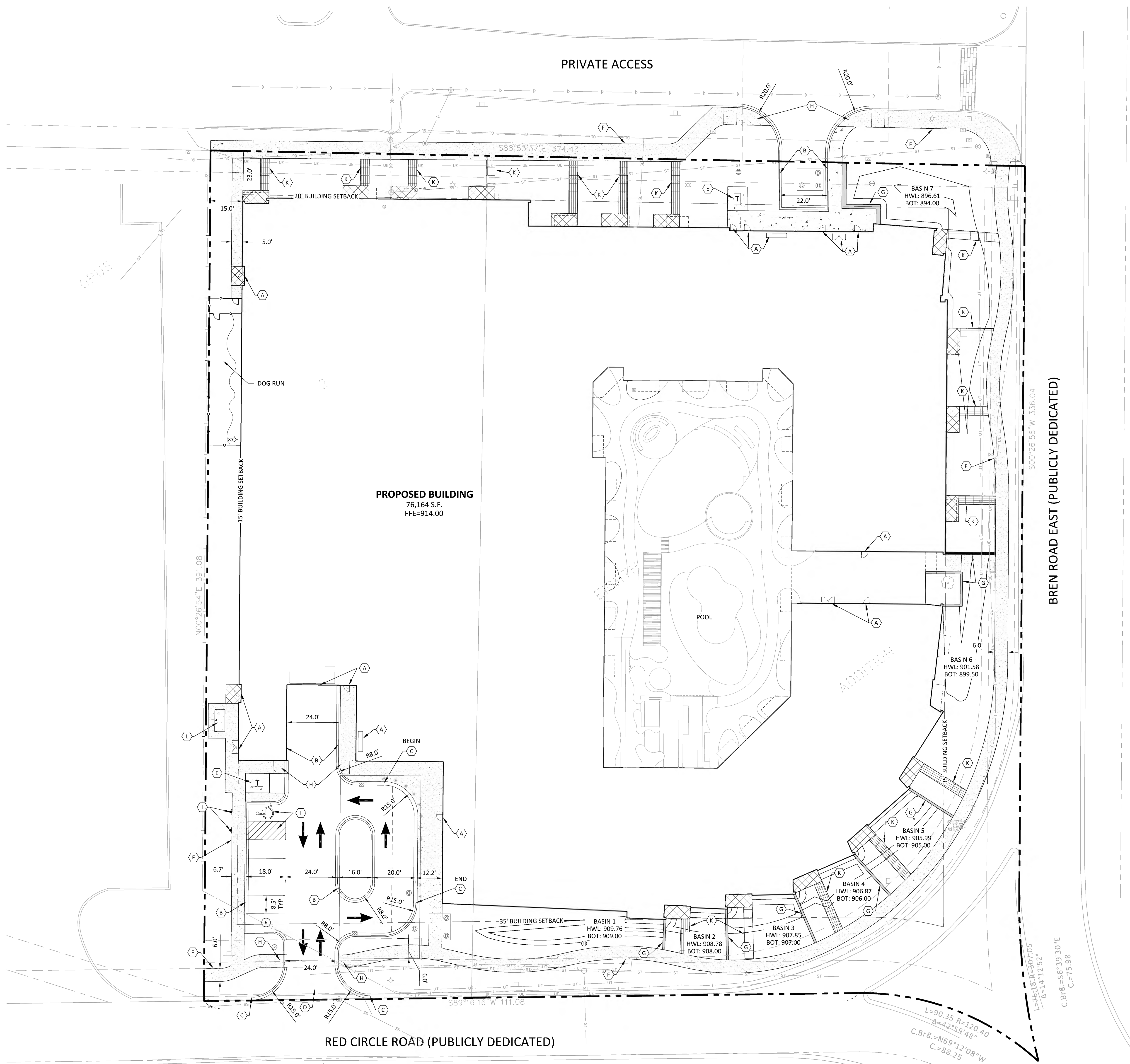
TLL BWF
DRAWN BY CHECKED BY

KEY PLAN

BREN ROAD
MULTIFAMILY

SITE PLAN

C3.01



LEGEND

PROPOSED	EXISTING	BOUNDARY LINE	STANDARD DUTY ASPHALT PAVING
[Symbol]	[Symbol]	CONCRETE CURB	[Symbol]
[Symbol]	[Symbol]	EASEMENT LINE	HEAVY DUTY ASPHALT PAVING
[Symbol]	[Symbol]	BUILDING LINE	CONCRETE PAVING
[Symbol]	[Symbol]	RETAINING WALL	CONCRETE SIDEWALK
[Symbol]	[Symbol]	WETLAND	PAVEMENT BY OTHERS (SEE ARCHITECTURAL PLANS)
[Symbol]	[Symbol]	TREE LINE	BRIDGE
[Symbol]	[Symbol]	SAW CUT LINE	
[Symbol]	[Symbol]	CHAIN LINK FENCE	
[Symbol]	[Symbol]	SIGN	
[Symbol]	[Symbol]	BOLLARD	
[Symbol]	[Symbol]	PARKING STALL COUNT	
[Symbol]	[Symbol]	KEY NOTE	
[Symbol]	[Symbol]	LIGHT POLE (BY OTHERS)	

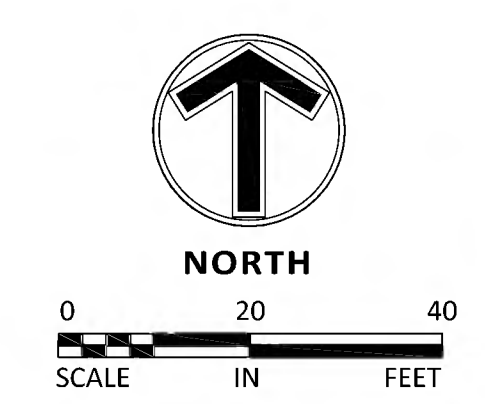
DEVELOPMENT SUMMARY

AREA	
SITE AREA	137,635 SF 3.16 AC
BUILDING	
UNITS	275 UNITS
DENSITY	87 UNITS/AC
OPEN SPACE REQUIRED (300 SF / UNIT)	82,500 SF
OPEN SPACE PROVIDED	35,290 SF
ZONING	
EXISTING ZONING	I-1
PROPOSED ZONING	PUD
SETBACKS	
FRONT YARD	35 FT
SIDE/REAR WEST	15 FT
SIDE/REAR NORTH	20 FT
PARKING	
PARKING REQUIRED (2 SPACES/UNIT, 1 TO BE ENCLOSED)	550 TOTAL 275 ENCLOSED
PARKING PROVIDED SURFACE ENCLOSED	401 6 395

DEVELOPMENT NOTES

1. NOTES PLACEHOLDER

- KEY NOTES**
- A. BUILDING, STOOPS, STAIRS (SEE ARCHITECTURAL PLANS)
 - B. B-612 CONCRETE CURB AND GUTTER
 - C. FLAT CURB AND GUTTER
 - D. CONCRETE APRON
 - E. TRANSFORMER
 - F. CONCRETE SIDEWALK
 - G. RETAINING WALL
 - H. ADA ACCESS LOCATION
 - I. ACCESSIBLE STALL STRIPING
 - J. ACCESSIBLE PARKING SIGN
 - K. BRIDGE
 - L. GENERATOR



THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/CI 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPOSITION OF EXISTING SUBSURFACE UTILITY DATA". THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. BY CONTACTING THE NOTIFICATION CENTER (Gopher State One FOR MINNESOTA), THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

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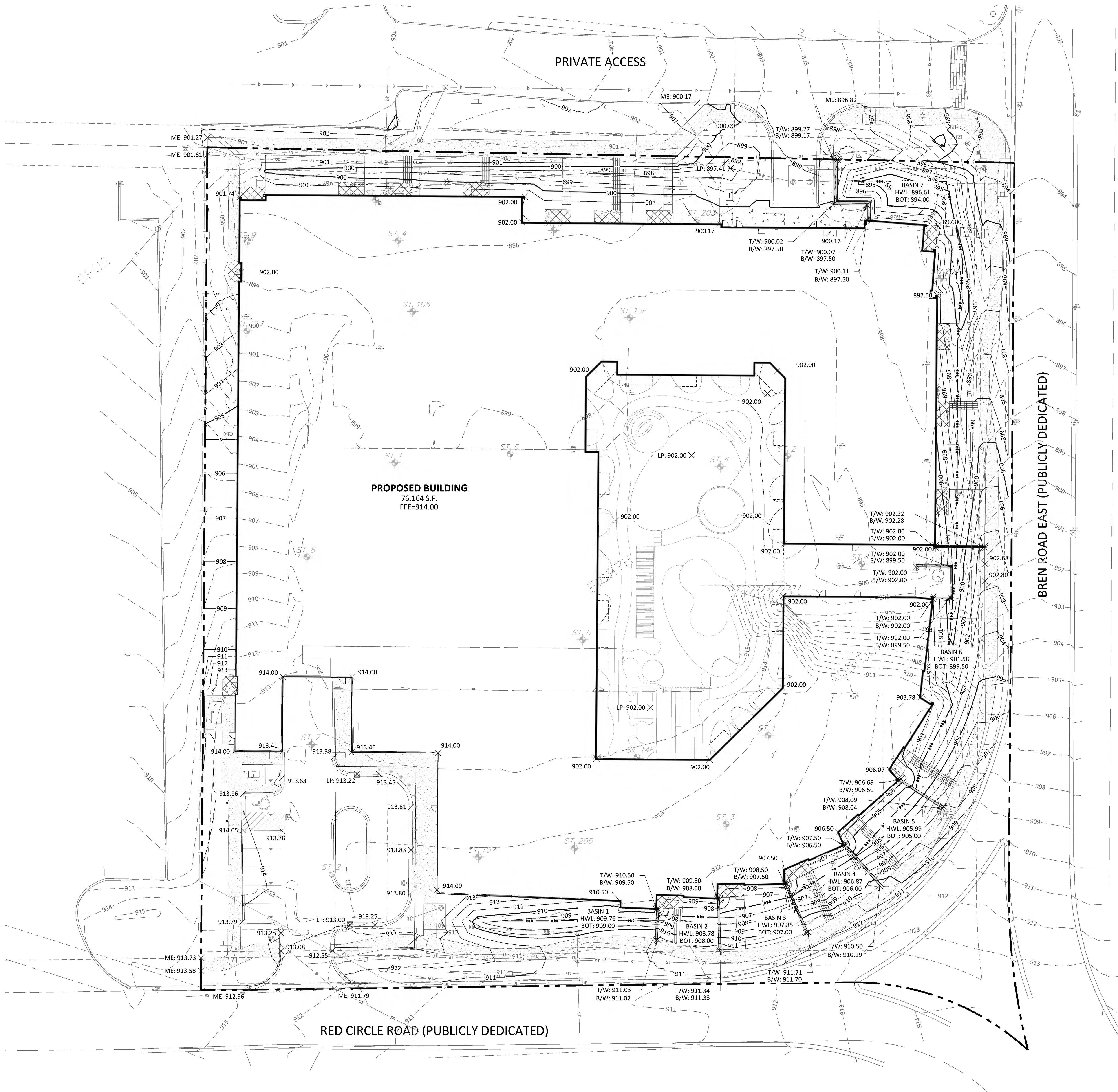
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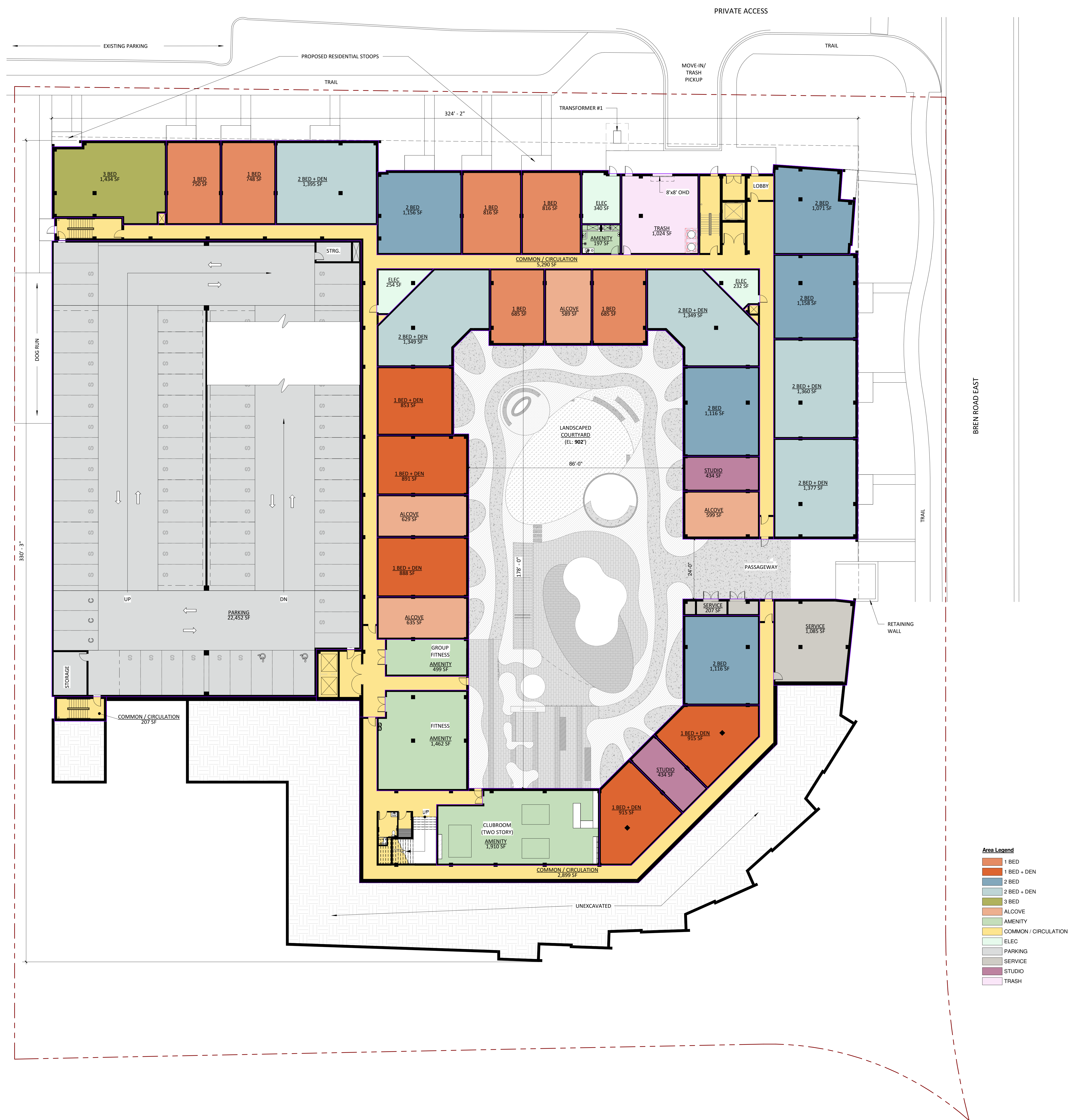
PROPOSED	EXISTING	BOUNDARY LINE	CONCRETE PAVING
(Symbol)	(Symbol)	(Symbol)	(Symbol)
(Symbol)	(Symbol)	CONCRETE CURB	CONCRETE SIDEWALK
(Symbol)	(Symbol)	STORM SEWER	PAVEMENT BY OTHERS (SEE ARCHITECTURAL PLANS)
(Symbol)	(Symbol)	DRAIN TILE	
(Symbol)	(Symbol)	BUILDING LINE	
(Symbol)	(Symbol)	RETAINING WALL	
(Symbol)	(Symbol)	CONTOUR	
(Symbol)	(Symbol)	WETLAND	
(Symbol)	(Symbol)	TREE LINE	
(Symbol)	(Symbol)	SPOT ELEVATIONS	
(Symbol)	(Symbol)	RIPRAP	
(Symbol)	(Symbol)	OVERFLOW ELEV.	
(Symbol)	(Symbol)	SOIL BORING	

GRADING NOTES
1. GRADING NOTES PLACE HOLDER

INFILTRATION BASIN CONSTRUCTION NOTES
1. INFILTRATION BASIN CONSTRUCTION NOTES PLACE HOLDER

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/CI 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).
IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

**NOT FOR
CONSTRUCTION**



Area Legend

1 BED
1 BED + DEN
2 BED
2 BED + DEN
3 BED
ALCOVE
AMENITY
COMMON / CIRCULATION
ELEC
PARKING
SERVICE
STUDIO
TRASH

LAND USE
APPLICATION
12/06/2022

ORIGINAL ISSUE:
09/19/22

REVISIONS:
No. Description Date

222521
PROJECT NUMBER

ESG DRAWN BY ESG CHECKED BY

KEY PLAN

BREN ROAD
MULTIFAMILY

LEVEL 1 FLOOR PLAN

A1.0

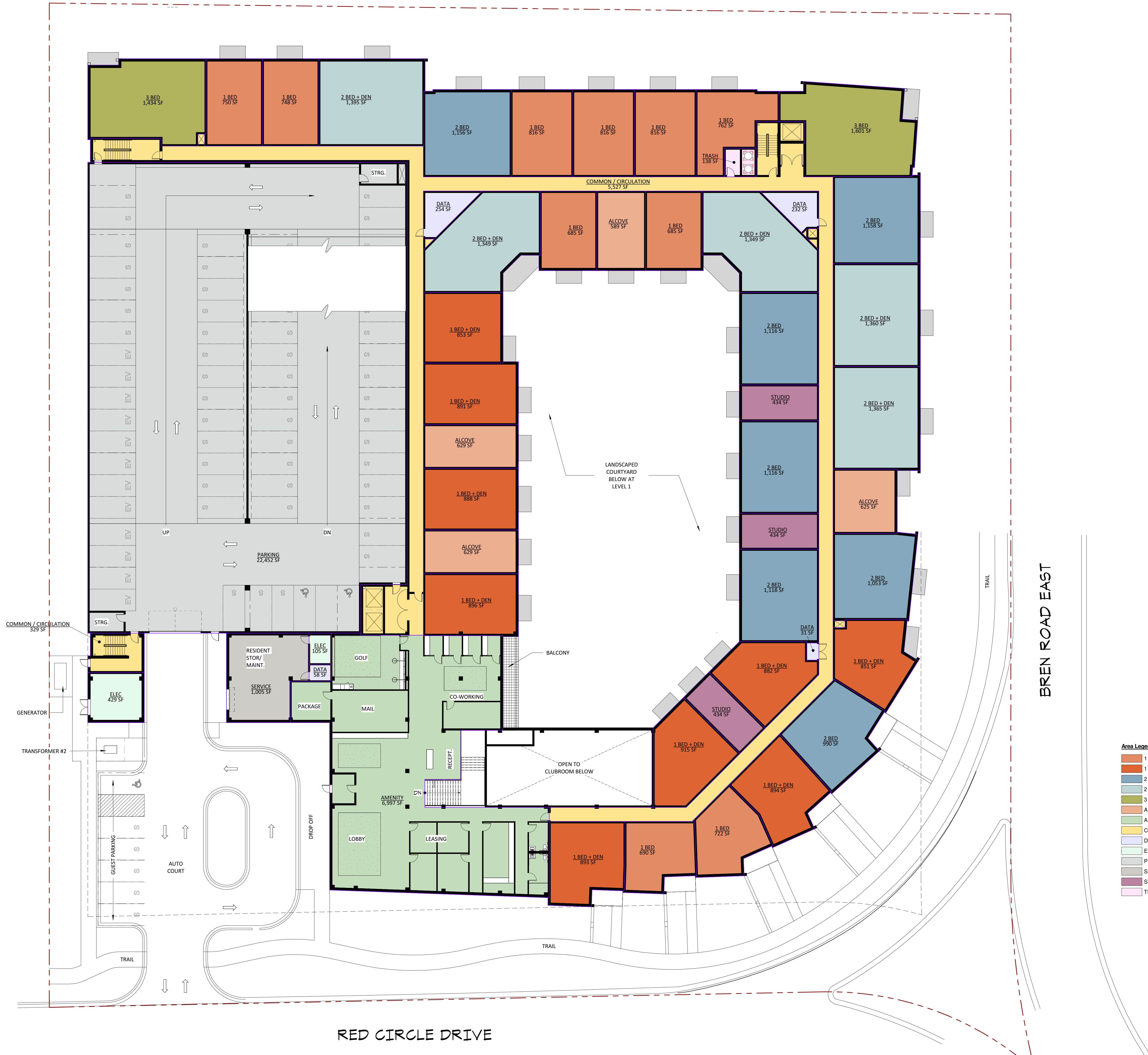
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota

Signature _____

Typed or Printed Name _____

License # _____ Date _____

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CONSTRUCTION**



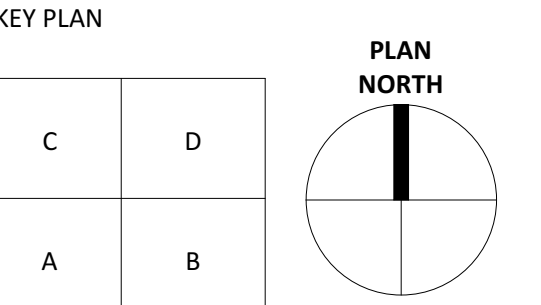
- Area Legend**
- 1 BED
 - 1 BED + DEN
 - 2 BED
 - 2 BED + DEN
 - 3 BED
 - ALCOVE
 - AMENITY
 - COMMON / CIRCULATION
 - DATA
 - ELEC
 - PARKING
 - SERVICE
 - STUDIO
 - TRASH

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BREN ROAD
MULTIFAMILY

LEVEL 2 FLOOR PLAN

A1.1

1 LEVEL 2 FLOOR PLAN
A1.1 1/16" = 1'-0"

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License # _____ Date _____

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CONSTRUCTION**



Area Legend

	1 BED
	1 BED + DEN
	2 BED
	2 BED + DEN
	3 BED
	ALCOVE
	COMMON / CIRCULATION
	ELEC
	PARKING
	STUDIO
	TRASH

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KEY PLAN

BREN ROAD
MULTIFAMILY

LEVEL 3-5 FLOOR PLAN

A1.2

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License # _____ Date _____

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CONSTRUCTION**



Area Legend

1 BED
1 BED + DEN
2 BED
2 BED + DEN
3 BED
ALCOVE
AMENITY
COMMON / CIRCULATION
DATA
PARKING
STUDIO
TRASH

LAND USE
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ESG DRAWN BY ESG CHECKED BY

KEY PLAN

BREN ROAD
MULTIFAMILY

LEVEL 6 FLOOR PLAN

A1.5

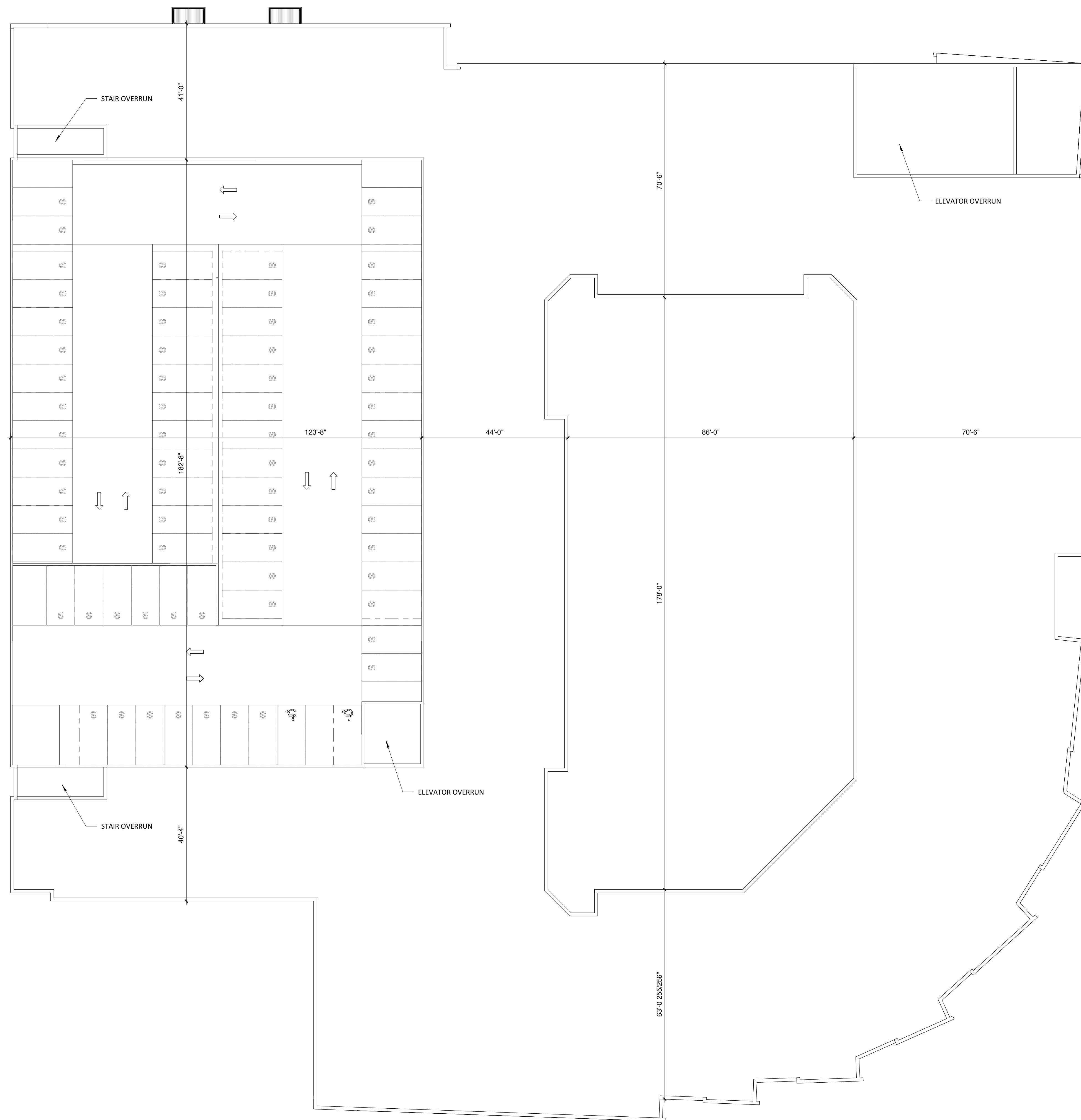
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Typed or Printed Name _____

License # _____ Date _____

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KEY PLAN

BREN ROAD
MULTIFAMILY

ROOF PLAN

A1.6

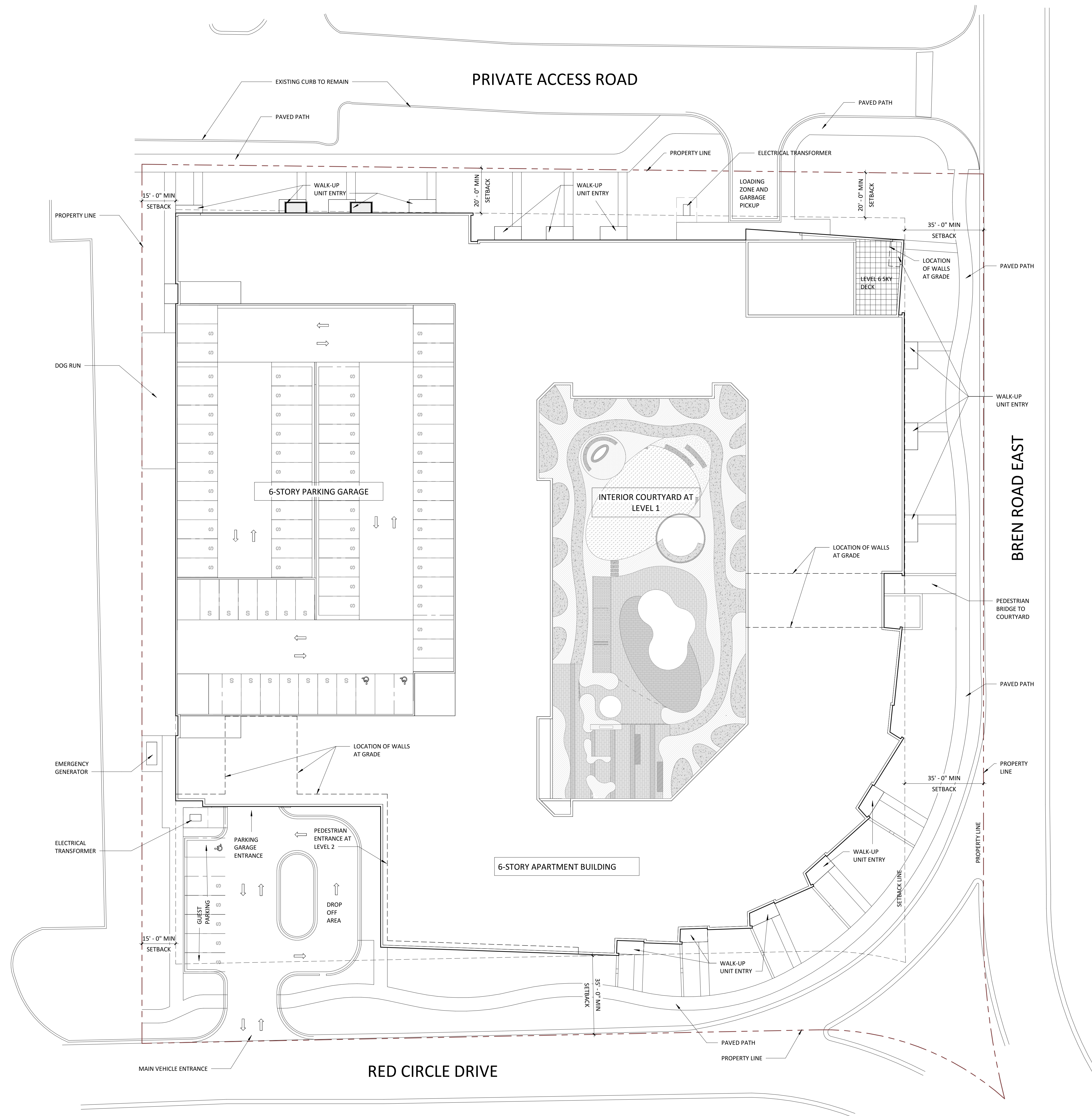
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Typed or Printed Name _____

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LAND USE
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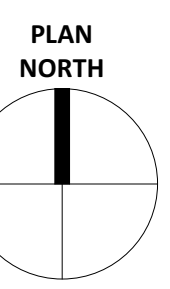
PROJECT NUMBER

ESG DRAWN BY

ESG CHECKED BY

KEY PLAN

C	D
A	B



BREN ROAD
MULTIFAMILY

ARCHITECTURAL SITE PLAN

A0.1

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Signature _____

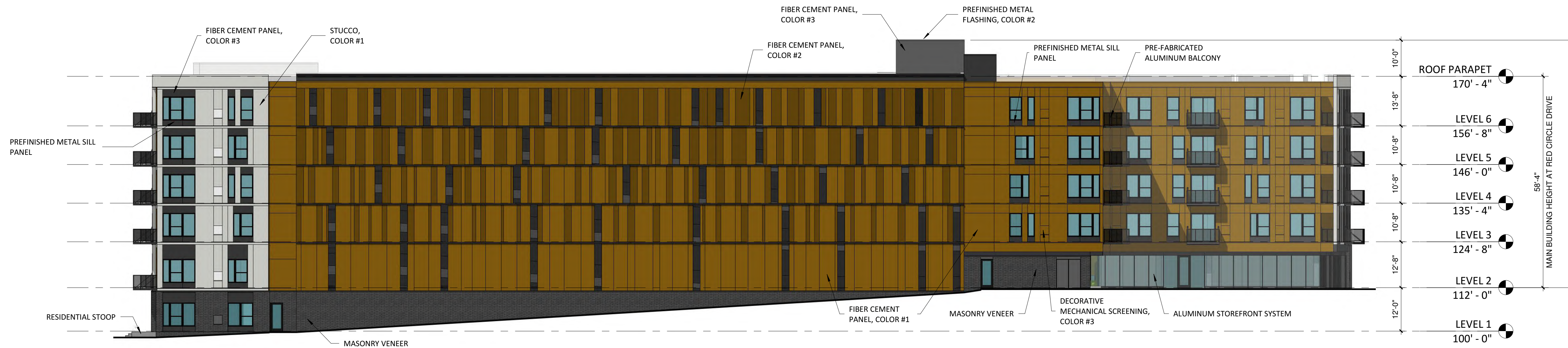
Typed or Printed Name _____

License # Date _____

**NOT FOR
CONSTRUCTION**



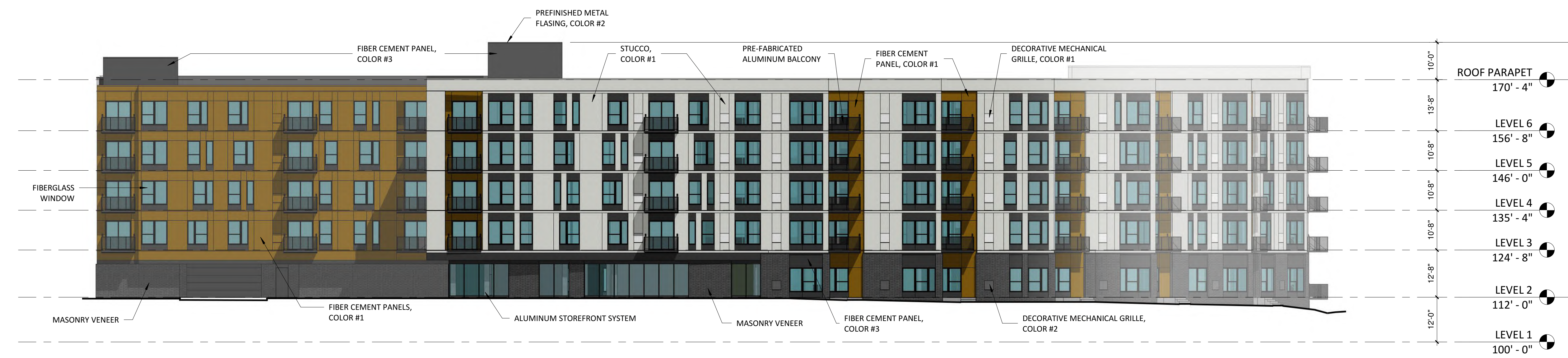
4 EAST ELEVATION
A3.0 1/16" = 1'-0"



3 WEST ELEVATION
A3.0 1/16" = 1'-0"



2 NORTH ELEVATION
A3.0 1/16" = 1'-0"



1 SOUTH ELEVATION
A3.0 1/16" = 1'-0"

LAND USE APPLICATION
12/06/2022

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222521
PROJECT NUMBER

ESG
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BREN ROAD
MULTIFAMILY

EXTERIOR ELEVATIONS
A3.0

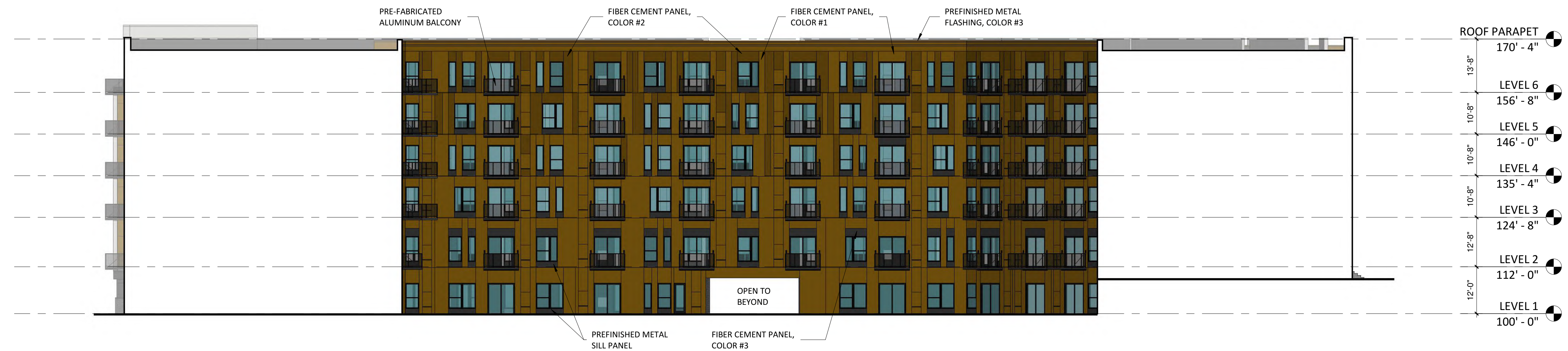
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Signature _____

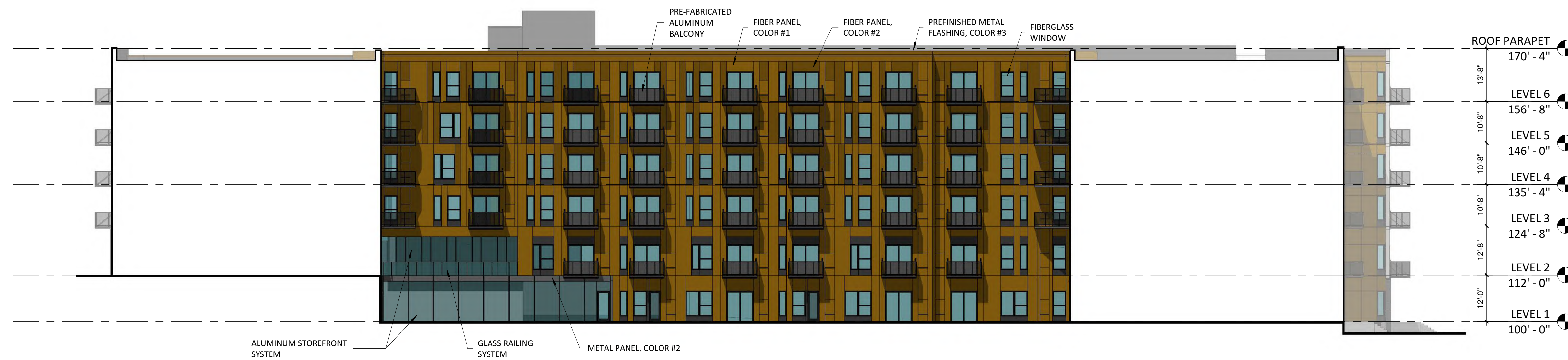
Typed or Printed Name _____

License # _____ Date _____

**NOT FOR
CONSTRUCTION**



4 EAST COURTYARD ELEVATION
A3.1 1/16" = 1'-0"



1 WEST COURTYARD ELEVATION
A3.1 1/16" = 1'-0"



2 SOUTH COURTYARD ELEVATION
A3.1 1/16" = 1'-0"



3 NORTH COURTYARD ELEVATION
A3.1 1/16" = 1'-0"

LAND USE
APPLICATION
12/06/2022

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REVISIONS:
No. Description Date

222521
PROJECT NUMBER

ESG DRAWN BY ESG CHECKED BY

KEY PLAN

BREN ROAD
MULTIFAMILY

COURTYARD ELEVATIONS

A3.1

**NOT FOR
CONSTRUCTION**

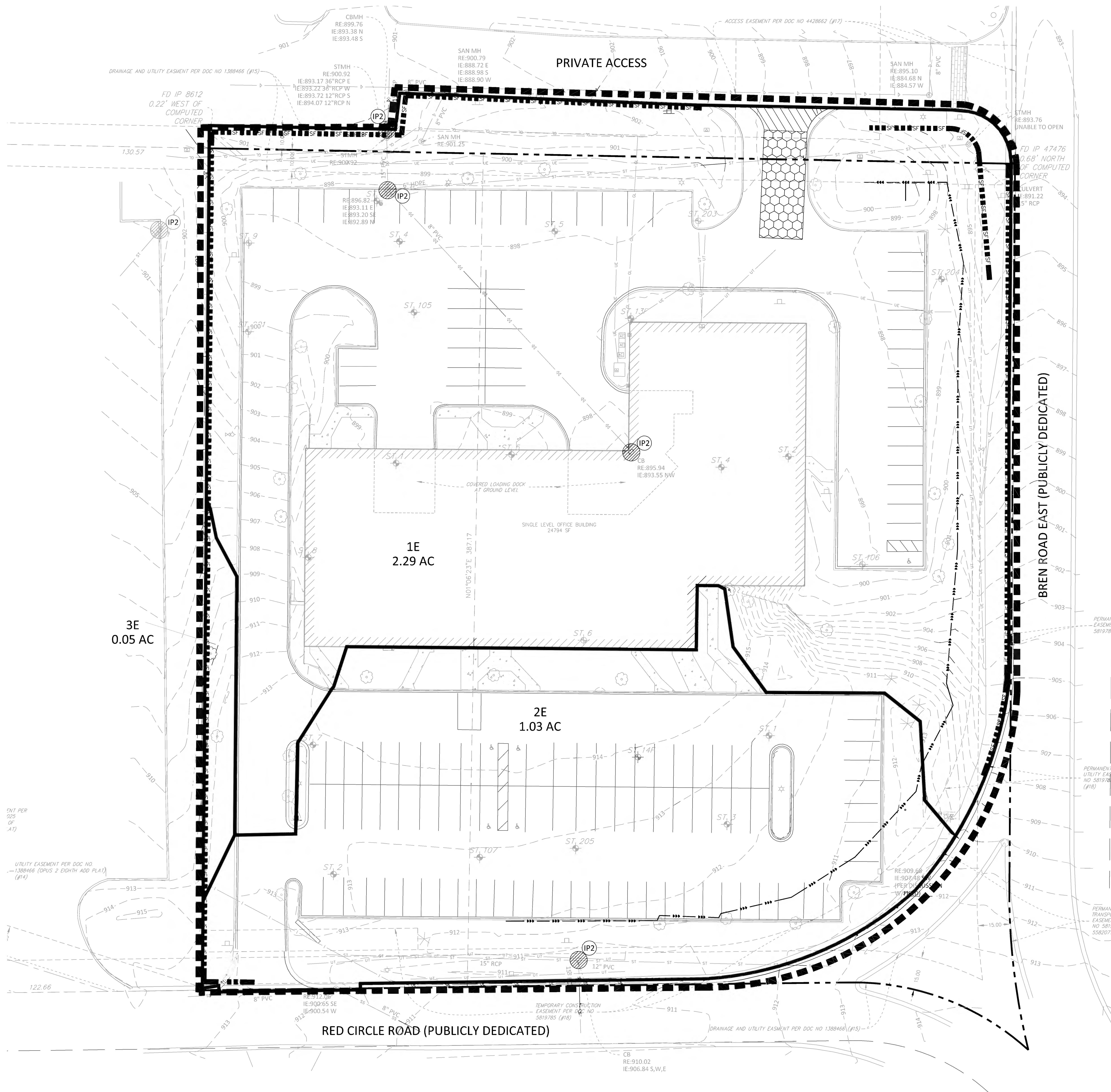
**LAND USE
APPLICATION
SUBMITTAL
12/06/2022**

ORIGINAL ISSUE:
09/19/22
REVISIONS:
No. Description Date

51166
PROJECT NUMBER
TLL BWF
DRAWN BY CHECKED BY
KEY PLAN

**BREN ROAD
MULTIFAMILY**
PHASE I EROSION
CONTROL PLAN

C5.01



LEGEND

PROPOSED	EXISTING	
		CONCRETE CURB
		STORM SEWER
		DRAINTILE
		CONTOUR
		RIPRAP
		OVERFLOW ELEV.
		BIO-ROLL
		SILT FENCE
		SILT DIKE
		LIMITS OF DISTURBANCE
		SOIL BORING
		DIRECTION OF OVERLAND FLOW
		TEMPORARY DIVERSION DITCH
		CHECK DAM
		LIMITS OF DRAINAGE SUB-BASIN
		INLET PROTECTION DEVICE
		TEMPORARY STONE CONSTRUCTION ENTRANCE
		TEMPORARY SEDIMENT BASIN
		TEMPORARY STABILIZATION MEASURES (SEED, MULCH, MATS OR BLANKETS AS OUTLINED IN THE SWPPP)
		TEMPORARY STORAGE AND PARKING AREA

EROSION CONTROL MATERIALS QUANTITIES

ITEM	UNIT	QUANTITY
SILT FENCE	LINEAR FEET	1,065
SILT DIKE	LINEAR FEET	0
BIO-ROLL	LINEAR FEET	0
CONSTRUCTION ENTRANCE	UNIT	1
INLET PROTECTION DEVICE (IP-1)	UNIT	0
INLET PROTECTION DEVICE (IP-2)	UNIT	6

* REFER TO SHEET C5.03 FOR GENERAL NOTES, MAINTENANCE NOTES, LOCATION MAPS, AND STANDARD DETAILS

NOTE TO CONTRACTOR

THE EROSION CONTROL PLAN SHEETS ALONG WITH THE REST OF THE SWPPP MUST BE KEPT ONSITE UNTIL THE NOTICE OF TERMINATION IS FILED WITH THE MPCA. THE CONTRACTOR MUST UPDATE THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITIONAL OR MODIFIED BMPs DESIGNED TO CORRECT PROBLEMS IDENTIFIED. AFTER FILING THE NOTICE OF TERMINATION, THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS, AND ALL REVISIONS TO IT MUST BE SUBMITTED TO THE OWNER, TO BE KEPT ON FILE IN ACCORDANCE WITH THE RECORD RETENTION REQUIREMENTS DESCRIBED IN THE SWPPP NARRATIVE.

