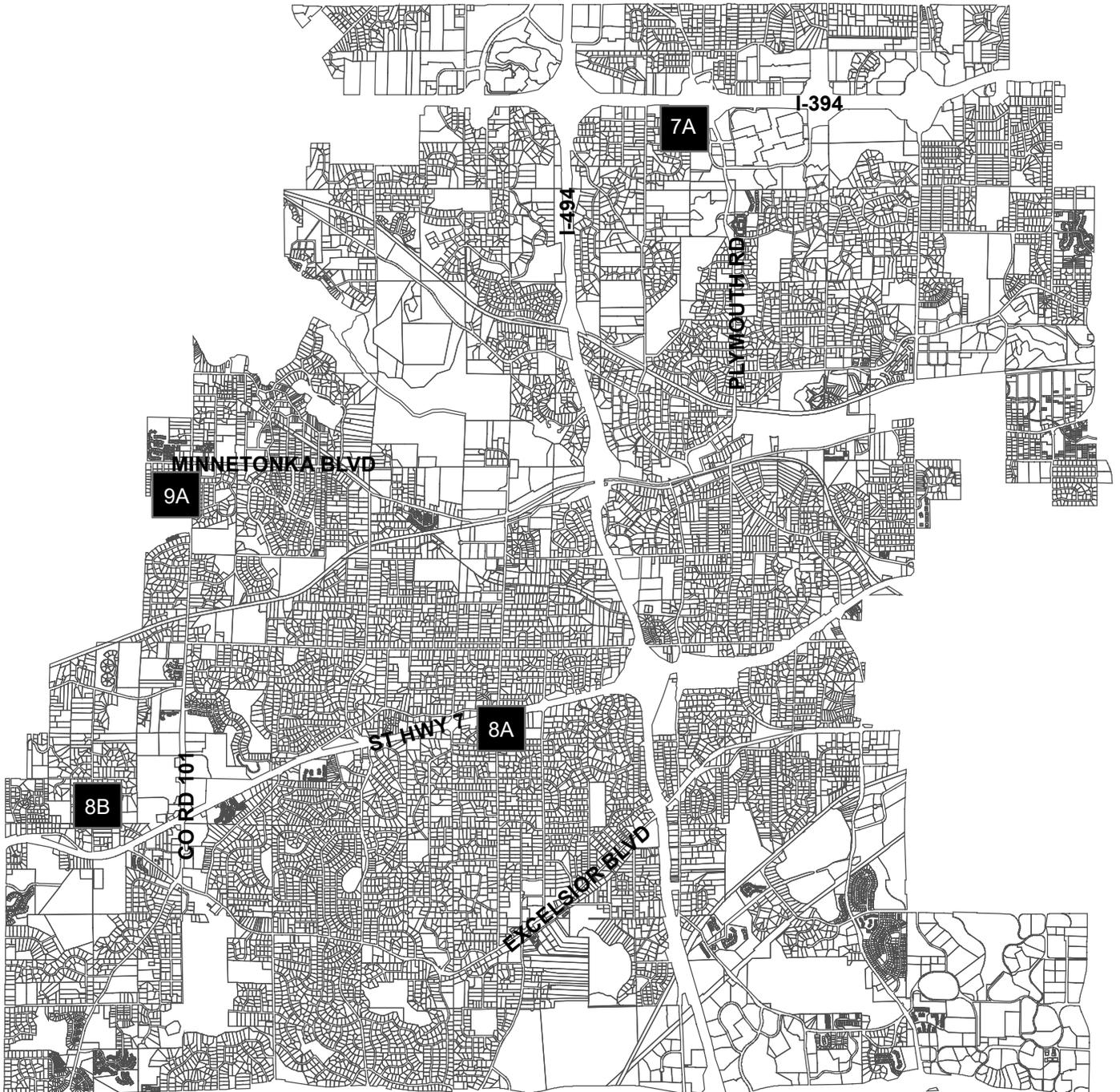




CITY OF
MINNETONKA

CITY OF MINNETONKA
PLANNING COMMISSION
OCT. 26, 2023

14600 Minnetonka Blvd. • Minnetonka, MN 55345
(952) 939-8200 • Fax (952) 939-8244
minnetonkamn.gov





**Planning Commission Agenda
Oct. 26, 2023
6:30 p.m.**

City Council Chambers – Minnetonka Community Center

1. Call to Order

2. Roll Call

3. Approval of Agenda

4. Approval of Minutes: Oct. 12, 2023

5. Report from Staff

6. Report from Planning Commission Members

7. Public Hearings: Consent Agenda Items

A. Amendment to the Ridgehaven Sign Plan as it pertains to the property at 13101 Ridgedale Drive.

Recommendation: Adopt the resolution approving the request (5 votes).

- Final decision subject to appeal
- Project Planner: Susan Thomas

8. Public Hearings: Non-Consent Agenda Items

A. Items concerning MidCountry Bank at 14617 Highway 7.

Recommendation: Recommend the city council adopt the ordinance and resolution approving the request (4 votes).

- To City Council (Nov. 13, 2023)
- Project Planner: Bria Raines

B. Preliminary plat approval of PRIORY WOODS, a three-lot subdivision at 4633 Sparrow Road.

Recommendation: Recommend the city council adopt the ordinance and resolutions approving the request (4 votes).

- To City Council (Nov. 13, 2023)
- Project Planner: Bria Raines

Planning Commission Agenda

Oct. 26, 2023

Page 2

9. Other Business

A. Items concerning a concept plan for townhome development at 3514, 3520, 3522 Co. Rd. 101.

Recommendation: Discussion topic only. No recommendation.

- To City Council (Nov. 13, 2023)
- Project Planner: Loren Gordon

10. Adjournment

Notices

1. Please call the planning division at (952) 939-8290 to confirm meeting dates as they are tentative and subject to change.
2. The following applications are tentatively scheduled for the Nov. 9, 2023 agenda.

Project Description	Barry Residence, VAR for a garage addition
Project Location	3345 Honeywood Lane
Assigned Staff	Drew Ingvalson
Ward Councilmember	Brian Kirk, Ward 1

Project Description	Fretham 13 th Addition, PPL for 16-lot subdivision
Project Location	2503, 2505, 2511, and 2615 Plymouth Road
Assigned Staff	Susan Thomas
Ward Councilmember	Rebecca Schack, Ward 2

**Unapproved
Minnetonka Planning Commission
Minutes**

Oct. 12, 2023

1. Call to Order

Chair Sewall called the meeting to order at 6:30 p.m.

2. Roll Call

Commissioners Hanson, Maxwell, Waterman and Sewall were present. Banks, Henry and Powers were absent.

City Planner Loren Gordon was present.

3. Approval of Agenda: The agenda was approved as submitted.

4. Approval of Minutes: Sept. 28, 2023

Hanson moved, second by Maxwell, to approve the Sept. 28, 2023 meeting minutes as submitted.

Hanson, Maxwell, Waterman and Sewall voted yes. Banks, Henry and Powers were absent. Motion carried.

5. Report from Staff

Gordon briefed the commission on land use applications considered by the city council at its meeting on Sept. 18, 2023:

- Adopted an ordinance rezoning the properties at 5432 Rowland Road and 5501 Baker Road from R-1 to R-2.
- Adopted a resolution approving the preliminary plat of Ridgedale 11th Addition at 12431 Wayzata Blvd.

The next planning commission meeting is scheduled to be held Oct. 26, 2023.

6. Report from Planning Commission Members: None

7. Public Hearings: Consent Agenda

No item was removed from the consent agenda for discussion.

Waterman moved, second by Hanson, to approve the item listed on the consent agenda as recommended in the staff report as follows:

- A. Conditional use permit for an attached accessory dwelling unit at 11621 Shady Oak Drive.**

Recommend that the city council adopt the resolution approving a conditional use permit for an accessory dwelling unit at 11621 Shady Oak Drive.

Hanson, Maxwell, Waterman and Sewall voted yes. Banks, Henry and Powers were absent. Motion carried and the item on the consent agenda was approved as submitted.

This item is scheduled to be reviewed by the city council at its meeting on Oct. 30, 2023.

8. Public Hearings

9. Adjournment

Hanson moved, second by Maxwell, to adjourn the meeting at 6:34 p.m. Motion carried unanimously.

By: _____
Lois T. Mason
Planning Secretary

Minnetonka Planning Commission Meeting

Agenda Item 7

Public Hearing: Consent Agenda

MINNETONKA PLANNING COMMISSION
Oct. 26, 2023

Brief Description Amendment to the sign plan for Ridgehaven Mall at 13145 Ridgedale Drive.

Recommendation Adopt the resolution approving the amendment.

Background

Ridgehaven Mall is located in the southwest quadrant of the Plymouth Road Crossroad/Interstate 394 interchange. The retail site is comprised of three properties. Lunds Byerlys and Target serve as anchor tenants, located on either end of the center. Smaller tenants are located on the subject property, situated between the anchors.



Signage on the subject property is governed by a sign plan approved in 1979 and subsequently updated in 1997 and 2003. Interestingly, neither anchor tenant is governed by sign covenants.

	Max. Letter Height	Max. Stacked Letter Height	Additional Information
1970	2 ft	3 ft	
1997	2 ft for tenants < 12,000 sq.ft. 3 ft for tenants >12,000 sq. ft.	silent	As there was no provision for stacking, staff enforced only max. letter height
2003	2 ft for tenants < 12,000 sq.ft. 3 ft for tenants >12,000 sq. ft.	5 ft for tenants > 12,000 sq. ft.	The sign plan was updated to specifically allow for the stacking of letters to accommodate a sign at Barnes and Noble Booksellers.

In 1997, planning staff approved a 3-foot by 36-foot sign based on tenant lease space information provided at that time: 12,100 sq. ft. In 2002, that sign was replaced by the existing 3-foot by 25-foot sign.

Proposal

Ulta Beauty recently submitted a sign permit application for the replacement of the existing sign with a roughly 5-foot by 12-foot sign. (The sign letters would have a maximum height of 36 inches, but the total sign height would be just under 5 feet.)



As part of the application, revised lease information was provided that indicated the tenant space was actually less than 12,000 sq. ft. Under the existing sign plan, the proposed 5-foot tall sign would not be allowed. Ulta is requesting an amendment to the sign plan to accommodate the proposed sign.

Staff Analysis

Staff finds that the proposed amendment is reasonable:

- The proposed sign would actually be smaller than previous tenant signs.

	Max. Height	Total Area
1997	3 ft	108 sq. ft.
2002 (existing)	3 ft	75 sq. ft.
Proposed	5 ft	58 sq. ft.

- The proposed sign would be appropriately scaled to the size of the tenant space.

Staff Recommendation

Adopt the resolution amending the sign plan for Ridgehaven Mall at 13145 Ridgedale Drive.

Originator: Susan Thomas, AICP, Assistant City Planner
Through: Loren Gordon, AICP, City Planner

Supporting Information

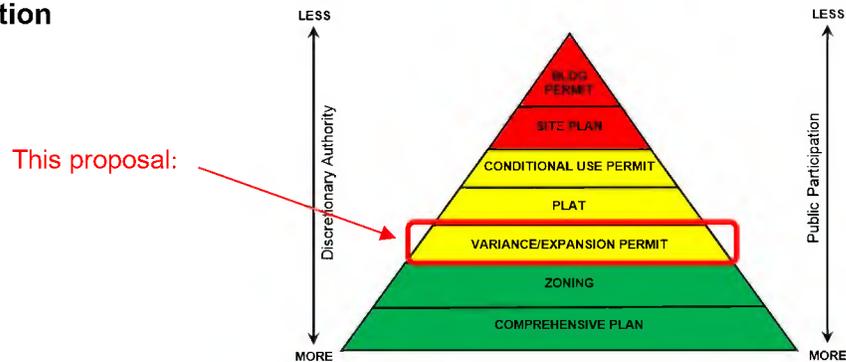
Surrounding Land Uses

North: Commercial space
South: Commercial space
East: Plymouth Road and commercial buildings beyond
West: Single-household residential

Planning

Guide Plan designation: Commercial
Zoning: PID, Planned I-394 District

Pyramid of Discretion



Motion Options

The planning commission has three options:

1. Concur with the staff recommendation. In this case, a motion should be made approving the requested amendment.
2. Disagree with staff's recommendation. In this case, a motion should be made to deny the applicant's request. This motion must include findings for the denial
3. Table the request. In this case, a motion should be made to table the item. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant, or both.