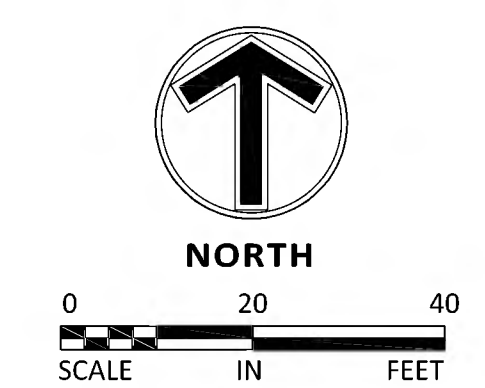
SOIL EROSION/SEDIMENTATION CONTROL OPERATION TIME SCHEDULE

CONSTRUCTION SEQUENCE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB	MAR	APR	MAY	JUN	
TEMPORARY CONTROL MEASURES																			
STRIP & STOCKPILE TOPSOIL																			
ROUGH GRADE / SEDIMENT CONTROL																			
TEMPORARY CONSTRUCTION ROADS																			
FOUNDATION / BUILDING CONSTRUCTION																			
SITE CONSTRUCTION																			
PERMANENT CONTROL STRUCTURES																			
FINISH GRADING																			
LANDSCAPING / SEED / FINAL STABILIZATION																			
STORM FACILITIES																			

NOTE: CONTRACTOR OR GENERAL CONTRACTOR TO COMPLETE TABLE WITH THEIR SPECIFIC PROJECT SCHEDULE

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/CI 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

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NOT FOR CONSTRUCTION

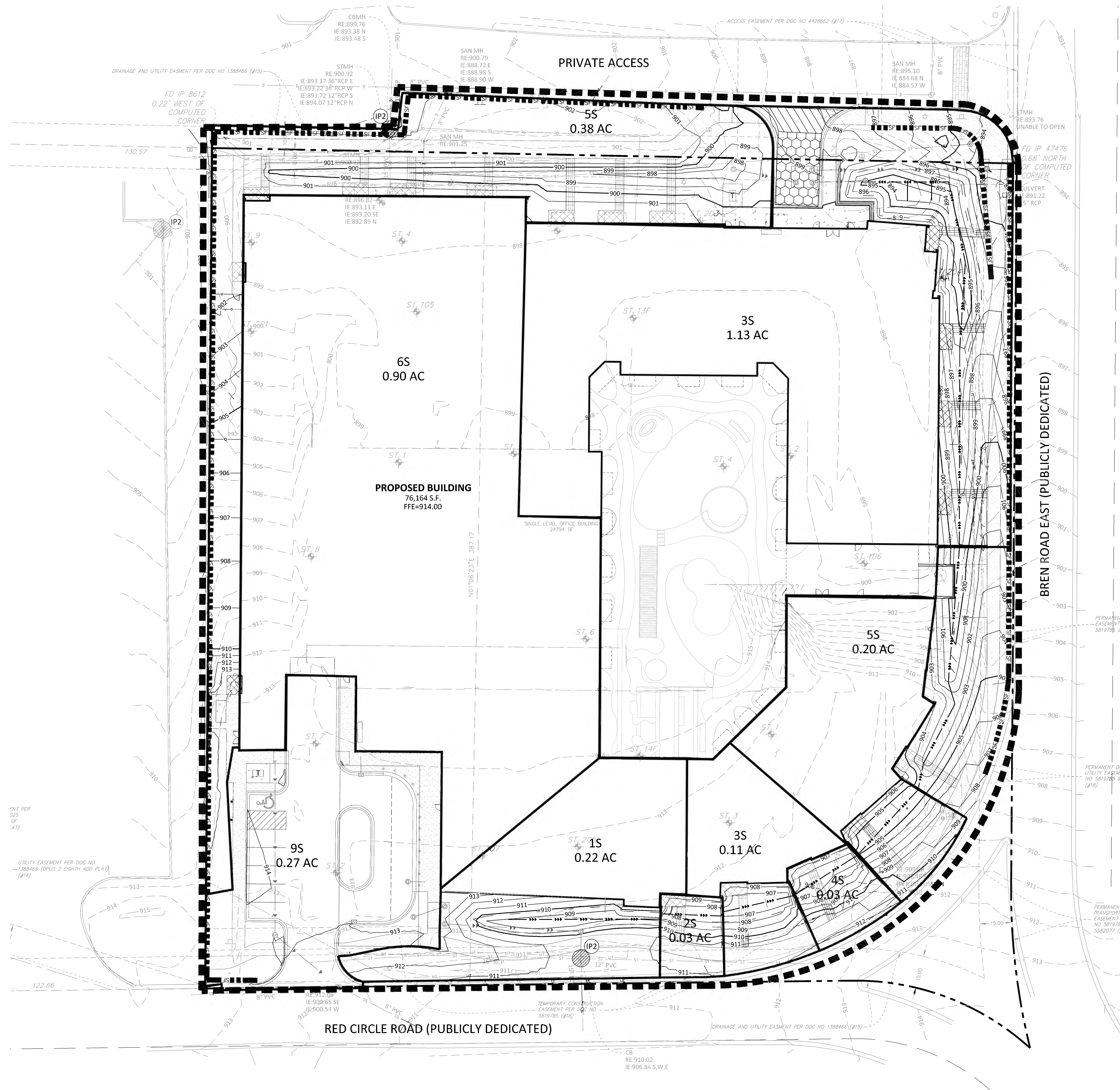
LAND USE APPLICATION
SUBMITTAL
12/06/2022

ORIGINAL ISSUE:
09/19/22
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No. Description Date

51166
PROJECT NUMBER
TLL BWF
DRAWN BY CHECKED BY
KEY PLAN

BREN ROAD
MULTIFAMILY
PHASE II EROSION
CONTROL PLAN

C5.02



LEGEND

PROPOSED	EXISTING	
		CONCRETE CURB
		STORM SEWER
		DRAINTILE
		CONTOUR
		RIPRAP
		OVERFLOW ELEV.
		BIO-ROLL
		SILT FENCE
		SILT DIKE
		LIMITS OF DISTURBANCE
		SOIL BORING
		DIRECTION OF OVERLAND FLOW
		TEMPORARY DIVERSION DITCH
		CHECK DAM
		LIMITS OF DRAINAGE SUB-BASIN
		INLET PROTECTION DEVICE
		TEMPORARY STONE CONSTRUCTION ENTRANCE
		TEMPORARY SEDIMENT BASIN
		TEMPORARY STABILIZATION MEASURES (SEED, MULCH, MATS OR BLANKETS AS OUTLINED IN THE SWPPP)
		TEMPORARY STORAGE AND PARKING AREA

EROSION CONTROL MATERIALS QUANTITIES

ITEM	UNIT	QUANTITY
SILT FENCE	LINEAR FEET	1,064
SILT DIKE	LINEAR FEET	0
BIO-ROLL	LINEAR FEET	0
CONSTRUCTION ENTRANCE	UNIT	1
INLET PROTECTION DEVICE (IP-1)	UNIT	0
INLET PROTECTION DEVICE (IP-2)	UNIT	3

NOTE TO CONTRACTOR

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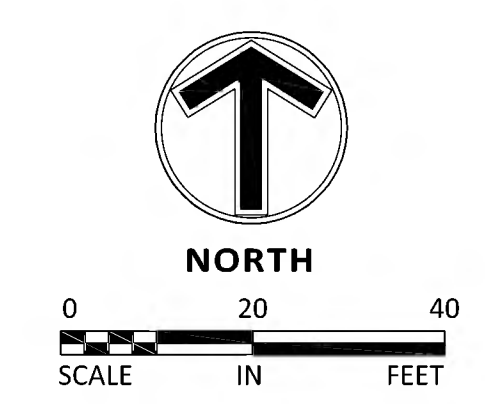
SOIL EROSION/SEDIMENTATION CONTROL OPERATION TIME SCHEDULE

CONSTRUCTION SEQUENCE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	JAN	FEB	MAR	APR	MAY	JUN	
TEMPORARY CONTROL MEASURES																			
STRIP & STOCKPILE TOPSOIL																			
ROUGH GRADE / SEDIMENT CONTROL																			
TEMPORARY CONSTRUCTION ROADS																			
FOUNDATION / BUILDING CONSTRUCTION																			
SITE CONSTRUCTION																			
PERMANENT CONTROL STRUCTURES																			
FINISH GRADING																			
LANDSCAPING / SEED / FINAL STABILIZATION																			
STORM FACILITIES																			

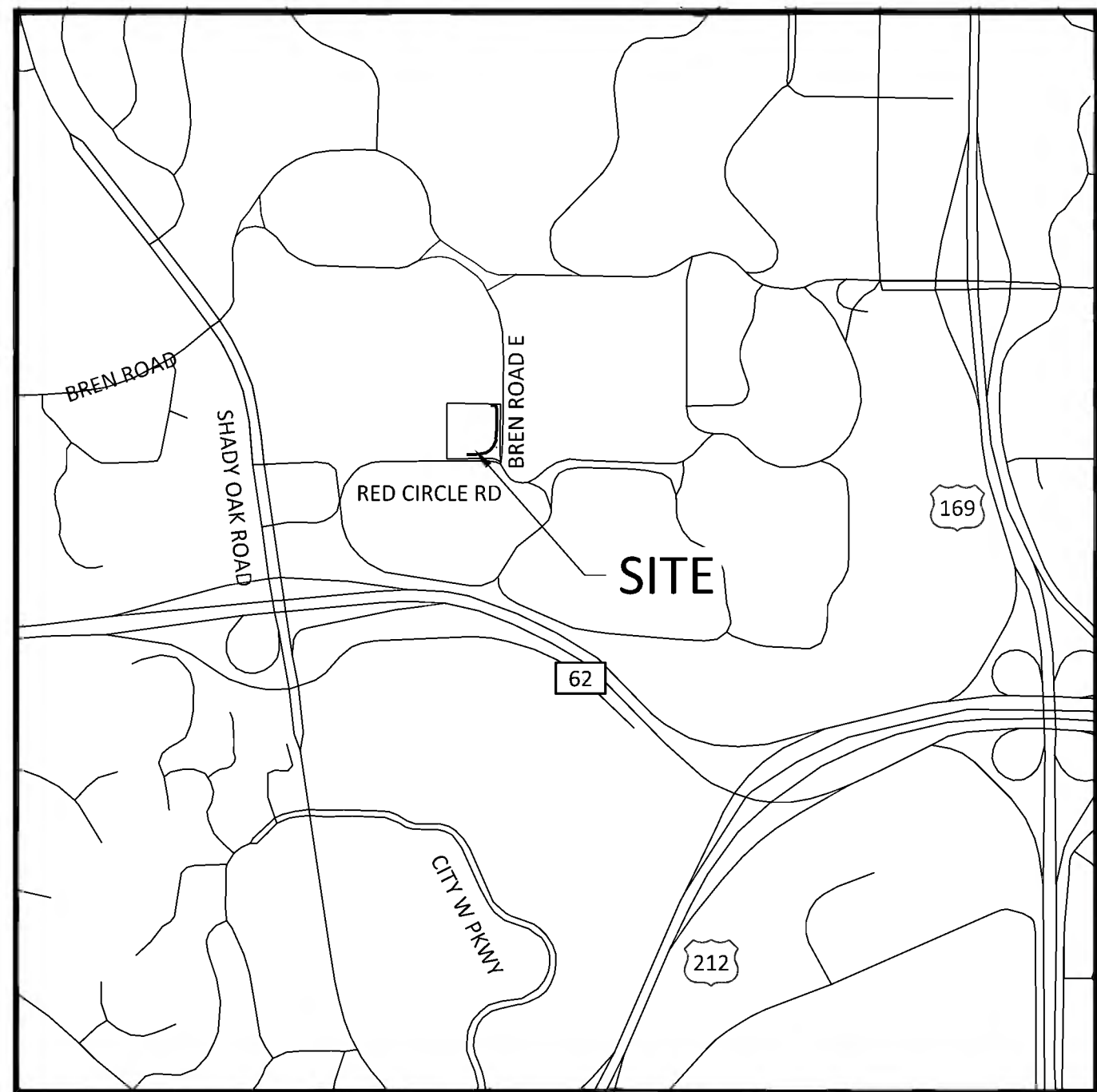
NOTE: CONTRACTOR OR GENERAL CONTRACTOR TO COMPLETE TABLE WITH THEIR SPECIFIC PROJECT SCHEDULE

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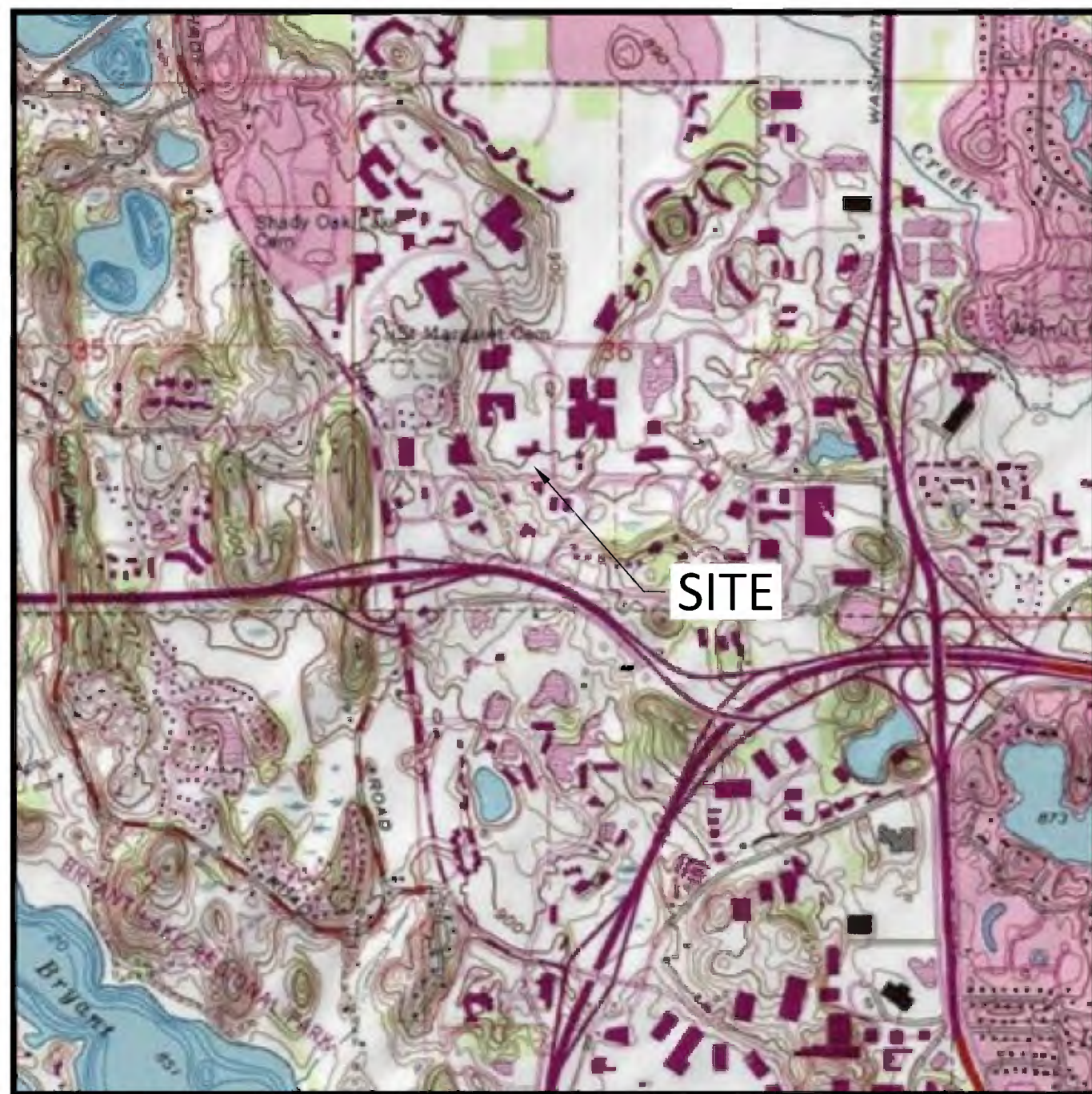


EROSION & SEDIMENTATION CONTROL NOTES & DETAILS / "SITE MAP"



SITE LOCATION MAP

NOT TO SCALE



USGS MAP

NOT TO SCALE

SEQUENCE OF CONSTRUCTION

- PHASE I:
1. INSTALL STABILIZED CONSTRUCTION ENTRANCES.
 2. PREPARE TEMPORARY PARKING AND STORAGE AREA.
 3. CONSTRUCT THE SILT FENCES ON THE SITE.
 4. INSTALL INLET PROTECTION DEVICES ON EXISTING STORM STRUCTURES, AS SHOWN ON THE PLAN.
 5. CONSTRUCT THE SEDIMENTATION AND SEDIMENT TRAP BASINS, AS REQUIRED.
 6. HALT ALL ACTIVITIES AND CONTACT THE CIVIL ENGINEERING CONSULTANT TO PERFORM INSPECTION OF BMP'S. GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT STORM WATER PRE-CONSTRUCTION MEETING WITH ENGINEER AND ALL GROUND DISTURBING CONTRACTORS BEFORE PROCEEDING WITH CONSTRUCTION.
 7. CLEAR AND GRUB THE SITE.
 8. BEGIN GRADING THE SITE.
 9. START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.

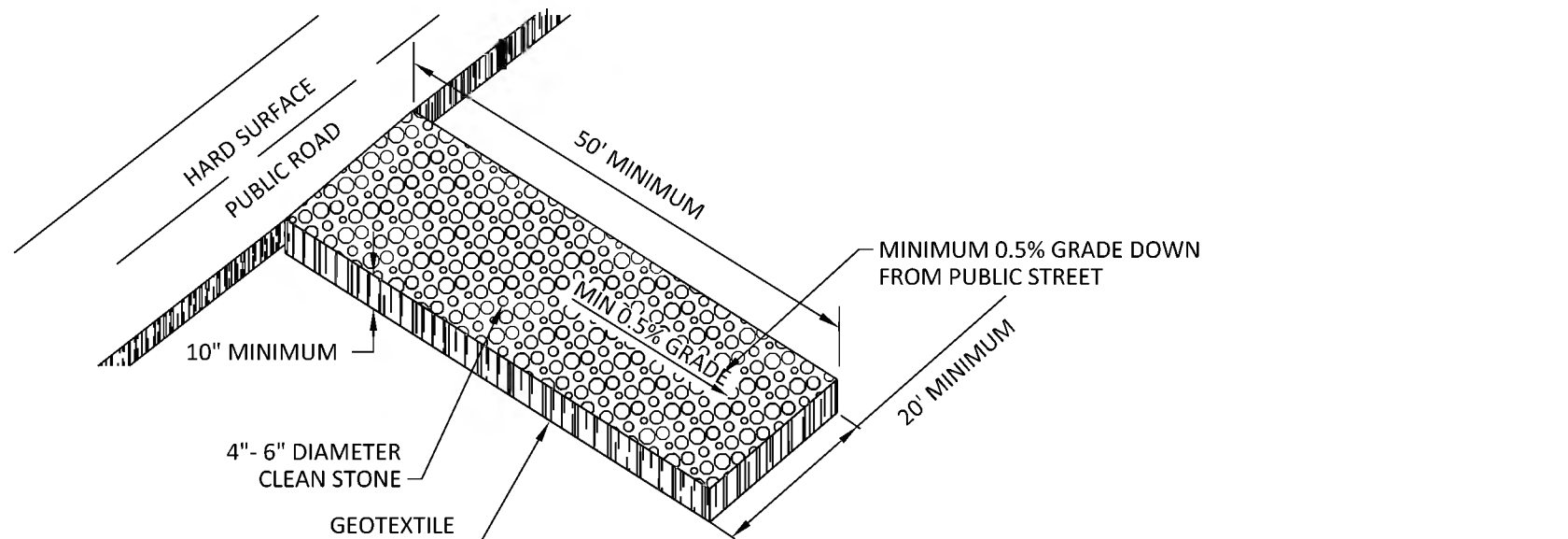
- PHASE II:
1. TEMPORARY SEED DENUED AREAS.
 2. INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
 3. INSTALL RIP RAP AROUND OUTLET STRUCTURES.
 4. INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
 5. PREPARE SITE FOR PAVING.
 6. PAVE SITE.
 7. INSTALL INLET PROTECTION DEVICES.
 8. COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
 9. REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED), IF REQUIRED BY THE CONTRACT.

GENERAL EROSION NOTES

1. CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME. WHERE A CONFLICT EXISTS BETWEEN LOCAL JURISDICTIONAL STANDARD SPECIFICATIONS AND SAMBATEK STANDARD SPECIFICATIONS, THE MORE STRINGENT SPECIFICATION SHALL APPLY.
2. THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS COMPRISED OF THIS DRAWING (EROSION & SEDIMENTATION CONTROL PLAN-ESC PLAN), THE STANDARD DETAILS, THE PLAN NARRATIVE, AND ITS APPENDICES, PLUS THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING & SUBMITTING THE APPLICATION FOR THE MPCA GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY. ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE SWPPP AND THE STATE OF MINNESOTA NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT (NPDES PERMIT, ISSUED AUGUST 1, 2018) AND BECOME FAMILIAR WITH THE CONTENTS. THE SWPPP AND ALL OTHER RELATED DOCUMENTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION.
4. CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES (BMP'S) AS REQUIRED BY THE SWPPP & PERMITS. CONTRACTOR SHALL OVERSEE THE INSPECTION & MAINTENANCE OF THE BMP'S AND EROSION PREVENTION FROM BEGINNING OF CONSTRUCTION AND UNTIL CONSTRUCTION IS COMPLETED, IS APPROVED BY ALL AUTHORITIES, AND THE NOTICE OF TERMINATION (NOT) HAS BEEN FILED WITH THE MPCA BY EITHER THE OWNER OR OPERATOR AS APPROVED ON PERMIT. ADDITIONAL BMP'S SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST TO OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION. (NOTE TO THE PREPARER: REVISE INSPECTION RESPONSIBILITY PER OPTIONS IN SWPPP NARRATIVE, SECTION 023701)
5. CONTRACTOR SHALL COMPLY WITH TRAINING REQUIREMENTS IN PART 21.1-21.3 OF THE GENERAL PERMIT.
6. BMP'S AND CONTROLS SHALL CONFORM TO FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AS APPLICABLE. CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY PERMITTING AGENCY OR OWNER.
7. ESC PLAN MUST CLEARLY DELINEATE ALL STATE WATERS. PERMITS FOR ANY CONSTRUCTION ACTIVITY IMPACTING STATE WATERS OR REGULATED WETLANDS MUST BE MAINTAINED ON SITE AT ALL TIMES.
8. CONTRACTOR SHALL MINIMIZE CLEARING TO THE MAXIMUM EXTENT PRACTICAL OR AS REQUIRED BY THE GENERAL PERMIT. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THE ESC PLANS SHALL BE CLEARLY DELINEATED (E.G. WITH FLAGS, STAKES, SIGNS, SILT FENCE, ETC.) ON THE DEVELOPMENT SITE BEFORE WORK BEGINS. GROUND DISTURBING ACTIVITIES MUST NOT OCCUR OUTSIDE THE LIMITS OF DISTURBANCE.
9. GENERAL CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE TRAILERS, AND TOILET FACILITIES.
10. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) MUST BE LIMITED TO A DEFINED AREA OF THE SITE AND SHALL BE CONTAINED AND PROPERLY TREATED OR DISPOSED. NO ENGINE DEGREASING IS ALLOWED ON SITE.
11. ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. A COMPACTED CLAY LINER IS NOT ACCEPTABLE. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE GROUND, AND THERE MUST NOT BE RUNOFF FROM THE CONCRETE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH MPCA REGULATIONS. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES. SELF-CONTAINED CONCRETE WASHOUTS ON CONCRETE DELIVERY TRUCKS ARE ALLOWED.
12. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLotation BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
13. DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
14. SOLID WASTE: COLLECTED SEDIMENT, ASPHALT & CONCRETE MILLINGS, FLOATING DEBRIS, PAPER, PLASTIC, FABRIC, CONSTRUCTION & DEMOLITION DEBRIS & OTHER WASTES MUST BE DISPOSED OF PROPERLY & MUST COMPLY WITH MPCA DISPOSAL REQUIREMENTS.
15. HAZARDOUS MATERIALS: OIL, GASOLINE, PAINT & ANY HAZARDOUS SUBSTANCES MUST BE PROPERLY STORED, INCLUDING SECONDARY CONTAINMENT, TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS TO STORAGE AREAS MUST BE PROVIDED TO PREVENT VANDALISM. STORAGE & DISPOSAL OF HAZARDOUS WASTE MUST BE IN COMPLIANCE WITH MPCA REGULATIONS.
16. ALL STORM WATER POLLUTION PREVENTION MEASURES PRESENTED ON THIS PLAN, AND IN THE SWPPP, SHALL BE INITIATED AS SOON AS PRACTICABLE AND PRIOR TO SOIL DISTURBING ACTIVITIES UPSLOPE.
17. DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS STOPPED SHALL BE TEMPORARILY SEED, WITHIN 14 DAYS OF INACTIVITY. SEEDING SHALL BE IN ACCORDANCE WITH MN/DOT SEED MIXTURE NUMBER 21-111 OR 21-112 DEPENDING ON THE SEASON OF PLANTING (SEE MN/DOT SPECIFICATION SECTION 2575.3) SEEDING METHOD AND APPLICATION RATE SHALL CONFORM TO MN/DOT SPECIFICATION SECTION 2575.3. TEMPORARY MULCH SHALL BE APPLIED IN ACCORDANCE WITH MN/DOT SPECIFICATION SECTION 2575.3F1 AND 2575.3G. ALTERNATIVELY, HYDRAULIC SOIL STABILIZER IN ACCORDANCE WITH MN/DOT SPECIFICATION SECTION 2575.3H MAY BE USED IN PLACE OF TEMPORARY MULCH.
18. DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS PERMANENTLY STOPPED SHALL BE PERMANENTLY STABILIZED. THESE AREAS SHALL BE STABILIZED IN ACCORDANCE WITH THE TIME TABLE DESCRIBED ABOVE. REFER TO THE GRADING PLAN AND/OR LANDSCAPE PLAN FOR VEGETATIVE COVER. (NOTE TO THE PREPARER: WHERE PERMANENT SEEDING IS NOT CALLED OUT IN THE GRADING AND/OR LANDSCAPE PLAN, REPLACE THE LAST SENTENCE IN THIS ITEM WITH THE FOLLOWING: SEED WET PONDS WITH MN/DOT SEED MIXTURE 310 "NATIVE WET TALL" BELOW THE HWL. SEED ALL OTHER AREAS WITH SEED MIXTURE 260 "COMMERCIAL TURF". SEEDING METHOD AND APPLICATION RATE SHALL CONFORM TO MN/DOT SPECIFICATION SECTION 2575.3.)
19. CONTRACTORS OR SUBCONTRACTORS WILL BE RESPONSIBLE FOR REMOVING SEDIMENT FROM CONVEYANCES & FROM TEMPORARY SEDIMENTATION BASINS THAT ARE TO BE USED AS PERMANENT WATER QUALITY MANAGEMENT BASINS. SEDIMENT MUST BE STABILIZED TO PREVENT IT FROM BEING WASHED BACK INTO THE BASIN, CONVEYANCES, OR DRAINAGEWAYS DISCHARGING OFF-SITE OR TO SURFACE WATERS. THE CLEANOUT OF PERMANENT BASINS MUST BE SUFFICIENT TO RETURN THE BASIN TO DESIGN CAPACITY.
20. ON-SITE & OFF-SITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BMP'S. STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE SITE MAP AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
21. TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS & CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB & GUTTER SYSTEMS OR CONDUITS & DITCHES.
22. SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION.
23. DUE TO THE GRADE CHANGES DURING THE DEVELOPMENT OF THE PROJECT, CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION CONTROL MEASURES (SILT FENCES, CHECK DAMS, INLET PROTECTION DEVICES, ETC.) TO PREVENT EROSION.
24. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY. THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.

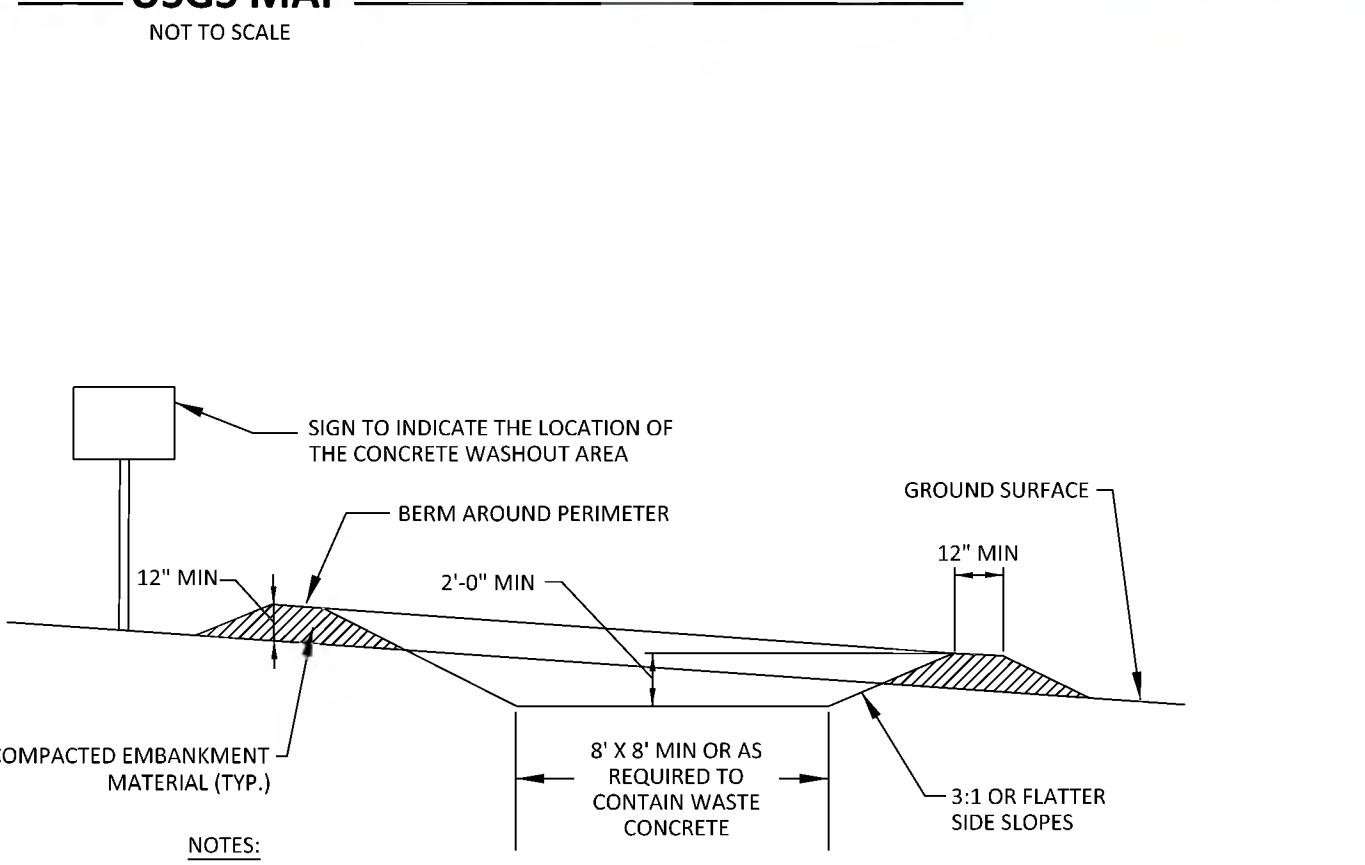
MAINTENANCE NOTES

- ALL MEASURES STATED ON THIS EROSION AND SEDIMENT CONTROL PLAN, AND IN THE STORM WATER POLLUTION PREVENTION PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. THE DESIGNATED CONTACT PERSON NOTED ON THIS PLAN MUST ROUTINELY INSPECT THE CONSTRUCTION ON SITE ONCE EVERY SEVEN DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:
1. ALL SILT FENCES MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/2 OF THE HEIGHT OF THE FENCE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
 2. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND THE SEDIMENT REMOVED WHEN THE DEPTH OF SEDIMENT COLLECTED IN THE BASIN REACHES 1/2 THE STORAGE VOLUME. DRAINAGE AND REMOVAL MUST BE COMPLETED WITHIN 72 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS (SEE PART 10.1-10.5 OF THE GENERAL PERMIT).
 3. SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS, MUST BE INSPECTED FOR EVIDENCE OF SEDIMENT BEING DEPOSITED BY EROSION. CONTRACTOR MUST REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS, AND REESTABLISH THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED SOIL. THE REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS. CONTRACTOR SHALL USE ALL REASONABLE EFFORTS TO OBTAIN ACCESS. IF PRECLUDED, REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) CALENDAR DAYS OF OBTAINING ACCESS. CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL LOCAL, REGIONAL, STATE AND FEDERAL AGENCIES AND RECEIVING ANY APPLICABLE PERMITS, PRIOR TO CONDUCTING ANY WORK.
 4. CONSTRUCTION SITE VEHICLE EXIT LOCATIONS MUST BE INSPECTED FOR EVIDENCE OF OFF-SITE SEDIMENT TRACKING ONTO PAVED SURFACES. TRACKED SEDIMENT MUST BE REMOVED FROM ALL OFF-SITE PAVED SURFACES, WITHIN 24 HOURS OF DISCOVERY, OR IF APPLICABLE, WITHIN A SHORTER TIME TO COMPLY WITH PART 9.11-9.12 OF THE GENERAL PERMIT.
 5. CONTRACTOR IS RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF TEMPORARY AND PERMANENT WATER QUALITY MANAGEMENT BMP'S, AS WELL AS ALL EROSION PREVENTION AND SEDIMENT CONTROL BMP'S, FOR THE DURATION OF THE CONSTRUCTION WORK AT THE SITE. THE PERMITTEE(S) ARE RESPONSIBLE UNTIL ANOTHER PERMITTEE HAS ASSUMED CONTROL (ACCORDING TO PART 3.1 TO 3.8 OF THE MPCA GENERAL PERMIT) OVER ALL AREAS OF THE SITE THAT HAVE NOT BEEN FINALLY STABILIZED OR THE SITE HAS UNDERGONE FINAL STABILIZATION, AND A (N.O.T.) HAS BEEN SUBMITTED TO THE MPCA.
 6. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT MUST BE REMOVED IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS (E.G., FUGITIVE SEDIMENT IN STREETS COULD BE WASHED INTO STORM SEWERS BY THE NEXT RAIN AND/OR POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS).
 7. ALL INFILTRATION AREAS MUST BE INSPECTED TO ENSURE THAT NO SEDIMENT FROM ONGOING CONSTRUCTION ACTIVITIES IS REACHING THE INFILTRATION AREA AND THESE AREAS ARE PROTECTED FROM COMPACTION DUE TO CONSTRUCTION EQUIPMENT DRIVING ACROSS THE INFILTRATION AREA.



TEMPORARY STONE CONSTRUCTION EXIT

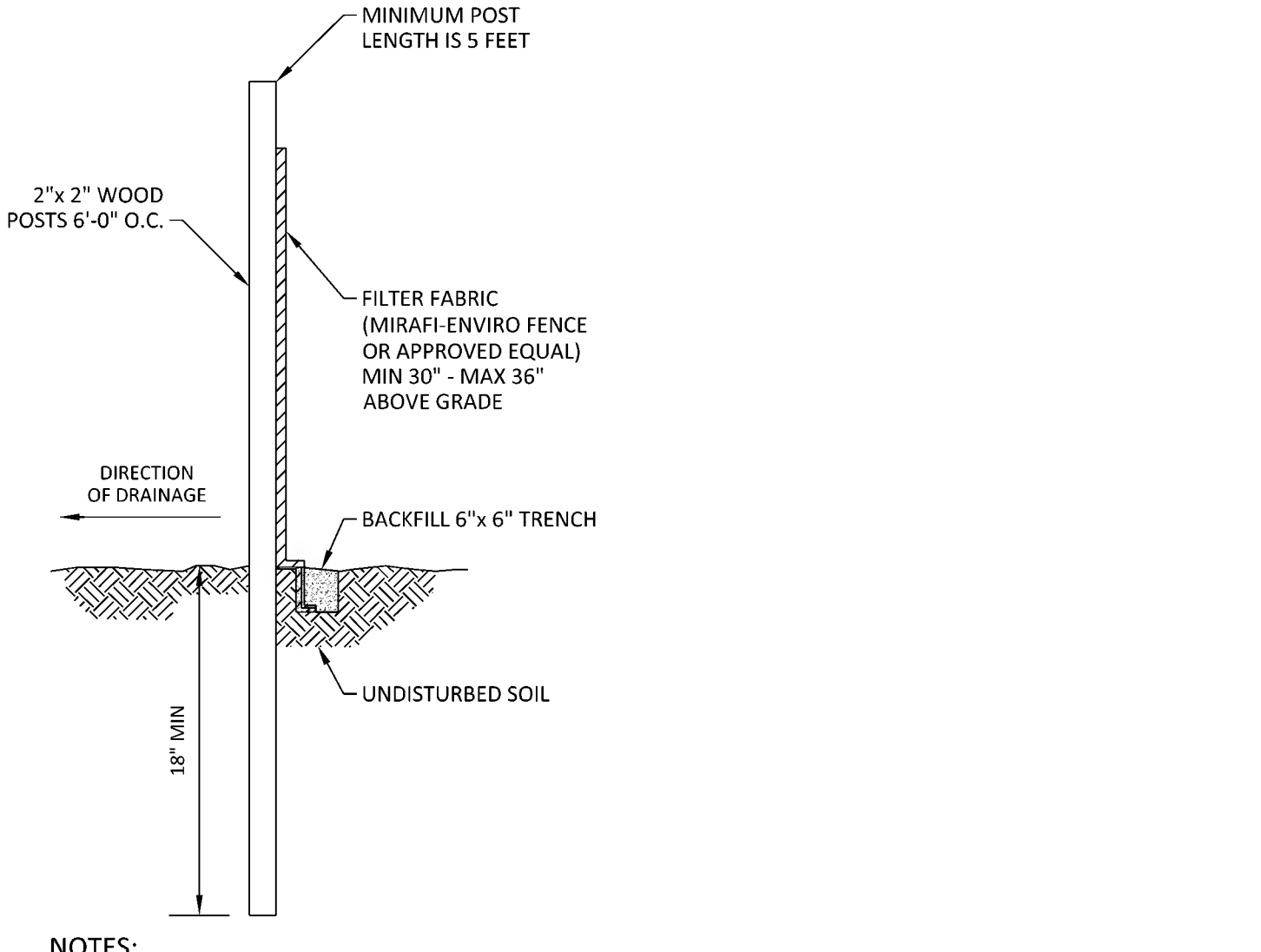
N.T.S.