Appeal

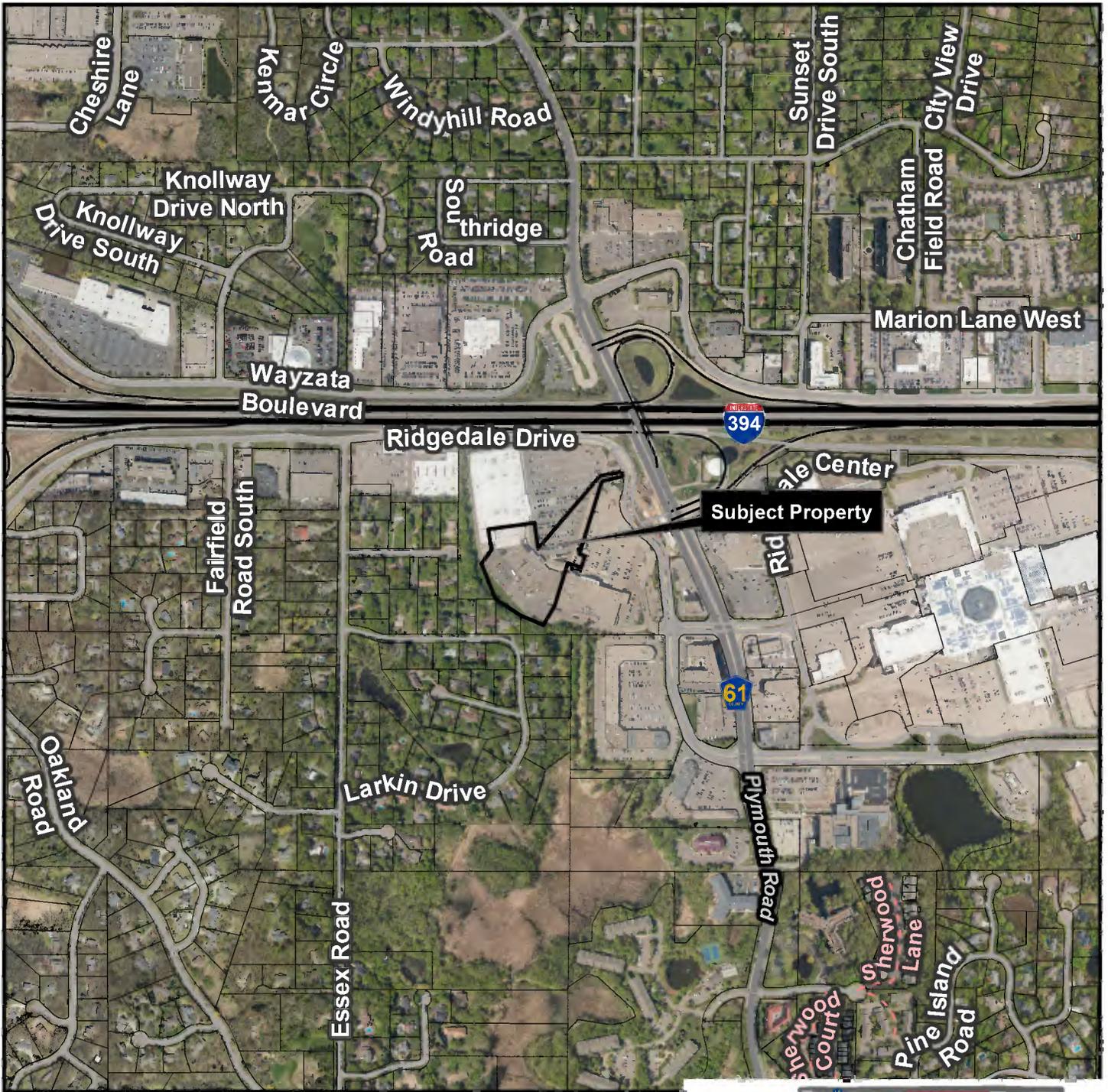
The planning commission's decision on the request is final and subject to appeal. Any person aggrieved by the planning commission's decision about the request may appeal such decision to the city council. A written appeal must be submitted to the planning staff within ten days of the date of the decision.

Neighborhood Comments

The city sent notices to 59 area property owners; no comments have received.

Deadline for Action

Jan. 25, 2024



Location Map

Project: Ulta Beauty
Address: 13145 Ridgedale Drive





9061 Woodlark Terrace. Boynton Beach, FL 33436

631.882.4718 ♦ gopermit@gmail.com

September 26, 2023

City of Minnetonka
14600 Minnetonka Blvd
Minnetonka, MN 55345

Re: Sign Variance Request
Ulta Beauty
13145 Ridgedale Drive
Minnetonka, MN

To Whom it May Concern:

Please allow this letter to serve as a request to apply for a sign variance for Ulta Beauty at the above referenced location.

The request to replace the current main identification “Ulta Beauty” wall sign with their new updated brand logo. The proposed sign will be in the same location as existing, will measure 57.87 sqft and have a 36” letter height.

The code states that lease spaces under 12,000 sqft allow a maximum letter height of 24” and lease spaces over 12,000 sqft allow a maximum letter height of 36”. Ulta Beauty’s location has a lease space of 11,010 sqft. The minimal difference of 990 sqft is why this request is being presented.

The proposed signage is the best application for this facade, we truly feel that abiding by the maximum letter height the signage will look lost on the facade and be a detriment to the shopping center and the surrounding tenants. Our new logo has a clean and modern look that will fit perfectly with the aesthetics in Ridgehaven. It should be noted that the letter height of the existing “Ulta Beauty” signage is a 36” overall height. We are asking that the existing letter height be allowed to remain for the new signage we are proposing.

If you have any questions or require additional information, please do not hesitate to contact me.

Thank you and I look forward to hearing from you!

Sincerely,

A handwritten signature in black ink, appearing to read "Jennifer Ronneburger", with a long, sweeping underline that extends to the left.

Jennifer Ronneburger
President, GoPermit

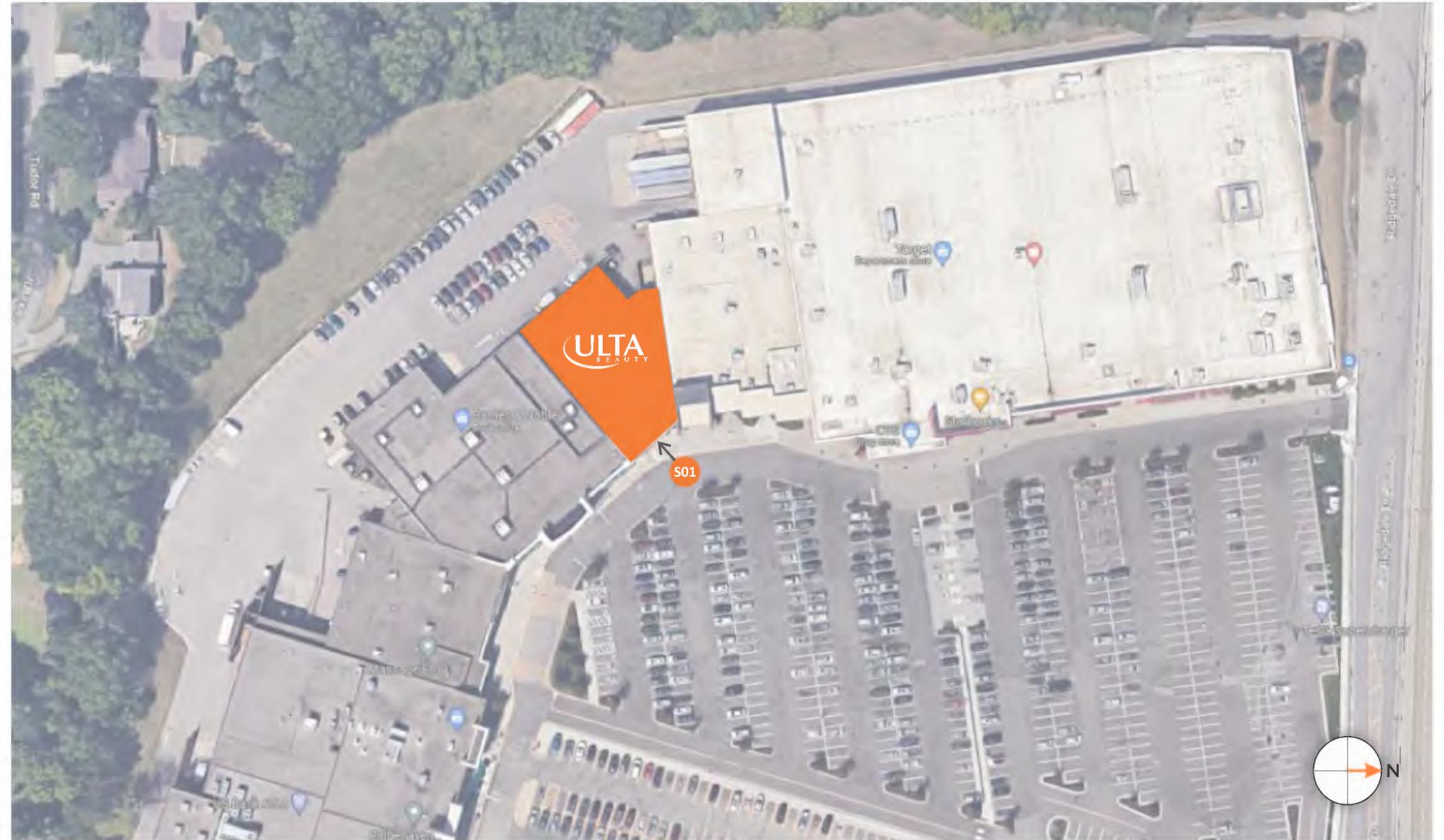


Store #65 Minnetonka, MN
13145 Ridgedale Drive
Minnetonka, MN 55305

SO #163900 | Created: 02.22.2023 | Last Revised: 08.17.2023

SIGN SCOPE INDEX

S01 (1) Set Illuminated Channel Letters



APPLICABLE CODE

- **WALL SIGNS:**
Lease space is 11,010 which would allow a 24" letter.
Lease space over 12,000 would allow to qualify for the 36"H
- Proposed S.F.: **57.87 SF** with a 36" letter



LL Signature: _____
 Printed Name: _____
 Company: _____
 Date: _____



Paint Note:
Patch and Paint sign band after removal
(color match to existing; color TBD)



EXISTING STOREFRONT LETTERS ARE 36" OAH

PROPOSED LETTERS ON EXISTING NORTHEAST ELEVATION

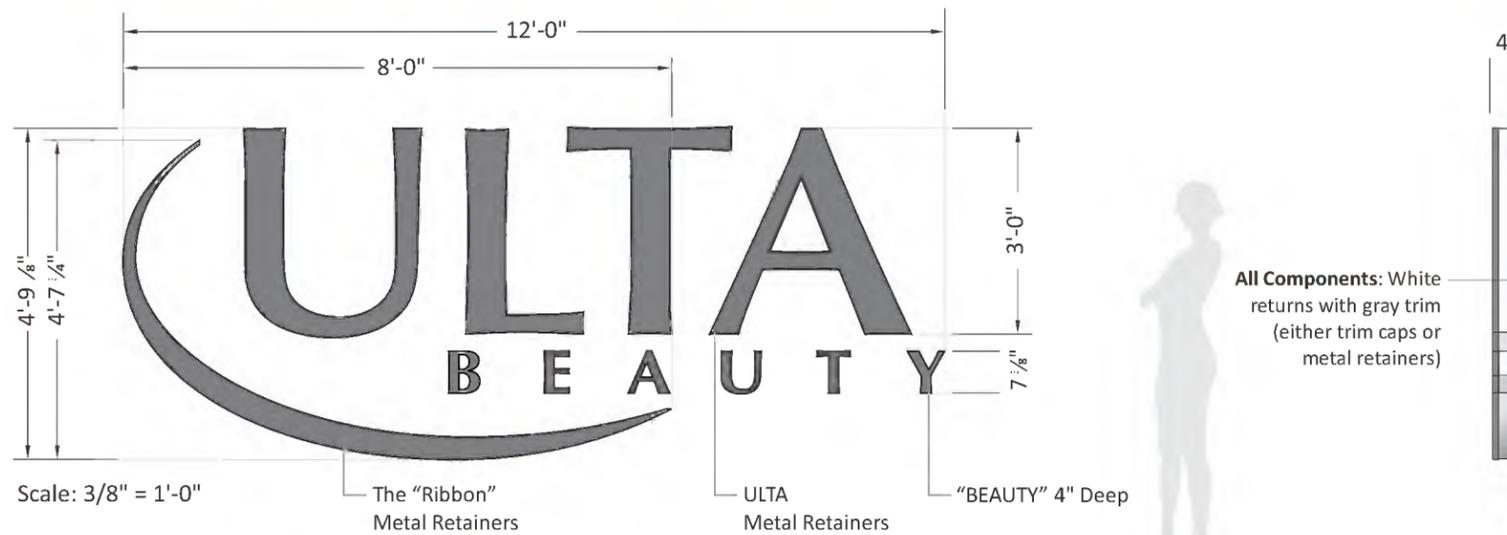
S01 (1) Set Illuminated Channel Letters

Revisions:
PP 06.13.2023 Raceway added
PP 06.19.2023 Raceway Removed
PP 07.05.2023 Changed to Non-Branded awning
...
...