- NOTES:
1. CONCRETE WASHOUT AREA SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
 2. CONCRETE WASHOUT AREA SHALL BE LINED WITH MINIMUM 10 MIL THICK PLASTIC LINER.
 3. VEHICLE TRACKING CONTROL IS REQUIRED IF ACCESS TO CONCRETE WASHOUT AREA IS OFF PAVEMENT.
 4. SIGNS SHALL BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE WASHOUT AREA, AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CONCRETE WASHOUT AREA TO OPERATORS OF CONCRETE TRUCKS AND PUMP TRUCKS. THE CONCRETE WASHOUT AREA SHALL BE REPAIRED AND ENLARGED OR CLEARED OUT AS NECESSARY TO MAINTAIN CAPACITY FOR WASTE CONCRETE.
 5. AT THE END OF CONSTRUCTION, ALL CONCRETE SHALL BE REMOVED FROM THE SITE AND DISPOSED OF AT AN ACCEPTED WASTE SITE.
 6. WHEN THE CONCRETE WASHOUT AREA IS REMOVED, THE DISTURBED AREA SHALL BE SEED AND MULCHED OR OTHERWISE STABILIZED IN A MANNER ACCEPTED BY THE CITY.

CONCRETE WASHOUT AREA

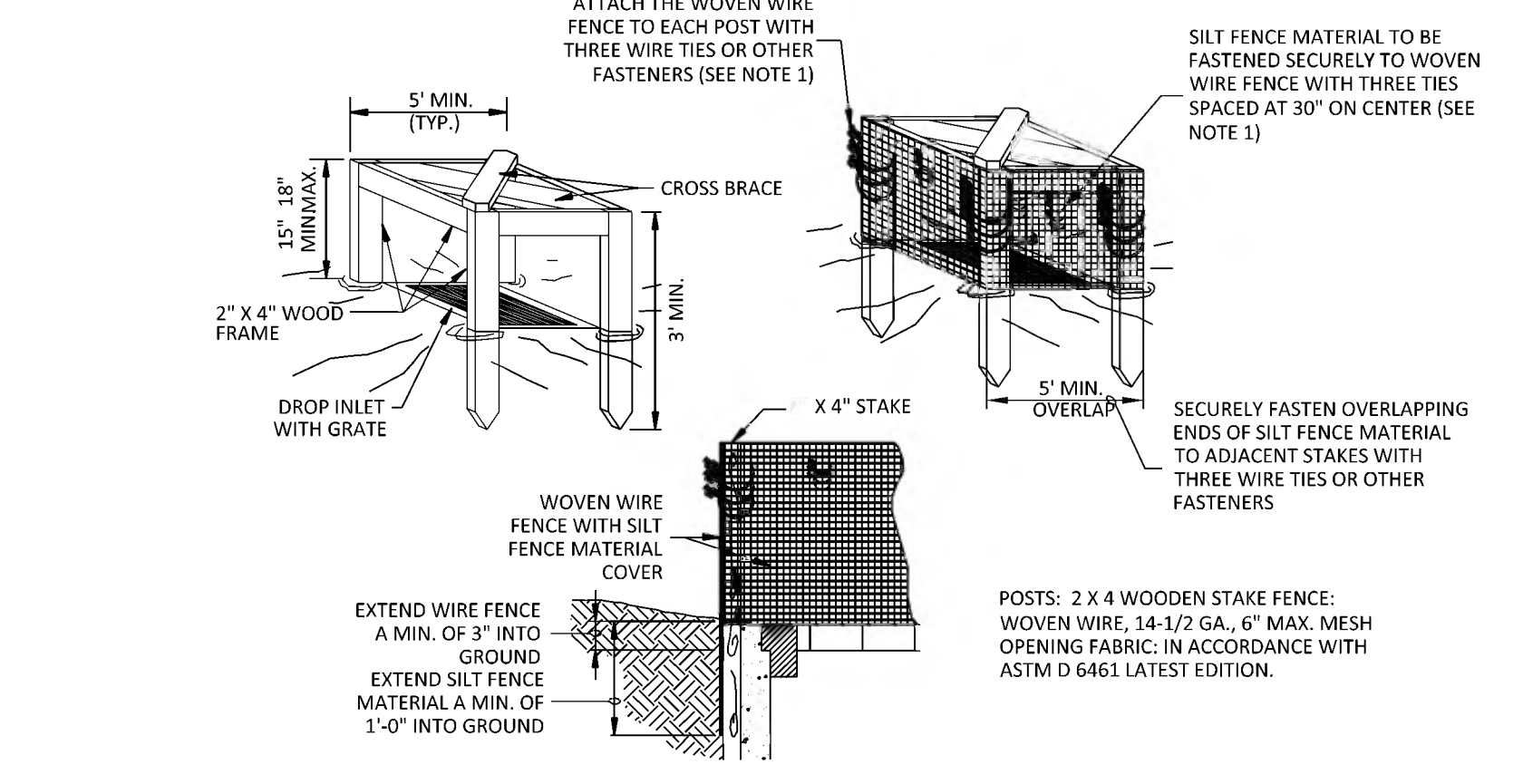
NOT TO SCALE



- NOTES:
1. MAXIMUM SPACING BETWEEN POSTS (CENTER TO CENTER) SHALL NOT EXCEED 6 FEET IN SPACING.
 2. A MINIMUM OF 5 FASTENERS PER POST

PREASSEMBLED SILT FENCE WOOD POSTS (MN/DOT 3886)

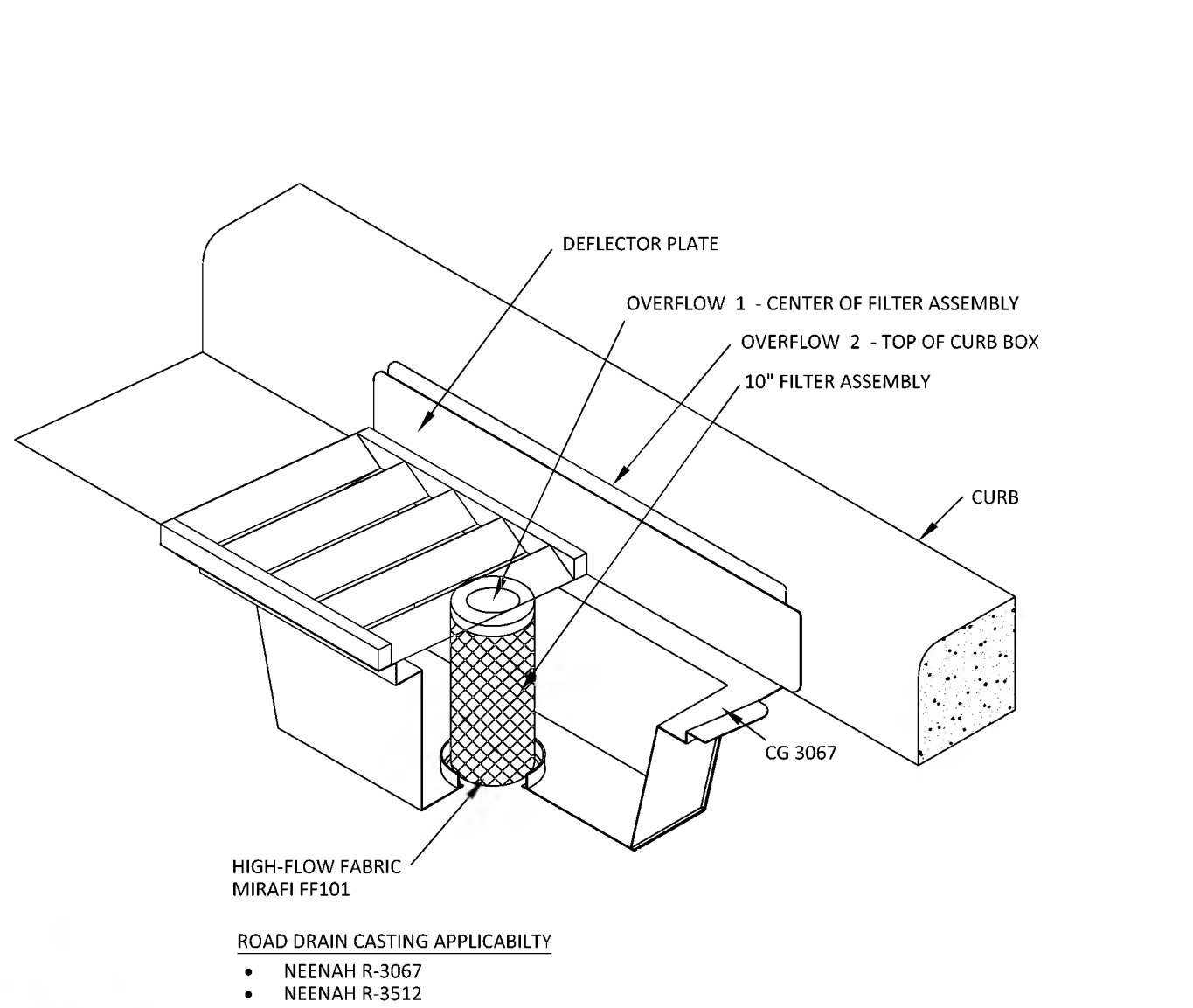
N.T.S.



1. ATTACH THE WOVEN WIRE FENCE TO EACH POST AND THE GEOTEXTILE TO THE WOVEN WIRE FENCE (SPACED EVERY 30\"/>
- 2. WHEN TWO SECTIONS OF SILT FENCE MATERIAL ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED ACROSS TWO POSTS.
- 3. MAINTENANCE SHALL BE PERFORMED AS NOTED IN THE SWPPP. DEPTH OF ACCUMULATED SEDIMENTS MAY NOT EXCEED ONE-HALF THE HEIGHT OF THE FABRIC. MAINTENANCE CLEANOUT MUST BE CONDUCTED REGULARLY TO PREVENT ACCUMULATED SEDIMENTS FROM REACHING ONE-HALF THE HEIGHT OF THE SILT FENCE MATERIAL ABOVE GRADE.
- 4. ALL SILT FENCE INLETS SHALL INCLUDE WIRE SUPPORT.

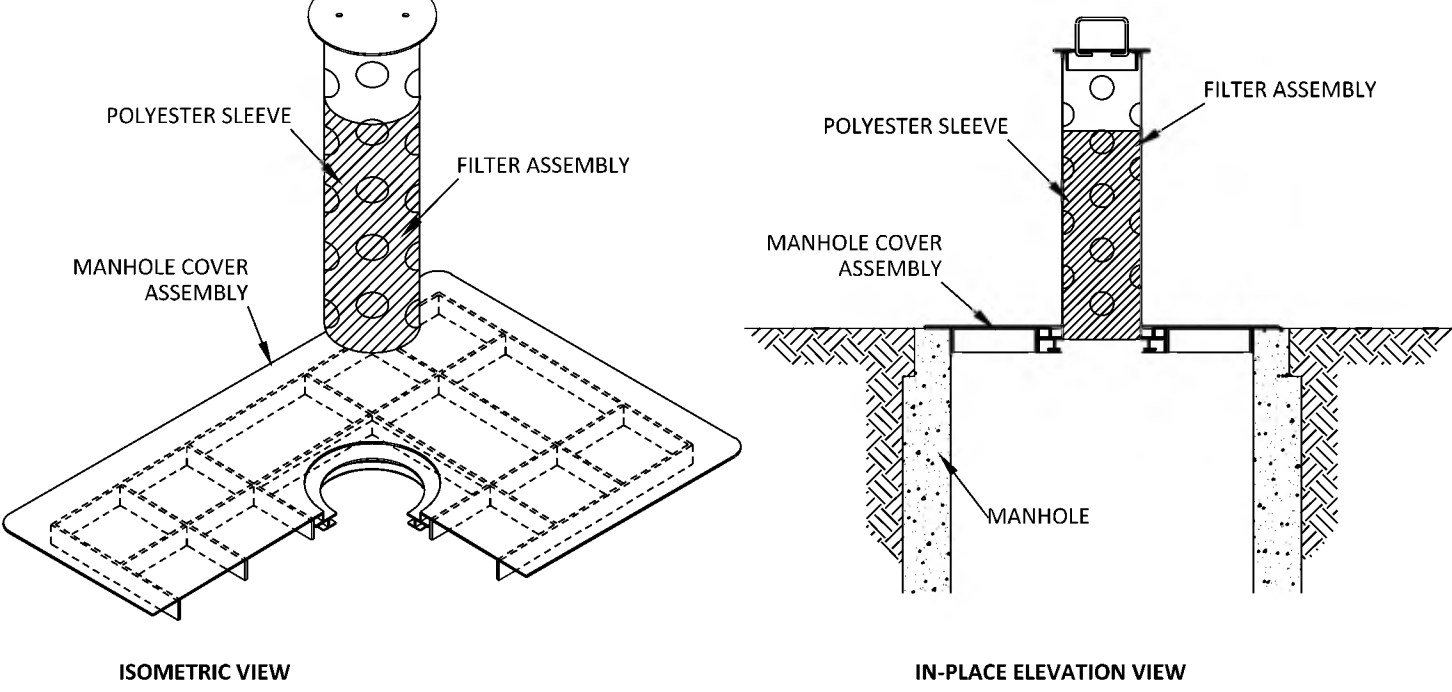
SILT FENCE INLET PROTECTION (IP-1)

NOT TO SCALE



ROAD DRAIN INLET PROTECTION (IP-2)

NOT TO SCALE



SPECIFICATIONS AND STANDARDS		AVAILABILITY STOCK
• AISC MANUAL OF STEEL CONSTRUCTION, 9TH EDITION		
• AWS STRUCTURAL WELDING CODE-STEEL, D1.1-94		
• 29 CFR 1926.558A SAFETY AND HEALTH STANDARDS		
DESIGN LOADS		
ALLOWABLE AXLE WEIGHT LOAD	20000 LBS.	
SAFETY FACTOR	2:1	
WATER FLOW RATE (THROUGH POLYESTER FILTER)	0.476 CFS @ 3\"/>	
	1.074 CFS @ 15\"/>	
	1.133 CFS @ 6\"/>	
MAXIMUM OVERFLOW RATE	1.575 CFS @ 12\"/>	

ROAD DRAIN INLET PROTECTION TOP SLAB MODEL (IP-3)

NOT TO SCALE

AREA SUMMARY IN ACRES

PAVEMENT AREA	0.88 AC±
BUILDING AREA	1.75 AC±
SEEDER AREA	0.85 AC±
TOTAL DISTURBED	3.47 AC±
PRE - CONSTRUCTION IMPERVIOUS	2.21 AC±
POST - CONSTRUCTION IMPERVIOUS	2.63 AC±

DEVELOPER/OWNER: GREYSTAR DEVELOPMENT CENTRAL, LLC 750 BERING DRIVE, SUITE 400 HOUSTON, TX 77057 832-269-0535	
SITE OPERATOR / GENERAL CONTRACTOR	
SUPERINTENDENT:	



Engineering | Surveying | Planning | Environmental

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the laws of the state of Minnesota.

Brian W. Frank
Registration No. 52728 Date: MM/DD/YYYY
If applicable, contact us for a wet signed copy of this plan which is available upon request at Sambatek's, Minnetonka, MN office.

NOT FOR CONSTRUCTION

LAND USE APPLICATION SUBMITTAL 12/06/2022

ORIGINAL ISSUE: 09/19/22

REVISIONS: No. Description Date

51166 PROJECT NUMBER

TLL DRAWN BY BWF CHECKED BY

KEY PLAN

BREN ROAD MULTIFAMILY
EROSION CONTROL NOTES & DETAILS

C5.03

**NOT FOR
CONSTRUCTION**

LAND USE
APPLICATION
SUBMITTAL
12/06/2022

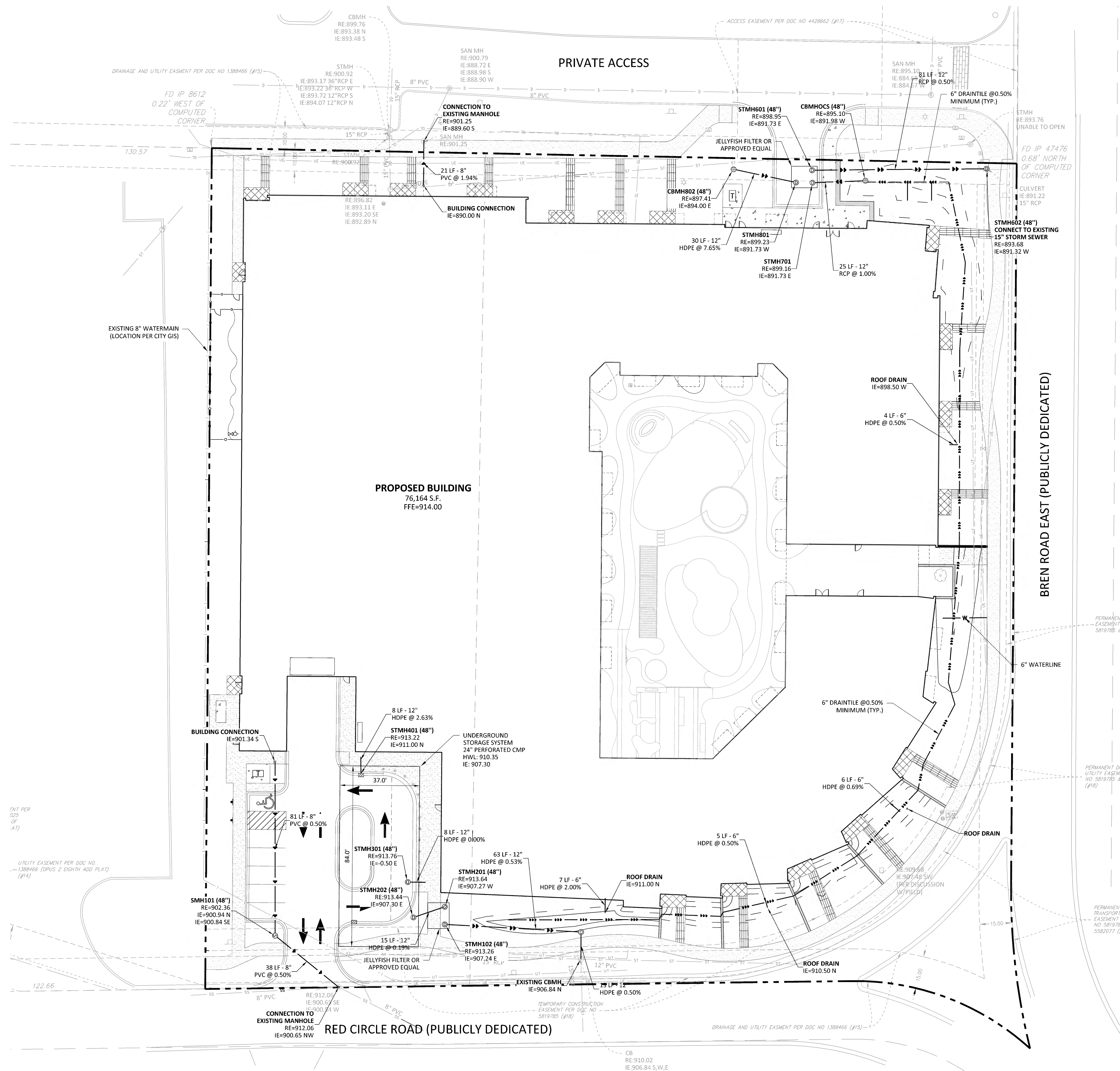
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TLL BWF
DRAWN BY CHECKED BY
KEY PLAN

BREN ROAD
MULTIFAMILY

UTILITY PLAN

C6.01



LEGEND

PROPOSED	EXISTING	
(Symbol)	(Symbol)	SANITARY SEWER
(Symbol)	(Symbol)	FORCEMAIN (SAN.)
(Symbol)	(Symbol)	STORM SEWER
(Symbol)	(Symbol)	DRAIN TILE
(Symbol)	(Symbol)	WATERMAIN
(Symbol)	(Symbol)	UNDERGROUND GAS LINE
(Symbol)	(Symbol)	UNDERGROUND TELEPHONE
(Symbol)	(Symbol)	UNDERGROUND ELECTRIC
(Symbol)	(Symbol)	CONCRETE CURB
(Symbol)	(Symbol)	EASEMENT LINE

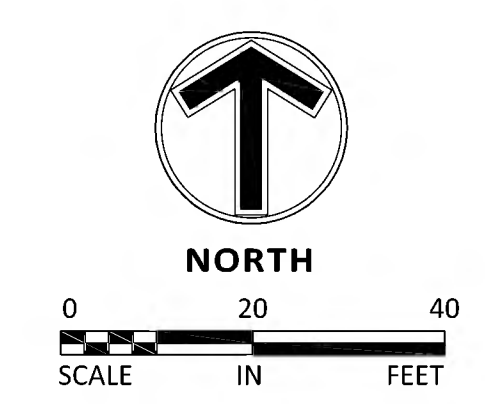
UTILITY CONSTRUCTION NOTES

- UTILITY CONSTRUCTION NOTES HOLDER

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/CI 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA), THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

IF THE CONTRACTOR ENCOUNTERS ANY DRAIN TILE WITHIN THE SITE, HE OR SHE SHALL NOTIFY THE ENGINEER WITH THE LOCATION, SIZE, INVERT AND IF THE TILE LINE IS ACTIVE. NO DRAIN TILE SHALL BE BACKFILLED WITHOUT APPROVAL FROM THE PROJECT ENGINEER.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.



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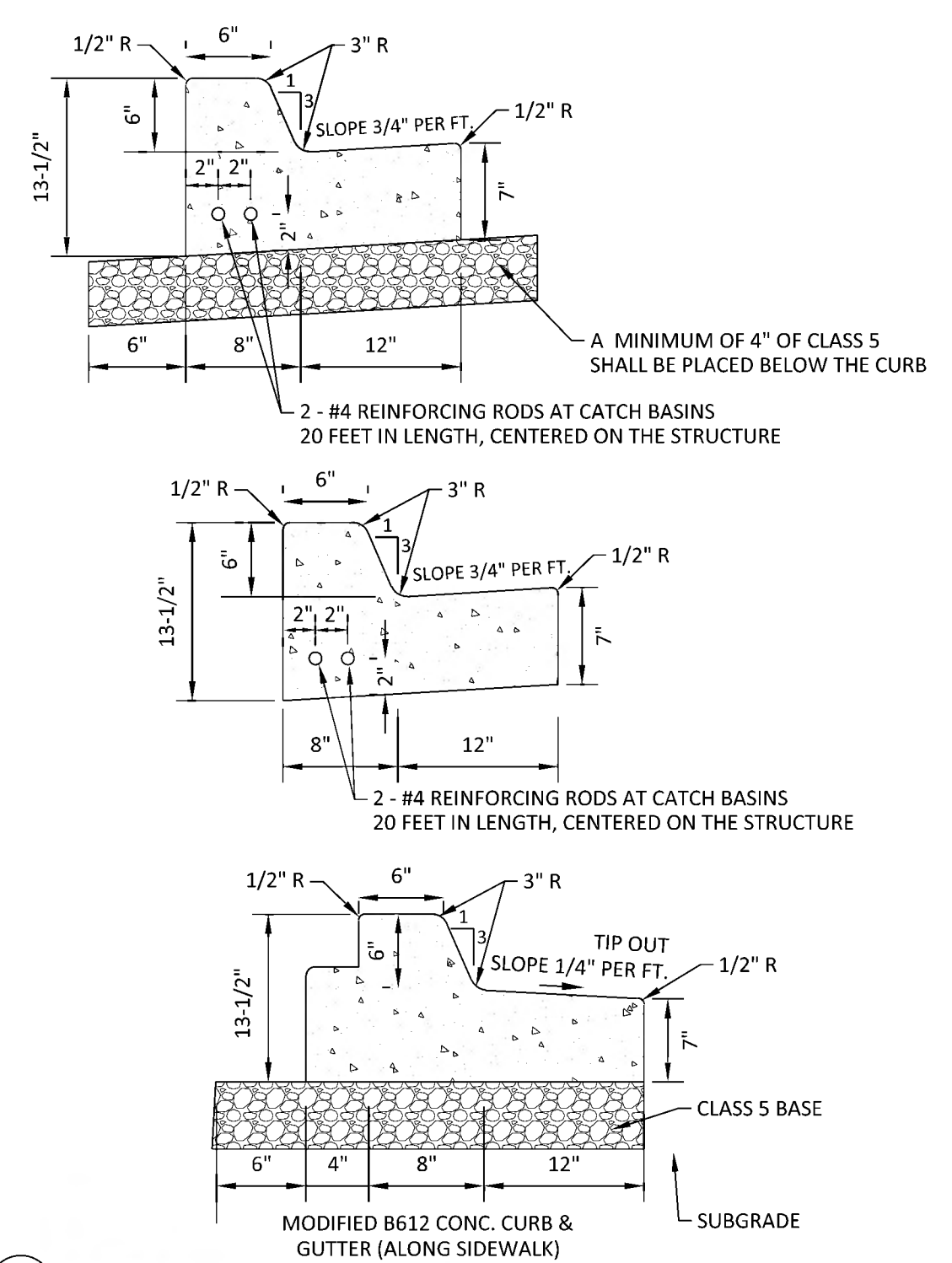
TLL BWF
DRAWN BY CHECKED BY

KEY PLAN

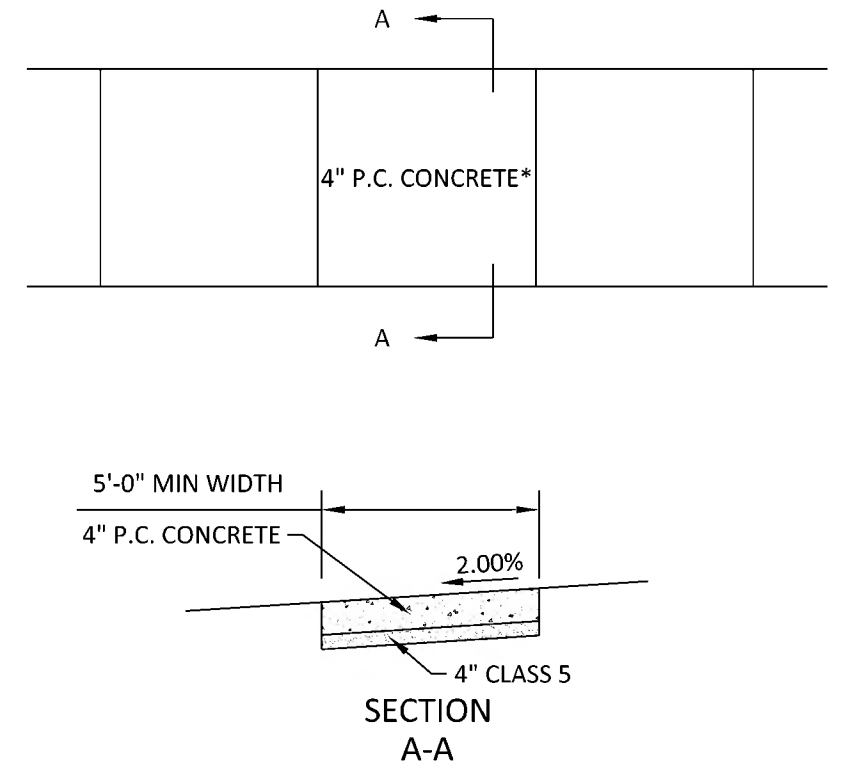
BREN ROAD
MULTIFAMILY

DETAILS

C9.01



01 B612 CONCRETE CURB & GUTTER
N.T.S.

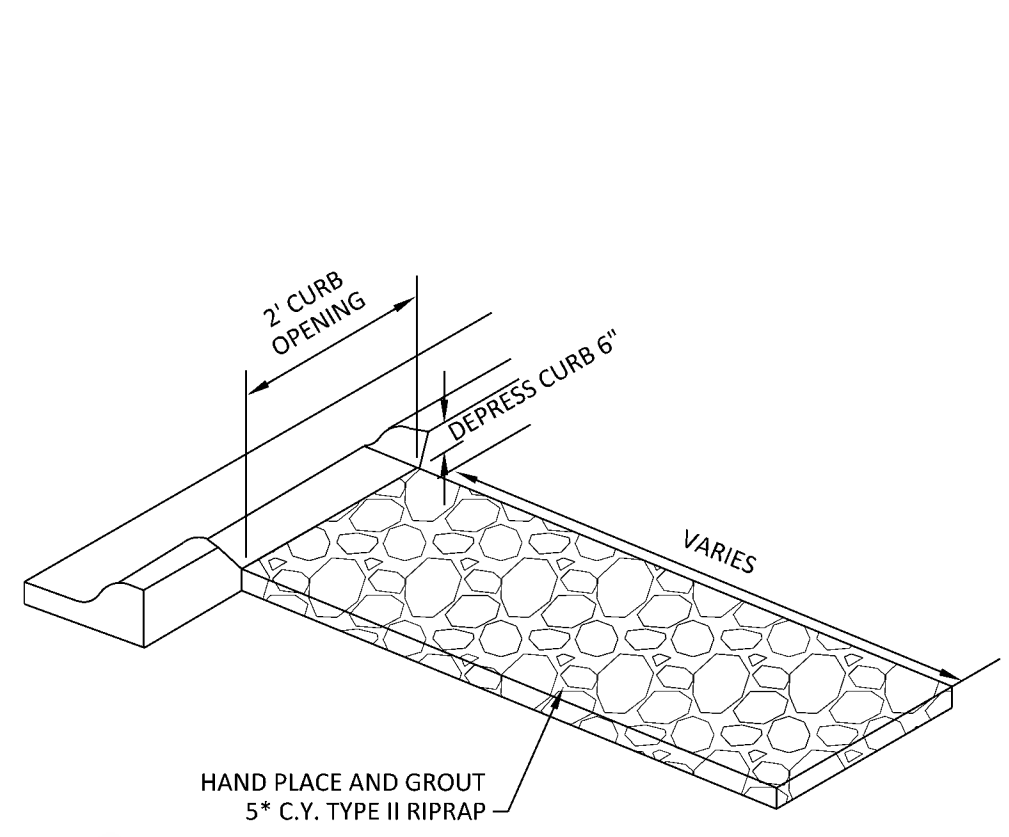


GENERAL NOTES FOR CONCRETE CONSTRUCTION:

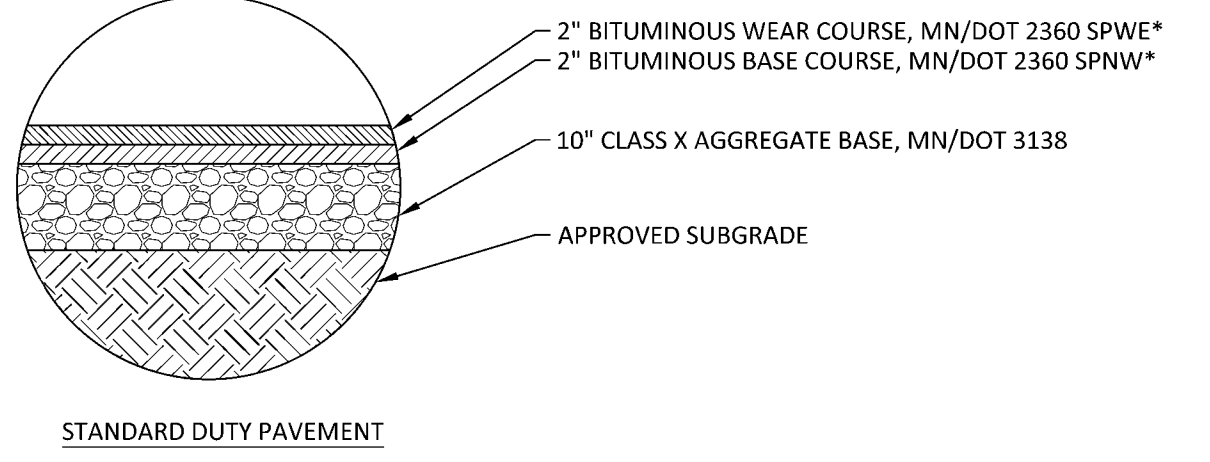
1. THE CONCRETE SIDEWALKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF MN/DOT "STANDARD SPECIFICATIONS FOR CONSTRUCTION".
2. SIDEWALK CONTROL JOINTS SHALL BE LOCATED AT 5 FEET ON CENTER. SIDEWALK EXPANSION JOINTS SHALL BE LOCATED AT A MAXIMUM OF 100 FEET ON CENTER, AND WHERE SIDEWALK ABUTS CURB, STRUCTURES, AND OTHER FIXED OBJECTS, SEAL ALL EXPANSION JOINTS. BROOM FINISH SHALL BE PERPENDICULAR TO THE LINE OF TRAFFIC.

*THE THICKNESS SHALL BE INCREASED TO A MINIMUM DEPTH OF INCHES AT DRIVEWAY CROSSINGS.

02 CONCRETE SIDEWALK
N.T.S.



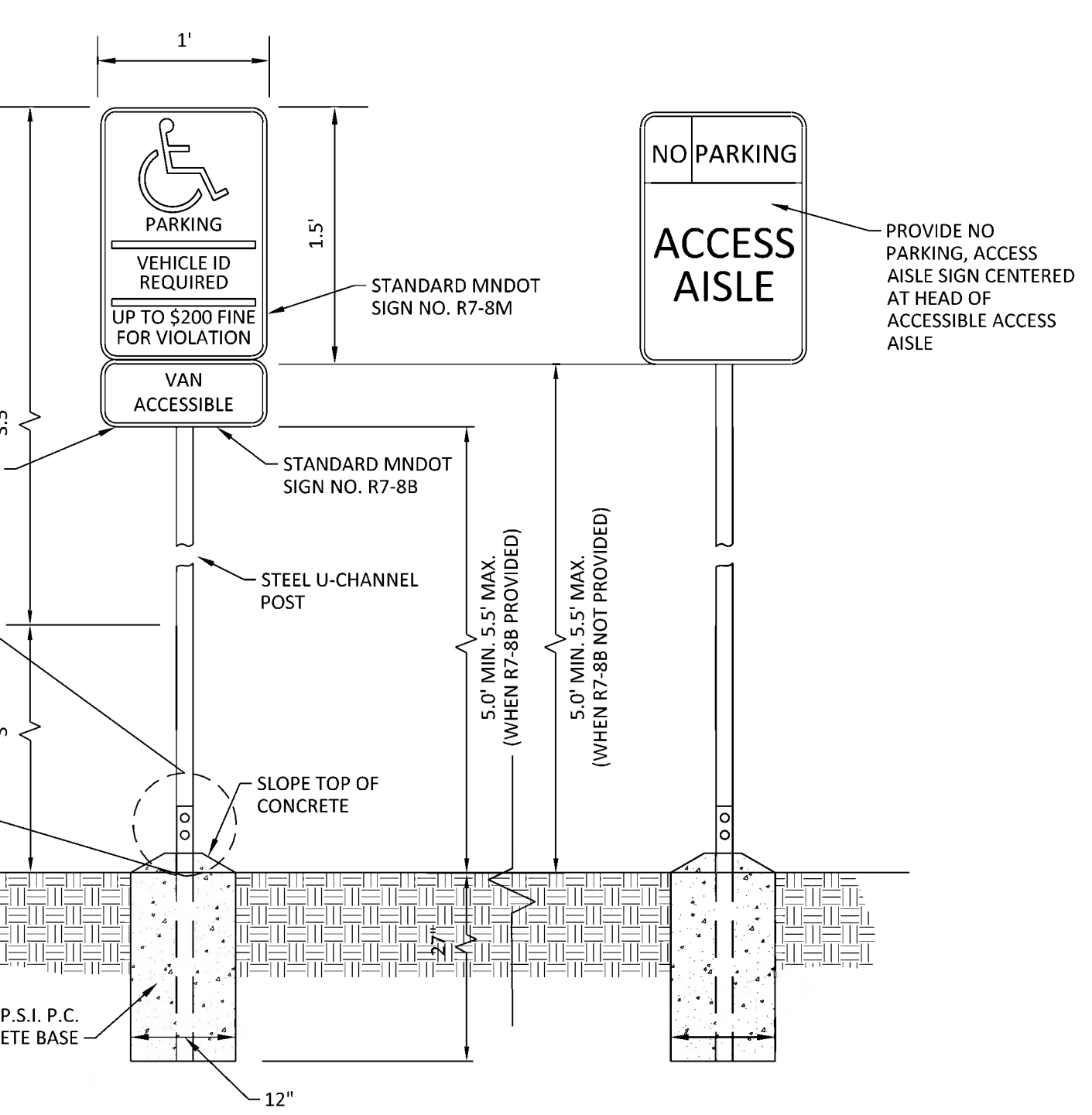
03 CURB CUT WITH RIPRAP
N.T.S.



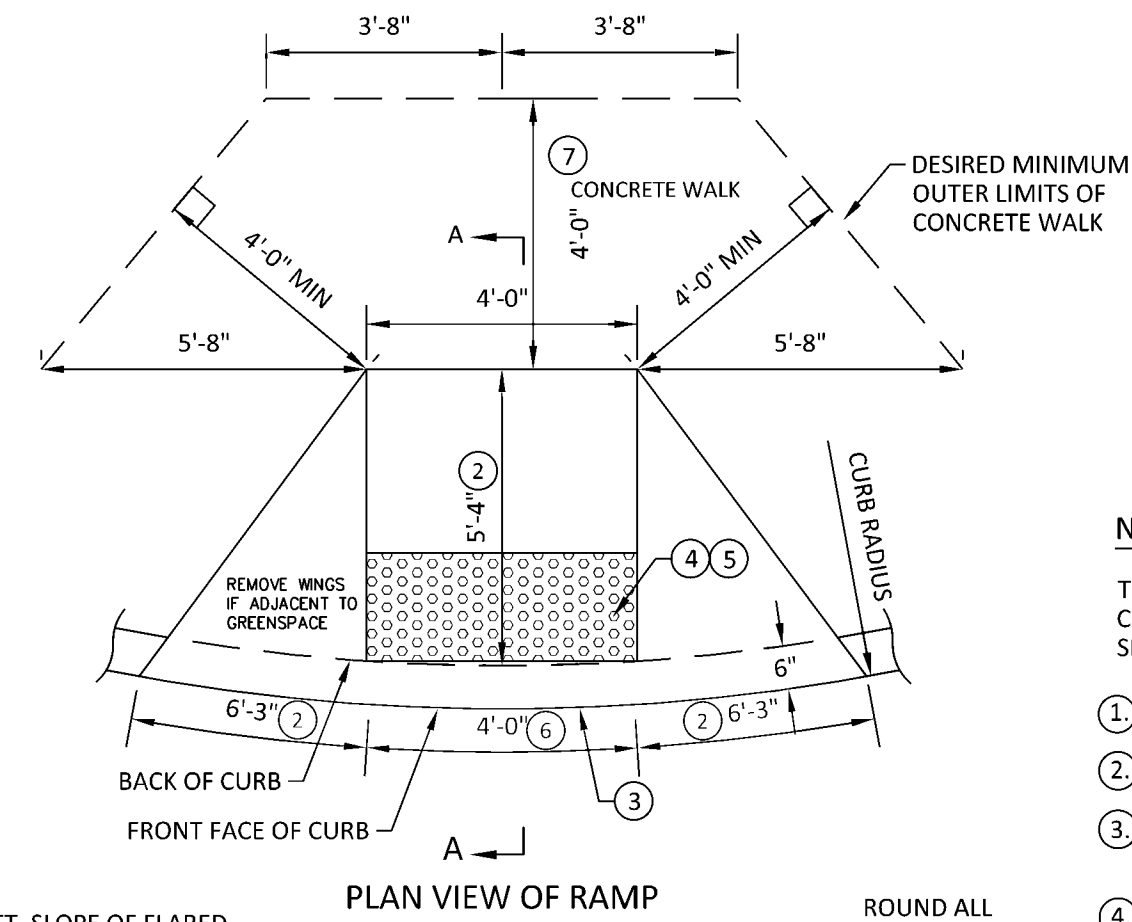
STANDARD DUTY PAVEMENT

* REFER TO BITUMINOUS MIX DESIGN REQUIREMENT PROVIDED BY THE GEOTECHNICAL CONSULTANT

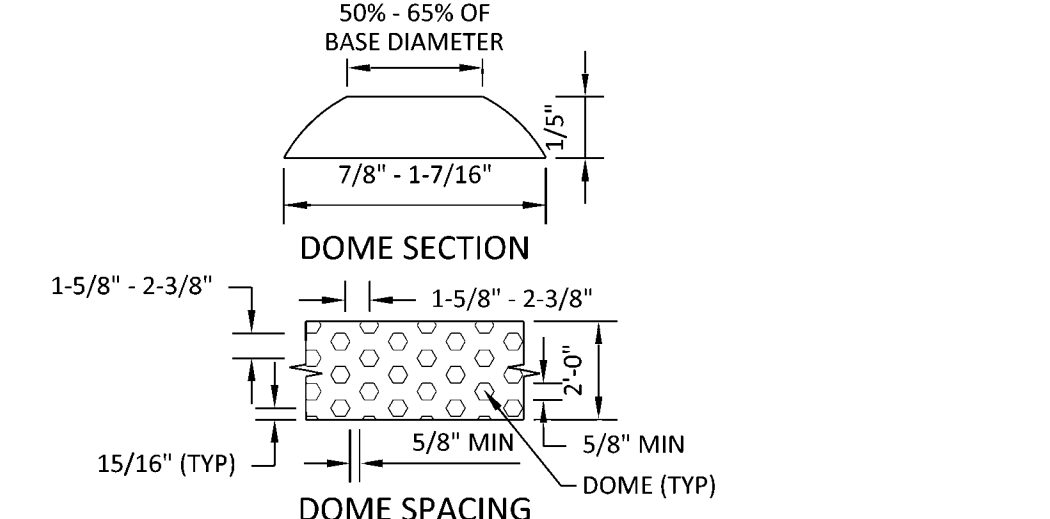
04 PAVEMENT SECTIONS
N.T.S.



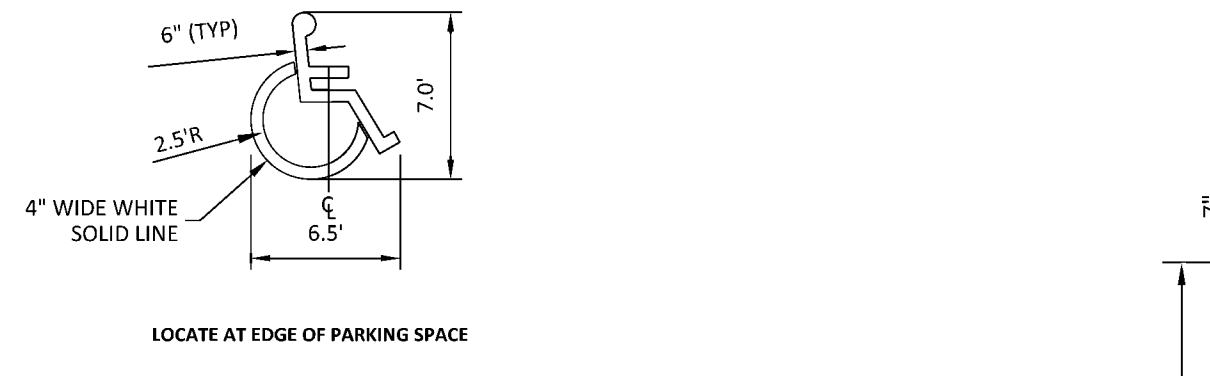
05 BREAKAWAY ACCESSIBLE PARKING SIGN
N.T.S.



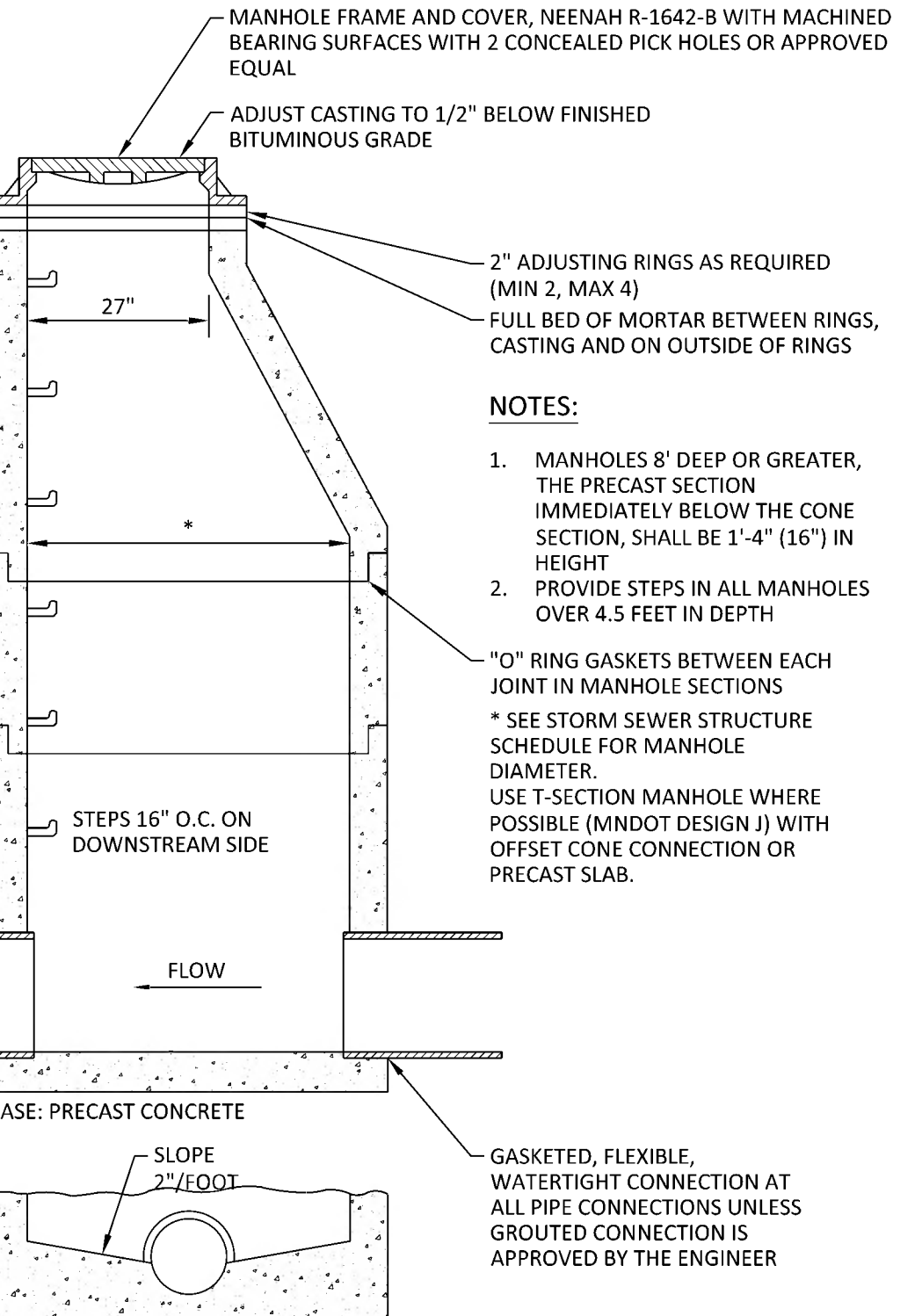
06 PEDESTRIAN CURB RAMP
N.T.S.



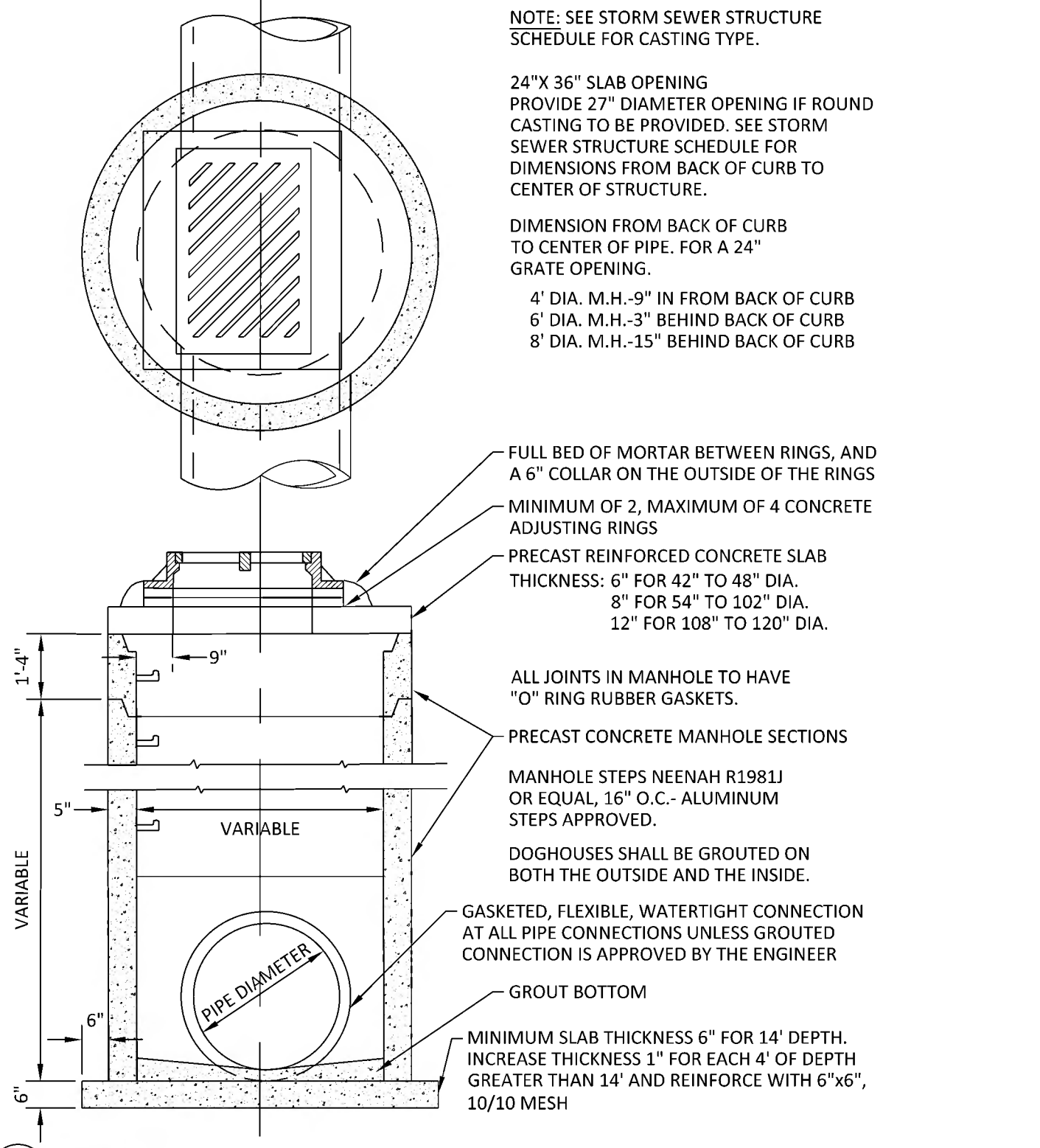
07 ACCESSIBLE PARKING SYMBOL
N.T.S.



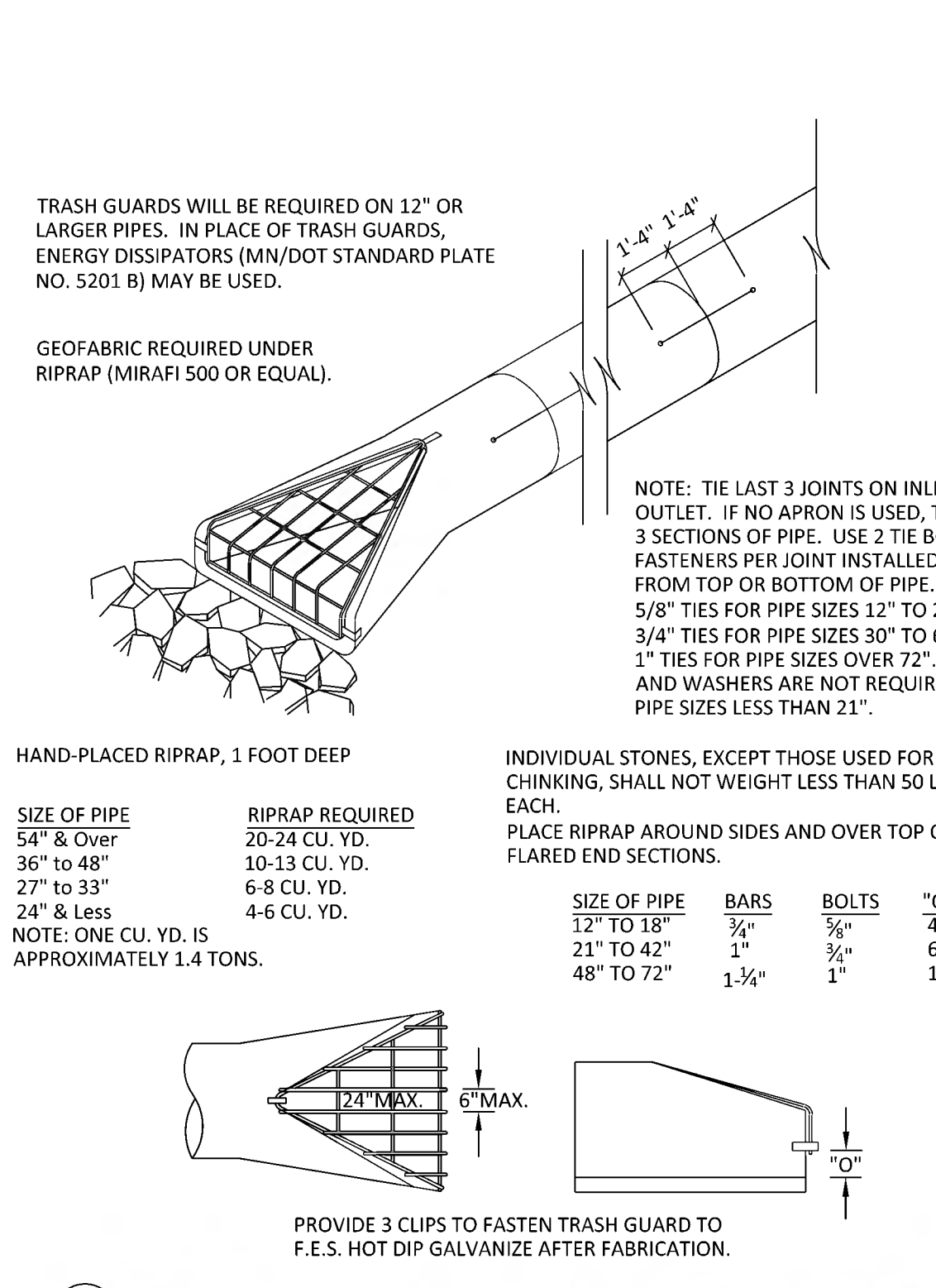
08 ADA/STANDARD STRIPING
N.T.S.



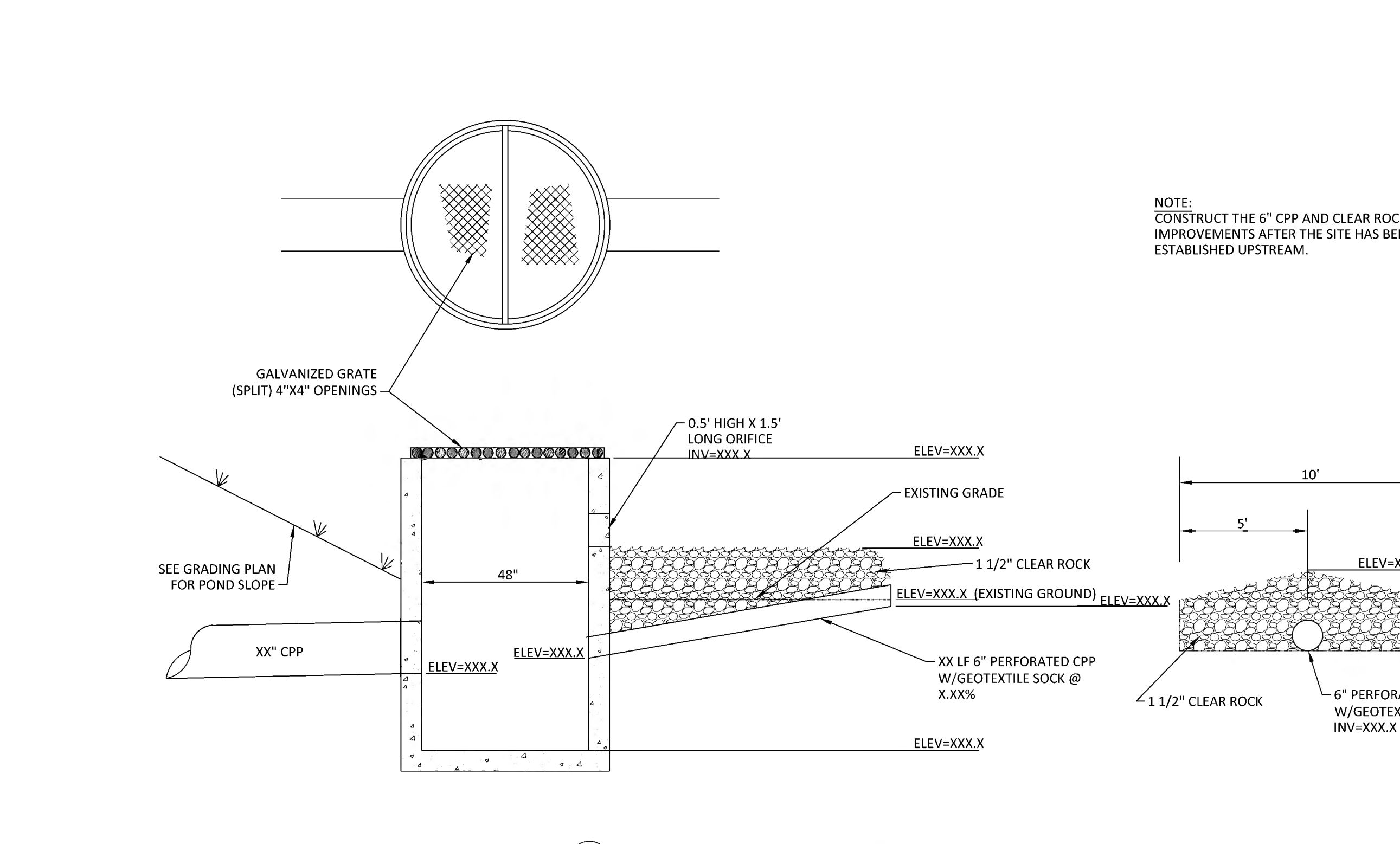
09 STORM SEWER MANHOLE
N.T.S.



10 CATCH BASIN MANHOLE
N.T.S.



11 FLARED END SECTION
N.T.S.



12 OUTLET CONTROL STRUCTURE
N.T.S.

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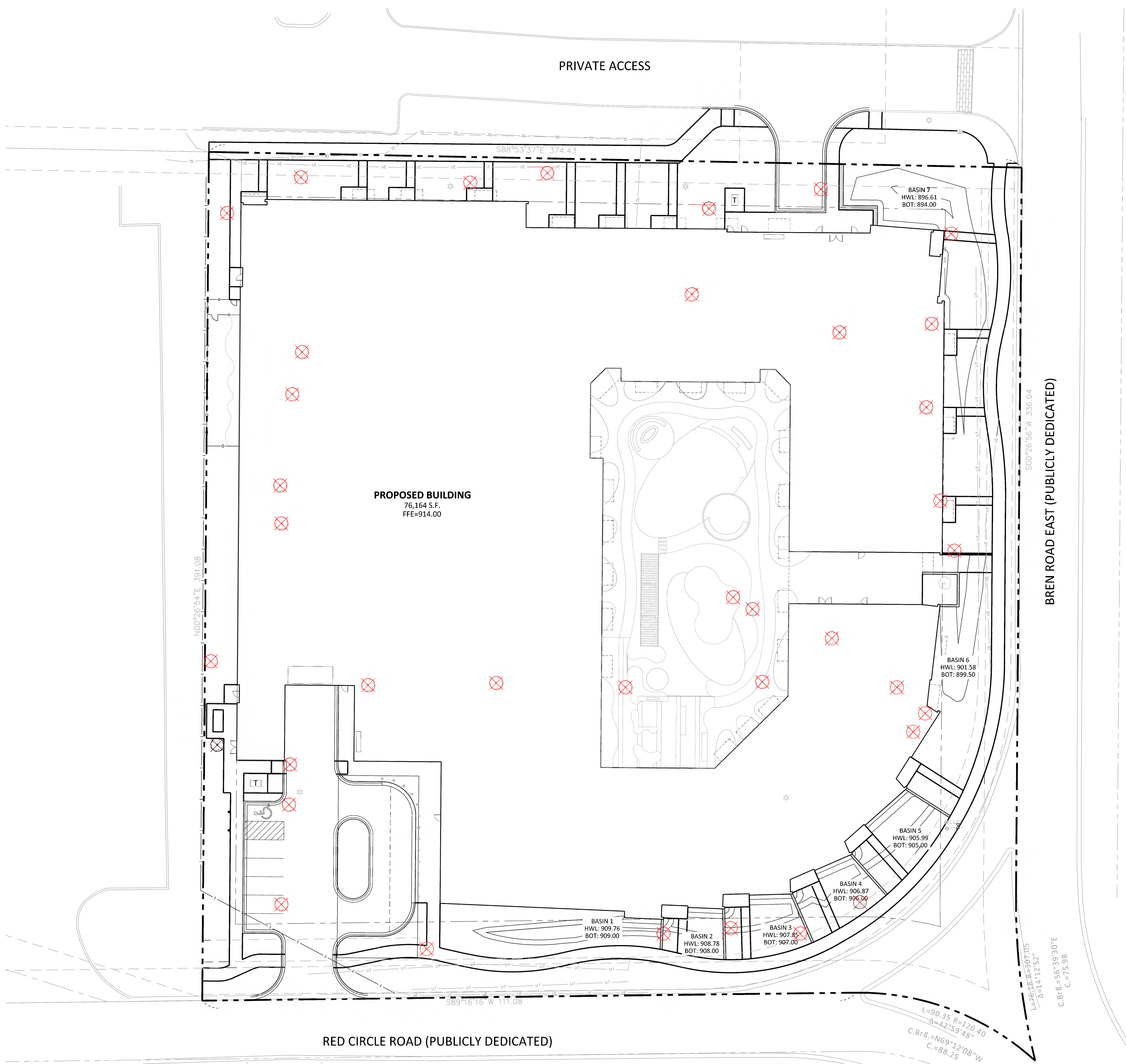
TLL BWF
DRAWN BY CHECKED BY

KEY PLAN

BREN ROAD
MULTIFAMILY

TREE INVENTORY

L1.01



LEGEND

PROPOSED	EXISTING	STANDARD DUTY ASPHALT PAVING
PROPERTY LIMIT	PROPERTY LIMIT	CONCRETE PAVING
CURB & GUTTER	CURB & GUTTER	CONCRETE SIDEWALK
EASEMENT	EASEMENT	
BUILDING	BUILDING	
RETAINING WALL	RETAINING WALL	
WETLAND LIMITS	WETLAND LIMITS	
TREELINE	TREELINE	
LANDSCAPE EDGING	LANDSCAPE EDGING	
STORM SEWER	STORM SEWER	
SANITARY SEWER	SANITARY SEWER	
FORCEMAIN (SAN.)	FORCEMAIN (SAN.)	
WATERDRAIN	WATERDRAIN	
YARD DRAIN	YARD DRAIN	
LIMITS OF DISTURBANCE	LIMITS OF DISTURBANCE	
TREE PROTECTION FENCE	TREE PROTECTION FENCE	
TREE TO BE REMOVED	TREE TO BE REMOVED	
SIGN	SIGN	
PIPE BOLLARD	PIPE BOLLARD	
RIPRAP	RIPRAP	

MINNETONKA LANDSCAPE CODE

- Development that is subject to landscape requirements in sections 300.27 and 300.31 must meet the minimum landscape requirements of the applicable section. Trees planted as part of a required landscaping plan may be counted as mitigation trees under this section, at the city's discretion.
- One inch for each inch in diameter of a deciduous tree removed and one foot for each foot in height of a coniferous tree removed.

CALCULATIONS

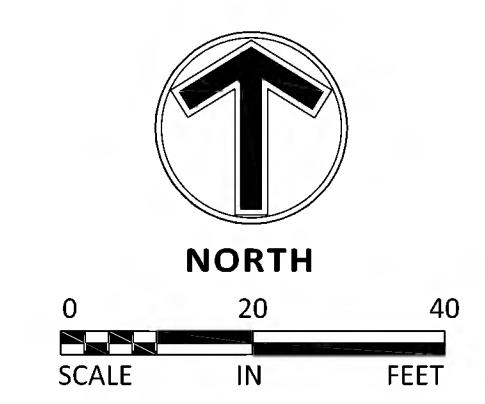
	EXISTING	REQUIRED
2 INCHES PER EVERY SIGNIFICANT TREE	6 TREES	12 INCHES
1 INCH PER EACH INCH IN DIAMETER OF HIGH PRIORITY TREES	83 INCHES	154 INCHES
1 FOOT PER EACH FOOT IN HEIGHT OF HIGH PRIORITY TREES	106 FEET	106 FEET

PLANT SCHEDULE

Tag	DBH	Height	Species	Notes	Type	Status
4090	21		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4091	27		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4092	22		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4093	11		River Birch (Betula nigra)		Deciduous	Remove
4094	17		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4095	8	18	Black Hills Spruce (Picea glauca)		Coniferous	Remove
4096	23		Littleleaf Linden (Tilia cordata)		Deciduous	Remove
4097	20		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4098	23		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4099	23		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
4100	7		Malus sp.		Deciduous	Remove
5001	15		Sugar Maple (Acer saccharum)		Deciduous	Remove
5002	7		Box Elder (Acer negundo)		Deciduous	Remove
5003	18	36	Black Hills Spruce (Picea glauca)		Coniferous	Remove
5004	18	36	Black Hills Spruce (Picea glauca)		Coniferous	Remove
5005	20	40	Black Hills Spruce (Picea glauca)		Coniferous	Remove
5006	23		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5007	9		Mulberry (Morus alba)		Deciduous	Remove
5008	14		Malus sp.		Deciduous	Remove
5009	24		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5010	24		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5011	21		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5012	27		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5013	23		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5014	17		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5015	25		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5017	7		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5018	11		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5019	15	30	Black Hills Spruce (Picea glauca)		Coniferous	Remove
5020	19		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5021	23		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5022	18		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5023	6		Malus sp.		Deciduous	Remove
5024	9		Malus sp.		Deciduous	Remove
5025	20		Littleleaf Linden (Tilia cordata)		Deciduous	Remove
5026	18		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5027	20		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove
5028	20		Green Ash (Fraxinus pennsylvanica)	Shows signs of EAB infestation	Deciduous	Remove

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASEE/38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. BY CONTACTING THE NOTIFICATION CENTER (FOR HER STATE ONE FOR MINNESOTA) THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.



PERSPECTIVE VIEW

BIRDSEYE VIEW OF AUTOCOURT



PERSPECTIVE VIEW

PEDESTRIAN VIEW OF AUTOCOURT



PERSPECTIVE VIEW

VIEW OF RED CIRCLE DRIVE FRONTAGE



PERSPECTIVE VIEW

PEDESTRIAN VIEW AT BREN ROAD NORTHEAST CORNER



PERSPECTIVE VIEW

PEDESTRIAN VIEW AT BREN ROAD UNDERPASS



PERSPECTIVE VIEW

PEDESTRIAN VIEW AT CORNER OF BREN ROAD AND RED CIRCLE DRIVE



PERSPECTIVE VIEW

PEDESTRIAN VIEW AT BREN ROAD LOOKING NORTH



PERSPECTIVE VIEW

VIEW OF NORTH ELEVATION



PERSPECTIVE VIEW

VIEW OF WEST ELEVATION



PERSPECTIVE VIEW

BIRDSEYE VIEW FROM SOUTHEAST



PERSPECTIVE VIEW

BIRDSEYE VIEW FROM NORTHEAST



PERSPECTIVE VIEW

BIRDSEYE VIEW FROM NORTHWEST



PERSPECTIVE VIEW

BIRDSEYE VIEW FROM SOUTHWEST



LANDSCAPE DESIGN VISIONING



LANDSCAPE DESIGN VISIONING



LANDSCAPE DESIGN VISIONING



LANDSCAPE DESIGN VISIONING





10701 BREN ROAD DEVELOPMENT

LAND USE APPLICATION
SUBMISSION DATE: DECEMBER 6, 2022

GREYSTAR

esg
ARCHITECTURE & DESIGN

URBAN ECOSYSTEMS

 **Sambatek**

LANDSCAPE RENDERINGS

PEDESTRIAN VIEW AT CORNER OF BREN ROAD AND RED CIRCLE DRIVE



LANDSCAPE RENDERINGS

PEDESTRIAN VIEW AT CORNER OF BREN ROAD AND RED CIRCLE DRIVE



LANDSCAPE RENDERINGS

PEDESTRIAN VIEW AT RED CIRCLE DRIVE LOOKING EAST



LANDSCAPE RENDERINGS

PEDESTRIAN VIEW AT BREN ROAD LOOKING NORTH



LANDSCAPE RENDERINGS

PEDESTRIAN VIEW AT NORTHEAST CORNER



LANDSCAPE RENDERINGS

STORM WATER CONVEYANCE WEIR DETAIL - DRY CONDITION



LANDSCAPE RENDERINGS

STORM WATER CONVEYANCE WEIR DETAIL - WET CONDITION



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

LANDSCAPE RENDERINGS

WALK UP UNIT BRIDGE CONNECTION - DRY CONDITION



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

LANDSCAPE RENDERINGS

WALK UP UNIT BRIDGE CONNECTION - DRY CONDITION



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

LANDSCAPE PLAN



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS



LANDSCAPE RENDERINGS

BIRDSEYE VIEW OF COURTYARD



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

LANDSCAPE RENDERINGS

PEDESTRIAN VIEW NORTH END OF COURTYARD



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

LANDSCAPE RENDERINGS

VIEW FROM GRILLING STATION



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

BIRDSEYE VIEW OF POOL



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

VIEW OF HOT TUB AND FIRE ELEMENT



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

MAIN OUTDOOR SEATING AREA AND FIRE ELEMENT



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

EVENING VIEW OF NORTH LAWN



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

EVENING VIEW OF MAIN SEATING AREA AND FIRE ELEMENT



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECT

LANDSCAPE RENDERINGS

WINTER VIEW OF NORTH LAWN



URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

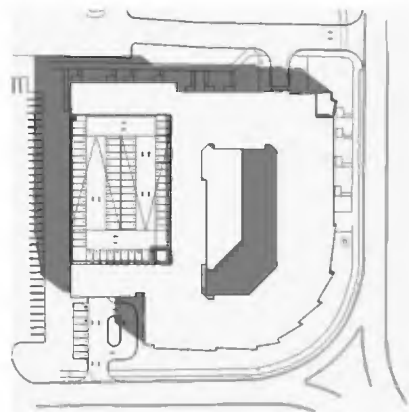
LANDSCAPE RENDERINGS

WINTER EVENING VIEW OF NORTH LAWN

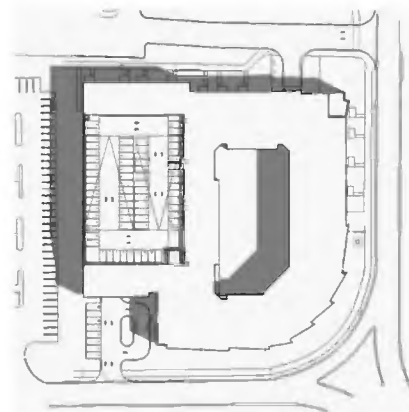


URBAN ECOSYSTEMS
LANDSCAPE ARCHITECTS

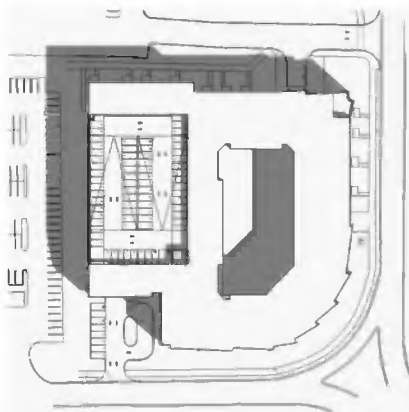
SOLAR STUDY



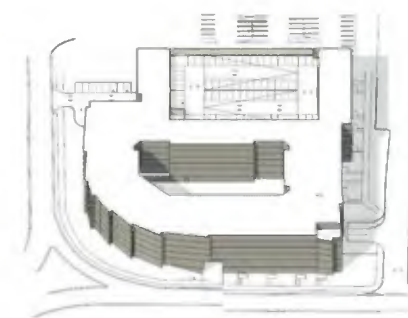
Courtyard Scheme 5/1, 10am



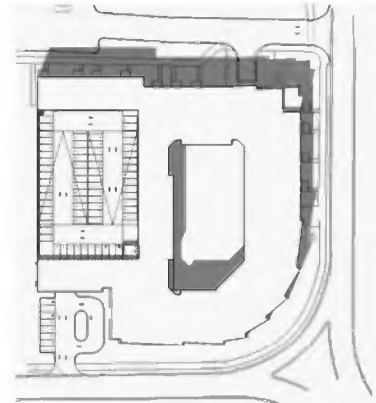
Courtyard Scheme 7/1, 10am



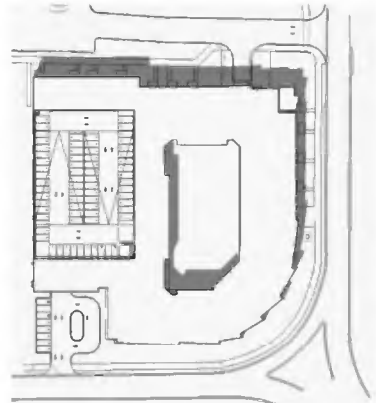
Courtyard Scheme 9/1, 10am



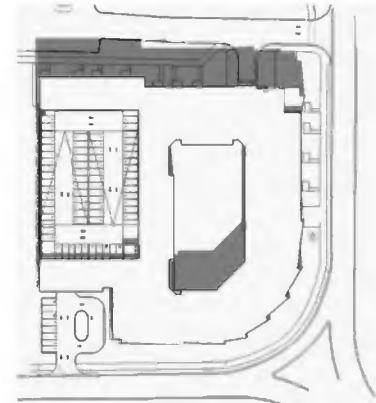
Courtyard Scheme East - 9/1, 10am



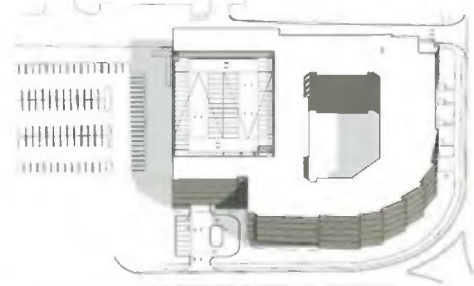
Courtyard Scheme 5/1, 1pm



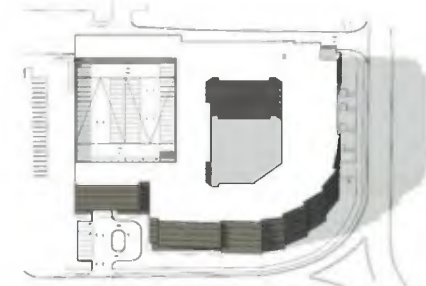
Courtyard Scheme 7/1, 1pm



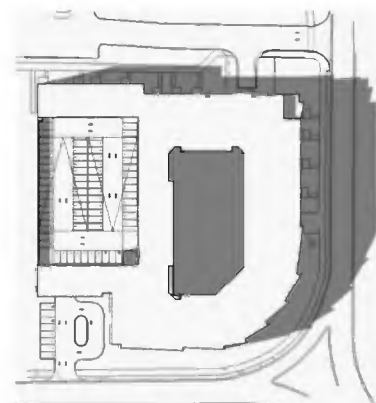
Courtyard Scheme 9/1, 12pm



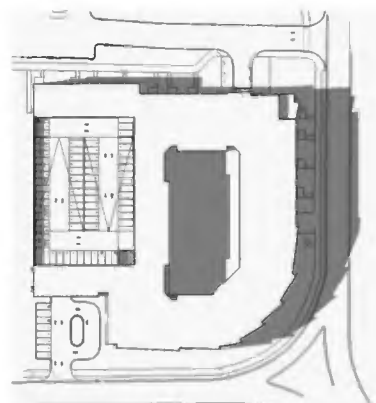
Courtyard Scheme South - 9/1, 10am



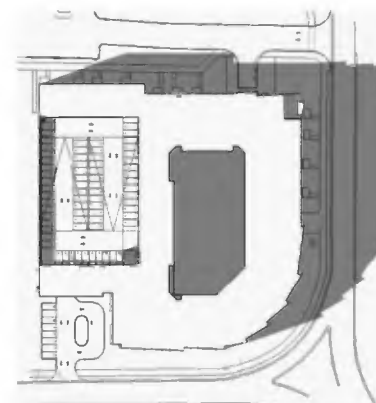
Courtyard Scheme South - 9/1, 4pm



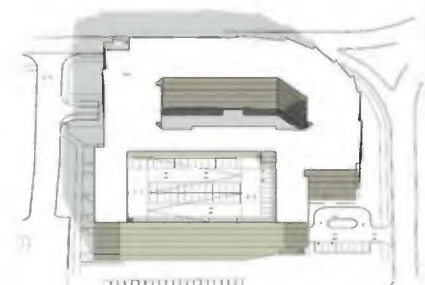
Courtyard Scheme 5/1, 4pm



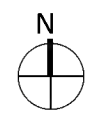
Courtyard Scheme 7/1, 4pm



Courtyard Scheme 9/1, 4pm



Courtyard Scheme West - 9/1, 4pm



SUSTAINABLE DESIGN: SITE

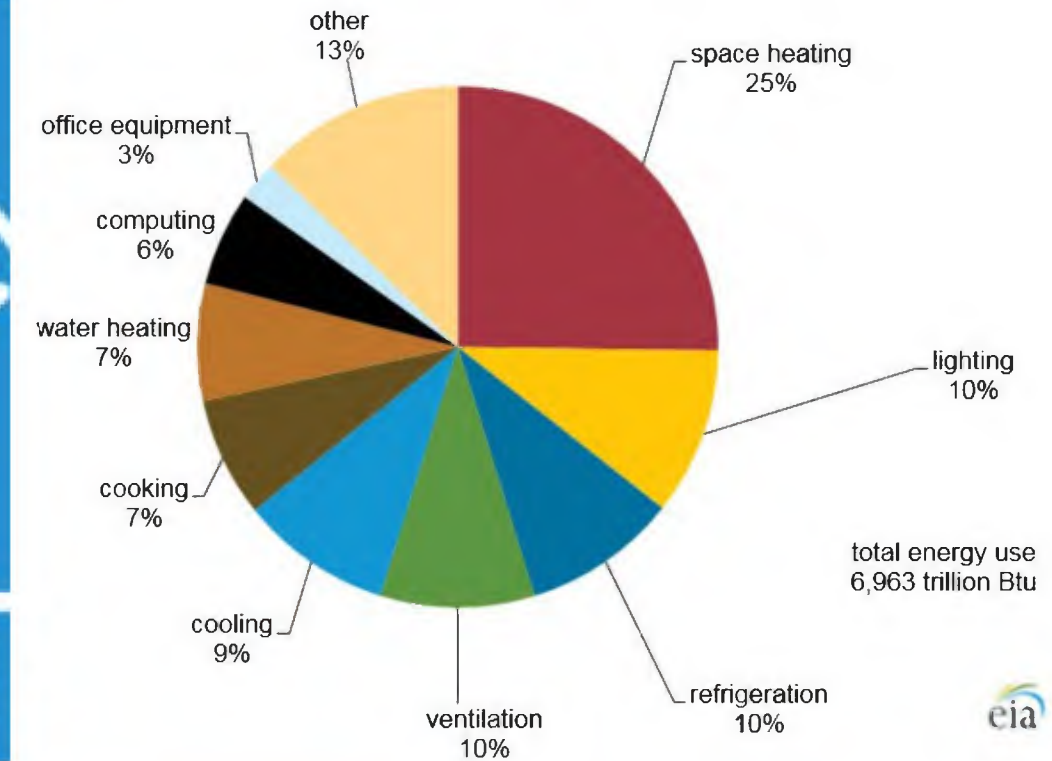
- **Access to transit** with adjacent light rail station
- Connecting to existing **bike/nature trails** and providing internal bike storage rooms
- **Stormwater management:** using a creative mix of surface and underground solutions integrated with the site's landscaping
- **Light-colored** site pavement and pavers to reduce heat island effect
- **Dark sky-compliant** site lighting
- **Native plant species** — pollinator/prairie/woodland shade/wildflower meadow gardens
- **Removal** of a significant area of high-maintenance **turf lawn** — less gas mowing /fertilizing/
- Less turf = less site irrigation
- **Smart irrigation systems** (rain/soil moistures, drip irrigation, highly efficient controllers/sprayheads)



SUSTAINABLE DESIGN: BUILDINGS

BUILDING EFFICIENCY & RENEWABLE ENERGY

- High R-Value building envelope — roof & walls
- High solar reflectance index (SRI) roof with SRI of 0.28 minimum
- Low-E insulated glazing
- **Tightly sealed building envelope** to reduce leakage and inefficiencies
- Efficient HVAC systems — explore options through the Xcel Energy EDA program
- Energy Star-rated appliances
- LED fixtures with **occupancy sensors**
- **Electrical vehicle** charging stations
- **WaterSense** plumbing fixtures
- **Low VOC** materials and paints
- **Low construction waste** due to panelized construction
- Solar Panel ready building



SUSTAINABLE DESIGN: OCCUPANTS

HEALTH & WELLNESS

- Design to encourage **physical activity** with well designed and convenient stairwells and exterior pathways
- **Visual connection** to the outdoors
- **Noise mitigation** with verified acoustical sound assemblies in walls and floors
- **Clean air** with at least MERV 8 air filters
- Access to **daylight** in rooms and amenity spaces
- **Low VOC** materials and paints
- Convenient common area **hand sanitizers and hand washing** locations
- Cleaning procedures, including **daily cleaning** of entrances, common areas, corridors, restrooms and offices



Ordinance No. 2023-

An ordinance approving a rezoning from I-1, industrial, to PUD, planned unit development and a master development plan for the property located at 10701 Bren Rd East

The City Of Minnetonka Ordains:

Section 1. Background

1.01 The subject property is located at 10701 Bren Rd East.

1.02 The property is legally described as:

Parcel 1:

Lot 5 and that part of Lot 6, which lies Easterly of the following described line: Beginning at a point on the South line of said lot 6 distant 123.00 feet Westerly from the Southeast corner of said Lot 6; thence North 1 degree, 09 minutes, 15 seconds East, a distance of 391.08 feet to a point on the North line of said Lot 6, distant 127.43 feet Westerly from the Northeast corner of said Lot 6 and said line there terminating; All in Block.1, Opus 2 Eighth Addition, Hennepin County, Minnesota

Torrens Property Certificate of Title No. 1211616

Parcel 2:

Permanent easement for pedestrian and vehicular access and ingress and egress for the benefit of Parcel 1 as set forth in that certain Easement Agreement dated September 20, 2007, recorded September 21, 2007, as Document No. 4428662

(T).