LL Signature: _____
 Printed Name: _____
 Company: _____
 Date: _____

Store #65

PM: KS	Address: 13145 Ridgedale Drive
Drawn By: CC	City State: Minnetonka, MN 55305
Date: 08.17.2023	Drawing Number: 163900-ELEV Page: 2



SCOPE

- Manufacture & install (1) new primary channel-letter sign.

GENERAL DESCRIPTION

- Channels letters constructed of aluminum (.040 returns, .063 backs, 4" depth).
- Returns are of pre-finished white aluminum
- Faces of white (2406) acrylic with full, 1st-surface coverage of perforated gray vinyl (3M 3635-0252 Gray)
- Faces secured with either 1" trim caps or 7/16" metal retainers (see Detail A for more detail)
- Letters are internally illuminated by white, 24V GE LED modules
- Power supplies are installed remotely (behind fascia)
- Letters installed onto exterior fascia with non-corrosive fasteners

SQUARE FOOTAGE

- 4'-9 7/8" x 12'-0" = 57.87

COLOR SCHEDULE

-  **ALL FACES:** 2406 White acrylic w/ 1st surface perforated vinyl application - 3M Custom Dual Color 3635-0252 Gray
-  **ALL RETURNS:** Pre-finished white (gloss)
-  **TRIM CAPS and METAL RETAINERS:** Painted to match PMS 431C Gray



All Components: White returns with gray trim (either trim caps or metal retainers)

ULTA & The Ribbon: These components require metal retainers - 7/16" (face) x 1 1/4" (return) - (painted to match ULTA's corporate gray)

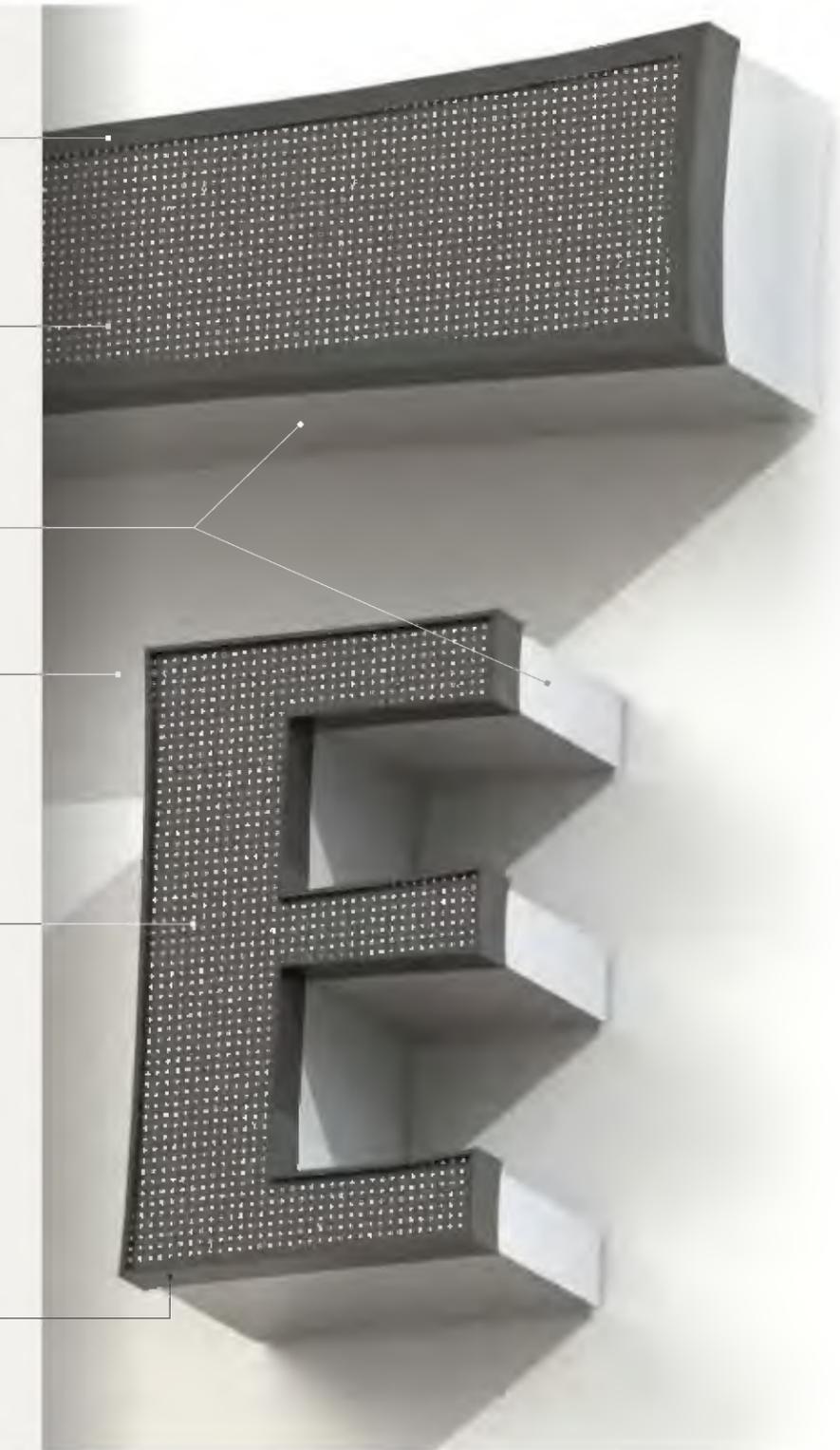
All Faces: 2406 White Acrylic with 1st Surface Application of perforated vinyl (3M 3635-0252 Gray)

All Components: White (pre-finished) 4" Deep Returns

Backer Board: 5/8" weatherproof plywood backing centered on fascia behind ULTA letter set. NOTE: Plywood is installed on backside of wall. (See elevation for dimensions)

All Faces: 2406 White Acrylic with 1st Surface Application of perforated vinyl (3M 3635-0252 Gray)

BEAUTY: Faces secured w/ 1" Trim Caps (painted to match ULTA's corporate gray)



Resolution No. 2023-

**Resolution approving an amendment to the sign plan for Ridgehaven Mall
at 13145 Ridgedale Drive**

Be it resolved by the Planning Commission of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 The subject property is located 13145 Ridgedale Drive. It is commonly referred to as Ridgehaven Mall and is legally described as:

Lot 2, Block 1, Ridgehaven Mall

Torrens Certificate No. 1535009

1.02 In 1979, the city approved a sign plan (sign covenants) for Ridgehaven Mall. The covenants do not include the anchor tenants, currently occupied by Lunds Byerlys and Target. The sign plan was updated in 1997 and 2003.

1.03 Ulta Beauty is requesting an amendment to the plan to accommodate a roughly 58-square-foot, five-foot-tall sign at its Suite S01 location. The amendment is outlined in Exhibit A of this resolution.

Section 2. Findings

2.01 The planning commission finds the requested amendment is reasonable, as:

1. The proposed sign would be smaller than previous tenant signs.
2. The proposed sign would be appropriately scaled to the size of the tenant space.

Section 3. Planning Commission Action.

3.01 The amendment, as outlined in Exhibit A of this resolution, is hereby approved.

Adopted by the Planning Commission of the City of Minnetonka, Minnesota, on Oct. 26, 2023.

Josh Sewall, Chairperson

Attest:

Fiona Golden, Deputy City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the Planning Commission of the City of Minnetonka, Minnesota, at a duly authorized meeting held on Oct. 26, 2023.

Fiona Golden, Deputy City Clerk

Exhibit A

**Ridgehaven Mall
Sign Plan
Approved Oct. 26, 2023**

Exterior Signage - Mall Tenants

- A. Tenant shall, at Landlord's option, be allowed to identify the Premises by means of an illuminated sign located on the exterior of the building and contained wholly within the limitations established by the Landlord.
- B. Tenants shall be limited to one sign on the building.
- C. Sign construction shall be individually channel letters illuminated from a neon source contained in each letter.
- D. Signs shall be Tenant's name, including logo and trademark.
- E. Location of sign shall be on the mall sign band within the Tenant's storefront (if the Tenant has an exterior storefront) or on the sign band as designated by the Landlord.
- F. For spaces in excess of 11,000 square feet, the height of the sign letters and logo shall not exceed thirty-six (36) inches and total sign height must not exceed sixty (60) inches.
- G. For spaces in less than 12,000 square feet, the height of the sign must not exceed twenty-four (24) inches
- H. All letters must be of white illumination. The store logo or trademark may be in color.

General Provisions

All temporary or permanent signs must meet the requirements of the City of Minnetonka sign ordinance, unless otherwise permitted by this covenant. City sign permits must be issued prior to display of all temporary or permanent signs.

Minnetonka Planning Commission Meeting

Agenda Item 8

Public Hearing: Non-Consent Agenda

MINNETONKA PLANNING COMMISSION
Oct. 26, 2023

Brief Description Items concerning a financial institution and a fast food restaurant, with drive-up windows, at 14617 Highway 7:

1. Rezoning from B-1, office, to B-2, limited business district;
2. Conditional use permit;
3. Site and building plan review, with variances.

Recommended Action Recommend the city council adopt the ordinance and resolution approving the proposal.

Background

The property at 14617 Highway 7 is approximately 40,500 square feet in area and is improved with a roughly 6,800 square foot building. The property is located within the B-1 Office Business District. In 1977, the subject property was redeveloped from a parking lot into an Edina Realty real estate office. In 2001, a conditional use permit was approved for a financial institution, Bayside Bank, to occupy the site. The subject property has had no major site or building changes since 2001. The building is currently occupied by MidCountry Bank.

Proposal

The applicant, Sperides Reiners Architects, Inc., on behalf of MidCountry Acquisition Corp., proposes to redevelop the MidCountry Bank site. The proposal includes the removal of the existing building, construction of a larger building, and introducing a restaurant with a drive-thru window to the site.

The proposed building is 3,900 square feet, an increase from the existing 3,449 square foot footprint. The restaurant includes an outdoor seating area and a drive-thru.



Image 1- Proposed Building

The proposal requires:

1. **Rezoning from B-1, office, to B-2, limited business.** A rezoning is required from the existing B-1 zoning to a B-2 zoning designation for the proposed restaurant. The city code does not permit a restaurant with or without drive-up facilities in the B-1 district.
2. **Conditional use permit, with variances.** A conditional use permit is required for any restaurant with or without drive-up facilities in the B-2 District. The proposed restaurant requires a variance to the conditional use permit standards for the following:

- Parking lot setback from the westerly right-of-way (ROW);
 - Building setback from the westerly ROW; and
 - Removal in excess of the permitted Significant and High-Priority Tree removal.
3. **Site and building plan review.** A site and building plan review is necessary for the construction and expansion of any commercial building or site. The increased building size requires an increase in the site parking demand, which the site can accommodate.

After staff review, a comprehensive plan amendment is not required for this proposal. For more information, see the "2040 Comprehensive Plan" section of this report.

Staff Analysis

A land-use proposal is comprised of many details. These details are reviewed by members of the city's economic development, engineering, fire, legal, natural resources, planning, and public works departments and divisions. The details are then aggregated into a few primary questions or issues. The analysis and recommendations outlined in the following sections of this report are based on the collaborative efforts of this larger staff review team.

- **Is the proposed rezoning appropriate?**

Yes. The proposed fast food restaurant could not be conditionally permitted at the subject property without the zoning change. The rezoning request is reasonable as the B-2 property would be a transition from the residential land use to the west to the more intense commercial use to the east of the subject property.

- **Is the proposed use generally appropriate?**

Yes. The existing financial institution use will remain at the site, with a restaurant use added. The restaurant use is allowed with a conditional use permit. The use is not uncommon on similarly zoned B-2 properties.

- **Is the proposed building and site design reasonable?**

Yes. Both the site and the building have been reasonably designed.

1. **Site.** The site is composed of various features that create a well-planned site.

- **Tenant separation.** The financial and restaurant communications tellers are located on opposite sides of the building so as not to interfere with the two drive-thru uses.
- **Trash enclosure.** The trash enclosure is located in the southeast corner of the southern parking lot. This is the most feasible location compared to those previously proposed by the applicant. Other locations would have impacted the queue lines and area of moving traffic, as opposed to the proposed location in the southern parking lot.

- **Stormwater feature.** The restaurant stacking area includes the installation of a rain garden in the center. This area is not large enough to satisfy stormwater management requirements; however, an underground stormwater chamber under the restaurant queue line does meet those requirements.
 - **Sidewalk connection.** The applicant proposes a sidewalk access continuing the existing connection from the southeast corner of the Williston Road and Highway 7 intersection to the subject property. The staff has added this as a condition of approval.
2. **Building.** The proposed exterior would be an attractive aesthetic compared to the existing site. The proposed materials are PVC wall panels, grey and beige stone, and metal wall panels. The materials are consistent with the architectural features of a commercial building. The earth tones are complimented by “MidCountry Green” accents.

Details of the trash enclosure were not provided. A condition has been added that the trash enclosure must match the proposed building and must be opaque.

- **Is the proposed parking and building setback variances reasonable?**

Yes. In 2000, a frontage road access to Williston Road was shifted, significantly impacting the subject property. The site was developed at that time. However, the donation of land to the Minnesota Department of Transportation for the frontage road realignment meant that it would be difficult for future development to meet setback requirements.

The proposal would meet one of the front yard setbacks but requires a variance to the front yard setback from the westerly ROW.

	Existing	Proposed	City Code Required
Westerly ROW setback*	62.64 feet	35.1 feet	50 feet
Easterly setback	10.2 feet	64.6 feet	50 feet
*Variance required			

Table 1 - Building setback standards

In general, the proposal utilizes features of the existing site. The existing parking lot setbacks are 6.4 feet from the east property line and 10 feet from the westerly ROW. The city code requires a minimum of 20 feet from the ROW to the north, west, and south; and 10 feet from the east property line. The existing site is non-conforming. The proposal does not change the location and size of the southern parking lot or the accesses. A variance is required to the western bank bypass drive aisle as, by modification, loses its non-conforming status. However, staff is not supporting the variance as the bypass lane is not necessary to the function of the site. Additional support for the removal of the bypass lane is noted in the supplemental report information section of the report.

	Existing	Proposed	City Code Required ¹
ROW setback from south	5 feet	5 feet	20 feet
ROW setback from west*	20 feet	10.3 feet	20 feet
Easterly setback	6.4 feet	6.4	10 feet
*Variance required			

Table 2 – Parking setback standards

• **Is the tree removal variance reasonable?**

Yes. All of the protected trees proposed for removal are immediately east of the existing structure, which would be impacted by the redevelopment of the site. Eleven of the thirteen trees on the site are protected by the tree ordinance, and of those, eight are proposed for removal.

Based on the location of the protected trees, it would be extremely difficult to remove the existing ones and meet the tree protection ordinance.

For more information, see the “Tree Protection” section of this report.

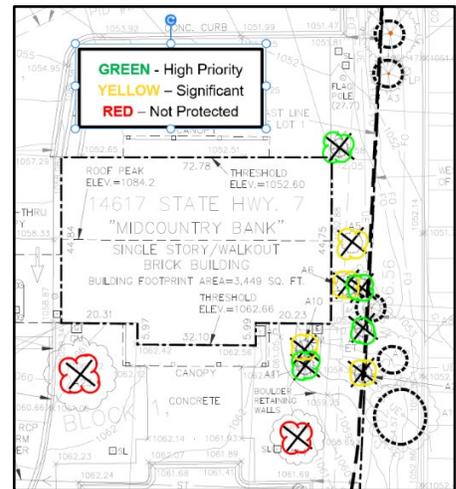


Figure 1 - Tree Removal Plan

Staff Recommendation

Recommend the city council adopt the ordinance and resolution approving a conditional use permit with variances and a final site and building plans at 14617 Highway 7.

Originator: Bria Raines, Planner
 Through: Susan Thomas, AICP, Assistant City Planner
 Loren Gordon, AICP, City Planner

¹ City Code 300.28 Subd. 12(b4) – This section contains the required parking setbacks.

Supporting Information

Subject Property The subject property is located in the B-1 – Office Business – zoning district and has a commercial land use designation in the 2040 Comprehensive Guide Plan.

Surrounding Property North: State Highway 7 and R-1 Low-Density Residential
South: Zoned R-1 Low-Density Residential
East: Commercial business, zoned B-2 Limited Business
West: Zoned R-1 Low-Density Residential

Proposed Building The proposed building would be 3,900 square feet in size. It would include an outdoor dining area, two drive-thru lanes, and parking stalls to the east and south of the principal structure. The westerly drive would serve the bank. It is covered with a canopy, which staff has recommended not be structurally attached to the building or anchored to the ground. This requirement is based on the assumption that consumer preference will continue to grow towards mobile or online users, therefore reducing or eliminating the need for drive-thrus in the future. The condition would require an “impermanent” canopy, which could be easily removed.

Traffic Study A traffic study was commissioned by city staff for the proposed project. The traffic consultant was requested to review the proposed plan, provide a traffic capacity analysis of the site and nearest intersections, a safety review for pedestrian and vehicular circulation, a parking demand review, and provide comments for parking and traffic improvements.

Traffic Analysis

In a capacity analysis, the intersections are ranked from level of service A through F. The level of service results are based on the average delay per vehicle, which corresponds to the delay threshold values shown in Table 7. Level of service A indicates the best traffic operation, while level of service F indicates where demand exceeds capacity. Overall intersection level of service A through D is generally considered acceptable in the Twin Cities area.

LOS Designation	Signalized Intersection Average Delay/Vehicle (seconds)	Unsignalized Intersection Average Delay/Vehicle (seconds)
A	≤ 10	≤ 10
B	> 10 - 20	> 10 - 15
C	> 20 - 35	> 15 - 25
D	> 35 - 55	> 25 - 35
E	> 55 - 80	> 35 - 50
F	> 80	> 50

Table 3 – Description of levels of service

The current level of service of the intersections surrounding the subject property are provided in Table 4.

Table 3. Existing Intersection Capacity Analysis

Intersection	Level of Service (Delay)		
	A.M. Peak Hour	M.D. Peak Hour	P.M. Peak Hour
TH 7 and Williston Road ⁽¹⁾	D (49 sec.)	C (25 sec.)	D (42 sec.)
South Service Road and Williston Road ⁽²⁾	A/B (11 sec.)	A/A (7 sec.)	A/A (8 sec.)
South Service Road and Southeast Access ⁽²⁾	A/A (4 sec.)	A/A (5 sec.)	A/A (4 sec.)
South Service Road and Northeast Access ⁽³⁾	A/A (1 sec.)	A/A (5 sec.)	A/A (5 sec.)
South Service Road and North Access ⁽³⁾	A/A (1 sec.)	A/A (1 sec.)	A/A (3 sec.)
South Service Road and Woodhill Road ⁽²⁾	A/A (8 sec.)	A/A (7 sec.)	A/A (10 sec.)

(1) Indicates a signalized intersection. The delay shown represents the overall intersection.
 (2) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.
 (3) Indicates an unsignalized intersection with no traffic control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.

Table 4 – Current level of service

The intersections are expected to have the following level of service in 2025.

Intersection	Level of Service (Delay)		
	A.M. Peak Hour	M.D. Peak Hour	P.M. Peak Hour
TH 7 and Williston Road ⁽¹⁾	D (54 sec.)	C (26 sec.)	D (45 sec.)
Lloyds Drive/South Service Road and Williston Road ⁽²⁾	B/D (35 sec.)	A/A (7 sec.)	A/A (9 sec.)
South Service Road and Southeast Access ⁽²⁾	A/C (16 sec.)	A/A (6 sec.)	A/A (5 sec.)
South Service Road and North Access ⁽³⁾	A/A (5 sec.)	A/A (6 sec.)	A/A (4 sec.)
South Service Road and Woodhill Road ⁽²⁾	B/C (16 sec.)	A/A (8 sec.)	A/A (9 sec.)

(1) Indicates a signalized intersection. The delay shown represents the overall intersection.
 (2) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.
 (3) Indicates an unsignalized intersection with no traffic control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.

Table 5 – Anticipated level of service (2025)

The traffic study does not anticipate the intersections to have any significant delay in traffic or increase in the level of service. While insignificant, it should be noted that the Highway 7 and Williston Road intersection is expected to have a service delay of one to five seconds, depending on the time of day.

In the Twin Cities area, level of service A through D is considered acceptable. All intersections are expected to continue operating within that range.

The traffic consultant provided comments concerning potential traffic issues at the site. Those comments are provided below.

- Incorporate traffic controls, signing, and striping at the site.
- Place “Do Not Enter” signs at the western access at the bank drive-thru exit.
- For safety reasons, adequately maintain the steps to the southern parking lot in the winter.
- Vehicles exiting the western access may have a slight obstructed view. However, this access currently exists, and no crashes were reported.

Comments not shown in the proposal are included as conditions of approval.

Parking Assessment

The proposal would provide 27 parking stalls for the two uses at the site: a financial institution and a restaurant with table service. The City of Minnetonka has recently adopted a new parking ordinance. This proposal meets the new parking.

	Tenant Size	City Code Required ²	ITE ³	Proposed
MidCountry Bank	2200 sq. ft.	7 stalls	8 stalls	27 stalls
Future tenant*	1700 sq. ft.	15 stalls	16 stalls	
Total		22 stalls	24 stalls	27 stalls

* The proposed tenant is unknown. Parking is calculated for a fast food restaurant, which requires more parking than a coffee shop.

Table 6 – Required parking stalls

Per the traffic study, the proposed parking available is sufficient.

Circulation

The traffic consultant provided one comment concerning potential circulation issues at the site.

Vehicles entering the northern access for the restaurant would need to head immediately left, which would conflict with vehicles exiting the site. (Shown in blue and red on Figure 2.) Operational impacts are expected to be minimal. However, if safety issues arise, a condition of approval would allow the city council to reasonably add or revise conditions to address any future unforeseen problems.

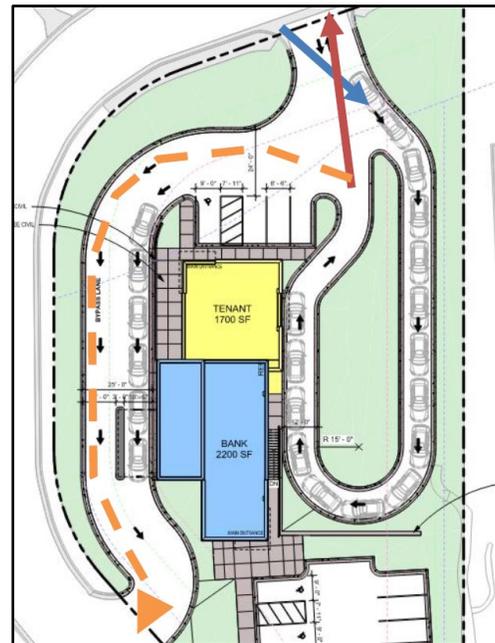


Figure 2 - Site circulation and bypass lane

Bypass Lane

One suggestion of the traffic study is the removal of a drive-thru bypass lane on the westerly side of the site. (Shown in orange on

² The newly adopted parking ordinance; approved May 1, 2023 by the City Council. The proposed ordinance requires three stalls per 1,000 sq. ft. for a general service and nine stalls per 1,000 sq. ft for a restaurant with a drive-thru.

³ The Institute of Transportation Engineers (ITE) requires 3.72 stalls per 1,000 sq. ft. for a drive-in bank, and a sit-down restaurant requires 9.44 stalls per 1,000 sq. ft.

Figure 2.) Albeit the proposal is reasonable, staff agrees that the bypass lane should be removed from the site plan.

1. Minimal internal circulation is expected in this area related to the bank use. The bypass lane would create unnecessary added internal circulation from users leaving the restaurant queue.
2. Vehicles existing on the western access have a slightly obstructed view of traffic coming from the east on the South Service road due to the curvature of the road. Traffic should be limited to bank users to mitigate potential traffic conflicts.
3. Removing the bypass lane would allow the site to meet the parking lot setback requirement from the ROW.

The removal of the bypass lane has been added as a condition of approval for this project.