1.03 Greystar Development is requesting rezoning from I-1, industrial, to PUD, planned unit development and a master development plan for the subject property. The amendment would allow for the construction of a six-story, 275-unit apartment building, with 10 percent of the units meeting affordability guidelines.

Section 2. Findings

- 2.01 The proposal is consistent with the OPUS area’s mixed-use designation in the comprehensive guide plan.
- 2.02 The proposal is consistent with City Council Policy 13.2, Affordable Housing Policy.
- 2.03 The proposal would not negatively impact the public health, safety, or general welfare.

Section 3.

- 3.01 Approval is subject to the following conditions:
1. The site must be developed and maintained in substantial conformance with the following plans unless modified by the conditions below:
 - Site Plan, dated Dec. 6, 2022
 - Exterior Elevations, dated Sept. 14, 2022
 - Grading and Drainage Plan, dated Sept. 19, 2022
 - Utility Plan, dated Sept. 19, 2022
 - Tree Inventory, Landscaping and Preservation Plan, dated Sept. 19, 2022 and Dec. 6, 2022
 2. The development must further comply with all conditions outlined in City Council Resolution No. 2023-XXX, Site and Building Plan approval, adopted by the Minnetonka City Council on _____, 2023.

Section 4. A violation of this ordinance is subject to the penalties and provisions of Chapter XIII of the city code.

Section 5. This ordinance is effective immediately.

Adopted by the City Council of the City of Minnetonka, Minnesota, on _____, 2023.

Brad Wiersum, Mayor
Attest:

Becky Koosman, City Clerk

Action on this ordinance:

Date of introduction: Jan. 30, 2023

Date of adoption:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Ordinance adopted.

Date of publication:

I certify that the foregoing is a correct copy of an ordinance adopted by the city council of the City of Minnetonka, Minnesota at a regular meeting held on _____, 2023.

Becky Koosman, City Clerk



**City Council Agenda Item 13A
Meeting of Jan. 30, 2023**

Title: On-sale intoxicating and Sunday on-sale intoxicating liquor licenses for Novel Spirits, LLC, dba The Social Kitchen & Libations, located at 12411 Wayzata Boulevard

Report From: Fiona Golden, Community Development Coordinator

Submitted through: Mike Funk, City Manager
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Novel Spirits, LLC, dba The Social Kitchen & Libations, has submitted an application for an on-sale intoxicating and a Sunday on-sale intoxicating license in the space previously occupied by Lake Shore Grill at Ridgedale Mall. The city council is required to hold two public hearings to consider the licenses and receive public testimony.

Recommended Action

Motion to grant the licenses.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Resources Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Financial Consideration

Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Subject: On-sale intoxicating and Sunday on-sale intoxicating liquor licenses for Novel Spirits, LLC, dba The Social Kitchen & Libations, located at 12411 Wayzata Boulevard

Background

Macy's Retail Holdings, Inc., is planning to reopen the restaurant space formally occupied by Lake Shore Grill. The restaurant closed in July 2020 after being open for more than 15 years. On [Nov. 14, 2022](#), the council approved a conditional use permit for The Social Kitchen & Libations to operate a sit-down restaurant in the space as it had been vacant for over a year.

Business Operation Description

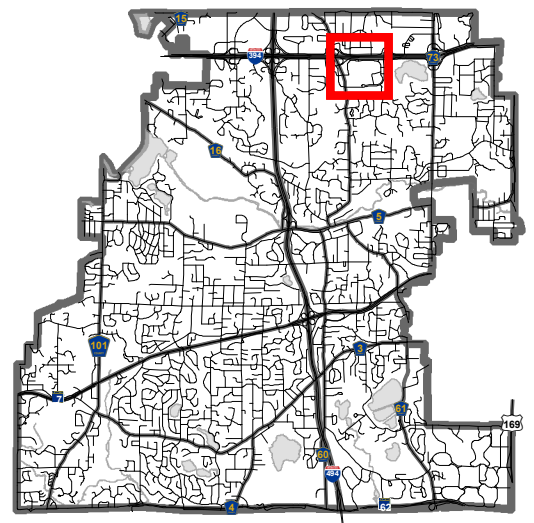
The restaurant will be open daily, Mon. - Fri. 11 a.m. – 8 p.m., Sat. 10 a.m. – 8 p.m., and Sun. 10 a.m. – 6 p.m. The Social Kitchen & Libations will be a fast-casual restaurant offering wood-fired pizzas, soups, salads, and small plates. The full-service bar will offer hand-crafted cocktails, beer, and wine. They anticipate having 15 hourly team members per shift. There is also a full-time general manager and supervisors to cover all shifts. All staff members responsible for serving liquor will be professionally trained by the Macy's Holding Inc. Alcohol Training Program. All new employees are trained upon being hired, along with annual training sessions for the whole team. The projected food-to-liquor ratio will be 88% food and 12% alcohol. The restaurant has seating for 117 indoor and 50 outdoor patrons. The restaurant space is undergoing minor changes to the indoor space, along with minor cosmetic changes to the outdoor signage. The restaurant has a projected opening date of March 3, 2023.

A complete application and license fees have been submitted. Staff mailed out postcards to area residents and businesses and didn't received any public comments. The police department's investigative report on this application is complete and will be forwarded to the council prior to the continued public hearing.



Location Map

Project: The Social Kitchen
Address: 12411 Wayzata Blvd



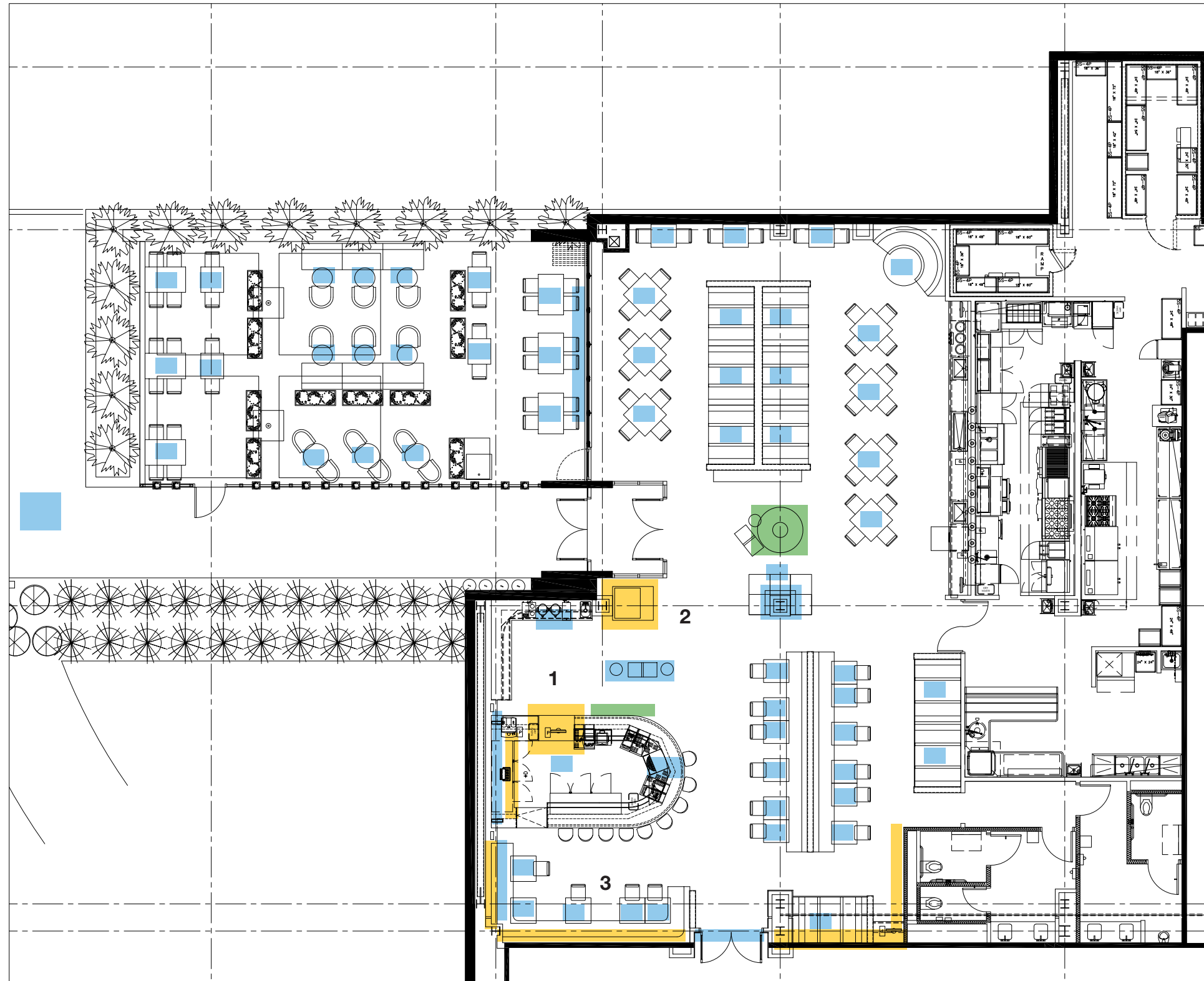
Updated Plan

Construction Areas

1. Remove Stainless Rack at bar, Install lower counter, match counter material
2. Install Coke Freestyle on a new built cabinet to match others
3. Paint indicated walls white

Graphics, Decor and Signage Areas

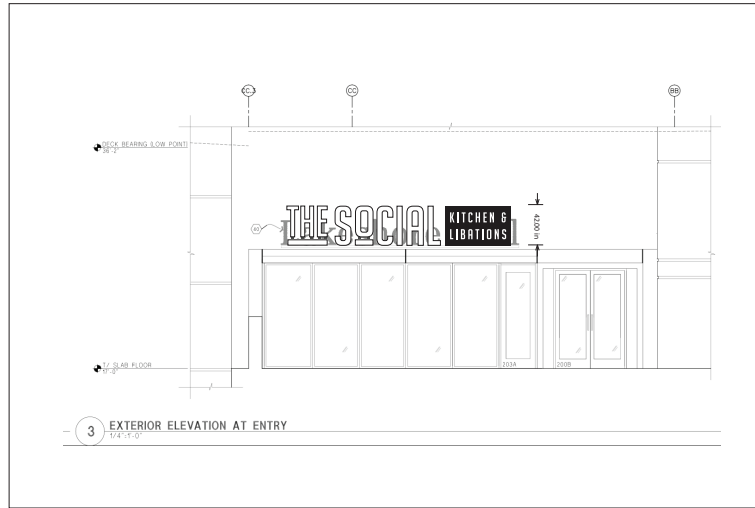
- Table Numbers
- Sidewalk Sign
- Exterior Sign
- Interior Sign
- Column Graphic
- Menu Board
- Paper Menu
- Art for old Monitor Wall
- Chandelier Plaque
- "Order Here" Sign
- Queue Line Fixtures
- Counter Display



Moving Areas

- Move round table to new location shown
- Remove Bar Stools as shown

1 // Exterior Logo Sign Option B // CHANNEL Mount



Paint background wall and North wall dark gray SW 7674 (by Macy's)

SW 7674
Peppercorn
Interior / Exterior
Location Number: 236-C7



- 42 inches tall
- White face, black returns
- Internally illuminated sign
- Individual letters sit on current armature
- Tagline is in white acrylic push-through metal and is internally illuminated
- Sign sits on remaining bar from old logo

The sign permit is reviewed administratively.



The New Menu

To Share

Popover Basket
Tomato Mozzarella
with Pesto flatbread
Avocado Toast
Guacamole and Chips
Buttermilk Chicken Tenders
Flatbread Roasted Harvest
Rotisserie Chicken

Soups

Chicken chili
Wild Rice
Soup of the Day
Soup and salad combo

Salads

Mandarin Chicken
Buttermilk Fried
Chicken Salad
Roasted Harvest Salad
Herald Caesar
Marketplace Sampler
Bison Burger Cobb Salad

Sandwiches

Cobblestone Turkey
New Chicken Sandwich with
Rotisserie Chicken
Chicken Caesar Wrap
Burger
Walleye Tacos

Favorites While they Last

Quiche of the Day
Chicken Pot Pie
Meatloaf
Walleye
Rotisserie Chicken Dinner

Family style

Chicken Pot Pie
Meatloaf
Rotisserie Chicken
Deli Salads by the
1/2 Pound or Pound

NA Beverages

Housemade Lemonade
Freestyle Fountain
Iced Teas
Espresso Drinks



**City Council Agenda Item 14A
Meeting of Jan. 30, 2023**

Title: Concept plan for Marsh Run II Development at 11816 Wayzata Blvd
Report From: Bria Raines, Planner
Submitted through: Mike Funk, City Manager
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Doran Development LLC has submitted a concept plan to redevelop the property at 11816 Wayzata Blvd. Doran Development provided concept plan details at the planning commission meeting on Jan. 19, 2023. The plan contemplates redevelopment of the property with the following:

- A 6-story, luxury market-rate apartment building with 197 units;
- A two-level parking garage with approximately 266 parking spaces and surface parking with approximately 41 parking spaces;
- Twenty percent of total units to be affordable units*; and
- Interior and outdoor amenity spaces for residents, such as a fitness room, business center, pool and pool area, grill station, and entertainment suite.



Marsh Run II Development - Building Perspective

* On Jan. 26, 2023, the Economic Development Advisory Commission (EDAC) will review the affordable housing components.

Recommended Action

Provide feedback intended to assist the applicant in preparing more detailed development plans. No formal action is required.

Strategic Profile Relatability

- Financial Strength & Operational Excellence
 - Sustainability & Natural Resources
 - Infrastructure & Asset Management
 - Safe & Healthy Community
 - Livable & Well-Planned Development
 - Community Inclusiveness
- N/A

Statement: N/A

Financial Consideration

- Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
- Financing sources: Budgeted Budget Modification New Revenue Source
- Use of Reserves Other [Enter]

Statement: N/A

Background

11816 Wayzata Blvd is located in the Oak Knoll neighborhood with two access points from Wayzata Blvd. The property is 4.33 acres in area and is improved with three two-story office buildings totaling 31,104 sq. ft. with associated parking and a stormwater management pond.

The site has a significant wetland, approximately 1.11 acres, between the stormwater management pond and the Federal Emergency Management Agency (FEMA) zone along the eastern property line. The office buildings were constructed in 1981 to the setback standards of that time. Any redevelopment of the site would be subject to current city code-defined wetland setbacks, watershed standards, and FEMA setbacks.



Marsh Run II site, 11816 Wayzata Blvd.



Wetland Delineation - Marsh Run II site

Concept Plan Review Process

The staff has outlined the following Concept Plan Review process for the proposal. At this time, a formal application has not been submitted.

- **Neighborhood Meeting.** An in-person neighborhood meeting was held on Nov. 29, 2022. The meeting was attended by Doran representatives, the selling property owners, two neighbors who live in The Birke, and four neighbors who live in the Fairfield Road neighborhood. Attendees expressed concerns about additional housing in the neighborhood, the resulting vehicular and pedestrian traffic (including dog walking), the location of trails, the disruption of wildlife, the obstruction to westerly sunset views, and the impact on the environment. Written comments from residents have been received and are attached.

Planning Commission Concept Plan Review. The planning commission reviewed the concept on Jan. 19, 2023. Members of the public noted the following concerns:

- Exacerbating existing on-street parking issues.
- More enforcement and “no parking” signs are needed on Fairfield Road.
- Increased foot traffic from dog walking and those not cleaning up after their pets. One resident requested waste receptacles along Fairfield Road to encourage dog walkers to pick up and dispose of their pet excrement.
- Redevelopment impacts to the environment (wetlands) and wild-life

The commission provided the following general comments:

- Appreciated the additional surface parking. Commissioners were concerned about repeated parking issues at The Birke.
 - Suggested utilizing the east office building, between Marsh Run II and The Birke, to correct existing off-street parking issues. Commissioners were curious about the future use at the east building and how it would affect the current parking issues.
 - Noted that the building design is well thought through. The scalloped curve on the building façade is interesting.
 - Most commissioners felt the building was “monolithic” and massing should be reduced.
 - Supported the concept of having three bedrooms units, 20% affordable units, and a dog walking trail available to the neighborhood.
- **Economic Development Advisory Commission.** The economic development advisory commission (EDAC) will review the affordable housing component of the concept should there be a request for financial assistance. The Marsh Run II Development is tentatively scheduled for the EDAC meeting on Jan. 26, 2023.
 - **City Council Concept Plan Review.** The city council concept plan review is intended as a follow-up to the planning commission and EDAC meetings and would follow the same format. No staff recommendations are provided, the public is invited to offer comments, and council members are afforded the opportunity to ask questions and provide feedback without any formal motions or votes.

Key Issues

City staff has identified the following considerations for any development of the subject property:

- **Change of land use:** A change from commercial to residential would change the characteristics of people traveling to and from the site. Further analysis of traffic impacts would be needed.
- **Site Plan:** The proposed site plan would intensify development on the site. Site circulation for residents and emergency response, snow removal, and pedestrian connections become more important as site use intensifies.
- **Wetland and Floodplain:** The site is subject to the Bassett Creek Watershed District requirements, which could require a more significant buffer than city code requirements. A reduced setback or buffer request may be necessary for the site.
- **Affordable housing opportunities:**
 - The proposed project would include a six-story apartment building with 197 units (40 affordable units). The units would be a mix of studio, one, two, and three-bedroom units. The developer is proposing to provide 20 affordable units at 60% AM and 20 affordable units at 80% AMI, with the following unit mix:

# of Units	Type	Rent Type	Rent
2	Studio (673sf)	Market	\$1,675
		60% AMI	\$1,233
12	1-BR (765sf)	Market	\$1,825
		60% AMI	\$1,320
6	2BR (1,179sf)	Market	\$2,700
		60% AMI	\$1,584
2	Studio (673sf)	Market	\$1,675
		80% AMI	\$1,644
10	1-BR (765sf)	Market	\$1,825
		80% AMI	\$1,761
6	2BR (1,179sf)	Market	\$2,700
		80% AMI	\$2,112
2	3BR (1745sf)	Market	\$3,850
		80% AMI	\$2,440

- Market-rate rents on the remaining 157 units range from approximately \$1,675 to \$3,850 per month.
- The developer is requesting \$4.6 million to assist with financing the affordable units and extraordinary project costs.
 - The EDAC will review the request for assistance at the EDAC meeting on Jan. 26.

- **Building Character:** Building elevations have been provided with character details to compliment the neighborhood. Input on building massing and desired character is important.

Key Topics

Staff requests council provides feedback on the following key topics.

- **Residential Use.** The Planned I-394 District (PID) is designated for mixed-use in the comprehensive plan. What is the council's opinion regarding the residential use of the site?
- **Site Plan.** The proposed site plan identifies the building location, vehicular and pedestrian connections, and some on-site amenities. Does the council have comments on the general location and organization of these elements?
- **Building Design.** Building elevations have been provided. Does the council have comments on the building massing, design elements, and materials?
- **Other Considerations.** What other land use-related items would the council like to comment on?

Next Steps

- **Formal Application.** If the developer/applicant chooses to file a formal application, notification of the application will be mailed to area property owners. Area property owners are encouraged to view plans and provide feedback via the city's website. Through recent website updates: (1) staff can provide owners with ongoing project updates, (2) owners can "follow" projects they are particularly interested in by signing up for automatic notification of project updates; (3) owners may provide project feedback on the project; and (4) and staff can review resident comments.
- **Council Introduction.** The proposal would be introduced at a city council meeting. At that time, the council would be provided another opportunity to review the issues identified during the initial concept plan review meeting and provide direction about any refinements or additional issues they wish to be researched and for which staff recommendations should be prepared.
- **Planning Commission Review.** The planning commission will review and subsequently make a recommendation to the city council on land use matters.
- **City Council Action.** The city council would take final action based on input from the planning commission, EDAC, professional staff, and the general public.

MINNETONKA PLANNING COMMISSION
Jan. 19, 2023

Brief Description	Concept plan review for Marsh Run II Redevelopment at 11816 Wayzata Blvd.
Action Requested	Discuss the concept plan with the applicant. No formal action is required.

Proposal

Doran Development has submitted a concept plan for the redevelopment of the Marsh Run II property located at 11816 Wayzata Blvd. The 4.33-acre property contains 3 two-story office buildings totaling 31,104 sq. ft. with associated parking and a stormwater management pond. The site is currently zoned PID, Planned I394 District, and guided for mixed-use in the 2040 Comprehensive Guide Plan. The mixed-use designation allows for both residential and commercial uses or either use alone; both are not required to exist on the site.

Doran contemplates redevelopment of the Marsh Run II property with the following:

- A six-story, luxury market-rate apartment building with 198 units. The building would include interior and outdoor amenity spaces for residents, such as:
 - Fitness room, business center, pool and patio area, grill station, and entertainment suites.
- An underground parking garage with approximately 279 parking spaces and surface parking with approximately 23 parking spaces; and

The architecture is influenced by the neighborhood and a nearby residential apartment project. While no formal action is required at this time, staff anticipates the following city actions may be necessary based on the current concept plan: (1) rezoning; (2) master development plan amendment; and (3) site and building plan approval.

Comparison to the Neighboring High-Density Residential

The subject property is west of the intersection of Wayzata Boulevard and Fairfield Road West. The concept would not be the first high-density development in the neighborhood. The neighborhood includes The Birke along Wayzata Boulevard and Fairfield Court, Fairfield Way, Fairfield Circle, and Bay hill condominiums along Fairfield Road West.

The Birke, a previous project of Doran Development, is the nearest high-density dwelling to the Marsh Run II property. As proposed, the building would be 68 feet in height and six stories, the same height as The Birke. The office building between the sites is a height of 26 feet and the townhomes to the north – on Fairfield Court and Fairfield Way – are 44 and 42 feet, respectively. The elevations of the neighborhood have been provided from the perspective of Wayzata Boulevard and I-394.



Concept Plan Review Process

The staff has outlined the following Concept Plan Review process for the proposal. At this time, a formal application has not been submitted.

- **Neighborhood Meeting.** A virtual neighborhood meeting was held on Nov. 29, 2022. The meeting was attended by Doran representatives, the selling property owners, two neighbors who live in the Birke, and four neighbors who live in the Fairfield Road neighborhood. Attendees expressed concerns about additional housing in the neighborhood, the resulting vehicular and pedestrian traffic (including dog walking), the location of trails, the disruption of wildlife, the obstruction to westerly sunset views, and the impact on the environment. One written comment has been received. (See pages A21-A26).

- **Planning Commission Concept Plan Review.** The purpose of the concept plan review is to allow commissioners to identify – for the developer and city staff – what they see as the positive components of a development concept and any issues or challenges they foresee. The concept plan review meeting will include a presentation by the developer of conceptual sketches and ideas but not detailed engineering or architectural drawings. No staff recommendations are provided, no motions are made, and no votes will be taken.
- **Economic Development Advisory Commission.** The economic development advisory commission (EDAC) will review the affordable housing component of the concept and any requests for financial assistance.
- **City Council Concept Plan Review.** The city council concept plan review is intended as a follow-up to the planning commission and EDAC meetings and would follow the same format. No staff recommendations are provided, the public is invited to offer comments, and council members are afforded the opportunity to ask questions and provide feedback without any formal motions or votes.

Key Issues

City staff has identified the following considerations for any development of the subject properties:

- **Change of land use:** A change from commercial to residential would change the characteristics of people traveling to and from the site. Further analysis of traffic impacts would be needed.
- **Site Plan:** The proposed site plan would intensify development on the site. Site circulation for residents and emergency response, snow removal, and pedestrian connections become more important as site use intensifies.
- **Wetland and Floodplain:** The site is subject to the Bassett Creek Watershed District requirements, which could require a more significant buffer than city code requirements. A reduced setback or buffer request may be necessary for the site.
- **Affordable housing opportunities:** The City of Minnetonka has a commitment to providing diversity in the type and size of housing units. The city's affordable housing policy requires the inclusion of affordable housing for low and moderate-income households in new multifamily developments. Economic Development staff are discussing a redevelopment tax increment financing (TIF) district. This would require a public hearing with the Economic Development Advisory Commission (EDAC) and a determination of assistance based on the proposed number of affordable units. Affordable housing and public assistance are the purviews of the EDAC.
- **Building Character:** Building elevations have been provided with character details to compliment the neighborhood. Input on building massing and desired character is important.

Key Topics

The staff has identified and requests planning commission feedback on the following key topics:

- **Residential Use.** The Planned I-394 District (PID) is designated for mixed-use in the comprehensive plan. What is the commission's opinion regarding the residential use of the site?
- **Site Plan.** The proposed site plan identifies the building location, vehicular and pedestrian connections, and some on-site amenities. Does the commission have comments on the general location and organization of these elements?
- **Building Design.** Building elevations have been provided. Does the commission have comments on the building massing, design elements, and materials?
- **Other Considerations.** What other land use-related items would the commission like to comment on?

Staff Recommendation

Staff recommends the planning commission provide feedback on the key topics identified by staff and any other land use-related items that the commission deems appropriate. This discussion is intended to assist the applicant in the preparation of more detailed development plans.

Originator: Bria Raines, Planner
Through: Susan Thomas, AICP, Assistant City Planner
Loren Gordon, AICP, City Planner

ADDITIONAL INFORMATION

Next Steps

- **Formal Application.** If the developer/applicant chooses to file a formal application, notification of the application will be mailed to area property owners. Area property owners are encouraged to view plans and provide feedback via the city's website. Through recent website updates: (1) staff can provide owners with ongoing project updates, (2) owners can "follow" projects they are particularly interested in by signing up for automatic notification of project updates; (3) owners may provide project feedback on the project; and (4) and staff can review resident comments.
- **Council Introduction.** The proposal would be introduced at a city council meeting. At that time, the council would be provided another opportunity to review the issues identified during the initial concept plan review meeting and provide direction about any refinements or additional issues they wish to be researched and for which staff recommendations should be prepared.
- **Planning Commission Review.** The planning commission will review and subsequently make a recommendation to the city council on land use matters.
- **EDAC Review.** The EDAC will review and subsequently make a recommendation to the city council on affordable housing and public finance.
- **City Council Action.** Based on input from the planning commission, EDAC, professional staff, and the general public, the city council would take final action.

Roles and Responsibilities

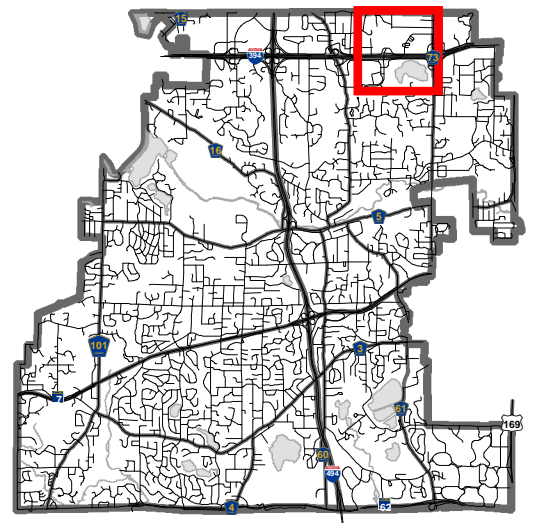
- **Applicants.** Applicants are responsible for providing clear, complete, and timely information throughout the review process. They are expected to be accessible to both the city and to the public and to respect the integrity of the public process.
- **Public.** Neighbors and the general public will be encouraged and enabled to participate in the review process to the extent they are interested. However, effective public participation involves shared responsibilities. While the city has an obligation to provide information and feedback opportunities, interested residents are expected to accept the responsibility to educate themselves about the project and review process, to provide constructive, timely, and germane feedback, and to stay informed and involved throughout the entire process.
- **Planning Commission.** The planning commission hosts the primary forum for public input and provides clear and definitive recommendations to the city council. To serve in that role, the commission identifies and attempts to resolve development issues and concerns before the council's consideration by carefully balancing the interests of applicants, neighbors, and the general public.

- **City Council.** As the ultimate decision-maker, the city council must be in a position to equitably and consistently weigh all input from their staff, the general public, commissioners, applicants, and other advisors. Accordingly, council members traditionally keep an open mind until all the facts are received. The council ensures that residents have an opportunity to participate in the process effectively.
- **City Staff.** The city staff is neither an advocate for the public nor the applicant. Rather, staff provides professional advice and recommendations to all interested parties, including the city council, planning commission, the applicant, property owners, and residents. Staff advocates for its professional position, not a project. Staff recommendations consider neighborhood concerns but necessarily reflect professional standards, legal requirements, and broader community interests.



Location Map

Project: Marsh Run II redevelopment
 Address: 11816 Wayzata Blvd





12/16/2022 1:47:12 PM



00' Overall building height, measured from grade



12/16/2022 1:47:13 PM

LEGAL DESCRIPTION (PROVIDED BY CLIENT)

Par 1:
Lot 8, Block 3, except the South 10 feet taken for widening Superior Boulevard; and That part of vacated Merrivale Ave. described as follows: beginning at a point on the East line of Lot 8, Block 3, in said plat distant 10.00 feet North of the Southeast corner of said Lot 8; thence East, parallel with the South line of said Lot 8, a distance of 6.00 feet; thence Northerly to the point of intersection with a line which runs parallel with and 50.00 feet West from the Easterly line of said Avenue, said point of intersection being 31.52 feet Southerly of the Easterly extension of the North line of said Lot 8, as measured along the last said parallel line; thence Northerly along said parallel line a distance of 31.52 feet of said Easterly extension of the North line of Lot 8; thence West along said Easterly extension, to the Northeast corner of said Lot 8; thence Southerly, along said East line of Lot 8, to said point of beginning; That part of Section Line lying South of the center line thereof and between the Westerly right-of-way line of Merrivale Ave. and a Northerly extension of the Westerly line of Lot 8, Block 3, "Boulevard Gardens, Hennepin Co. Minn."

Par 2:
Lots 9, 10, 11, 12 and 13, Block 3, "Boulevard Gardens, Hennepin Co. Minn.", except the South 10 feet of said Lots 9, 10, 11, 12 and 13 and except that part of said Lots 11, 12 and 13 that is designated and delineated as Parcel 10C on the Minnesota Department of Transportation Right-of-Way plat No 27-37.

GENERAL NOTES

- This survey was prepared without the benefit of a title commitment. No search for easements or restrictions recorded or unrecorded, if any, was made by the surveyor.
- Bearings shown are based upon Hennepin County Coordinate System, NAD 83, 1986 Adjustment, US Survey Foot.
- Subject property contains 240,039 Sq. Ft. or 5.51 acres.

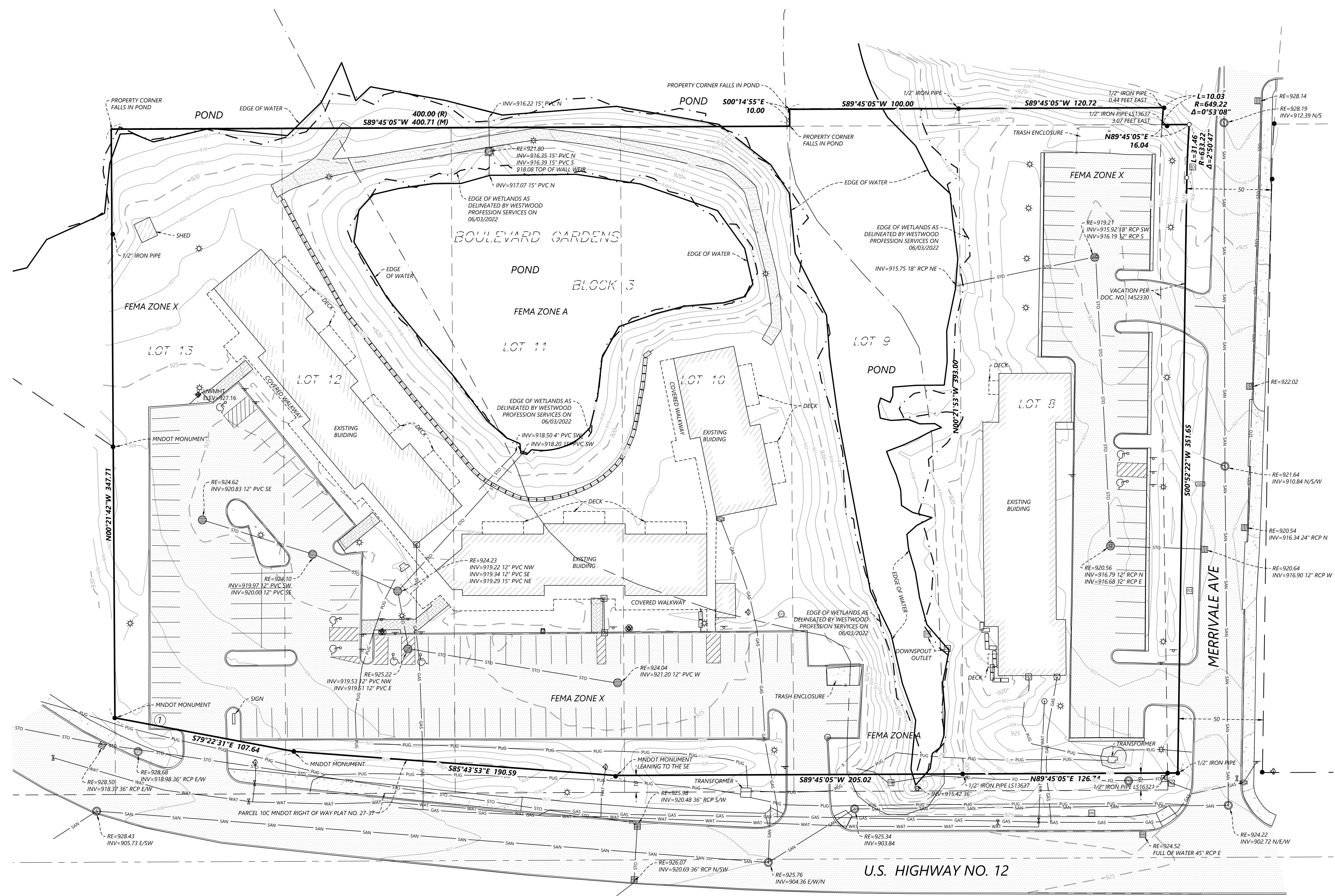
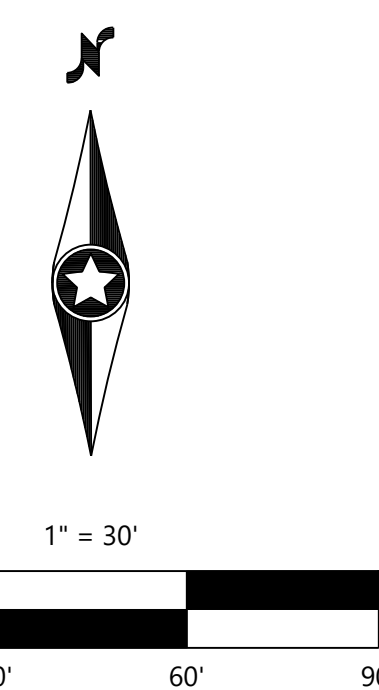
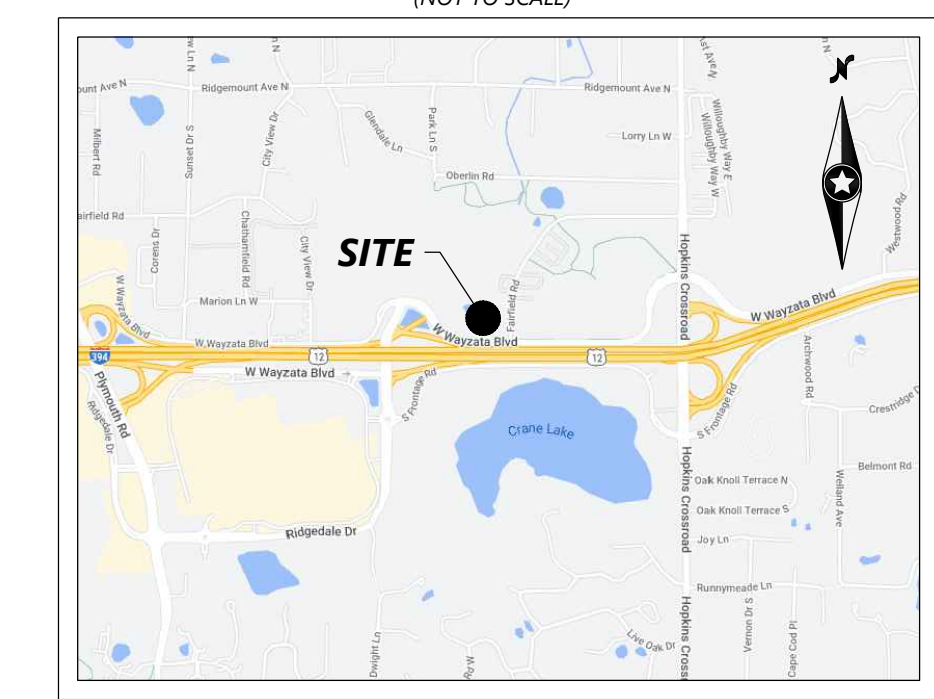
POSSIBLE ENCROACHMENTS

- Parking lot curb over the south property line.