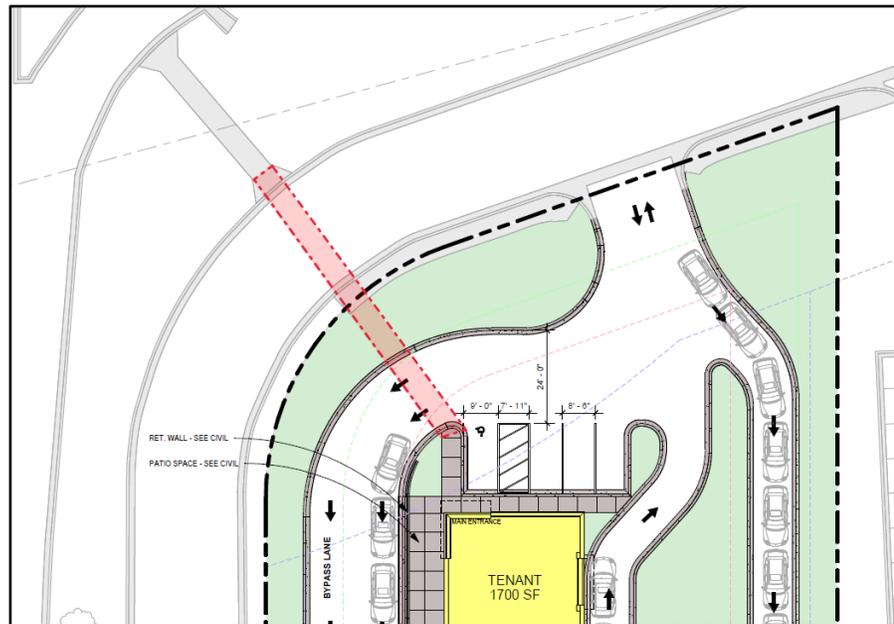


Figure 3 - Proposed Sidewalk placement

Glen Lake Trail

A trail exists connecting Glen Lake, looping north along the western side of Williston Road to the intersection with Highway 7, east across along the southern side of Service Road, and south along the western side of Wood Hill Road. Staff has included a condition of approval that a sidewalk access must connect the subject property to the existing path. The recommended location would be a straight line continuing the pedestrian crossing from the southeast corner of the intersection of Highway 7 and Williston Road.

Stormwater

New development and redevelopment of existing sites must meet specific stormwater management rules, which include runoff rate

control, runoff volume control, and water quality treatment. Best management practices (BMPs) are incorporated into stormwater management plans to control the volume of water leaving the site while improving water quality by reducing pollutant loading. Further, the rate of stormwater runoff leaving a site "post-development" must be less than or equal to the rate of runoff leaving the site "pre-development."

The plans generally meet the city's stormwater management rule.

Tree Protection

The proposal would not meet the tree protection ordinance. The proposal requests the following removals.

	Allowed removals	Proposed removals
High-Priority	One tree (35%)	Four trees (100%)*
Significant	Three trees (50%)	Four trees (57%)*
*Variances required.		

Table 7 - Tree Removal Rates

Based on discussions with the applicant, there is no alternative plan where the existing building could be removed and the number of tree removals reduced.

The site redevelopment requires minimum landscaping and mitigation requirements as outlined in the ordinance.⁴ This means that, based on the project value, the required landscaping value for the project site is determined.

CUP Standards

The proposal would meet the general conditional use permit standards as outlined in City Code 300.21 Subd.2:

1. The use is consistent with the intent of this ordinance;
2. The use is consistent with the goals, policies and objectives of the comprehensive plan;
3. The use does not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements;
4. The use is consistent with the city's water resources management plan;
5. But for the setback and tree removal variances, the use is in compliance with the performance standards specified in section 300.28 of this ordinance; and

⁴ City Code 300.27 Subd. 15 – The ordinance required minimum landscaping value is based on the project value. The project value has not been provided. The required landscaping value will be calculated at the time of the permit review.

6. The use does not have an undue adverse impact on public health, safety or welfare.

The proposed financial institution with a drive-up window would meet the conditional use permit standards outlined in City Code 300.21 Subd.4(d) for a use with a drive-up window in the B-2 district:

1. drive-up windows and stacking areas must not be located adjacent to any residential parcel;

Finding: The drive-up windows are not adjacent to residential properties.

2. stacking areas must provide for a minimum of six cars per aisle; and

Finding: The proposed site plan illustrates a stacking area for at least six vehicles for either drive-thru.

3. public address system must not be audible from any residential parcel; and

Finding: As a condition of this resolution, public address systems shall not be audible from any residential parcel.

4. must be subordinate to and associated with a permitted use located within a building on the site.

Finding: The drive-up window is associated with a financial institution that is a principal use.

The proposal would meet the specific conditional use permit standards for a fast food restaurant with a drive-thru window as outlined in City Code 300.21 Subd. 4(f):

1. Shall be located only on sites having direct access to minor arterial streets or service roads;

Finding: The subject property is accessed via the Highway 7 service road.

2. Public address systems shall not be audible from any residential parcel;

Finding: This has been included as a condition of approval.

3. Stacking for a minimum of six cars per aisle shall be provided within applicable parking lot setbacks;

Finding: The proposed site plan illustrates a stacking area for at least six vehicles for either drive-thru.

4. Shall not be permitted when traffic studies indicate significant impacts on the levels of service as defined by the Institute of Traffic Engineers of adjacent streets and intersections; and

Finding: The traffic study does not anticipate the redevelopment would impact levels of service beyond the current conditions.

5. The building shall be set back at least 100 feet and screened from any adjacent property designated in the comprehensive plan for residential use.

Finding: The proposed building would be located over 100 feet from the closed residential building and is separated by a ROW.

SBP Standards

By City Code §300.27, Subd. 5, the city will consider compliance with the following standards when evaluating site and building plans. The proposed apartment development would meet these standards.

1. Consistency with the elements and objectives of the city's development guides, including the comprehensive plan and water resources management plan;

Finding: The proposal has been reviewed by city planning, engineering, and natural resources staff and found to be generally consistent with the city's development guides, including the water resources management plan.

2. Consistency with this ordinance;

Finding: Apart from the variance, the proposal would comply with the standards of the CUP ordinance and the site and building plan review. As is outlined in the associated resolution, the variance standard is met.

3. Preservation of the site in its natural state to the extent practicable by minimizing tree and soil removal and designing grade changes to be in keeping with the general appearance of neighboring developed or developing areas;

Finding: The proposal would require a variance to the tree protection ordinance for the removal of significant and high-priority trees beyond permitted by city code. As is outlined in the associated resolution, the variance standard is met.

4. Creation of a harmonious relationship of buildings and open spaces with natural site features and with existing and future buildings having a visual relationship to the development;

Finding: The proposed redevelopment would incorporate more landscaping on the site than currently exists. The landscaping plan proposed trees, shrubs, grasses, and perennials; increasing the number of natural features.

5. Creation of a functional and harmonious design for structures and site features, with special attention to the following:
 - a) An internal sense of order for the buildings and uses on the site and provision of a desirable environment for occupants, visitors, and the general community;
 - b) The amount and location of open space and landscaping;
 - c) Materials, textures, colors, and details of construction as an expression of the design concept and the compatibility of the same with the adjacent and neighboring structures and uses; and
 - d) Vehicular and pedestrian circulation, including walkways, interior drives, and parking in terms of location and number of access points to the public streets, the width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, and arrangement and amount of parking.

Finding: The proposed redevelopment would create an additional use to the underutilized site. The site plan would create an appropriate physical order to the site, allowing a minimum of six vehicles stacking on-site for each drive-thru.

6. Promotion of energy conservation through design, location, orientation, and elevation of structures, the use and location of the glass in structures, and the use of landscape materials and site grading.

Finding: As a new construction, the proposed restaurant would include energy efficiency/conservation components required by the state building code.

7. Protection of adjacent and neighboring properties through reasonable provision for surface water drainage, sound and sight buffers, preservation of views, light, and air, and those aspects of design not adequately covered by other regulations which may have substantial effects on neighboring land uses.

Finding: The proposal would visually and physically alter the property and the immediate area. However, it is not anticipated to negatively impact adjacent or neighboring properties.

Variance Standard

By City Code §300.07, a variance may be granted from the requirements of the zoning ordinance when: (1) it is in harmony with the general purposes and intent of the ordinance; (2) it is consistent with the comprehensive plan; and (3) when an applicant establishes that there are practical difficulties in complying with the ordinance. Practical difficulties mean that the applicant proposes to use a property in a reasonable manner not permitted by the ordinance, the plight of the landowner is due to circumstances unique to the property not created by the landowner, and the variance, if granted, would not alter the essential character of the locality.

2040 Comprehensive Plan Guide

The subject property is guided for office use. Within the 2040 Comprehensive Plan, the definition of an office land use is as follows:

The office land use district provides locations for administrative, executive, professional or other offices and related service uses, such as financial institutions, lodging, daycare and similar uses. It is not intended for retail uses that serve the general public. The office designation can be used, if designed appropriately, as a transitional use between residential and more intense commercial districts.

It is the opinion of staff that the proposed office and restaurant uses on the site are an appropriately designed transition from the westerly residential land uses to the easterly commercial uses, therefore meeting the intention of the office land use definition.

Land Use Category	Zoning District (or Permitted Use within Specified Base District)
Commercial Uses	
Office	B-1 Office
	B-2 Limited Business
	B-3 General Business
	I-1 Industrial
	Planned I-394 District
	Planned Unit Development District
Commercial/Retail	B-2 Limited Business
	B-3 General Business
	Planned I-394 District
	I-1 Industrial ^(a)
	Planned Unit Development District

Table 2- Land Use Category Compared to Zoning Ordinance

Additionally, the 2040 Comp Plan provides an implementation table with the permitted uses within each land use category. As shown in Table 8, a comparison of the land use category to the zoning ordinance, the office land use allows uses from all business and industrial districts including B-2, limited office use, which would

conditionally permit a restaurant with or without drive-thru facilities at the subject property.

The proposed conditional use permit and rezoning would allow the restaurant use without the need for a comprehensive plan amendment.

Natural Resources

Best management practices must be followed during the course of site preparation and construction activities. This would include the installation and maintenance of erosion control fencing.

Neighborhood Comments

The city sent notices to 24 area property owners and residents and received no written comments to date.

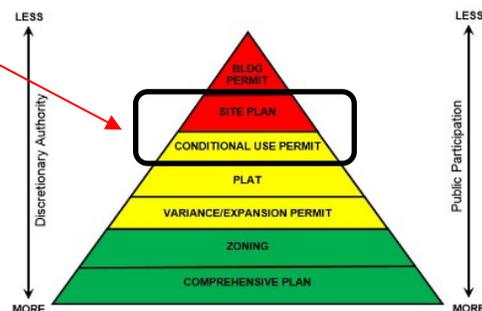
Commission Action

The planning commission will make a recommendation to the city council; a recommendation requires a majority vote of the commission. The planning commission has the following options:

1. Concur with staff's recommendation. In this case, a motion should be made recommending the city council approve the conditional use permit with associated variances and the site and building plan review.
2. Disagree with the staff's recommendation. In this case, a motion should be made recommending the city council deny one or more aspects of the proposal. The motion should include findings for denial.
3. Table the request. In this case, a motion should be made to table the item. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant, or both.

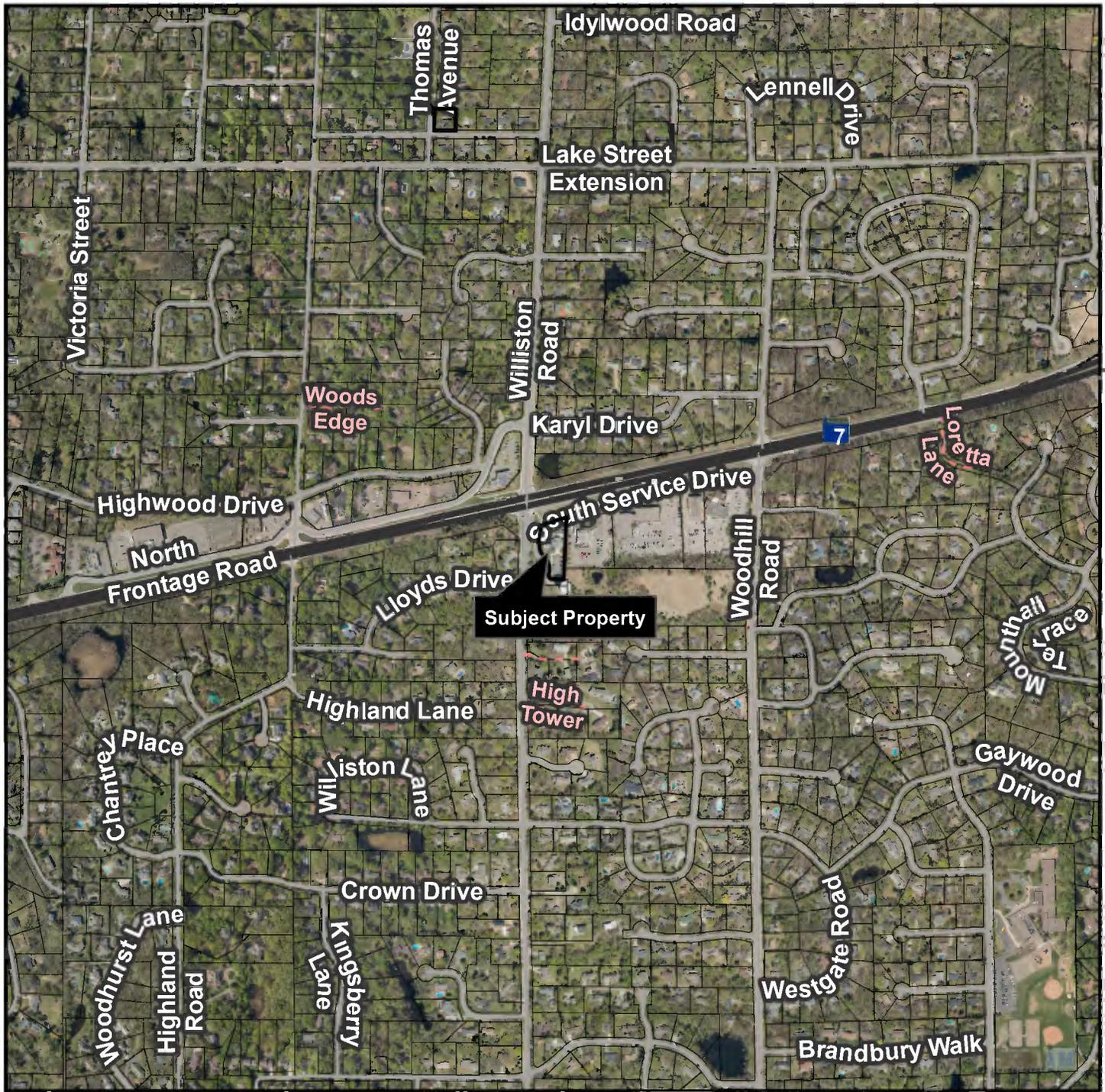
Pyramid of Discretion

This proposal:



Deadline for Action

Dec. 4, 2023



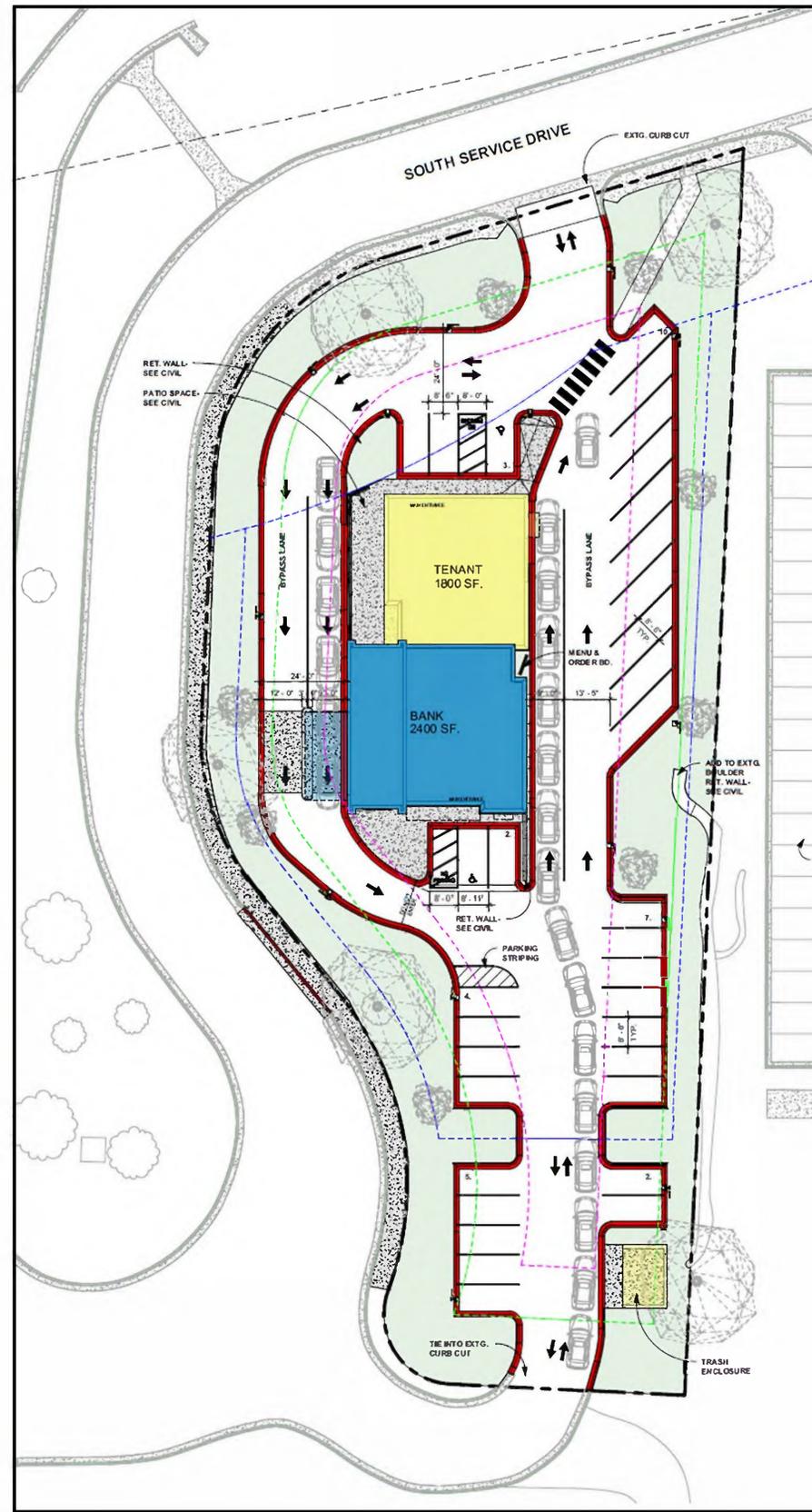
Location Map

Project: MidCountry Bank
 Address: 14617 Hwy 7

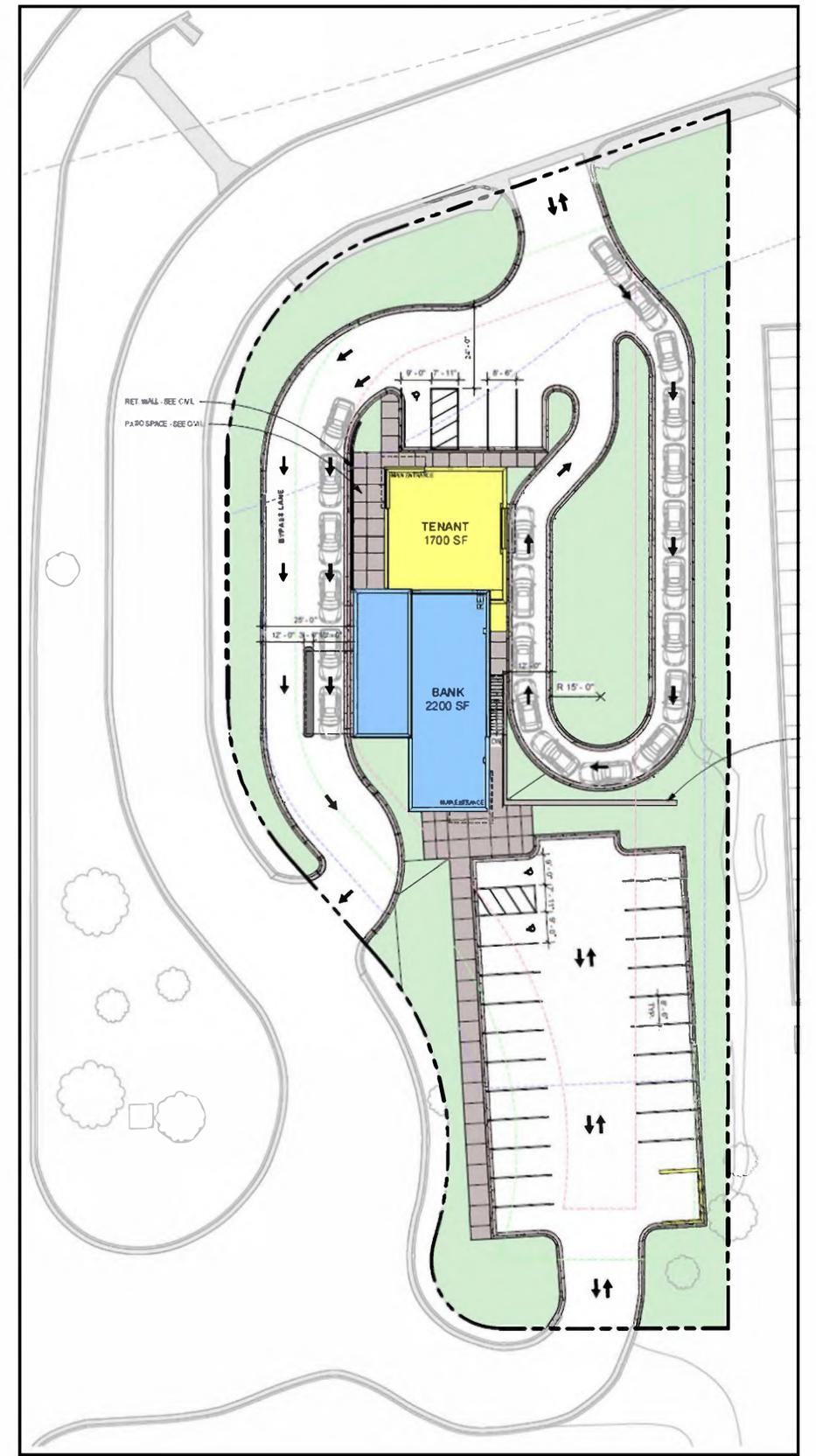




Existing Site



Previous Proposal



Current Proposal

MIDCOUNTRY BANK - REDEVELOPMENT

14617 MN-7 MINNETONKA, MN 55345

PROJECT TEAM

OWNER

MIDCOUNTRY BANK
14617 MN-7
MINNETONKA, MN 55345
CHRIS VOSBEEK
952-400-2821
CHRIS.VOSBEEK@MIDCOUNTRYBANK.COM

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SCOTTG@ECIMN.COM
VP OPERATIONS:
JON WOLFF
612-272-3409
JONW@ECIMN.COM

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EDEN PRAIRIE, MN 55344
PROJECT CONTACT
JEFF GEARS
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JEFFG@SRA-MN.COM

MECHANICAL PLUMBING

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NAME
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EMAIL@ADDRESS.COM

ELECTRICAL

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SUITE #
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NAME
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STRUCTURAL ENGINEER

Company
ANY STREET
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NAME
PHONE
EMAIL@ADDRESS.COM

CIVIL ENGINEER

CIVIL SITE GROUP
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GOLDEN VALLEY, MN 55422
DAVE KNAEBLE
763-234-7523
DKNAEBLE@CIVILSITEGROUP.COM