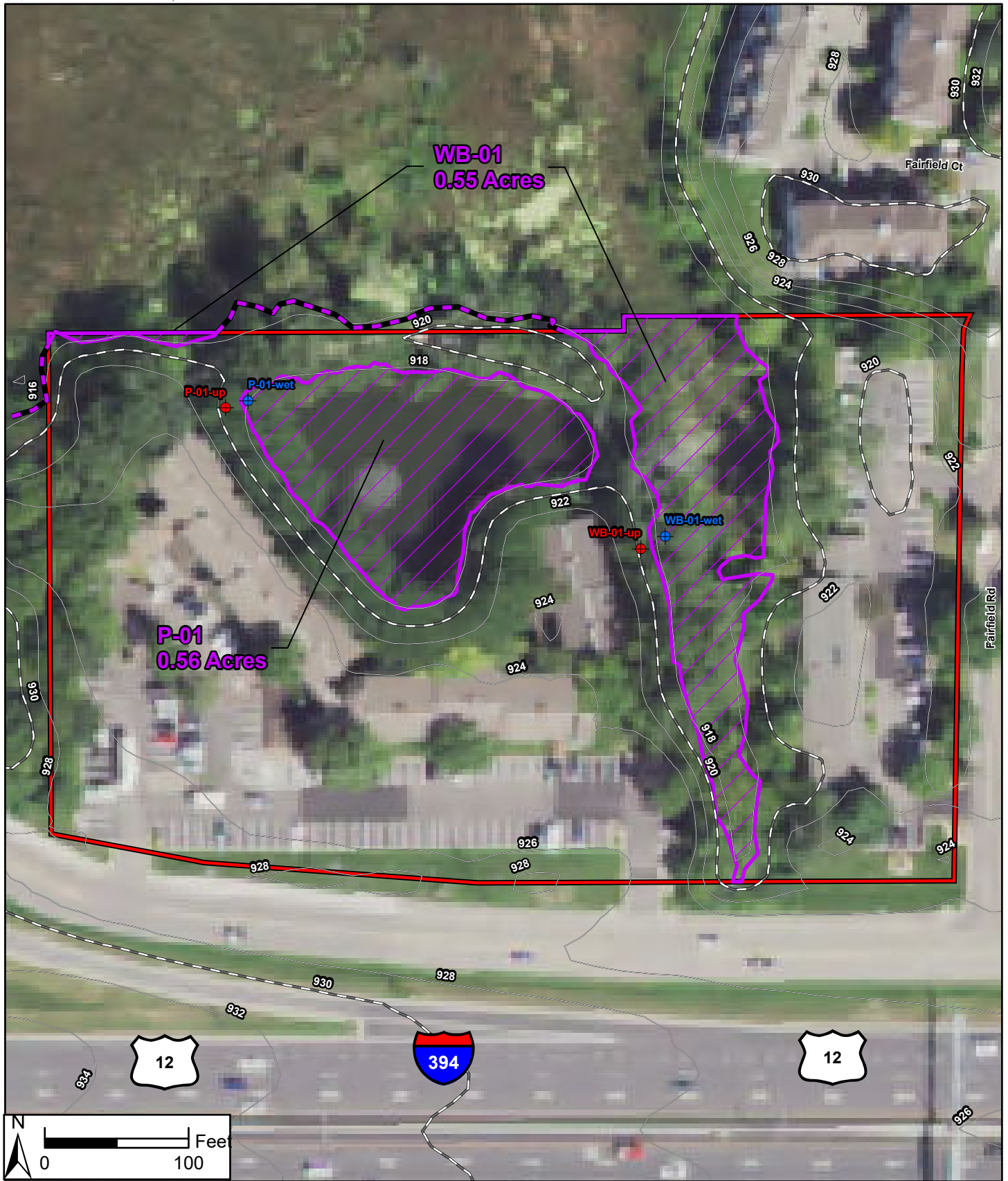
LEGEND

⊙	SANITARY MANHOLE	—	BOUNDARY LINE
⊕	SEWER CLEANOUT	---	RIGHT-OF-WAY LINE
⊖	STORM MANHOLE	- - -	LOT LINE
⊗	CATCH BASIN	- · - · -	EASEMENT LINE
⊘	FLARED END SECTION	- - - - -	SECTION LINE
⊙	CATCH BASIN MANHOLE	- · - · -	WETLAND LINE
⊖	DOWNSPOUT	- · - · -	FEMA LINE
⊗	HYDRANT	- · - · -	GAS LINE
⊘	GATE VALVE	- · - · -	PLUG
⊙	POST INDICATOR VALVE	- · - · -	POWER UNDERGROUND
⊕	ELECTRIC BOX	- · - · -	SAN
⊖	ELECTRIC METER	- · - · -	SANITARY SEWER
⊗	TELEPHONE BOX	- · - · -	STO
⊘	HAND HOLE/JUNCTION BOX	- · - · -	STORM SEWER
⊙	FIBER OPTIC MANHOLE	- · - · -	TUG
⊕	FIBER OPTIC PEDESTAL	- · - · -	TELEPHONE UNDERGROUND
⊖	NATURAL GAS METER	- · - · -	WAT
⊗	NATURAL GAS VALVE	- · - · -	WATERMAIN
⊘	STEELWOOD POST	- · - · -	FO
⊙	SIGN	- · - · -	FIBER OPTIC
⊕	HANDICAPPED STALL	- · - · -	X
		- · - · -	FENCE LINE
		- · - · -	CURB & GUTTER
		▨	CONCRETE SURFACE
		▩	BITUMINOUS SURFACE
		▧	GRAVEL SURFACE
		●	FOUND MONUMENT (SEE LABEL)
		○	SET MONUMENT
		⊖	1/2" IRON PIPE LSS3596

VICINITY MAP
(NOT TO SCALE)





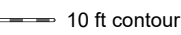
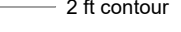


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Data Source(s): Westwood (2022); Census Bureau (2017); USDA NAIP Hennepin County (2017).

Legend

-  Onsite Delineated Wetland Boundary
-  Offsite Delineated Wetland Boundary
-  Upland Sample Point
-  Wetland Sample Point
-  10 ft contour
-  2 ft contour

**11800, 11812, 11824,
11900 Wayzata Blvd.**

City of Minnetonka
Hennepin County, Minnesota

Delineated Wetlands

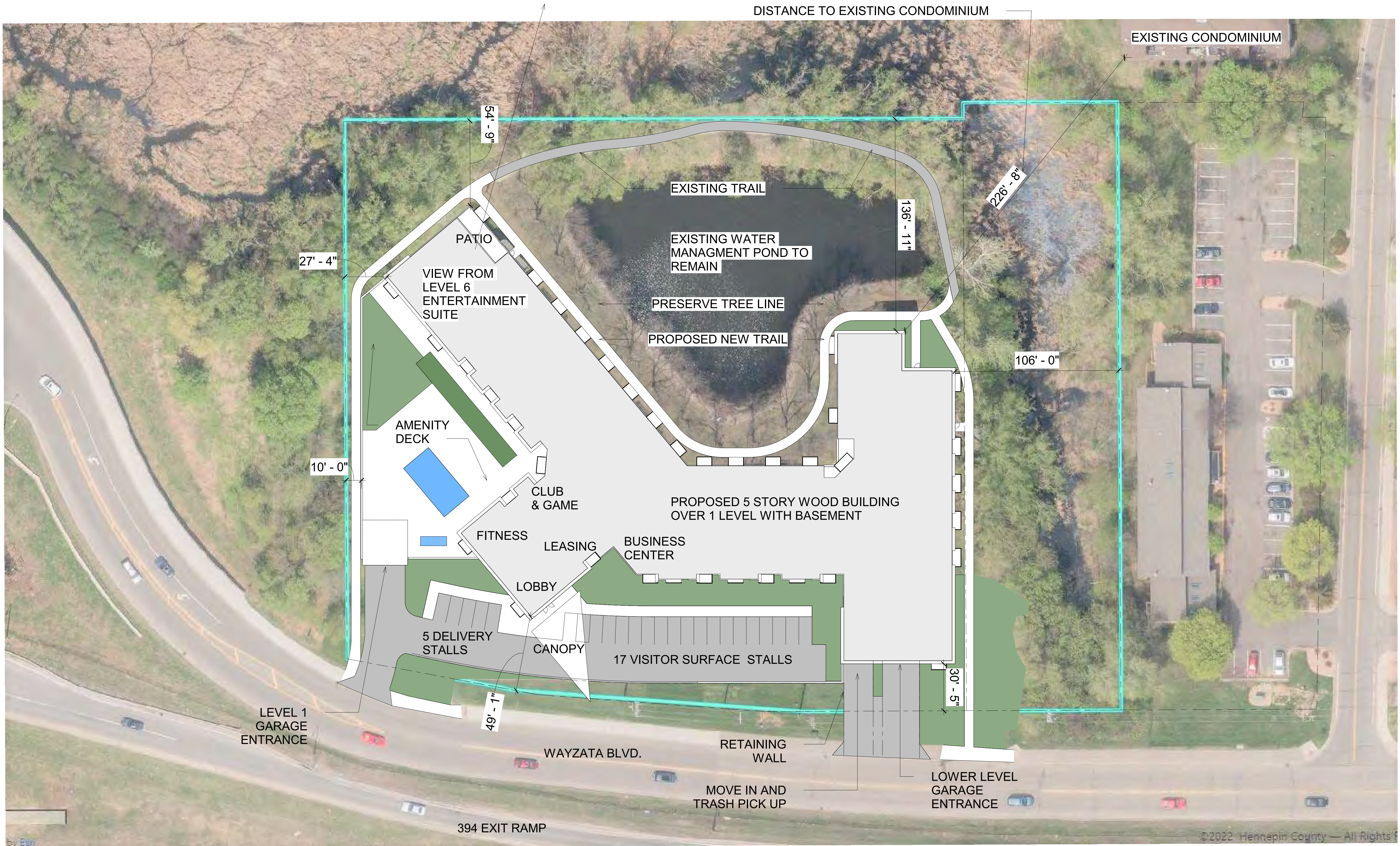
Westwood
Toll Free (888) 937-5150 westwoodps.com
Westwood Professional Services, Inc.



by Esri

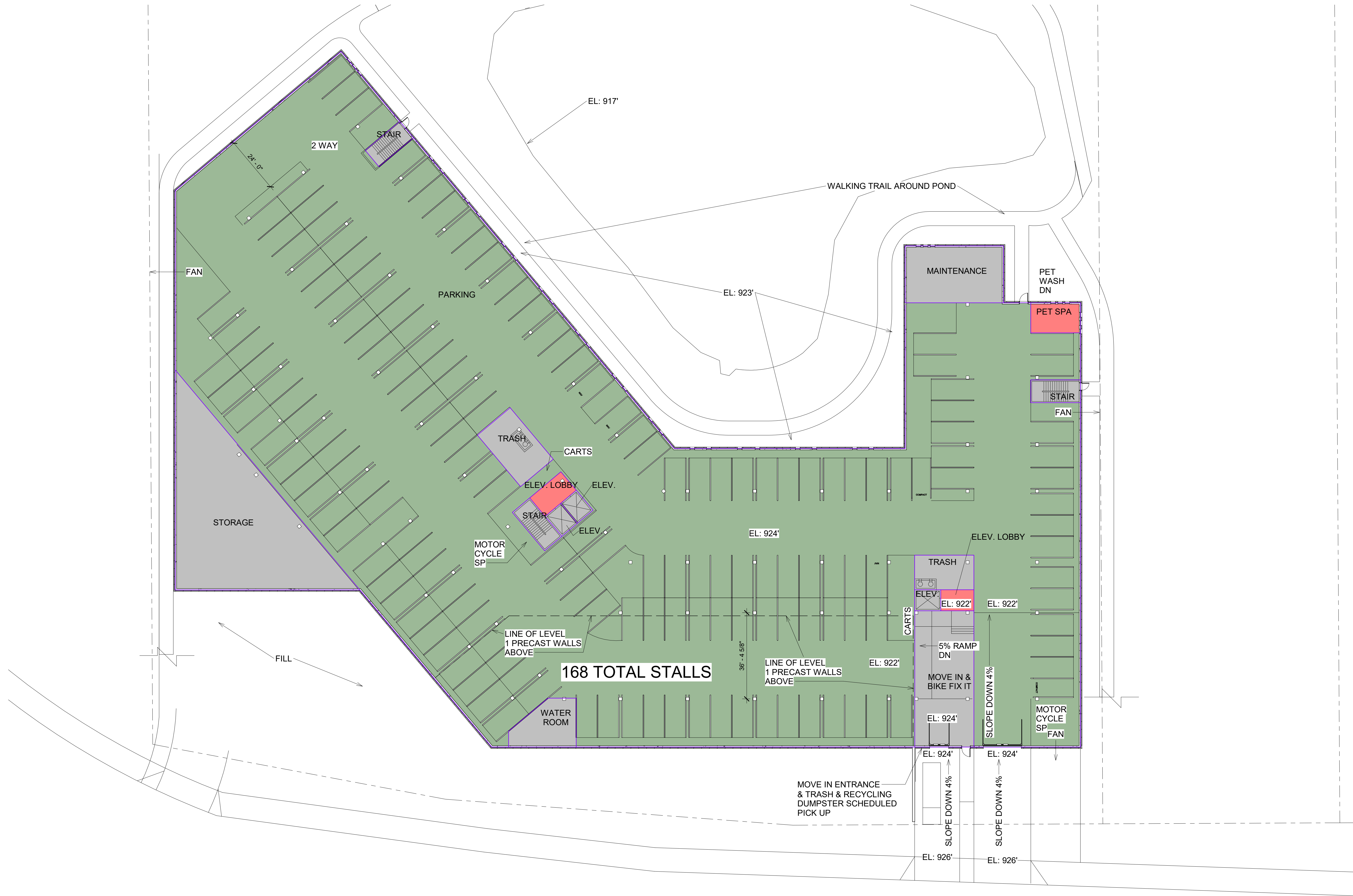
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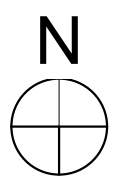
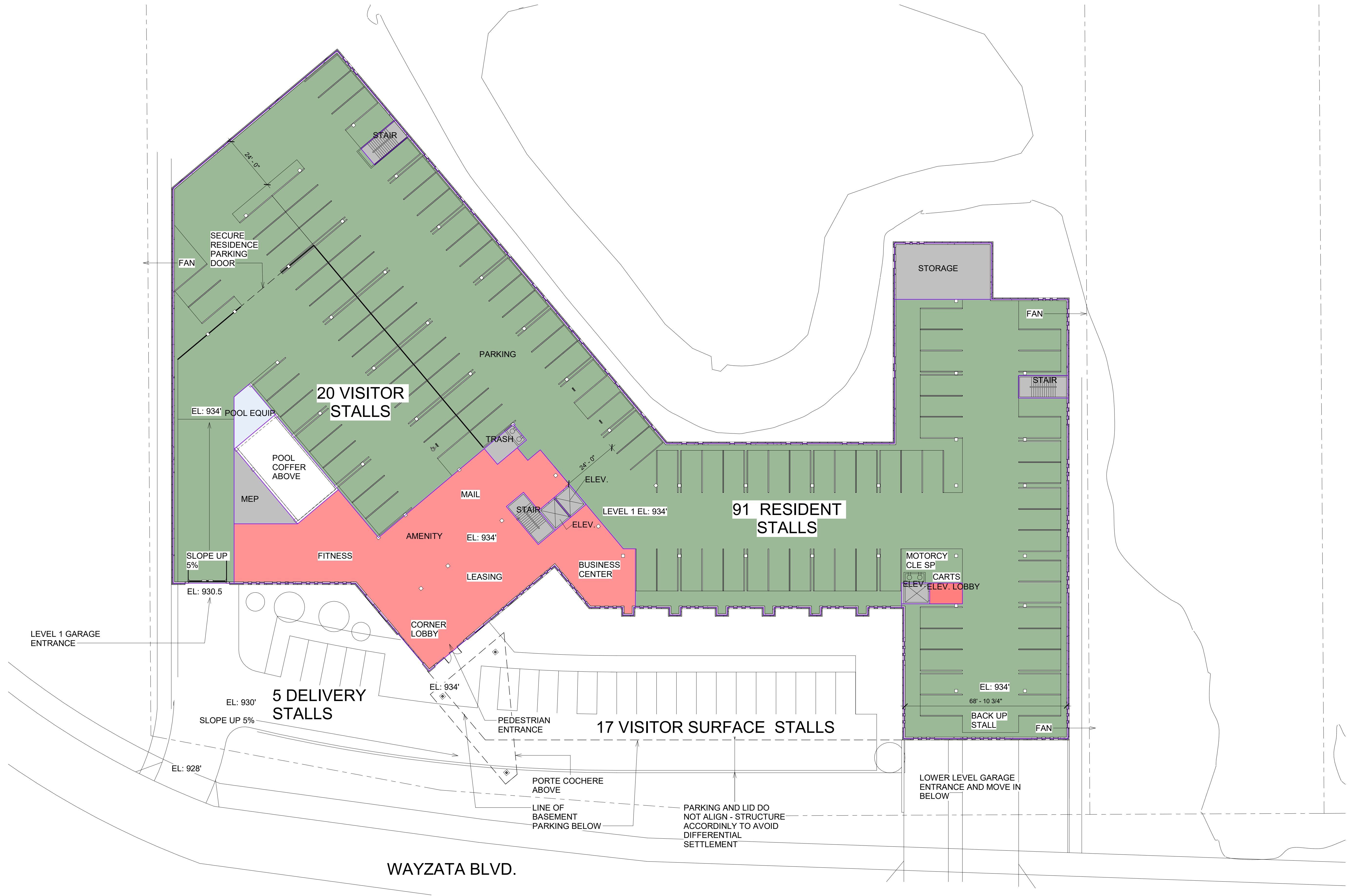


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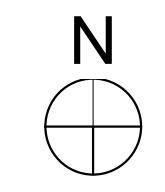
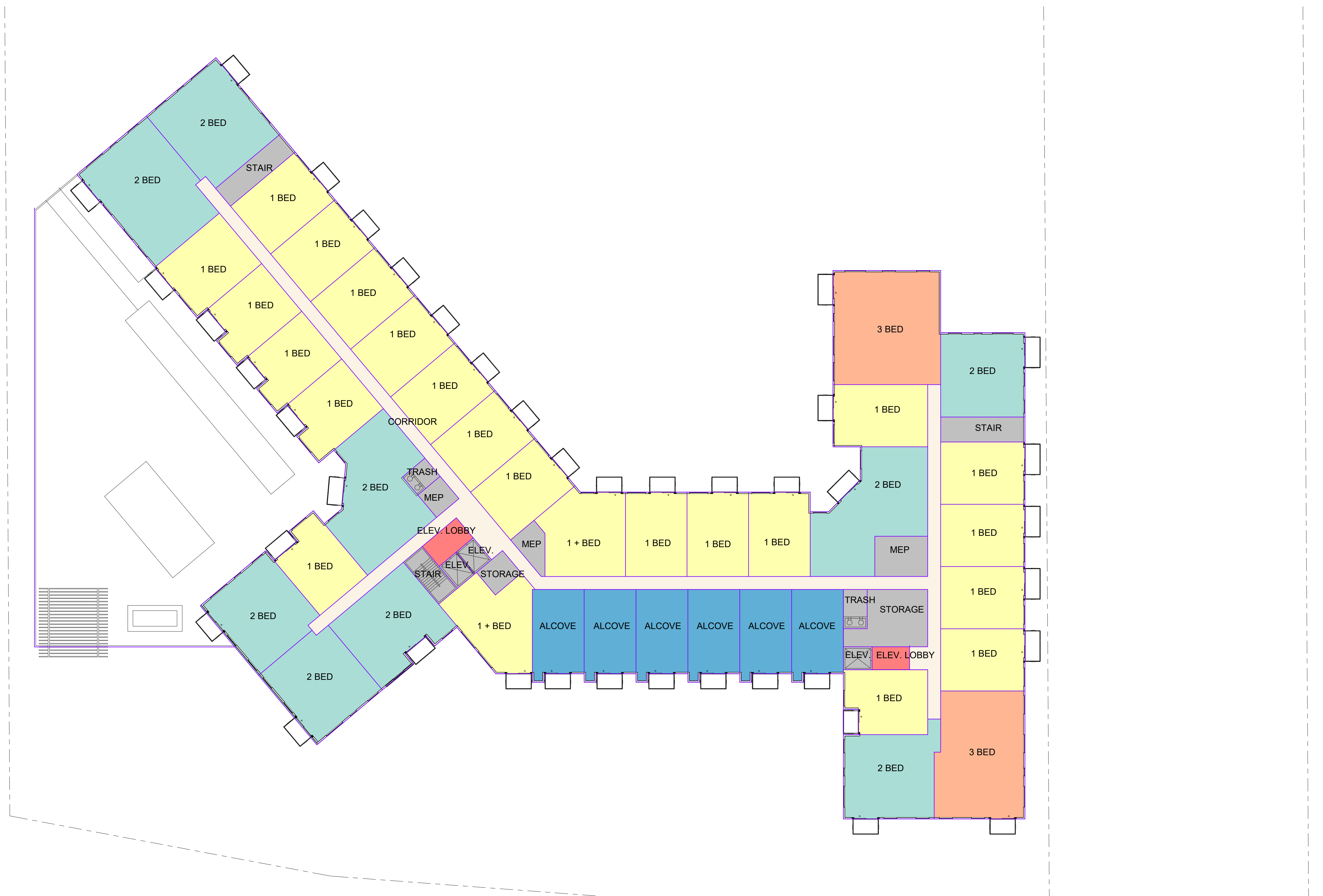
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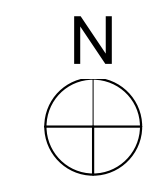
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12/16/2022 1:47:23 PM



12/16/2022 1:47:24 PM









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DORAN
ARCHITECTURE

MARSH RUN II REDEVELOPMENT

11816 Wayzata Blvd, Minnetonka

EXTERIOR RENDERING

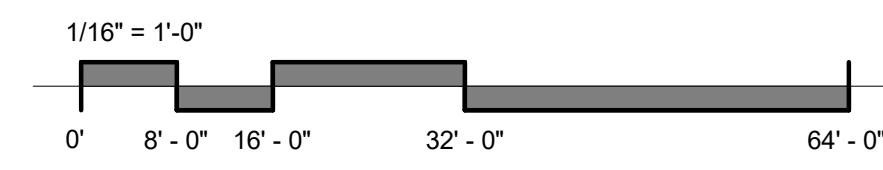
A 2.02



② WEST ELEVATION
1/16" = 1'-0"



① SOUTH ELEVATION
1/16" = 1'-0"



12/16/2022 1:47:28 PM



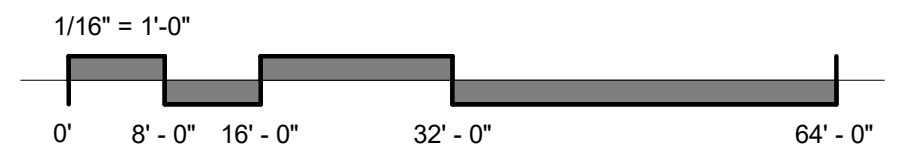
③ EAST ELEVATION
1/16" = 1'-0"



② NORTH ELEVATION
1/16" = 1'-0"



① NORTH ELEVATION - ANGLED
1/16" = 1'-0"



12/16/2022 1:47:31 PM

9 AM

12 PM

3 PM

6 PM

MAR / SEPT 21ST



JUNE 21ST



DEC 21ST



PORTION OF RESIDENCE LOWER LEVEL 1 IN SHADOW AT THIS TIME AT RED LINE

SHADOW MODEL 3D VIEW FROM ABOVE

12/16/2022 1:47:31 PM

12-8-2022

City of Minnetonka Planning Division-members:

Loren Gordon, Ashley Cauley, Drew Ingvalson, Brian Raines, Susan Thomas, and Julie Wischnack

City of Minnetonka Planning Commission-members:

Derrick Banks, Alex Hanson, Matt Henry, Amanda and, John Powers, Joshua Sewall, and David Waterman

Members I would like to thank you in advance for reading this letter. I write specifically regarding a newly proposed apartment building by Doran Development at 11900 Wayzata Blvd. I will break down this letter by addressing 3 specific areas relating to this project, the Birke, other apartment complexes in this area, and finally the proposed redevelopment project.

Birke:

At city council meetings to discuss the construction members of the Westridge Homeowners Association, the Gables Homeowners Association and the Bay Hill Condominium complex all voiced concerns with the size of the apartment and its appearance in our neighborhood along with traffic and parking problems. The original proposal was scaled down ever so slightly when it was discovered that the original heights would indeed infringe on the sun light that the southern most residents of the Gables would experience.

Simply put the gargantuan size of the Birke does not blend in with the neighborhood. The Birke kind of sticks out like a sore thumb. Somehow this project was approved to move forward without one single street level parking space available for tenants. The only street level parking spaces are 3 or 4 for guest parking, perhaps 3 for handicap parking, and a couple for future tenants.

This lack of proper parking availability has resulted in an overabundance of parked cars on the west side of Fairfield Rd with no regard for the mailbox or fire hydrants as they have proven their lack of adhering to laws requiring certain distances from the mailbox, fire hydrants as well as parking way too close to intersections in violation of those state mandated distances. In addition, often vehicles violate the city's no overnight parking regulation that unless it has changed, does not permit overnight parking on any street within the City.

Recently the city did approve requests to post no parking signs all along the west side of Fairfield Rd. this road is simply not wide enough for parked cars which when present do not allow safe room for north and south bound traffic when passing by them. While the situation has improved there are still far too many violators that completely disregard the no parking posting, often parking right next to those signs. The city has responded by issuing some citations to violators. Given the fact that the weather has turned cold enough to make the use of the pool on the second outside level of the Birke unusable, many of those visitors might not be coming for a visit as the pool is not in available. Thus, the parking violators might return when the weather warms once again.

We have heard the Birke's claim that they have plenty of inside parking for all their tenants and guests. We heard from a former tenant that the cost associated with purchasing an interior parking space has resulted in tenants taking up the guest parking areas as there has been little to no monitoring of the

interior parking. The cost of an interior parking space is supposedly \$150/month per car, a cost that many cannot afford or do not wish to incur. Having no surface parking within the boundaries of the lot means that these parking issues will likely continue.

The Birke advertised a pet park would be included as one of the amenities. What they have provided might be more accurately referred to as a pet cage. It does exist along the east side of the building. It is about 35-40 feet long and 5-6 feet wide with a 6 ft chain link fence on three sides and the building's exterior on the fourth side. Many of the tenants are seen walking their dogs with on a leash along the sidewalks at the Gables which, if they properly picked up and disposed of their pets droppings would not be a problem. Unfortunately, all too often they either do not pick up after their pet or some that simply drop the pet waste bag at the curb or under a pine tree.

Construction of the Birke was advertised to provide Luxury Apartments. Tenants who initially rented a unit were quick to leave when the lease was up due to the noise from adjacent units affecting their use of their apartment. One former tenant advised that he could hear each time that the tenant in the unit above him used their bathroom. He also mentioned that he could look up and pretty much follow the foot traffic of the tenant above hearing each step of the occupants above moving about. He felt that the walls, ceilings, and floors were paper thin and in no way constructed to be noise inhibiting.

At the initial meetings at the city where residents could ask questions, there was a concern with the possible removal of large mature trees along what the west side of this property's lot was. Doran representatives stated that they would not remove those trees. Those trees were removed during the early phases of the construction. Thus, their assurance to neighboring residents and the council proved to be false.

Comparable apartment complexes:

It is simply hard to understand just why the Birke and the new proposal need to be so high, except for them to have as many apartments as possible without regard to the neighborhood. At the initial open city meetings members of Doran claimed that it was not financially possible for them to scale down the apartment. However, there are plenty of apartments close that are not nearly as high or large. Here are a few:

The Luxe located next to the Ridgedale YMCA is a 4 story luxury apartment. Luxury to this building meant taller than normal ceilings, 2 by 6 walls instead of 2 by 4. Insulation between interior walls adjoining neighboring units, insulation within the floor levels, far better than construction grade interior finishes. I happen to know this to be the case as before they were very far along in the construction my wife and I met with the owner to learn more about these apartments.

The Avidor is a 55 and older complex built on the southwest corner of the Ridgedale shopping project. It has 5 levels.

The Ridgeway apartments is on the east side of the Ridgedale YMCA, and it has 3 apartment level floors and one that is a lower level below grade parking garage.

The 1700 apartment building on the west side of Plymouth Road at the Ridgedale area is another luxury apartment constructed with a roof top pool and has 5 levels for apartments and the ground level that

houses commercial spaces. I have personally been inside an apartment in this building and am certain that it qualifies as being luxurious.

There are two important features that each of these complexes possess. **First and most importantly, none of them are constructed near any residential housing.** Second, they all have adequate surface level parking for tenants and guests.

Doran's redevelopment proposal:

The project calls for razing the present 2 level office building and replacing it with a 6-story 190 unit apartment building. Their proposal includes approximately 260 interior parking stalls and an additional 40 street level parking spaces.

I attended the November 29th informational meeting they had at the Birke where they presented plans for the project and had their Director of Development and architect present along with others.

The land behind the current office building or to the north consists of a wetlands / marsh area that is home to many types of waterfowl and animals. It is a natural habitat that they live within. Included in this area are a pair of Trumpeter Swans that return every year to this area. All this wildlife and waterfowl are safe from being hunted and human traffic in this area. Just to the east of this office building is a grove of large trees that has a creek running through the middle of them flowing into a pond within the wetlands. This wetland provides the perfect type of habitat for Trumpeter Swans. As well as for the vast number of waterfowl and animals.

Doran's plan puts their building so close to the wetlands and this grove of trees making it pretty much impossible to avoid disturbing this habitat. One must realize that construction equipment will get even closer to and cause more harm to this natural environment. On their overview layout they marked down that they would not destroy any tree over 30 ft tall along the wetlands or the grove of trees. Based on their statement that they would not tear out trees along the Fairfield Road during the Birke construction and then turning around and removing those trees should be evidence enough that they have no problem in saying one thing and doing another. Maybe that was a construction cost factor but none the less, they proved themselves too not be honest.

The Doran plan also includes a walking path extending out around a small pond in the wetlands. Walking a dog in this area will certainly tend to disturb the current habitat.

That grove of trees pretty much blocks our view of the present office building acting as a much welcomed buffer of sound and view. The proposed project's height will eliminate that feature. Even smaller than 30 foot trees in height contribute to this buffer. The project as proposed will reach heights of well over 50 feet without considering the height of any roof top mechanical equipment that might be in their plans.

The current office building has been in its location for many years. In the early 1980's I was employed by a company who had an office in this building. We were on the lower level, east side all the way back toward the wetlands. This suite has a deck that overlooked the wetlands. My company moved from that location when they felt that rumors about this building gradually sinking downward made this location no longer suitable for their needs. If there was any truth in that fear, this 6 story apartment

complex would most certainly require the installation of pilings to support the structure which would likely cause more disruption to the area.

I live in the south building of the Westridge HOA. Our complex has 4 buildings, each facing one another in a rectangular format. Our largest building abuts the wetlands along our properties western property line. The land slopes rapidly from the back of this building to the wetlands such that walking behind this building is a chore. I mention this simply to emphasize that our residents do not infringe upon the habitat. It is an unfortunate reality but if this project is approved as presently presented, the sunsets that we all enjoy will be greatly diminished by a 50+ apartment building suddenly altering the views.

We realize that buildings eventually simply wear out their useful life or even usefulness for the present needs. Ever since Covid arrived we have noticed less vehicles parked around the present office building. With the workforce shifting to at homework and so many office buildings not being completely full the need for another office building in this area is perhaps gone.

We are wondering why a replacement should be a gargantuan 6 story apartment complex. This type of building would fit in nicely much closer to the West End area located west of 394 near Highway 100 as there are several apartment buildings of similar size and construction. This area however is not West End. This area is not a strictly commercial area like the West End area. While we offer objections to the size of this proposed building, a far greater concern is the effects such a project will have on the waterfowl and wildlife animals that call this habitat their home.

As a possible alternative, perhaps scaling down could be considered along with moving as close as possible to the frontage road and on the western corner of this property. This would enable the wetlands and present trees along the north and east sides of this area to remain protected. A scaled down version could leave plenty of room for surface parking and quite possibly leave an open area that could feature grass, planted trees and a walking area for tenants. A feature like this would provide a welcome area to pet owners providing a space for them to enjoy the outdoors without having to walk some distance either in a west or east direction along blacktop and cement that presently consumes all the areas outside of this property as presently it is adjacent to paved roads and a sidewalk.

As a featured amenity that no other apartment building has, perhaps even consider a couple of outdoor pickle ball courts. This is an ever growing sport enjoyed by all ages of participants. Provide something unique to your complex and that is pleasing to see instead of a huge box sticking out of the ground.

Please consider requiring changes to the present design. We are sure that from the inside of these large apartments that the view is enjoyable, however from the outside, not so much. Let's not turn this pristine area into a Westend. Westend is a great area, but it is completely a commercial development. Even the comparable apartment structures in our area are within commercial developments.

Thank you for taking the time to read my letter. I realized after receiving the Doran Nov 17, 2023, letter that due to a prior out of town engagement I will not be able to attend the City Council meeting on Jan 9, 2023. Upon discussions with several neighbors, I felt that we should make our concerns to your commissions, so they are available for you to have at the time of the Minnetonka Planning Commissions meeting on December 15th.

Happy Holidays to All,

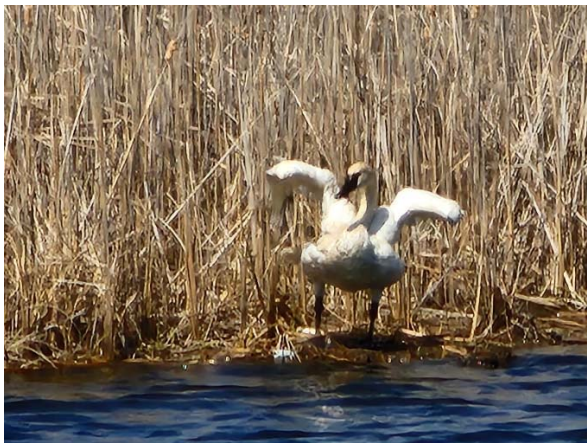
Charlie Ross

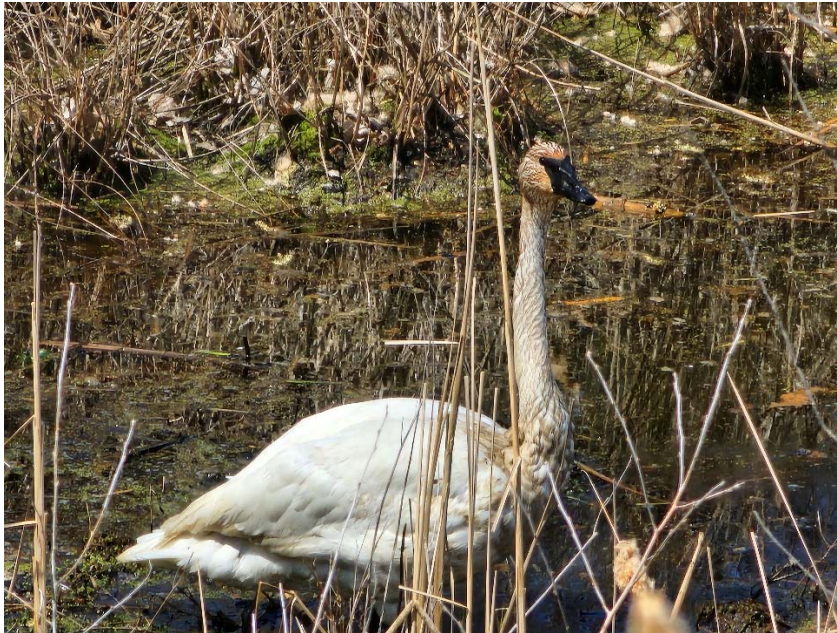
Westridge HOA President

992 Fairfield Ct

Minnetonka, MN 55305

[cross@theclaimstation.net](mailto:ross@theclaimstation.net)





Marsh Run II

Thursday, January 19, 2023 1:01 PM

Subject	Marsh Run II
From	Peter Vickerman
To	Bria Raines
Sent	Thursday, January 19, 2023 9:27 AM

Good morning Bria,

I was reviewing the plans for the Marsh Run II proposal and had a couple comments/questions:

- I generally support the new proposal as it will significantly increase the value of the property and thus provide additional tax revenue to the city, county, and school district. I note that the 2022 total tax for the property is \$68,152.04 whereas the 175-unit Birke is \$564,009.58. With more units proposed for Marsh Run II, it can be expected that the total tax will be even higher for this project.
- There appeared to be two site plans in the submission, one had one access to the frontage road and the other had two. From a pedestrian and bicycle standpoint, one access would be preferable.
- The current bike/ped path along the frontage road could use improvement.
 - This trail can serve as a great way for these future residents to get to businesses at Westridge Market and Ridgedale but is currently not a great experience.
 - Ideally the trail would be moved away from back of curb with street trees to better buffer users. If the trail is pulled back enough, a car waiting to turn could be between the street and the trail, limiting conflicts between drivers leaving the site and bikers and pedestrians.
 - The intersections and ramps could use improvement as well as there are currently large turn radii that increase the crossing distance.
 - The ramps also face 45 degrees into the frontage road (they are diagonal) vs just facing east/west since this is the only real movement anyone would do.

Thank you,

Peter Vickerman

11550 Oberlin Road

Re: Attached for the City Council Meeting 1.30.23

Tuesday, January 24, 2023 8:49 AM

Subject	Re: Attached for the City Council Meeting 1.30.23
From	Jim Reid
To	Sherry Weis; Bria Raines
Sent	Monday, January 23, 2023 3:36 PM

Correction - Updated attachment

On Mon, Jan 23, 2023 at 3:32 PM Jim Reid wrote:

Hello Sherry:

I would like to provide the following homeowner comments to the city, and hence, be part of the packet delivered to City Council for the next meeting on Monday January 30, 2023.

Please confirm the same.

City of Minnetonka, MN City Council Meeting Monday January 30, 2023 Doran - Marsh Run II Redevelopment

Coordination and Communication with the Neighborhood

- What database is the developer using to connect with the neighbors?
- What is the schedule for future meetings and discussions? (Unfortunately, It seems it does not include many neighbors that should be included)

Parking Planning and Management

The developer has indicated, at the recent planning commission meeting, they are planning an added double decked parking lot along Fairfield Road and to be added to the retained East Building, where are those renderings and plans detailing it?

RE: That revised parking space:

What will be the total number of available spots be, once it is completed?

Are those same number of spots intended to serve as surface parking for all of the following?:

1. The full compliment of all tenants and visitors in the remaining east office building on Fairfield Road?
2. Plus: Be the sole surface parking for the entire Marsh Run II Redevelopment at 11816 Wayzata Blvd and it's reported nearly 200 units?
3. And: provide for the historically proven needed surface parking for the 175 Birke Units?
4. In addition: Serve Pool and General Visitors to the 175 Birke units/residents?
5. What steps are planned to prevent parking on Wayzata Blvd, as has occurred on Fairfield Road?
6. What have been the parking ratios pre Birke (tenant/parking space) and what will they be with the added 375 units?

Density

The developer has plans to only develop the same footprint for Marsh Run II that currently exists, although, isn't that ultimately already limited by the scope of their purchase?

Environmental Impact

The developer plans to add a new trail to the north of the Marsh Run II Redevelopment for residents to walk their dogs, etc.

1. What impact will that cause on the pre existing and current habitat in that same area?
2. What is the land acreage to building square footage ratios of the current Birke site as well as the proposed new Marsh Run II? In order to provide density comparisons.
3. What were the same commercial development acreage ratios prior to both of these Doran developments?
4. What traffic studies (before and after) have been done for the Birke and Marsh Run II, projecting out the completed 375 new units?
5. What noise abatement studies (before and after) have been done for the Birke and Marsh Run II, projecting out the completed 375 new units?
6. What water flow impact studies (before and after) have been done for the Birke and Marsh Run II, projecting out the completed 375 new units?
7. What new landscaping and added green space is planned for the Marsh Run II development?
8. What land mass areas and residential areas will no longer have sun and now be shaded, when, where and for how long?

Others

Given the plans to pretty much duplicate the look of the Birke into Marsh Run II, could that result in a new domineeringly large (375 Units) apartment campus complex in the midst of a long established residential area?

Since, Doran has already purchased this area, what was it's intentions if development of this scale was not an option?

When do the leases terminate in the East Building? What are the developers plans for when those leases end? Might there be further development? If so what and how much?

What's been the history of the occupancy rates thus far at the Birke?

What is the financial demographic resident profile for the Birke vs actual and in the planning of Marsh Run II?

Sincerely and Best,
Jim Reid

Homeowner 1000 Fairfield Spur Minnetonka, MN 55305

From: [Fiona Golden](#)
To: [Fiona Golden](#)
Subject: Marsh Run II - Concept Plan
Date: Wednesday, January 25, 2023 8:56:42 AM

From: Sara Maloney
Sent: Monday, January 23, 2023 9:38 PM
To: Loren Gordon <lgordon@minnetonkamn.gov>
Cc: Julie Wischnack <jwischnack@minnetonkamn.gov>
Subject: Marsh Run II - Concept Plan

Hello Loren:

Per our phone conversation today, please include this email in the packet for the City Council meeting scheduled for January 30, 2023.

Attention Mayor Wiersum; City Council Members; and City of Minnetonka Planning Officials:

RE: Marsh Run II – Concept Plan

I live in the Gables of Westridge, a community of 104 townhomes, located on the east side of Fairfield Road and directly north of the Birke. We are one of three (3) common interest communities in the neighborhood. There are 20 townhomes in the Westridge association located on the west side of Fairfield Road, and Bayhill Condos, consisting of 66 condo units, is directly north of the Westridge, also on the west side of Fairfield Road.

I attended the neighborhood meeting on November 29, 2022, and the Planning Commission Meeting on January 19, 2023. Please consider watching the planning commission meeting because some of my neighbors spoke on issues that have affected many of us. These issues began once the Birke opened, and we are concerned another high-density building will compound those issues. Most involve additional traffic and parking, but we're also concerned about the wetlands and how a new building, even larger than the Birke, will harm the environment and wildlife. I hope we can work together to resolve the current issues before considering another high-density building.

Parking is one of the greatest concerns since the Birke residents and guests have been parking on Fairfield Road. It's too narrow for vehicles to pass in both directions if vehicles are parked on one side. Apparently, guests are reluctant to park in the underground ramp, so it's important the Birke make a greater effort to urge their guests to stop parking illegally on Fairfield Road, or in private parking areas in the neighborhood associations.

It's not just guests, but also residents who do not use the underground ramp. Doran stated the Birke charges tenants for an additional parking space, and some residents do not want to pay the extra fees. I think they need to consider a policy change since there is no option for legal off-street parking in the area, unless they are allowed to park 24/7 in the commercial parking lot on the west side of Fairfield Road.

Some of the planning commissioners suggested the Birke commit to using the commercial parking lot as a long-term parking solution. They also suggested the Birke be more proactive in educating their residents and guests. Both of those solutions would be helpful. I also wonder if it's possible for them to add additional street level parking on the south side of the Birke off Wayzata Boulevard.

We also need to work on a plan to slow the traffic on Wayzata Boulevard. It's especially tricky near the entrance to 394. Vehicles travelling west on Wayzata Boulevard, with the intention of taking a left onto the 394-entrance ramp, have difficulty seeing oncoming traffic. If another large building is added with an

entrance close to the 394-entrance ramp, this will be more of a hazard. It's also dangerous to bike on Wayzata Boulevard, so a dedicated bike path would be a great addition.

Finally, I'm concerned about having such a high-density project so close to the wetlands. It's important to many of us to preserve the natural environment and assure us that extra care be taken both during and after construction. The commissioners addressed a sun/shadow study and stated the building as proposed will cast a shadow on the nearby townhomes, and they'd need to lower it. I also would like to know if the lack of sun will affect the vegetation in the wetlands. I'd also urge Doran to incorporate more native plants to a new building as well as the Birke.

Thank you in advance for reading my concerns and please contact me directly if you have any questions.

Sara Maloney

- He was more concerned with the density in relation to the number of units per buildable area. Using the entire property to calculate density is misleading.
- He encouraged the applicant to keep working with neighbors.