LANDSCAPE ARCHITECT

CIVIL SITE GROUP
5000 GLENWOOD AVENUE
GOLDEN VALLEY, MN 55422
NAME
PHONE
E-MAIL

GENERAL NOTES

- STUD FRAMING EXTENDED TO STRUCTURE ABOVE SHALL HAVE 3" X 3 5/8" GALVANIZED STUD TRACK AT TOP. STUD FRAMING SHALL BE 3/4" FROM TOP OF TRACK AND HAVE NO MECHANICAL FASTENING TO ALLOW FOR 3/4" DEFLECTION.
- VERIFY ALL EXISTING CONDITIONS, DIMENSIONS, AND ALIGNMENT OF WALLS. BRING ANY DISCREPANCIES TO THE ARCHITECT'S ATTENTION PRIOR TO FABRICATION/ CONSTRUCTION BEGINS.
- CONTRACTOR TO INSTALL EQUIPMENT PER MANUFACTURER'S REQUIREMENTS.
- HOLD 1/2" CLEARANCE BETWEEN FLOOR AND GYPSUM BOARD. FILL GAP BETWEEN BOTTOM EDGE OF GYPSUM BOARD AND FLOOR WITH SEALANT. STRIKE SEALANT SMOOTH AND FLUSH WITH FACE OF PARTITION. REMOVE EXCESS SEALANT FROM PARTITION AND FLOOR.
- CHANGES IN FLOOR MATERIALS SHALL BE LOCATED AT THE CENTERLINE OF THE DOOR LEAF OR AS SHOWN ON THE FLOOR FINISH PLAN.
- VERIFY LOCATION OF ACCESS PANELS WITH MECHANICAL AND ELECTRICAL DRAWINGS FOR ACCESS TO MECHANICAL AND ELECTRICAL ITEMS.
- SEAL PENETRATIONS IN FIRE RATED ASSEMBLIES AND SMOKE BARRIERS TO MEET REQUIRED RATINGS. UTILIZE UL APPROVED METHODS.
- PROVIDE FIRE TREATED BLOCKING AS REQUIRED TO SUPPORT ALL CABINETS, SHELVES, BUILT INS, EQUIPMENT OR ACCESSORIES. COORDINATE WITH VENDOR DOCUMENTS WHERE SUCH CONDITIONS APPLY.
- NOTIFY THE ARCHITECT IF ELECTRICAL/ COMMUNICATION/ HVAC/ PLUMBING/ ITEMS DEPICTED CONFLICT WITH ADA REQUIREMENTS OR INDUSTRY STANDARDS.
NOTE: ALL DEVICES AND CONTROLS TO BE INSTALLED WITHIN A MAXIMUM OF 4" OF EACH OTHER HORIZONTALLY (NOT 16" O.C.) AND ALIGN THE BOTTOMS OF EACH ITEM. IN THE VERTICAL POSITION ALIGN THE ITEMS ON CENTERLINES.
- DURING CONSTRUCTION, AREA SHALL BE KEPT CLEAN AND ORDERLY.
- LIGHTING, EXIT LIGHTING INFORMATION, ELECTRICAL, DATA AND TELEPHONE INFORMATION SHOWN ARE FOR ELECTRICAL CONTRACTORS REFERENCE ONLY. CONTRACTOR SHALL ENSURE COORDINATION OF ELECTRICAL ITEMS WITH BUILDING CONSTRUCTION AND EQUIPMENT AND SHALL OBTAIN THE NEEDED INFORMATION TO PROVIDE A COMPLETE AND WORKING INSTALLATION.
- CONSTRUCTION SHALL BE IN ACCORDANCE WITH STATE AND LOCAL CODES.
- PROVIDE GFI ELECTRICAL OUTLETS AT LOCATIONS REQUIRED BY CODE.

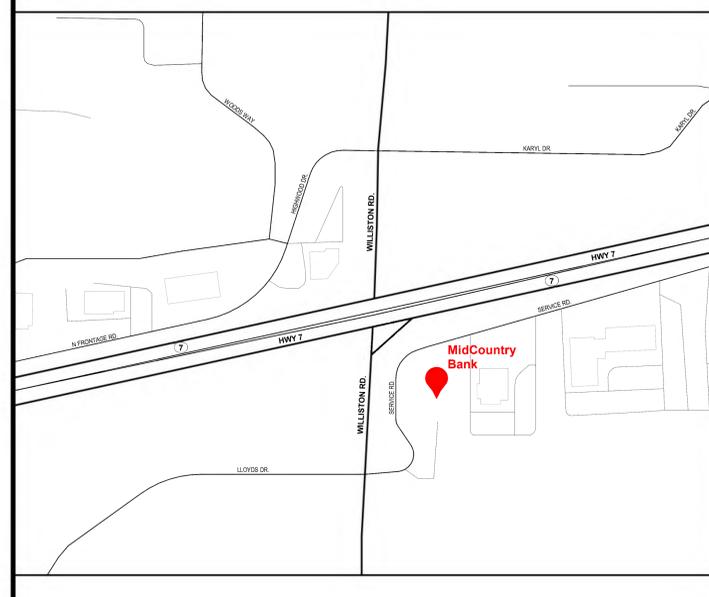
PROJECT IMAGE



SHEET INDEX

SHEET	SHEET NAME	ISSUE RECORD	VARIANCE APPLICATION	CONDITIONAL USE PERMIT	SITE AND BUILDING PLAN REVIEW
A000	TITLE SHEET				
A001	ABBREVIATIONS, SYMBOLS & TYPICAL MOUNTING HEIGHTS				
A010	LIFE SAFETY PLANS				
A020	BUILDING SYSTEMS				
A021	PARTITION TYPES				
A022	PARTITION TYPE DETAILS				
A030	ARCHITECTURAL DEMOLITION SITE PLAN				
A040	ARCHITECTURAL SITE PLAN				
A100	DEMOLITION PLAN				
A200	SITE PLAN & FLOOR PLANS				
A230	FINISH PLAN, LEGEND, AND SCHEDULE				
A230	REFLECTED CEILING PLANS				
A240	ROOF PLAN				
A300	EXTERIOR ELEVATIONS				
A301	EXTERIOR RENDERINGS				
A310	CANOPY				
A400	BUILDING SECTIONS				
A420	WALL SECTIONS				
A421	WALL SECTIONS				
A500	EXTERIOR DETAILS				
A520	ROOF DETAILS				
A600	SCHEDULES / FRAMES TYPES				
A700	CASEWORK SCHEDULE				
A701	INTERIOR ELEVATIONS				
A710	INTERIOR SECTION & DETAILS				
A800	FURNITURE PLAN				

LOCATION MAP



PRELIMINARY
NOT FOR
CONSTRUCTION

ISSUE #	DATE	DESCRIPTION
08/11/2023	08/11/2023	VARIANCE APPLICATION
08/11/2023	08/11/2023	USE PERMIT APPLICATION
08/11/2023	08/11/2023	SITE AND BUILDING PLAN REVIEW