Chair Sewall called for a brief recess.

B. Concept plan review for Marsh Run II Redevelopment at 11816 Wayzata Blvd.

Chair Sewall introduced the proposal and called for the staff report.

Raines reported. Staff recommends commissioners provide feedback on the key topics identified by staff and any other land-use-related items that commissioners deem appropriate. This discussion is intended to assist the applicant in the preparation of more detailed development plans.

Tony Kuechle, Doran and Co., applicant, stated that:

- The applicant plans on acquiring both properties. The concept plan only covers 11816 Wayzata Blvd. That acquisition would help with the neighborhood's lack of parking.
- The design of the building would be similar to The Birke.
- Neighbors expressed issues with a lack of parking, mass of the building, the trail disrupting natural habitat and the number of units.
- The proposal would cover the existing footprint.
- Three options were provided for exterior materials to receive feedback.
- The current buildings have been deemed blighted.
- The applicant would apply for TIF funds.
- The applicant is discussing with staff having ten percent of the units meet 80 percent area-median-income (AMI) affordable-housing requirements.
- The applicant supports prohibiting parking on Fairfield Ave. The applicant is willing to cover the cost of the city adding more "no parking" signs.
- The property the applicant purchased on the east could be assigned parking from 5 p.m. to 8 a.m.
- Raines did a great job explaining the concept plan.
- He was available for questions and looking forward to receiving feedback.

Powers asked Mr. Kuechle if he would have done anything differently on The Birke project. Mr. Kuechle answered that he would double the amount of surface parking. The internal, heated, dry visitor parking is not being used. The units maintain 96 percent occupancy.

Powers asked how many electric vehicle chargers would be provided. Mr. Kuechle responded that 10 percent of the stalls would have electric chargers right away and the

facility would have the ability for 20 percent of the stalls to provide electric chargers if needed in the future.

In response to Henry's question, Mr. Kuechle noted that the wetland is a man-made wetland and the applicant would agree to not install the trail if that is what neighbors and the city would prefer.

In response to Maxwell's question, Mr. Kuechle explained that the concept plan would raise the ground area of the office building about two feet. When the building would be excavated, a clay bathtub would be created to prevent water coming in. The lake to the south determines the underwater groundwater level. The developer learned that through development of the Birke; found an effective way to fix that issue with the Birke; and would do the same fix for the concept plan proposal. The building would sit two feet above the ordinary-high-water level.

In response to Waterman's question, Mr. Kuechle explained how restrictions had to be put in place to prevent non-tenants and non-guests-of-tenants of the Birke from parking vehicles for extended periods of time in the free, underground parking area. The proposal would have more surface parking available for tenants and an additional parking lot available for after-business-hour parking.

Chair Sewall confirmed with Mr. Kuechle that there would be receptacles available to collect dog waste.

The public hearing was opened.

Jane Gordon, a resident of Bay Hill Condominiums, stated that:

- She was glad that the proposal would replace an unattractive building and make the area more attractive.
- She is representing many residents of Bay Hill Condominiums.
- When a vehicle parks on Fairfield Road, it becomes a one-lane road.
- She requested more parking signs be installed that state "no parking" instead of signs that have a circle and slash around a "P."
- She called the police when seven vehicles were parked on the street and a police officer ticketed the vehicles.
- She would like the amount of parking for the proposal repeated before the end of the meeting.
- Residents of the Birke use parking stalls that belong to Bay Hill Condominiums and the office building because they do not want to pay for underground parking at the Birke.
- She is concerned that there would not be enough parking stalls available for the proposal's tenants.
- The left turn to get to I-394 is very dangerous since it is an uncontrolled intersection and the sight-line is obstructed.

- She supports new residents joining the area, but wants to make sure that there would be enough parking and traffic hazards would be fixed.
- She was available for questions.

Mr. Kuechle stated that:

- He agrees that there needs to be more “no parking” signs on Fairfield Road that make it clear that no parking is allowed the entire stretch of the street and not just the spots in front of the signs.
- He explained that the Birke has 1.4 parking stalls per living unit. The concept plan would have 1.52 stalls per living unit. He agreed that there is currently a parking issue that he is working to solve.

Pam Lewis, 980 Fairfield Court, stated that:

- The building is not attractive, but the neighborhood behind it has a beautiful wetland area. There is a lot of wildlife including deer and swans.
- She is still adjusting to the addition of the Birk. It is a giant building.
- She had predicted that visitors, Uber drivers and door-dash drivers would not want to use underground parking stalls. She appreciated the applicant acknowledging the problem, but the neighbors are stuck living with the problem.
- There is still a problem of vehicles being parked on Fairfield Road and in the guest parking stalls.
- Dog walkers walk through the neighborhood to Overland Park and leave bags of dog poop along the road.
- An even bigger building would exasperate the parking problem.
- She understands that housing and density make sense at the proposed location.
- She would like the grove of trees preserved to provide a buffer of her view of the site; provide privacy; and provide habitat for wildlife.
- A left-turn lane to I-394 and bike lane would make the area safer. Bicyclists currently travel on the sidewalks.
- There would be more of a parking shortage when the office building site would be redeveloped in the future.
- The proposed building would add too much density.

Charlie Ross 992 Fairfield Court, stated that:

- There is a lot of wildlife including deer, swans and birds that he fears would disappear.
- No trees should not be cut down because they provide a buffer between the proposed building and the neighborhood.
- The trees on the boulevard were removed when the Birke was built.

- There are three “no parking” signs on Fairfield Road. He counted 10 vehicles parked illegally on the street last night.
- There are no bags or dog waste receptacles on Fairfield Road.
- The proposed building would be too gargantuan. Similar huge apartment buildings in other communities are not located in residential areas.
- He asked for the acreage of the site.
- He did not understand why this building would be so huge.

No additional testimony was submitted and the hearing was closed.

Mr. Kuechle stated that:

- The site is 4.03 acres in size.
- The site would require less grading than the Birke which would reduce the amount of tree removal.
- The Birke site did not remove any trees that were not approved for removal by the tree protection ordinance and approved in a tree removal plan.
- A tree preservation plan and tree removal plan would be submitted if a formal application is submitted to the city.
- A tree survey would be completed if the applicant moves forward with submitting an application.
- A grove of trees located between the two buildings is located in a wetland, so they are not allowed to be removed.
- Pet waste bags are available for anyone on the Birke property, but the applicant cannot legally locate them on someone else’s property or in a public-street right of way. He would be happy to provide bags to the neighbors.
- The concept plan would have 43 surface parking stalls. The Birke has 7 surface parking stalls.

Maxwell stated that:

- She appreciated that the footprint of the proposed building would align with the existing footprint to prevent a large amount of impervious surface from being added so close to the wetland.
- She favors decreasing the height of the building one story to help the neighbors be more comfortable with the mass of the building.
- The site is a reasonable place to have a multi-family building with its location on a frontage road.
- She would prefer a building with a little more design character.
- She supports keeping the trail to help keep dog walkers off of the road.

Hanson stated that:

- He enjoyed his tour of the Birke. He appreciates that the developer has been transparent and has learned from the mistakes done with the Birke.
- It was quiet in the Birke. He assumed the proposed building would be made of the same quality.
- He would like to learn the apartment sizes that would be available in the proposed building.
- He likes the trail around the building and would like it to be pervious like gravel or limestone.
- He encouraged the applicant to educate the residents on parking regulations.
- He suggested the applicant keep neighbors informed on the status of the buffer located between the proposed building and the neighbors.

Powers stated that:

- He was happy that the proposal would have more surface parking than the Birke.
- He does not like the appearance of the Birke, but he is glad Doran would create a complimentary building.
- He suggested that the office building parking lot be made a permanent parking solution.
- He loves the trail for walking dogs.
- Residents need to be taught what to do to be good neighbors.
- He would like 20 percent of the units to be affordable at 80 percent AMI.

Henry stated that:

- He appreciated the comments from neighbors and the applicant responding to the comments.
- The building would be too massive. He would like something with more character that would complement the area.
- He suggested moving the pool so it would have more sun.
- The uncontrolled intersection to reach I-394 is dangerous and needs to be addressed.
- The east parking lot should be a long-term solution.

Waterman stated that:

- He supports the proposal. It is a nice idea.
- He likes that the existing footprint would be utilized.
- He likes the third rendering of the building the best to help decrease the view of the mass. It is large to look at from the north to the south.
- He likes the dog run and trail.
- He encouraged the applicant to provide 20 percent of the units at 80 AMI.
- He looks forward to seeing an application in the future.

- He would like the parking lot on the east to be a permanent parking solution.

Powers stated that:

- He would like the developer to utilize the dog run area to also attract birds and other wildlife.

Chair Sewall stated that:

- He liked that the proposed building would use the same footprint as the existing building to prevent more impervious surface from being added.
- Not having to change the zoning is helpful.
- He supports the trail and would like the trail to be a continuous loop.
- He likes that the proposal would have more surface area parking than the Birke.
- He invited residents to call police when witnessing parking and pet waste violations.
- The mass of the building would be too large. The shadow would get too close to the existing condominium building. People do not own a view, but it is unreasonable for one building to cast a shadow over another property. That is the biggest challenge for the proposal.
- He likes this site better than the Birke's site.
- He looks forward to seeing the process move forward.

The city council is scheduled to review the concept plan at its meeting on Jan. 30, 2023.

9. Adjournment

Hanson moved, second by Powers, to adjourn the meeting at 9:46 p.m. Motion carried unanimously.

By: _____
Lois T. Mason
Planning Secretary

City Council Agenda Item 14B
Meeting of Jan. 30, 2023

Title: Concept plan for Ridgewood Road Villas located at 18116 Ridgewood Road

Report From: Ashley Cauley, Senior Planner

Submitted through: Mike Funk, City Manager
Loren Gordon, AICP, City Planner

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

Ron Clark Construction has submitted a concept plan that contemplates the removal of the existing home and accessory structures for the construction of 13 one-story, villa-style homes. The roughly 23-acre site is located at 18116 Ridgewood Road and includes an adjacent, unaddressed parcel.



Figure 1: Existing Conditions



Figure 2: Concept

Recommended Action

Provide feedback intended to assist the applicant in preparing more detailed development plans. No formal action is required.

Strategic Profile Relatability

- | | |
|--|--|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input checked="" type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input type="checkbox"/> Community Inclusiveness |
| <input checked="" type="checkbox"/> N/A | |

Financial Consideration

Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Concept Plan Review Process

The staff has outlined the following Concept Plan Review process for the proposal. At this time, a formal application has not been submitted.

- **Neighborhood Meeting.** A neighborhood meeting was held on Jan. 19, 2023. Seventeen people were in attendance. Residents asked questions about the access, price points of the homes, construction timelines, and site logistics (particularly during construction). They also raised concerns and commented about:
 - ✓ Diversity in the housing style and design within the development.
 - ✓ Cohesive and complementary building materials to the Marshes of Meadowwoods development.
 - ✓ Stormwater, snow storage, tree preservation, and wetland protections.
 - ✓ Buffering to mitigate for headlights at the intersections and encouraged stewardship responsibilities within the development.
- **Planning Commission Concept Plan Review.** The commission reviewed the conceptual plan on Jan. 19, 2023. Five residents appeared to address the commission. Commissioners encouraged the “spirit” of Marshes of Meadowwoods to be incorporated into the development. They expressed concerns related to price point, density, and stormwater. They questioned if the villa-style housing type was enough of a public benefit to support rezoning to a planned unit development (PUD), zoning.
- **City Council Concept Plan.** The city council concept plan review is intended as a follow-up to the planning commission meeting and would follow the same format. No staff recommendations are provided, the public is invited to offer comments, and council members can ask questions and provide feedback without formal motions or votes.

Key Topics

Staff requests council provides feedback on the following key topics:

- **Number of lots:** The concept plan suggests constructing 13 one-story, villa-style homes. Access to eight lots would be via a new, public cul-de-sac. The remaining lots would be accessed via a series of shared drives and driveways.
- **Housing type:** The concept plan suggests the construction of villa-style, association-maintained houses. The homes range between 2,800 and 3,500 square feet, with two-car garages. The city’s Affordable Housing Policy does not require the inclusion of affordable units in detached-for-sale housing developments.
- **Access:** Marshes of Meadowwoods, west of the site, were approved in 2003. A road and utility easement was secured in the area between Lindsey Lane and the eastern property line. This easement was secured to allow the future connection to the property under contemplation.

- **Other Considerations:** What other items would the council like to comment on?

Next Steps

- **Formal Application.** If the developer/applicant chooses to file a formal application, notification of the application will be mailed to area property owners. Area property owners are encouraged to view plans and provide feedback via the city's website. Through recent website updates: (1) staff can provide owners with ongoing project updates; (2) owners can "follow" projects they are particularly interested in by signing up for automatic notification of project updates; (3) owners may provide feedback on the project; and (4) staff can review resident comments.
- **Council Introduction.** The proposal would be introduced at a city council meeting. At that time, the council would be provided another opportunity to review the issues identified during the initial concept plan review meeting and provide direction about any refinements or additional issues they wish to be researched and for which staff recommendations should be prepared.
- **Planning Commission Review.** The planning commission will review and subsequently make a recommendation to the city council on land use matters.
- **City Council Action.** The city council would take final action based on input from the planning commission, professional staff, and the general public.

Since the Planning Commission meeting:

Comments were received after the planning commission meeting. Those comments are attached.

MINNETONKA PLANNING COMMISSION
Jan. 19, 2023

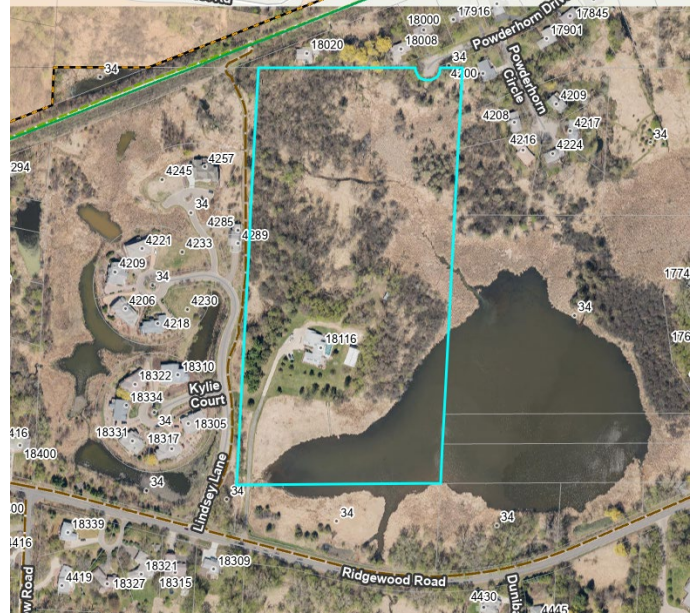
Brief Description	Concept plan for Ridgewood Road Villas at 18116 Ridgewood Road
Action Requested	Discuss the concept plan with the applicant. No formal action is required.

Background

The property at 18116 Ridgewood Road is roughly 19.6 acres in size. The property is held in common ownership with an adjacent, 3.43-acre, unaddressed parcel.

A substantial amount of the properties are encumbered by wetlands and associated floodplain. The staff has received a wetland delineation for the property but has not yet received a detailed survey or conducted a detailed analysis of the property (these occur at the time of a formal application review).

The properties are zoned R-1, low-density residential, and designed for low-density development in the 2040 Comprehensive Guide Plan.



Concept Plan

Ron Clark Construction has submitted a concept plan that contemplates the removal of the existing home and accessory structures for the construction of 13 one-story, villa-style homes. A formal proposal based on the concept plan would likely require the following city actions: (1) rezoning from R-1, low-density residential to PUD, Planned Unit Development; (2) adopting a master development plan; and (3) preliminary and final plats.

Review Process

The staff has outlined the following review process for the proposal. At this time, a formal application has not been submitted.

- **Neighborhood Meeting.** The developer is holding a neighborhood meeting before the Jan. 19th planning commission meeting. A summary of the meeting will be provided at the planning commission meeting.
- **Planning Commission Concept Plan Review.** The planning commission Concept Plan Review is intended as a follow-up to the neighborhood meeting. The objective of this meeting is to identify major issues and challenges in order to inform the subsequent review and discussion. The meeting will include a presentation by the developer of conceptual sketches and ideas but not detailed engineering or architectural drawings.

No staff recommendations are provided, the public is invited to offer comments, and planning commissioners are afforded the opportunity to ask questions and provide feedback without any formal motions or votes.

- **City Council Concept Plan Review.** The city council Concept Plan Review is intended as a follow-up to the planning commission meeting and would follow the same format as the planning commission Concept Plan Review. No staff recommendations are provided, the public is invited to offer comments, and council members are afforded the opportunity to ask questions and provide feedback without any formal motions or votes.

Key Issues

Staff requests commission and council comment/ feedback on the following key issues and any other issues the commission and council deem appropriate. The comments/ feedback provided are intended to assist Ron Clark Construction should the company choose to put together a formal application package. However, the commission and council decisions on any formal redevelopment application are not suggested or restricted by concept plan review comments or feedback:

- **Number of lots:** The concept plan suggests the construction of 13 one-story, villa-style homes. Access to eight lots would be via a new, public cul-de-sac. The remaining lots would be accessed via a series of shared drives and driveways. The conceptual density would be 2.7 units per acre. Comments related to the number and configuration of lots are requested.
- **Housing type:** The concept plans suggest the construction of villa-style, association-maintained houses. The homes would range between 2,800 and 3,500 square feet, with two-car garages. Comments related to the general neighborhood concept are requested.
- **Access:** Marshes of Meadowwoods, west of the site, was approved in 2003. A road and utility easement was secured in the area between Lindsey Lane and the eastern property line. This easement was secured to allow the future connection to the property under contemplation. Comments related to the conceptual access are requested.

Staff Recommendation

Staff recommends the planning commission provide comments and feedback to assist the applicant with future direction that may lead to the preparation of more detailed development plans.

Originator: Ashley Cauley, Senior Planner
Through: Loren Gordon, AICP, City Planner

ADDITIONAL INFORMATION

Next Steps

- **Formal Application.** If the developer chooses to file a formal application, notification of the application will be mailed to area property owners. Property owners are encouraged to view plans and provide feedback via the city's website. Through recent website updates: (1) staff can provide residents with ongoing project updates, (2) residents can "follow" projects they are particularly interested in by signing up for automatic notification of project updates; (3) residents may provide project feedback on the project; and (4) and staff can review resident comments.
- **Council Introduction.** The proposal would be introduced at a city council meeting. At that time, the council would be provided another opportunity to review the issues identified during the initial Concept Plan Review meeting and to provide direction about any refinements or additional issues they wish to be researched and for which staff recommendations should be prepared.
- **Planning Commission Review.** The planning commission would hold an official public hearing for the development review and would subsequently recommend action to the city council.
- **City Council Action.** Based on input from the planning commission, professional staff, and the general public, the city council would take final action.

City Roles and Responsibilities

- **City Council.** As the ultimate decision maker, the city council must be in a position to equitably and consistently weigh all input from their staff, the general public, planning commissioners, applicants, and other advisors. Accordingly, council members traditionally keep an open mind until all the facts are received. The council ensures that residents have an opportunity to participate effectively in the process.
- **Planning Commission.** The planning commission hosts the primary forum for public input and provides clear and definitive recommendations to the city council. To serve in that role, the commission identifies and attempts to resolve development issues and concerns prior to the council's consideration by carefully balancing the interests of applicants, neighbors, and the general public.
- **City Staff.** City staff is neither an advocate for the public nor the applicant. Rather, staff provides professional advice and recommendations to all interested parties, including the city council, planning commission, the applicant, and residents. Staff advocates for its professional position, not a project. Staff recommendations consider neighborhood concerns but necessarily reflect professional standards, legal requirements, and broader community interests.



LOCATION MAP

Project: Ron Clark Construction
Location: 18116 Ridgewood Road





7500 West 78th Street
Edina, MN
55439

(952) 947-3000
fax (952) 947-3030

Monday, December 19, 2022

Loren Gordon
City of Minnetonka
14600 Minnetonka Blvd
Minnetonka, MN 55435

RE: Ridgewood Road Villas

Ron Clark Construction is proposing a 13-unit Villa home neighborhood on the existing property located at 18116 Ridgewood Road.

The proposed single family home neighborhood would have thirteen (13) one story villa homes. Each home will have a 2-car garage with additional parking for 2 cars in each driveway, including 7 guest parking stalls. There is also additional guest parking along the public street and in the cul-de-sac.

The overall goal is to work in balance with the existing natural elements of the site while adding new housing to create a small neighborhood that adds to the city inventory. The One Level design is of high demand for existing Minnetonka residents that are looking for a ‘Lifestyle’ option while still staying in Minnetonka. This also opens their existing homes to young families looking to raise their children while attending Minnetonka schools.

The first step in this process is to determine if the city agrees that changing the zoning for this parcel to a low-density residential PUD is a good use for the property. We feel strongly that it does by creating new housing, improving water quality going into the adjacent wetland and the Riley Bluff Creek Watershed District, creating value for the existing area, and increasing the tax base.

Planning

The proposed development plan is for thirteen (13) single family Villa homes located on a new public road that will connect to the existing public road (Lindsey Lane) on the West boundary of our site. Utilizing a new access point for the site will allow for the existing driveway to be removed and returned to natural buffer for the existing wetland.



MN Builder License # 1220
www.RonClark.com
www.RonClark.com

When the Marshes of Meadowood project was approved by the City and County, the County required a road and utility easement be created across all of Outlot C, this is where we are creating our access to the public street Lindsey Lane.

Zoning: The current zoning for the site is R-1 and is occupied by a single-family home and various outbuildings. The proposed zoning would be requesting the property be rezoned to a PUD.

Housing and Architecture: The single family Villa homes will be planned and designed to fit into the existing neighborhood but also will add a new style of housing not currently in the neighborhood. We are planning a mix of home designs that range between 2800 and 3500 square feet. The home designs will offer two car-plus garages, sunrooms, porches, and decks with a variety internal floorplans and exterior styles. The overall Architecture is still in the development stage but if you look at the neighborhoods, of our active projects you will get a feel of the quality of the homes and the variety of the exterior designs. We are committed to energy efficient building practices as part of the Minnesota Green Path Builder program.

Site Design: The lots are being configured on the 23.99-acre property to create an opportunity to preserve the majority of existing trees and take advantage of the beautiful wetland and wooded views around the boundaries of the site.

We have investigated and documented the wetlands and existing trees on the site. We will provide complete reports and tree preservation plans for future applications.

Stormwater Management: Stormwater runoff from the site will be collected and routed to the new biofiltration basin created in the middle of the site as both a functional and esthetic amenity to store and filter water into the soils below it. The system will be designed to meet the requirements of both the City and the Watershed District.



7500 West 78th Street
Edina, MN
55439

(952) 947-3000
fax (952) 947-3030

Home Owners Association: A Home Owners Association will be planned for the neighborhood. The association will maintain each homes lawn care and snow removal along with maintain any common area landscaping.

Development Team:

Project Architect – Whitten Associates, Inc. – Tim Whitten and Heather Maanum

Project Engineer – Champion Engineering - Marty Champion

Developer – Ron Clark Construction – Mike Waldo



MN Builder License # 1220
www.RonClark.com
www.RonClark.com



SITE PLAN
SCALE: 1" = 50'-0"

12.19.22



AERIAL SITE PLAN

12.19.22



Doc No 8492392 12/15/2004 04:30 PM
Certified filed and or recorded on above date:

Office of the County Recorder
Hennepin County, Minnesota

Michael H. Cunniff, County Recorder

Deputy 7

TransID 81145

Fees

\$15.00 DOC

\$4.50 SUR

\$19.50 Total

2004 AND PRIOR TAXES PAID
TAXPAYER SERVICES
TRANSFER ENTERED

DEC 15 2004

HENNEPIN COUNTY MINN.
[Signature]
DEPUTY

ROAD AND UTILITY EASEMENT

Custom Home Bldrs Title

THIS ROAD AND UTILITY EASEMENT is given on November 19, 2004 by Meadow Woods LLC, a Minnesota limited liability company ("Owner"), to the CITY OF MINNETONKA, a Minnesota municipal corporation (City"), in accordance with the following:

1. **Ownership.** Owner is the fee owner of the property legally described on attached Exhibit A ("Property").

2. **Grant of Easement.** For valuable consideration, Owner conveys to the City an easement for utilities and public right-of-way purposes ("Easement") over, under, and across the real property in Hennepin County, Minnesota, legally described on attached Exhibit B (Easement Area) and depicted on attached Exhibit C.

3. **Scope of Easement Rights.** The Easement includes the right of the City, its contractors, employees, agents and assigns to:

- a. locate, construct, reconstruct, operate, maintain, inspect, alter and repair within the Easement Area a public roadway, storm sewer, sanitary sewer and water facilities, ground surface drainage ways and sidewalks, or other public facilities or

improvements of any type that are not inconsistent with a public right-of-way use;
and

b. cut, trim, or remove from the Easement Area trees, shrubs, or other vegetation that in the City's judgment unreasonably interferes with the City's easement or facilities.

4. **Duration of Easements.** The Easement is permanent and remains in effect in perpetuity.

5. **Warranty of Grantor.** The Owner warrants that it is the owner of a fee simple interest in the Property, that it has the right to grant the Easement, and that the Property is free and clear of any lien, encumbrance, easement, restriction, covenant or condition, except for those filed of record with the County Recorder or Registrar of Deeds for Hennepin County, Minnesota.

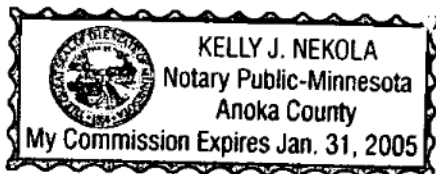
6. **Easement Runs with Land.** The Easement runs with the land and is binding on the Owner, its heirs, successors and assigns.

Meadow Woods LLC

By Bruce A. Nedegaard
Bruce A. Nedegaard
Its President and Chief Manager

STATE OF MINNESOTA }
 Anoka } SS
COUNTY OF HENNEPIN }

The foregoing instrument was acknowledged before me this 19 day of November, 2004 by Bruce A. Nedegaard, the President and Chief Manager of Meadow Woods LLC, a Minnesota limited liability company, on behalf of the limited liability company.



Kelly J. Nekola
Notary Public

THIS INSTRUMENT WAS DRAFTED BY:

Hinshaw & Culbertson LLP (RAW)
3100 Campbell Mithun Tower
222 South Ninth Street
Minneapolis, MN 55422
(612) 334-2516

For City use only:

Planning File # _____

Date of Council approval _____

EXHIBIT A

Lots 1 and 2, Block 1, Marshes of Meadowwoods

Lots 1 through 3, inclusive, Block 2, Marshes of Meadowwoods

Lots 1 through 6, inclusive, Block 3, Marshes of Meadowwoods

Lots 1 through 6, inclusive, Block 4, Marshes of Meadowwoods

Outlots A, B, C, D, E and F, Marshes of Meadowwoods, Hennepin County, Minnesota
according to the recorded plat thereof.

EXHIBIT B
EASEMENT AREA

Outlot C, Marshes of Meadowwoods



OF MEADOWWOODS

C.R. DOC. NO. _____

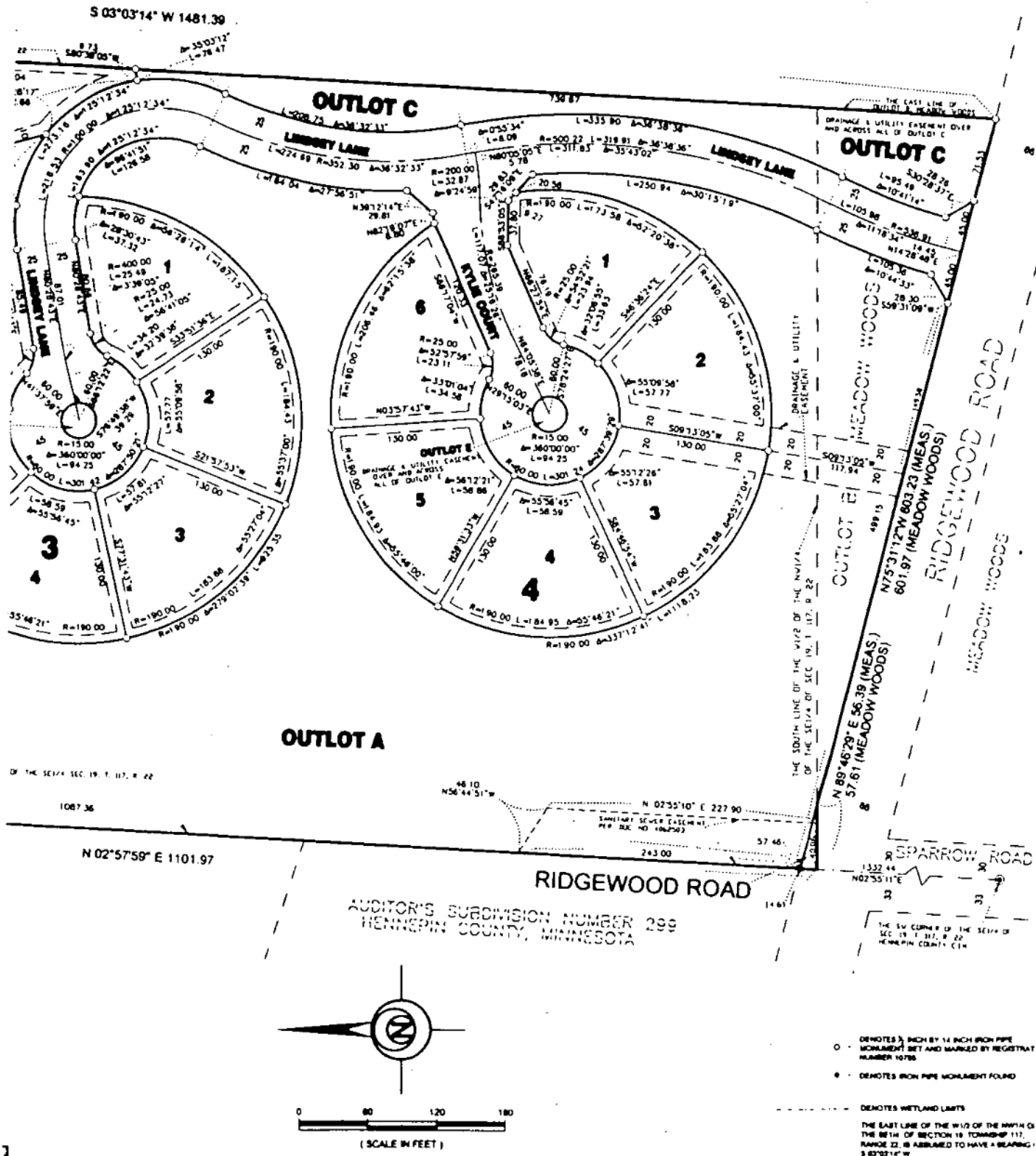


EXHIBIT C



14600 Minnetonka Blvd. | Minnetonka, MN 55345 | 952-939-8200 | eminnetonka.com

To: Planning Commission
From: Loren Gordon, City Planner
Date: Jan. 19, 2023
Subject: Change Memo for the Jan. 19th Planning Commission meeting

ITEM 9A – Ridgewood Villas

The attached neighborhood comments were received after distribution of the planning commission packet.

ITEM 9B – Marsh Run II Redevelopment

There is an error in the Staff Recommendation section of the report. Please make the following change:

An in-person ~~virtual~~ neighborhood meeting was held on Nov. 29, 2022.

The attached neighborhood comments were received after distribution of the planning commission packet.

From: [Joseph Boyer](#)
To: [Ashley Cauley](#)
Subject: 18116 Ridgewood Rd, Joe Boyer's thoughts
Date: Wednesday, January 18, 2023 1:10:55 PM
Attachments: [Opposition to Builder Proposed Roadway2.pdf](#)
[Best roadway location.pdf](#)
[Max tree loss, erosion-proposed plan.pdf](#)
[Proposed roadway.pdf](#)

Hi Ashley,

I am a homeowner in Marshes of Meadowwoods. 4218 Lindsey Ln, Minnetonka, MN 55345. I am very concerned that this development will negatively impact the use and enjoyment of my property and that of my neighborhood. Please find my comments attached. I have a surgery scheduled for Friday AM so my availability to comment at the commission meeting is not sure. It sounds like this is the most important of meetings in establishing the development. Therefore It is imperative that my thoughts are communicated to the commissioners.

My home foundation is set at 908 the cul de sac at the proposed development is also 908. The public service road proposed is obscure but will most likely face at my home as stated in my maps. I propose a better road servicing the development. see map. A shrub buffer or landscape berm is needed at the cul de sac to screen automobile lighting as the cars and delivery vehicles wind around the cul de sac. Many signature trees are lost with the builders proposed road easement.

See attachments below.

Please let me know of the successful receipt of this message.

Thank you!

Joseph Boyer



18116 Ridgewood Road site.

Opposition to Builder proposed Roadway.

Determined by Joe Boyer, 4218 Lindsey Lane Minnetonka, MN. Marshes of Meadowwoods.

Date 1/16/2023

Purpose, to significantly reduce negative impact of the proposed roadway on Marshes of Meadowwoods neighborhood. The owners of Marshes of Meadowwoods have purchased properties paid for the roads, maintain the wetlands at great expense. A request to interrupt access and take land for this development ought not create a perpetual nuisance for our members. Therefore, it is my view that the developer's flag lot road access be limited to the first ground suitable for access to their existing private driveway, thereby limiting the distance into Marshes of Meadowwoods property. There are numerous inconveniences associated with this intrusion, mostly short-term annoyances (3-5 years) such as road blocking and dirt. My main concern is with long term headlight pollution, often overlooked but super annoying, without insightful planning great damage occurs to others in the form of anxiety and decreased property values.

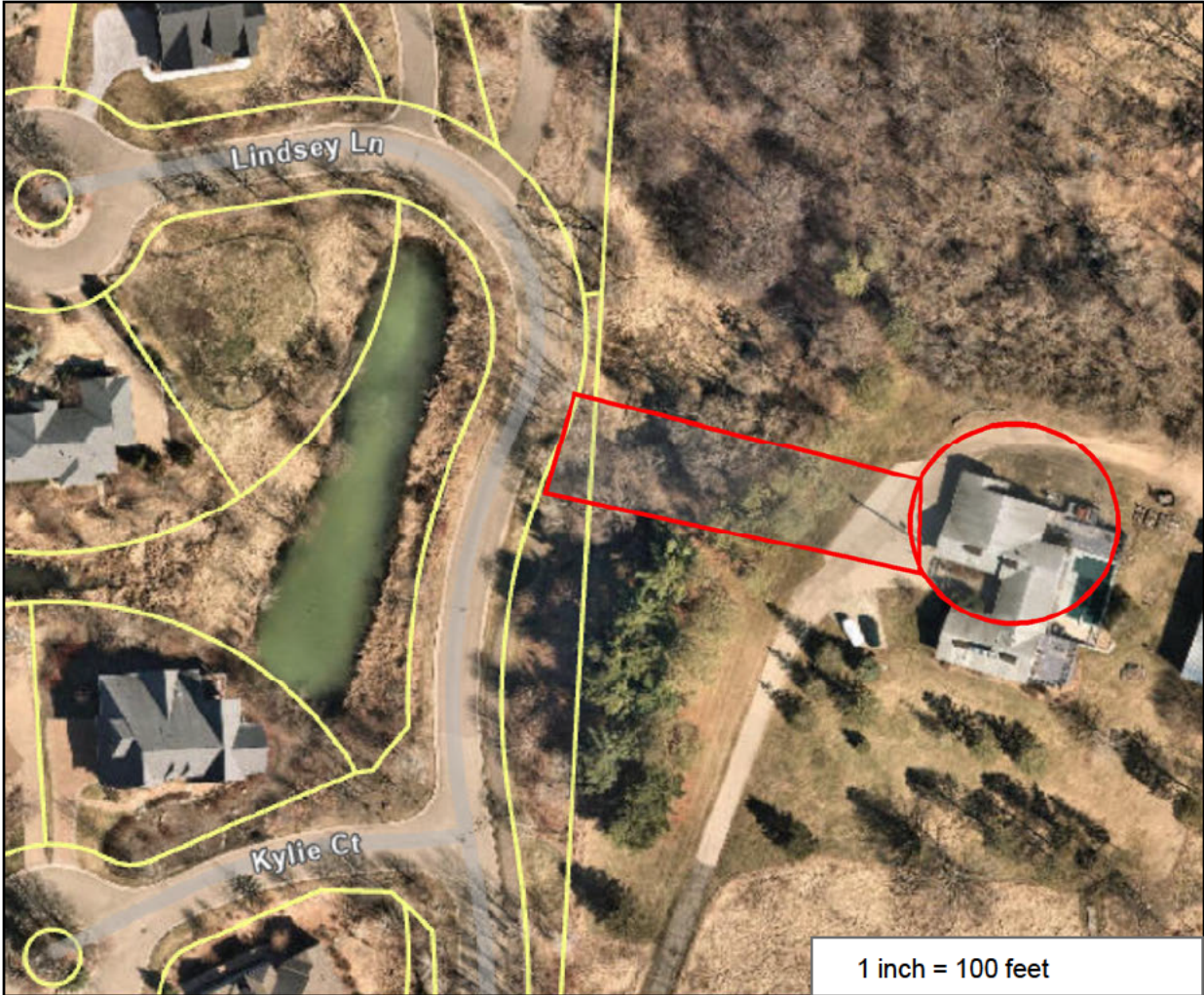
Negative impact of proposed plan.

1. The proposed roadway excavation is extensive and requires removal of approximately 20 significant white pine trees ranging in size from 15 to 24 inches in diameter.
2. The holding pond area is insufficient for the hardcover density created from housing and roadway construction. Additionally, the existing pond depression has no outlet so flooding will occur.
3. The cul-de-sac elevation and road alignment directly impact the privacy of at least five of my neighbors and our home due to automobile activity turning like a searchlight on a swivel in the dark as headlight spillover shines directly through our windows into our homes most private spaces. When you consider 10-26 trips in the morning hours and the same amount in the evening you can imagine the intensity and angst as a result. Please keep your excess light trash on your side of the property line.

Better results by moving the proposed road.

1. Moving the driveway to Kylie intersection, saves excavation impact with better use of existing drive area. May align with existing sewer and water stub. Additionally, the car lights are shining downhill mostly to the south over the lake wetland when leaving the development.
2. Mostly eliminates excess water runoff onto Marshes of Meadowwoods wetland property.
3. Decreases uphill slope of road to new development from 10% to 6.3%.
4. Preserves trees for aesthetic appeal as well as wildlife predator bird habitat.
5. Creates a level approach thereby increasing safety at the public trail cross walk.
6. Sewer and water trench and public road should align so that tree damage and erosion is mitigated.
7. Better safety as a streetlight currently exists at Kylie Court intersection.

It is our hope that the current driveway be demolished, and wetland restored when roads are completed to reduce hardcover impact on Lake Zimmerman.



No results

Comments:

Proposed roadway

This data (i) is furnished 'AS IS' with no representation as to completeness or accuracy; (ii) is furnished with no warranty of any kind; and (iii) is not suitable for legal, engineering or surveying purposes. Hennepin County shall not be liable for any damage, injury or loss resulting from this data.

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No results

Comments:

tree loss2

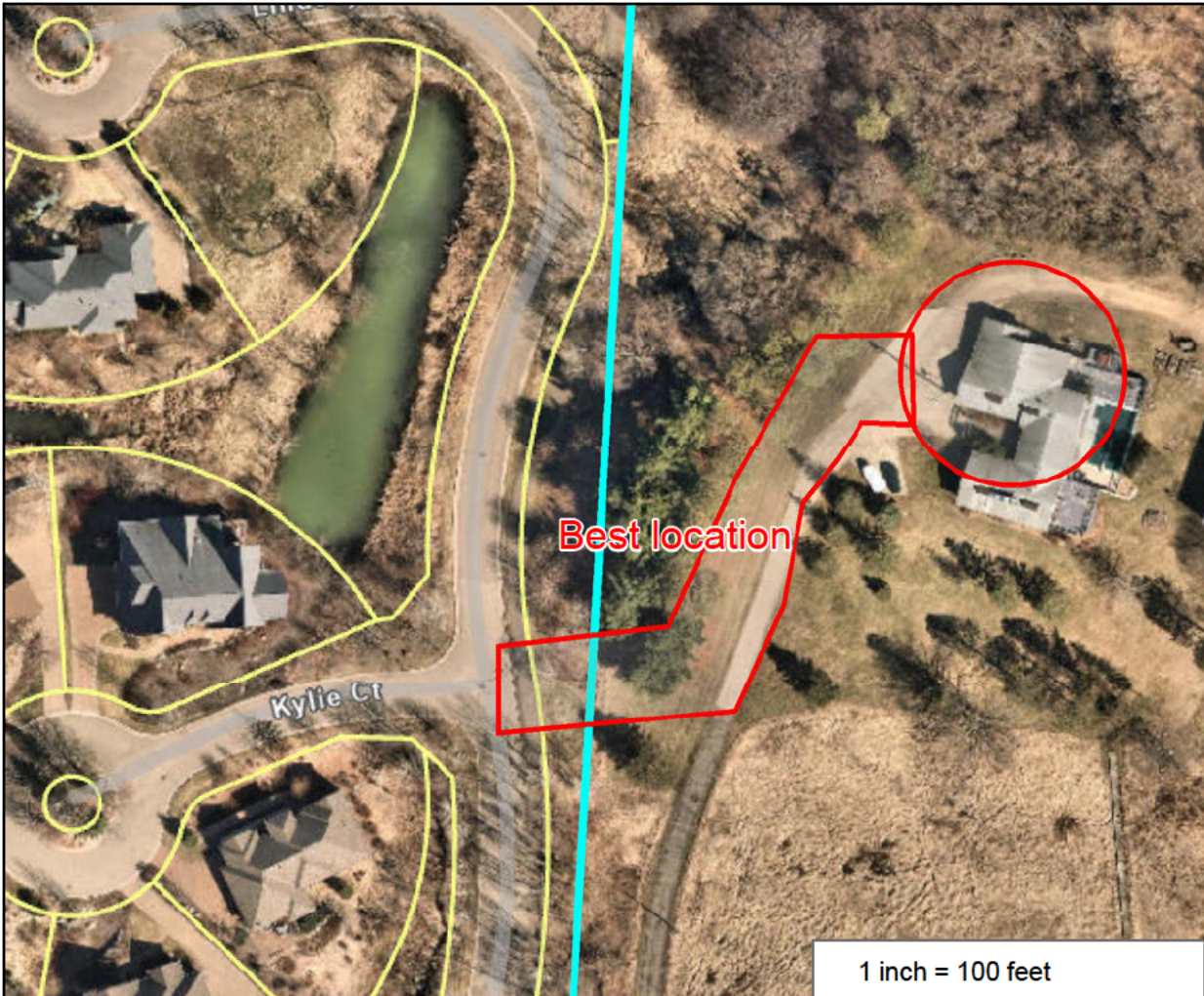
This data (i) is furnished 'AS IS' with no representation as to completeness or accuracy; (ii) is furnished with no warranty of any kind; and (iii) is not suitable for legal, engineering or surveying purposes. Hennepin County shall not be liable for any damage, injury or loss resulting from this data.

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Hennepin County Property Map

Date: 1/16/2023



PARCEL ID: 1911722420001

OWNER NAME: Lisa A Millhone Et Al

PARCEL ADDRESS: 18116 Ridgewood Rd, Minnetonka MN 55345

PARCEL AREA: 19.7 acres, 858,027 sq ft

A-T-B: Abstract

SALE PRICE: \$850,000

SALE DATE: 05/2011

SALE CODE: Warranty Deed

ASSESSED 2021, PAYABLE 2022
PROPERTY TYPE: Residential
HOMESTEAD: Homestead
MARKET VALUE: \$929,900
TAX TOTAL: \$13,963.98

ASSESSED 2022, PAYABLE 2023
PROPERTY TYPE: Residential
HOMESTEAD: Homestead
MARKET VALUE: \$2,248,500

Comments:

Best roadway location

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Ashley Cauley

From: Bryan Rice <[REDACTED]>
Sent: Tuesday, January 17, 2023 1:06 PM
To: Ashley Cauley
Subject: FW: [EXTERNAL] Ridgewood Villa Meetings virtual options
Attachments: Best roadway location.pdf; Max tree loss, erosion-proposed plan.pdf; Opposition to Builder Proposed Roadway2.pdf; Proposed roadway.pdf

Hi Ashley

I see on-line that you are the Project Manager for the proposed Ridgewood Villa project. I am sending this correspondence to you as note Joe sent these to Brad and Loren of the City earlier today. I believe you were the person we worked with on our build at 4230 Lindsey Lane, and was so impressed on your diligence and related. We are just a couple of weeks from being completed, and our really pleased with everything. Thanks again.

We are the new neighbors in Joe's communications, and if the proposed roadway is an accurate depiction that would be a concern as the incoming lights would directly shine on our main living area. If truly need to be in this general area, at the least we would ask that you consider moving the intersection south 30-40' - basically positioning the intersection (and lights) through the yellow lined easement areas having less effect on any property owner.

Joe's Best location of course would work as well, and may be safer for all parties with one intersection. Not sure how this effects the Kylie Court folks, as not there quite yet.

Anyways, thanks for your leadership with this and we will trust that you will help find a win/win solution for all. Call on any questions.

Bryan and Becky Rice

[REDACTED]

Sent from [REDACTED] for Windows

Marsh Run II

Thursday, January 19, 2023 1:01 PM

Subject	Marsh Run II
From	Peter Vickerman
To	Bria Raines
Sent	Thursday, January 19, 2023 9:27 AM

Good morning Bria,

I was reviewing the plans for the Marsh Run II proposal and had a couple comments/questions:

- I generally support the new proposal as it will significantly increase the value of the property and thus provide additional tax revenue to the city, county, and school district. I note that the 2022 total tax for the property is \$68,152.04 whereas the 175-unit Birke is \$564,009.58. With more units proposed for Marsh Run II, it can be expected that the total tax will be even higher for this project.
- There appeared to be two site plans in the submission, one had one access to the frontage road and the other had two. From a pedestrian and bicycle standpoint, one access would be preferable.
- The current bike/ped path along the frontage road could use improvement.
 - This trail can serve as a great way for these future residents to get to businesses at Westridge Market and Ridgedale but is currently not a great experience.
 - Ideally the trail would be moved away from back of curb with street trees to better buffer users. If the trail is pulled back enough, a car waiting to turn could be between the street and the trail, limiting conflicts between drivers leaving the site and bikers and pedestrians.
 - The intersections and ramps could use improvement as well as there are currently large turn radii that increase the crossing distance.
 - The ramps also face 45 degrees into the frontage road (they are diagonal) vs just facing east/west since this is the only real movement anyone would do.

Thank you,

Peter Vickerman

11550 Oberlin Road

Handout from the neighborhood meeting
that was referenced during PC meeting

Meeting with Ron Clark - Questions

Because we have only recently seen the current site plan, we have a number of questions.

The following list of questions to ask Ron Clark in this order:

1. Are you willing to agree that all of your construction equipment, supplies and employee/subcontractor parking will always be kept on your property with none of it ever on Lindsey Lane?

2. In your December 19 letter to Minnetonka, there are a few questions that we have as follows:
 - You state that the housing will blend with the existing neighborhood designs. Are you willing to commit to the following?
 - Each house will have a similar amount to brick or stone on the front that our homes have.
 - There will be more than five designs so no more than two homes will have the same basic design. None of our homes look the same.
 - The exterior siding material will be like ours with no vinyl siding material of any kind.
 - Almost all of our homes have paver driveways which both enhance the house design as well as contribute to mitigating storm water runoff. Will you do paver driveways here to match our neighborhood as you have done on some of your other neighborhoods?
 - Can all of the driveways hold two large SUVs?

 - On site design, you mention that you will have a tree preservation plan. Are you willing to go beyond the requirements of Minnetonka Ordinance 2021-24 to preserve the over one hundred year old grove of pine trees? If any of these majestic pines trees are removed, will you commit to replace each of them with pine trees having at least 6 caliper inch diameter?

 - In your storm water management paragraph, it is mentioned that a biofiltration basin will capture all of the runoff. Our neighborhood found that a couple of basins cannot capture all of the water runoff so we incorporated paver driveways, rain gardens next to each home and our streets have varied curb design to encourage water flow. We are very concerned that your inadequate

water/ snow management will negatively impact our neighborhood conservation efforts with your untreated salted water flowing directly in our ponds without any filtration.

- There is no mention in your letter on how your development will interface with all of the adjacent wetlands other than eliminating the current entrance road.. Our neighborhood has wetland conservation markers ten feet from the wetlands throughout our neighborhood prohibiting inappropriate activities such as fertilizing or planting a lawn within the ten-foot wetland set back. Will you commit to do the same?
- There is no mention in the letter about how the public road will interface with the walking trail. This is a safety issue that requires more than a simple stop sign. Perhaps a speed bump or two before the stop sign will make this car/people interface safer particularly since the public road will be coming downhill.
- Are you willing to plant spruce trees along the west side Lindsey Lane across from where your public street intersects with Lindsey Lane to mitigate car lights into our homes?
- Will your neighborhood be a 55+ community?

- Reviewed the concept plan for Saville Flats located in the southeast corner of the Excelsior Blvd. and Co. Rd. 101 intersection.

6. Report from Planning Commission Members

Powers appreciated the great job public works staff did plowing the streets after the heavy snow falls.

7. Public Hearings: Consent Agenda

No item was removed from the consent agenda for discussion.

Waterman moved, second by Maxwell, to approve the item listed on the consent agenda as recommended in the staff report as follows:

A. Items concerning Groveland Elementary School at 17310 Minnetonka Blvd.

Recommend that the city council adopt the attached resolution approving a conditional use permit and site plan review with a variance and expansion permit for Groveland Elementary at 17310 Minnetonka Blvd.

Henry, Maxwell, Powers, Waterman, Hanson and Sewall voted yes. Banks was absent. Motion carried and the item on the consent agenda was approved as submitted.

8. Other Business

A. Concept plan review for Ridgewood Road Villas at 18116 Ridgewood Road.

Chair Sewall introduced the proposal and called for the staff report.

Cauley reported. Staff recommends commissioners provide comments and feedback to assist the applicant with future direction that may lead to the preparation of more detailed development plans.

Mike Waldo, Ron Clark Construction, representing the applicant, stated that:

- He attended a meeting with neighbors and heard their concerns. It opened up a good dialogue that will be continued.
- The style of home would be similar to Legacy Oaks. It would be a high-quality product. The ranch-style residences with walk outs or look outs would be designed for empty nesters, but there would be no age restriction.
- A wetland delineation, tree survey and preliminary grading plan have already been completed to make sure the proper buffers to the wetland would be maintained and the site would work with the concept plan.

- A stewardship agreement would be considered for an area on the north and the buffer areas.
- They spoke with the neighbor on the north and adjusted the position of the proposed house that would be located nearest her property and plant trees to provide her residence with a buffer in response to her comments.
- Evergreens would be added on the west side of Lindsey Lane and the last 200 feet of the proposed road would have a downslope to deflect headlights when vehicles enter Lindsey Lane from the proposed road.
- The proposed residences would have customized fronts.

Tim Whitten, Whitten and Associates, landscape planner and architect for the applicant, stated that:

- The product is designed for empty nesters and is a needed product in Minnetonka.
- The proposal would provide housing for seniors who want to downsize and frees up their larger single-family residences.

Mr. Waldo stated that he and Mr. Whitten would be available for questions and appreciate the commissioners' feedback.

Chair Sewall looks forward to learning how many units per buildable acre the site has if an application with detailed plans is submitted in the future.

In response to Waterman's question, Mr. Waldo stated that the concept plan is already in compliance with the tree protection ordinance.

Jay Jensen, 4209 Lindsey Lane, stated that:

- He has a paver driveway, conservation easement on his property and all of the stormwater runoff is contained on his property.
- There is a stewardship agreement managed by the city.
- He wants to protect the otters he has seen near his property.
- He does not want the proposal to have a detrimental impact.
- He would appreciate the applicant meeting with the neighbors again before the applicant submits an application to the city.
- Lindsey Lane is a narrow public street. He would like a driveway created for construction traffic to minimize the impact to Lindsey Lane.
- He encouraged paver driveways to be used to help with water infiltration and have a better appearance.
- He would like the proposed road moved to the south to save some very old pine trees and minimize the slope that carries water down the hill.
- His backyard has natural vegetation instead of grass. He would like the proposal to have the same conservation markers in their backyards as he does to protect the wetlands.

- He suggested a traffic control device like a stop sign or speed bump be added to the public road to protect the pedestrians and bikers using the regional trail.
- Mr. Waldo mentioned that spruce trees would be added along Lindsey Lane to minimize the headlight impact.
- He appreciated everyone's time.

Mary Ann Coleman, 18317 Kylie Court, stated that:

- She is 100 percent against the project.
- She has seen 24 eagles perch in the trees where the proposed houses would be located and eat from the pond.
- There are otters, egrets, blue herons, ducks, geese and two trumpeter swans in the area.
- She was worried about construction vehicles jeopardizing the safety of trail users.
- Coyotes and deer live in the area.
- She questioned what a "green designated home" meant.
- The area would be harmed by the proposal.
- She did not find a road easement on the plat drawing. The minutes from a meeting held Sept. 15, 2003 state that a road and utility easement was provided to the city on Outlot C.
- A migratory bird permit may be required since eagles feed from the pond.
- She requested the project be scaled down. She wants the wild life to stay there.
- The proposal would decrease her property value.

Alicia Copa, 18008 Powderhorn Drive, stated that:

- Since Deephaven Cove was constructed, her property has had massive flooding and she has spent thousands of dollars to install and maintain culverts.
- Her variance application to construct an addition to her house within the wetland setback was denied.
- The wildlife is amazing.
- She has lost half of an acre lot because of the Deephaven Cove water redistribution.
- She was concerned the proposal would disturb the natural water runoff and cause more home and property damage.

John Coleman, 18317 Kylie Court, stated that:

- He thought Legacy Oaks has high density, limited trees and is a bland development due to a lack in the variety in colors.
- This proposal should have affordable housing.

- He requested that the conservation done for the marshes continue with this property.
- He will continue to review recorded documents for the property.
- He asked what interesting things could be done with the large piece of property that the current concept plan would not change. He thought something could be done to enhance the area.

Ms. Coleman stated that the six years of construction done to create Legacy Oaks scared away wildlife. She requested that the construction equipment stay on the applicant's property and not use Lindsey Lane.

Pat Montgomery, 17833 Powderhorn Drive, stated that:

- The utility ditch in the rear of his property is 30 to 40 feet wider than it was 15 years ago.
- He thinks the Marshes of Meadowwood cause more stormwater to travel onto his property.
- The wetland delineator he hired said that the utility ditch was blocked in three areas. One was from beavers making a damn and two were caused by the stormwater pipes becoming plugged from dirt.
- The residents of Powderhorn Circle have standing water on their properties March through August. He was concerned their properties would end up like his.
- His lot used to have .65 acres not in a wetland. Now only the first 10 feet of property from his back door is not wetland.
- He was concerned that water movement that cannot be predicted or controlled would impact the residents on Powderhorn Drive more and more.
- He was concerned when the city offered him curb and gutter at no cost.
- He opposes a street connecting with Powderhorn Drive.

No additional testimony was submitted and the hearing was closed.

Hanson confirmed with Cauley that Lindsey Lane is a public street and the proposed street in the concept plan would be public.

In response to Henry's question, Gordon explained how the Marshes of Meadowwoods' developer died during its development. It is a unique conservation project that restored wetlands that were being used as a golf course and created some home sites. The concept plan would only change the upland areas and not change the wetland areas of the property.

Hanson stated that:

- He appreciated the presentation from the applicant and for the neighborhood speakers being organized.
- Ron Clark has been a patient and flexible developer who listened to neighbors during previous projects. He values that.
- He supports making driveways wide enough for two vehicles to be parked on the driveways and still being able to open their doors.
- He encouraged the applicant to find ways for the proposal to be harmonious with its unique neighbor to the west.
- Like Chair Sewall, he is interested in learning how many units per buildable acre the site has if an application with detailed plans is submitted in the future.
- He would like to see the aesthetics of the proposed houses blending with the neighboring houses.

Waterman stated that:

- He appreciated the comments from neighbors.
- He agrees that Ron Clark development does a good job working with neighbors and doing a high-quality project.
- There is a lot to like about the concept plan.
- The site is a large, developable piece of property.
- He supports villa-style residences that support a little more density than R-1 single-family lots to free up other housing and creates natural affordability. This style of house with a size of 2,300 to 2,500 square feet would provide more affordability than what would be built on a lot zoned to meet R-1 requirements.
- He was a little concerned the residences would be too close together.
- He thought a public benefit would be the single-floor living to justify a planned unit development (PUD). The property on the north could also be utilized to provide a public benefit.
- The proposal should complement The Marshes of Meadowwoods.
- Issues to be careful of include stormwater runoff, snow management and the water table.
- He appreciated the work already done with neighbors to agree upon buffering.
- The concept plan would already comply with the tree ordinance.
- He was not sure if he was comfortable with the density.

Powers stated that:

- He likes Ron Clark development a lot.
- He thought the concept plan was too dense.
- He did not think houses that would sell for over \$1 million would qualify as a public benefit.
- Legacy Oaks was a very good project.

- He was concerned with stormwater runoff. The effects would be far reaching.
- The ability to effectively provide stormwater management must be determined before deciding how many houses would be appropriate.

Henry stated that:

- He would like to learn more about the possibility of providing effective stormwater management before commenting on other facets of the concept plan. He has full faith in each staff expert tasked with reviewing an application's stormwater management requirements.
- He appreciated the neighbors providing feedback on what is seen in the area on a day-to-day basis.
- He thanked the developer for working with the neighbors.
- The development on the west is unique. He encouraged the applicant to preserve the environment as much as possible.
- He likes the preservation of 19 acres.
- The proposal would be too dense and the houses would be located too close together.

Maxwell stated that:

- The property is unique and provides an opportunity to be good stewards of the beauty of Minnetonka.
- There would be too much density.
- The concept plan would have too much impervious surface in an area that already has a lot of water issues.
- She would like to see a thorough water runoff and stormwater management plan.
- She thought a two-vehicle garage might work in this case.
- She likes the extra surface parking for visitors.
- She did not think adding villa-style houses is enough of a public good to justify a PUD. The northern part of the property may be able to be utilized as a public good.

Chair Sewall stated that:

- The biggest issue is water. The worst-case scenario is that the property stays the way it is now. The best case is a decrease in the amount of water runoff caused by stormwater management features completed with the proposal.
- He would like the spirit of the conservation practices being done across the street applied to the proposal.
- He favors rezoning, but did not know if there is a strong enough case to justify a PUD.

- He was more concerned with the density in relation to the number of units per buildable area. Using the entire property to calculate density is misleading.
- He encouraged the applicant to keep working with neighbors.

Chair Sewall called for a brief recess.

B. Concept plan review for Marsh Run II Redevelopment at 11816 Wayzata Blvd.

Chair Sewall introduced the proposal and called for the staff report.

Raines reported. Staff recommends commissioners provide feedback on the key topics identified by staff and any other land-use-related items that commissioners deem appropriate. This discussion is intended to assist the applicant in the preparation of more detailed development plans.

Tony Kuechle, Doran and Co., applicant, stated that:

- The applicant plans on acquiring both properties. The concept plan only covers 11816 Wayzata Blvd. That acquisition would help with the neighborhood's lack of parking.
- The design of the building would be similar to The Birke.
- Neighbors expressed issues with a lack of parking, mass of the building, the trail disrupting natural habitat and the number of units.
- The proposal would cover the existing footprint.
- Three options were provided for exterior materials to receive feedback.
- The current buildings have been deemed blighted.
- The applicant would apply for TIF funds.
- The applicant is discussing with staff having ten percent of the units meet 80 percent area-median-income (AMI) affordable-housing requirements.
- The applicant supports prohibiting parking on Fairfield Ave. The applicant is willing to cover the cost of the city adding more "no parking" signs.
- The property the applicant purchased on the east could be assigned parking from 5 p.m. to 8 a.m.
- Raines did a great job explaining the concept plan.
- He was available for questions and looking forward to receiving feedback.

Powers asked Mr. Kuechle if he would have done anything differently on The Birke project. Mr. Kuechle answered that he would double the amount of surface parking. The internal, heated, dry visitor parking is not being used. The units maintain 96 percent occupancy.

Powers asked how many electric vehicle chargers would be provided. Mr. Kuechle responded that 10 percent of the stalls would have electric chargers right away and the

Neighborhood feedback received since the
Planning Commission meeting

From: [Sarah--comcast](#)
To: [Ashley Cauley](#)
Subject: Ridgewood development
Date: Thursday, January 19, 2023 4:50:18 PM

Hello Ashley,

Nice to talk with you the other day. Below are some of the ideas that we spoke about:

I'm writing to you concerning the proposed development along Ridgewood Road. I am opposed to changing the zoning from R1 to a PUD, allowing the building of 13 townhome "villas" in a single family residential neighborhood. This would fill beautiful open natural space around the lake with high density housing.

This is a beautiful property with a lake, mature trees, wetlands, wildlife, and open space. I've driven by this property for more than twenty years and I always enjoy the view and open space. I enjoy walking the wooded trail through the former golf course; part of the trail still has a bit of woods and trees. This area would be lined with townhomes if this development goes through. Open space, mature trees, wetlands, and wildlife are characteristics I appreciate in Minnetonka. This proposed development would detract from many of these elements.

Allowing a developer to build high density townhomes on the small portion of high land in this property is not in character with the neighborhood. I remember when the golf course was replaced with the current Marshes of Meadow Woods development. At that time, city planners told how high density developments occur along busier corridors, such as near Target or Lakewinds shopping areas. This property is not along a busy corridor. Why would zoning be changed to allow higher density detracting from the natural character of the area?

During the development of the golf course, a city environmental specialist spoke of the value of the lake on this property, and there was talk of trying to make this area a park. I ask that the city try to preserve lands like this instead of making a deal with developers that allow for more dense developments in residential areas.

Sincerely,
Sarah Stevenson

From: [John Coleman](#)
To: [Ashley Cauley](#)
Cc: [Loren Gordon](#)
Subject: Re: Marshes of Meadowwoods
Date: Tuesday, January 24, 2023 9:53:20 PM
Attachments: [Ridgewood Villas Concept Plan Review.pdf](#)

Thanks Ashley,
Please include the attached document as background for the Planning Commission and City Council
Appreciate your help!
John

Ridgewood Road Villas Development Project Concept Review Meeting

Neighborhood Concerns...

John Coleman
18317 Kylie Ct

1/24/2023



- **Proposed Ridgewood Road Villas Concept is modeled on Ron Clark’s Legacy Oaks Project...**
 - Conceptual Plan mirrors a drive thru impression of Legacy Oaks; high density, repetitive design, limited tree/landscape cover, years of construction impact on the surrounding neighborhood...
- **2004 Marshes of Meadowwoods, a Minnetonka showcase: “the most aggressive conservation orientated project the City had taken on” !**
 - **Focused on visually attractive designs**, framed by trees, blending the homes into wetland. Limiting perception of density by clustering homes in wheels. Reduction from potential of 63 →13+2 units. Supporting Mtka “Open Spaces” by moving back from Ridgewood Rd-adding landscape screening. A Trail for the public benefit linking neighborhoods to the south to LRT. Extensive water mgmt; 5 Drainage ponds, Homes have Raingardens, Paver driveways... the demonstrated results = improved water quality, enhanced natural environment, plus Otters, Cranes, Eagles and Swans !
 - **Don’t go backwards!!** Only a few opportunities to develop 20 acre sites left in Mtka. Is there potential for the public’s benefit in the northern half of the property that remains undeveloped? Site includes large portion of the lake how should that be managed and protected long term? Is a Conservation Easement and Stewardship program appropriate? Can the Ridgewood Villas improve on Minnetonka’s 2004 goals, or at least be environmentally and aesthetically consistent with Marshes!
- **Will the Developer commit to Affordable Housing Allocation?:** Marshes has ~14% Affordable Housing Continue to implement the “Economic Development Authority’s policy that 10 to 20 percent of new multi-family units should be affordable housing and ensure long-term affordability within new developments”.
- **Will the Developer consider lower density options and / or remain R1 ?**
 - *2003/2004 discussions in the Council’s minutes included; a potential development plan of 7 lots R1 with no flood plane impact (see next page), also the 2004 Traffic Study results assumed 5 new homes on Dunibar (recently approved) and only 7 on Millhone property*

* Will the Developer commit to

- Add Permanent-Green Landscaping into Marshes to manage Headlight pollution impact?
- Commit to keep all Constriction Parking and Materials on their 23 acre site – reducing impact and safety on Lindsey Ln and Public Trail?
- Codify Restrictions on Lawn chemicals, plantings i.e 10ft transition zones outside of wetland areas?
- Add Rain Gardens spread through out the project , not just 2 holding ponds to manage stormwater?
- Commit to Paver Driveways allowing water infiltration in place?
- Wetland Conservation Markers on each lot thereby preventing landscaping in the wetland setback area?
- Grading & Drainage Plan Public and private roads designed and appropriately sloped to prevent the higher elevation of the Project to drain contaminants such as fertilizers into the Marshes Stewardship - Lindsey Lane area and into existing Marshes ponds Conservation watershed?
- Snow Removal Plan that details appropriate spaces such that the contaminated road snow is not pushed down into Marshes Conservation Stewardship area i.e Lindsey Ln and into ponds?

1/24/2023

*not prioritized

Legacy Woods

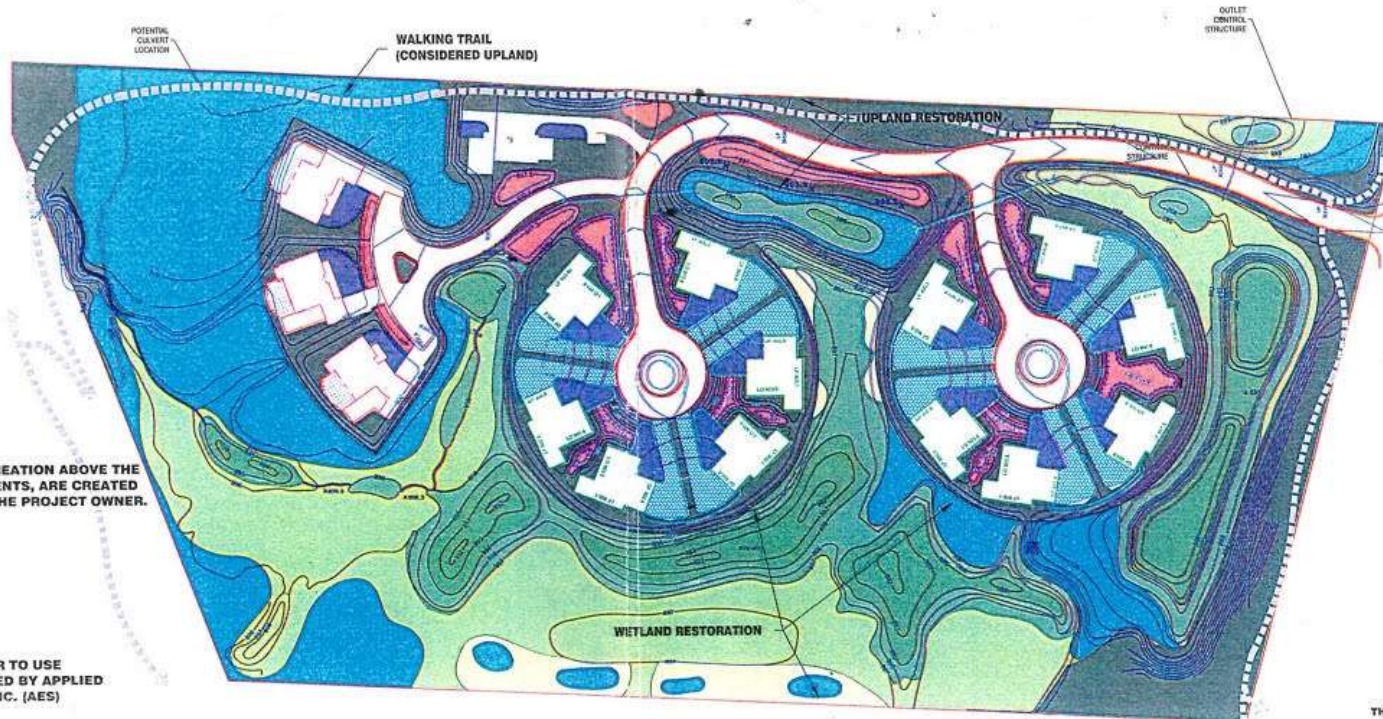
High Density
both
Physically &
Visually



1/24/2023

RESTORATION PLAN: MEADOWWOODS

COLOR DRAWING



ADDITIONAL WETLAND CREATION ABOVE THE REGULATORY REQUIREMENTS, ARE CREATED AT THE DISCRETION OF THE PROJECT OWNER.

LANDSCAPE CONTRACTOR TO USE SPECIFICATIONS PROVIDED BY APPLIED ECOLOGICAL SERVICES INC. (AES)

THIS RESTORATION PLAN WAS DEVELOPED BY APPLIED ECOLOGICAL SERVICES INC. (AES) MODIFIED BY HEI WITH AES PROMISSION.

Legend

- Project Boundary
- TYPE 2: Wet Prairie/Meadow-3.02 Ac.
- TYPE 3: Shallow Emergent-1.73 Ac.
- TYPE 4: Deep Emergent-1.43Ac.
- TYPE 2/6: Strub Swamp-0.35 Ac.
- TYPE 2/7: Hardwood Swamp-3.56 Ac.
- Rain Garden Biofilter-0.62 Ac.
- Upland Forest/Woodland-4.64 Ac.
- Private Garden Area-0.39 Ac.
- Proposed Underdrain
- Wetland Edge
- Proposed Stormsewer

PERFORMANCE CRITERIA:
ACTUAL WETLAND FILL: 2.89 AC

WETLANDS REQUIRED:
CITY: 10.36 AC.
WCA: 2.90 AC. NWC
2.90 AC. PVC

WETLANDS ACTUAL:
CITY: 10.51 AC.
WCA: 1.29 AC NWC
1.87 AC NWC(25% OF 7.46 AC.)
3.07 AC TOTAL WCA

UPLAND RESTORATION AREA= 4.64AC. ACTUAL PVC

VERTICAL SCALE:				
HORIZONTAL SCALE:				
001	RLH	6-21-04	As requested by owner	
002	RLH	7-09-04	As requested by BAER	
003	JRE	12-09-03	REVISED FOR SUBMITTAL	
REV.	NAME:	DATE:	REVISION DESCRIPTION:	

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME, OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DATE: _____ REC. NO. _____

HUMPHREY ENGINEERING

145 Main Street
P.O. Box 252
Rosedale, MN 54028
Bus: 715-698-3448 Fax: 715-698-3441

Engineers
Land Surveyors
Construction Managers

PROJECT:
MEADOWWOODS TOWNHOMES
CITY OF MINNETONKA, MN

RESTORATION PLAN

SHEET NO. 8

DRAWN BY: JRE

CHECKED BY: RLH

AWG FILE: 294-00PRES.DWG

Protect the Marshes' Conservation Easements and the Pond-Lake Ecosystem :

Will a formal plan to managed stormwater and salt snow, chemical, pond and wetland management be put in place?

Remediate Headlight Pollution:

Will Developer add Evergreen Trees to shield existing homes as shown?

1/24/2023



Reduce Density:

Maintain and enhance the perception of "Open Spaces" and improve visuals from Ridgewood Rd

Will the Developer restrict the impression of "manicured lawns"

Will the Developer vary home designs, frame homes with trees to blend into the wetland environment?

Legacy Oaks Example: Construction Impacts Neighborhoods For Years

2015...

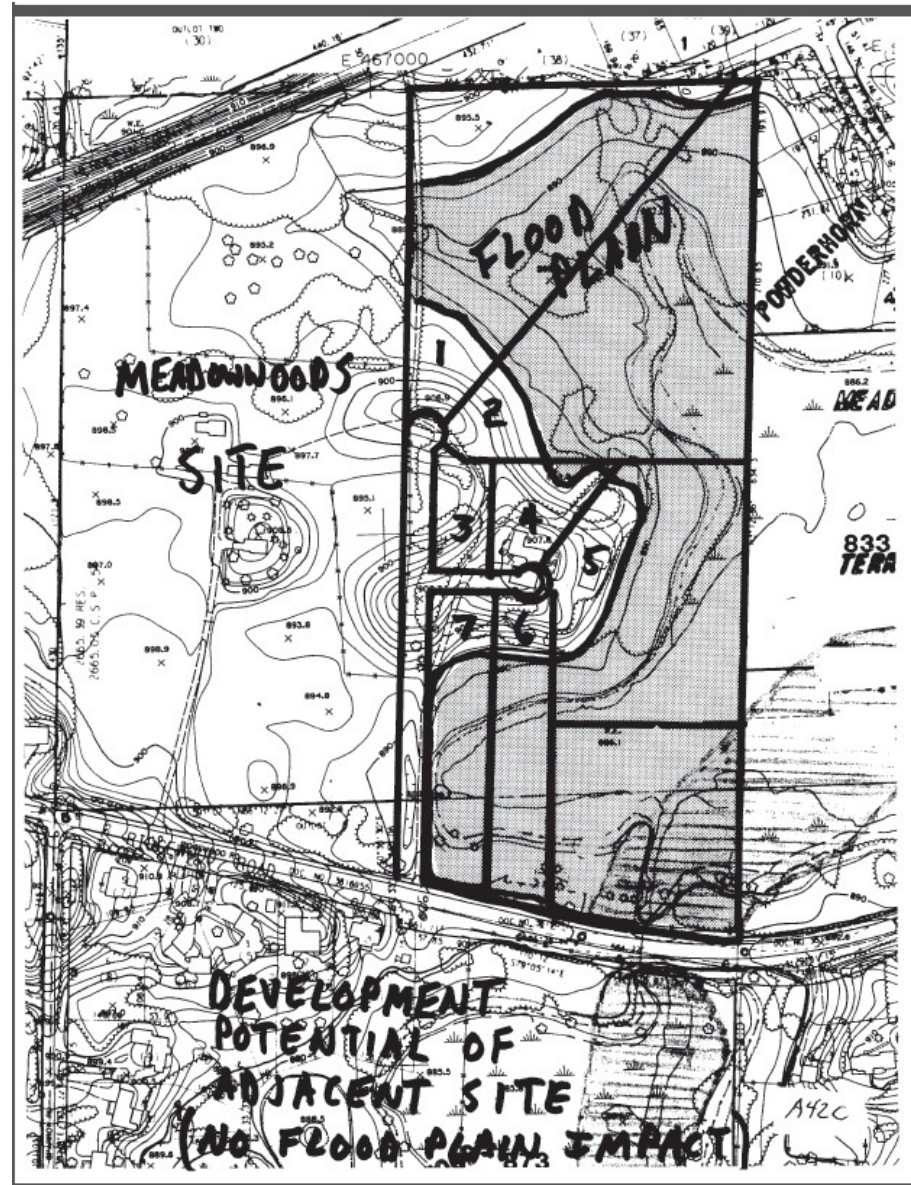
2018...

2021



Other Options:

During Marshes' project approval process in 2003, discussions around future development in the neighborhood considered the potential for 7 homes on the (Zimmerman) Millhone property – while maintaining R1 zoning



From: [Mary Ann Donahue](#)
To: [Ashley Cauley](#)
Subject: Ridgewood Development/Monday Council Meeting
Date: Tuesday, January 24, 2023 8:01:25 PM
Attachments: [Eagles.pdf](#)

Hi Ashley,

Would you please add this for our meeting?

See you Monday!!
Mary Ann Coleman

The Bald and Golden Eagle Protection Act

- The Eagle Act (16 U.S.C. 668-668c), enacted in 1940, and amended several times since then, prohibits anyone, without a permit issued by the Secretary of the Interior, from taking bald eagles, including their parts, nests, or eggs. ...The Act defines take as; pursue, shoot, shoot at, poison, wound, kill, capture, trap, collect, **molest or disturb**. "**Disturb means to agitate or bother a bald** or golden eagle to a degree that causes, or is likely to cause, based on the best scientific information available, 1) injury to an eagle, 2) a decrease in its productivity, by substantially interfering with normal breeding, feeding, or sheltering behavior, or 3) nest abandonment, by substantially interfering with normal breeding, feeding, or sheltering behavior." In addition to immediate impacts, this definition also covers impacts that result from human-induced alterations initiated around a previously used nest site during a time when eagles are not present, if, upon the eagles' return, such alterations agitate or bother an eagle to a degree that injures an eagle or substantially interferes with normal breeding, feeding, or sheltering habits and causes, or is likely to cause, a loss of productivity or nest abandonment.
- A violation of the Act can result in a criminal fine of \$100,000 (\$200,000 for organizations), imprisonment for one year, or both, for a first offense. Penalties increase substantially for additional offenses, and a second violation of this Act is a felony.

1/24/2023

Picture taken 1/14/23: 1st pair is here now, up to 10 pairs will come as the Pond opens !





**City Council Agenda Item 15A
Meeting of Jan. 30, 2023**

Title: Boards and commissions appointments and reappointments

Report from: Sarissa Falk, Executive Assistant

Submitted through: Mike Funk, City Manager
Moranda Dammann, Assistant City Manager

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

On Jan. 31, 2023 the appointment terms will expire for the Minnetonka Boards and Commissions members listed below. Staff recommends these members should continue to serve in their respective capacities. Also listed below are the candidates Mayor Wiersum recommends appointing to fulfill the park board and senior advisory board vacancies.

Recommended Action

Approve the following reappointments to the Minnetonka Boards and Commissions:

- Ann Duginske Cibulka, to the economic development advisory commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Lee Jacobson, to the economic development advisory commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Melissa Johnston, to the economic development advisory commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Katie Semersky, to the park board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Korey Beyersdorf, to the park board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- David Waterman, to the planning commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.

Subject: Boards and commissions reappointments and appointments

- Derrick Banks, to the planning commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Carol Seiler, to the senior advisory board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Edwin Avalos, to the sustainability commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Matt Henry, to the sustainability commission, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.

Approve the following appointments to the Minnetonka Boards and Commissions:

- Anne Hanley, to the park board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Elizabeth DiLorenzo, to the park board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Catherine Goset, to the senior advisory board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Pamela Rixe, to the senior advisory board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.
- Ted Nearman, to the senior advisory board, to serve a two-year term, effective Feb. 1, 2023 and expiring on Jan. 31, 2025.

Strategic Profile Relatability

- | | |
|--|---|
| <input type="checkbox"/> Financial Strength & Operational Excellence | <input type="checkbox"/> Safe & Healthy Community |
| <input type="checkbox"/> Sustainability & Natural Resources | <input type="checkbox"/> Livable & Well-Planned Development |
| <input type="checkbox"/> Infrastructure & Asset Management | <input checked="" type="checkbox"/> Community Inclusiveness |
| <input type="checkbox"/> N/A | |

Statement:

Financial Consideration

- Is there a financial consideration? No Yes [Enter estimated or exact dollar amount]
- Financing sources: Budgeted Budget Modification New Revenue Source
- Use of Reserves Other [Enter]

Statement: N/A

Background

On January 31, 2023, the appointment terms will expire for some members of the economic development advisory commission, park board, planning commission, senior advisory board and sustainability commissions. All of them are eligible to be reappointed, and they each have indicated an interest in continuing to serve another term. The members listed above have been valuable and productive members, and staff recommends the reappointment of the eligible members listed.

Section 2.02 of the City Charter states members of such bodies [boards and commissions] will be appointed by the mayor, subject to approval by the city council. The city also strives to have balanced and diversified boards and commissions. This highlights the importance of revisiting the recruitment process each year to ensure the city is reaching all residents and improving accessibility of the application. Anne Hanley, Elizabeth DiLorenzo, Catherine Goset, Pamela Rixe and Ted Nearman applied during the December 2022 boards and commissions recruitment period, and was subsequently interviewed at the Jan. 23, 2023 city council study session. Mayor Wiersum reviewed the city council scoring sheets and recommends appointing Anne Hanley and Elizabeth DiLorenzo to serve two-year terms on the park board to fill vacancies and Catherine Goset, Pamela Rixe and Ted Nearman to serve two-year terms on the senior advisory board to fill vacancies.



**City Council Agenda Item 16
Meeting of Jan. 30, 2023**

Title: Closed session to conduct city manager performance evaluation;
pursuant to Minnesota Statute § 13D.05, subd. 3(a)

Report From: Moranda Dammann, Assistant City Manager

Submitted through: Corrine Heine, City Attorney

Action Requested: Motion Informational Public Hearing
Form of Action: Resolution Ordinance Contract/Agreement Other N/A
Votes needed: 4 votes 5 votes N/A Other

Summary Statement

The employment agreement between the city and City Manager Mike Funk requires the city council to conduct annual performance evaluations with the city manager in the same manner as other non-union employees.

Recommended Action

Motion to convene in closed session, pursuant to Minnesota State Statute § 13D.05, subd. 3(a), for the purpose of evaluating the performance of City Manager Mike Funk for the evaluation period June 27, 2022, through December 31, 2022.

Strategic Profile Relatability

Financial Strength & Operational Excellence Safe & Healthy Community
 Sustainability & Natural Environment Livable & Well-Planned Development
 Infrastructure & Asset Management Community Inclusiveness
 N/A

Financial Consideration

Is there a financial consideration? No Yes
Financing sources: Budgeted Budget Modification New Revenue Source
 Use of Reserves Other [Enter]

Background

On June 27, 2022, the City of Minnetonka entered into an Employment Agreement with Mike Funk as city manager. The agreement requires the city council to conduct annual performance evaluations with the city manager in the same manner as other non-union employees.

Minnesota State Statute § 13D.05, subd. 3(a) allows a public body to close a meeting to evaluate the performance of an individual who is subject to its authority. The public body must identify the individual to be evaluated prior to closing the regular portion of the meeting.

Subject: Closed session to conduct city manager performance evaluation; pursuant to Minnesota Statute § 13D.05, subd. 3(a)

However, state statute gives discretion to the employee to keep the meeting open. Mr. Funk has expressed his preference to keep the meeting closed.

This agenda item will take place during the closed portion of the regular council meeting, therefore it is appropriate the city council conduct the review, which includes goal setting, with the city manager during this time.

Minnesota State Statute § 13D.05, subd. 3(a) also requires the public body to summarize its conclusions regarding the evaluation at its next open meeting. The next open meeting is a study session scheduled for Monday, Feb. 6, 2023. The mayor will summarize the conclusions during this meeting.

After the council approves the motion and immediately prior to going into closed session the mayor will need to announce the following two statements: 1.) that at the conclusion of the closed meeting, no other regular business of the city will be discussed and that the regular meeting is adjourned; and 2.) that he will summarize the conclusions of the performance evaluation at the February 6 study session meeting.