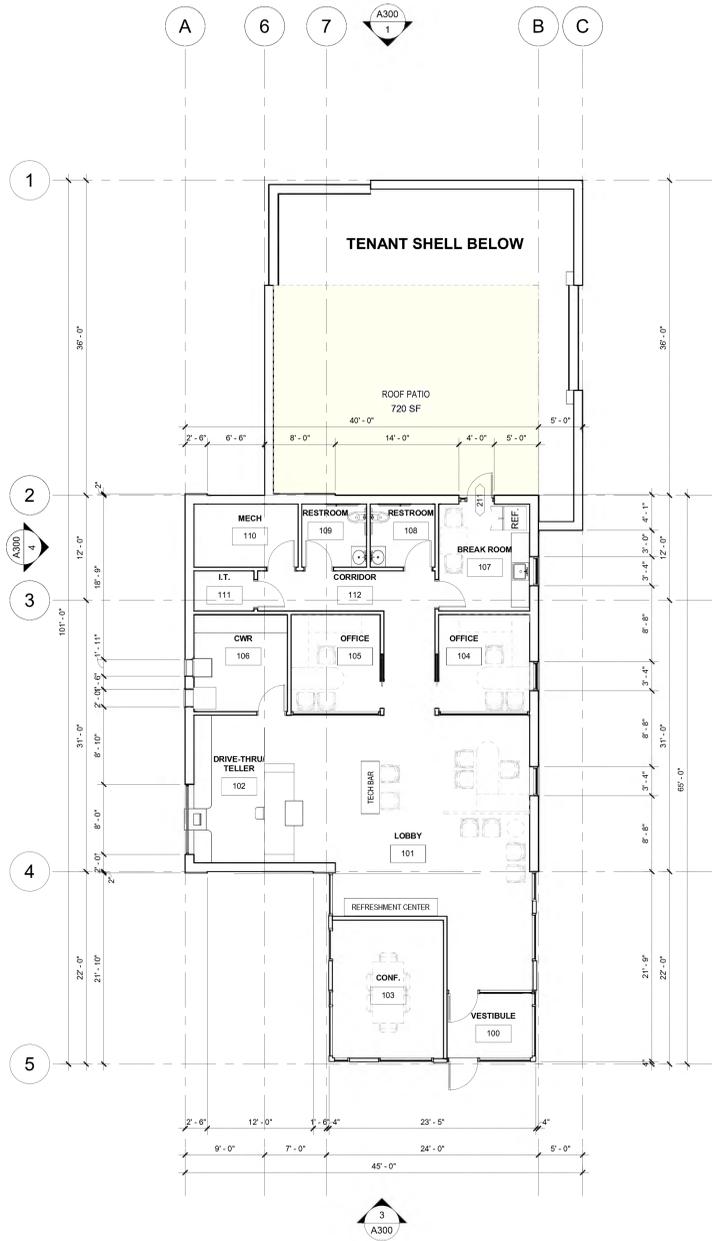
MIDCOUNTRY BANK

14617 MN-7 MINNETONKA, MN
55345

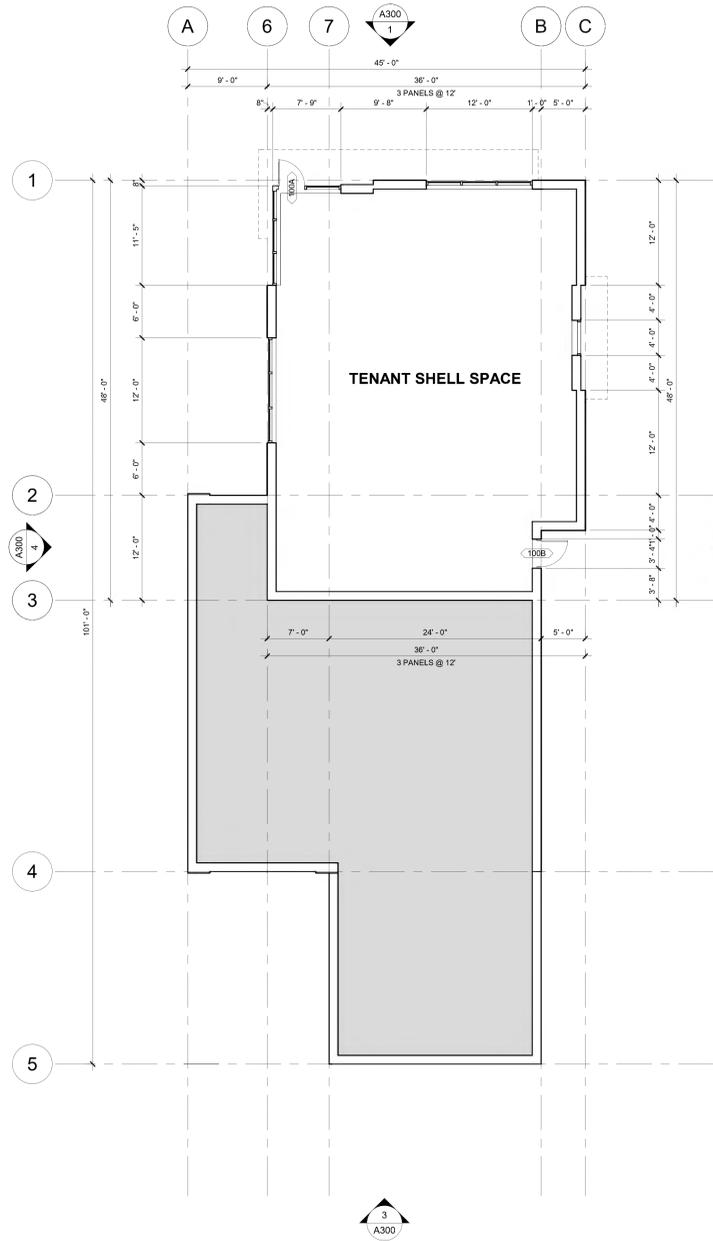
TITLE SHEET

PROJECT NO: 22-111
DRAWN BY: JK
CHECKED BY: JK

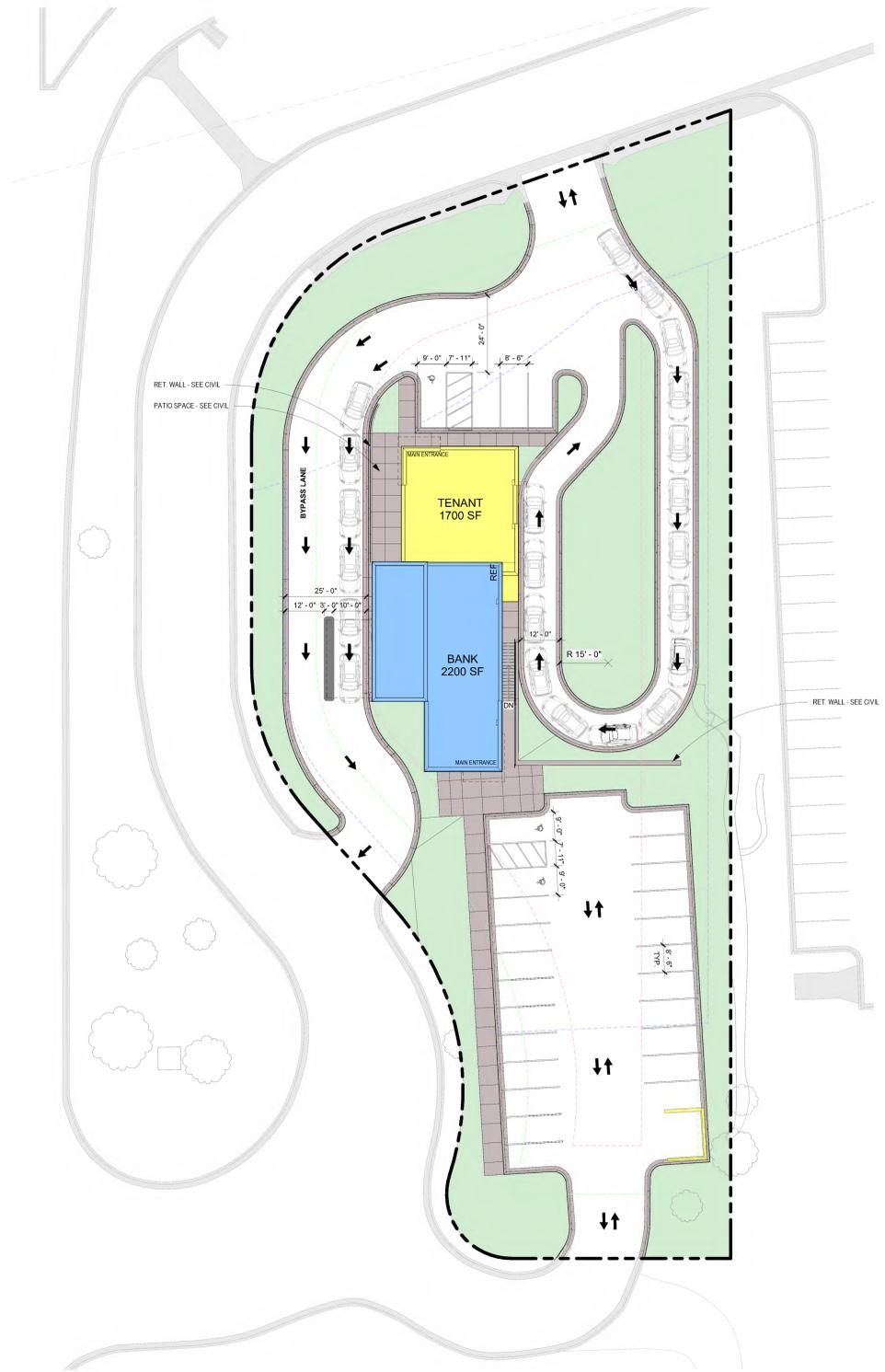
A000



2 UPPER LEVEL
A200 SCALE: 1/8" = 1'-0"



1 LOWER LEVEL
A200 SCALE: 1/8" = 1'-0"



3 SITE PLAN
A200 SCALE: 1" = 20'-0"

PRELIMINARY
NOT FOR
CONSTRUCTION

ISSUE #	DATE	DESCRIPTION
08/11/2023	08/11/2023	CONTRACT APPLICATION FOR PERMIT
08/11/2023	08/11/2023	APPLICATION FOR PERMIT
08/11/2023	08/11/2023	SEE AND BUILDING PLAN REVIEW

MIDCOUNTRY BANK

14617 MN-7 MINNETONKA, MN
55345

SITE PLAN & FLOOR
PLANS

PROJECT NO: 23-111
DRAWN BY: JG
CHECKED BY: JG

A200

PRELIMINARY
NOT FOR
CONSTRUCTION

ISSUE #	DATE	DESCRIPTION
08/11/2023	08/11/2023	ISSUE APPLICATION FOR PERMIT
08/11/2023	08/11/2023	APPLICATION
08/11/2023	08/11/2023	SEE AND BUILDING PLAN REVIEW

EXTERIOR
ELEVATIONS

PROJECT NO: 23-111
DRAWN BY: JG
CHECKED BY: JG

A300

8/11/2023 11:40:56 AM

EXTERIOR MATERIALS %

NORTH EXTERIOR ELEVATION	
PRECAST	13% (153 sf.)
STONE LOOK	40% (472 sf.)
GLAZING	17% (201 sf.)
WOODLOOK	30% (348 sf.)
METAL PANEL	00% (0 sf.)
TOTAL	100% (1,174 sf.)

EAST EXTERIOR ELEVATION	
PRECAST	11% (217 sf.)
STONE LOOK	63% (1,273 sf.)
GLAZING	12% (233 sf.)
WOODLOOK	08% (162 sf.)
METAL PANEL	07% (141 sf.)
TOTAL	100% (2,026 sf.)

SOUTH EXTERIOR ELEVATION	
PRECAST	20% (136 sf.)
STONE LOOK	00% (0 sf.)
GLAZING	25% (174 sf.)
WOODLOOK	33% (223 sf.)
METAL PANEL	22% (150 sf.)
TOTAL	100% (683 sf.)

WEST EXTERIOR ELEVATION	
PRECAST	53% (1,012 sf.)
STONE LOOK	14% (266 sf.)
GLAZING	22% (418 sf.)
WOODLOOK	03% (61 sf.)
METAL PANEL	07% (139 sf.)
TOTAL	100% (1,894 sf.)

TOTAL BUILDING	
PRECAST	26% (1,518 sf.)
STONE LOOK	35% (2,011 sf.)
GLAZING	19% (1,024 sf.)
WOODLOOK	14% (794 sf.)
METAL PANEL	07% (430 sf.)
TOTAL	100% (5,777 sf.)

STONE BLOCK LOOK PRECAST PANELS:

REFERENCE STONE

PRECAST CONCRETE - BUFF - FORMLINER - STONE BLOCK (PC-1)

PRECAST CONCRETE - GREY - FORMLINER - STONE BLOCK (PC-2)

FORMLINER

17003 LP

PRECAST EXAMPLES

DESIGN INTENT

THE INTENT IS THAT ALL PRECAST PANELS (PC-1, OR PC-2) ARE TO EMULATE THE LOOK OF THE STONE BLOCK SAMPLES SHOWN TO THE LEFT BY THE USE OF TINTED PRECAST BLENDS (BUFF BLEND & GREY BLEND) USING THE FORM LINER (OR SIMILAR) SHOWN ABOVE.

THE RIGHT TWO IMAGES ARE EXAMPLES FROM THE MANUFACTURER OF PROJECTS WITH SIMILAR INTENDED STONE BLOCK LOOK FINISHES.

PRECAST CONCRETE - BLACK/DARK BRONZE STAIN - ACID ETCH (PC-3)

WOODLOOK PVC SIDING - HONEY OAK (PS-1)

PRE-FINISHED METAL FLASHING - EXTRA DARK BRONZE (PFMF-1)

METAL PANEL - EXTRA DARK BRONZE (MPC-2)

ALUMINUM FRAMED STOREFRONT - EXTRA DARK BRONZE - THERMALLY BROKEN (ASF-1)

EXTRA DARK BRONZE

METAL PANEL - MIDCOUNTRY GREEN (MPC-1)

METAL PANEL - ACCENT GREY (MPC-3)





1
A301 BANK EXTERIOR VIEW - SW
SCALE: 1/2" = 1'-0"



2
A301 BANK EXTERIOR VIEW - SE
SCALE: 1/2" = 1'-0"



3
A301 TENANT EXTERIOR VIEW - NW
SCALE: 1/2" = 1'-0"



4
A301 TENANT EXTERIOR VIEW - NE
SCALE: 1/2" = 1'-0"

PRELIMINARY
NOT FOR
CONSTRUCTION

ISSUE #	DATE	DESCRIPTION
08/11/2023	08/11/2023	ISSUE APPLICATION FOR PERMIT
08/11/2023	08/11/2023	APPLICATION
08/11/2023	08/11/2023	SEE AND BUILDING PLAN REVIEW

MIDCOUNTRY BANK

14617 MN-7 MINNETONKA, MN
55345

EXTERIOR
RENDERINGS

PROJECT NO: 23-111
DRAWN BY: JG
CHECKED BY: JG

A301

MIDCOUNTRY BANK

MINNETONKA, MINNESOTA

ISSUED FOR: CITY SUBMITTAL



ARCHITECT:
 SRa ARCHITECTURE + INTERIORS
 6442 CITY WEST PARKWAY, #300
 EDEN PRAIRIE, MN 55344
 CONTACT: ERIC REINERS
 612-209-3749

DEVELOPER / PROPERTY OWNER:
 MIDCOUNTRY BANK FSB
 14617 WASHINGTON AVE S, SUITE 900
 BLOOMINGTON, MN 55439
 CONTACT: CHRIS VOSBEEK
 952-400-2821

ENGINEER / LANDSCAPE ARCHITECT:
 CIVIL SITE GROUP
 5000 GLENWOOD AVE
 GOLDEN VALLEY, MN 55422
 CONTACT: DAVE KNAEBLE
 612-615-0060

SURVEYOR:
 KEMPER & ASSOCIATES, INC
 721 OLD HWY 8 NW
 NEW BRIGHTON, MN 55112
 CONTACT: MARK KEMPER
 651-631-0351

GEOTECHNICAL ENGINEER:
 AMERICAN ENGINEERING TESTING, INC.
 550 CLEVELAND AVENUE NORTH
 ST. PAUL, MN 55114
 CONTACT: ROB FLICKINGER
 651-659-9001

ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "Gopher State One Call" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.



Know what's below.
 Call before you dig.

PRELIMINARY:
 NOT FOR
 CONSTRUCTION

MIDCOUNTRY BANK
 14617 STATE HIGHWAY 7, MINNETONKA, MN 55345
MIDCOUNTRY BANK FSB
 7825 WASHINGTON AVE S, SUITE 900, BLOOMINGTON, MN 55439

PROJECT
 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

David J. Knaeble
 David J. Knaeble
 DATE: 07/21/23 LICENSE NO. 48776

ISSUE/SUBMITTAL SUMMARY
 DATE DESCRIPTION
 07/21/23 CITY SUBMITTAL

SHEET INDEX	
SHEET NUMBER	SHEET TITLE
C0.0	TITLE SHEET
C1.0	REMOVALS PLAN
C1.1	TREE PRESERVATION PLAN
C2.0	SITE PLAN
C2.1	TRUCK TURNING MOVEMENT PLAN
C3.0	GRADING PLAN
C4.0	UTILITY PLAN
C5.0	CIVIL DETAILS
C5.1	CIVIL DETAILS
C5.2	CIVIL DETAILS
L1.0	LANDSCAPE PLAN
L1.1	LANDSCAPE PLAN NOTES & DETAILS
SW1.0	SWPPP - EXISTING CONDITIONS
SW1.1	SWPPP - PROPOSED CONDITIONS
SW1.2	SWPPP - DETAILS
SW1.3	SWPPP - NARRATIVE

DRAWN BY: BN, AM REVIEWED BY: DK
 PROJECT NUMBER: 22450

REVISION SUMMARY
 DATE DESCRIPTION

TITLE SHEET
C0.0
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MIDCOUNTRY BANK
14617 STATE HIGHWAY 7, MINNETONKA, MN 55345
MIDCOUNTRY BANK FSB
7825 WASHINGTON AVE S, SUITE 900, BLOOMINGTON, MN 55439

PROJECT

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

David J. Knaeble
David J. Knaeble
DATE: 07/21/23 LICENSE NO. 48776

ISSUE/SUBMITTAL SUMMARY

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REMOVALS PLAN

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REMOVAL NOTES:

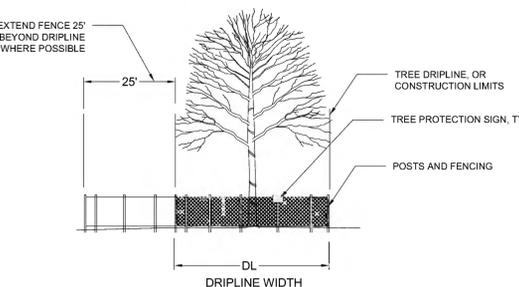
- ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
- SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
- REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
- REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
- EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
- REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
- EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
- PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
- MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS. FOR ANY WORK ON ADJACENT PRIVATE PROPERTY, THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE ADJACENT PROPERTY OWNER PRIOR TO ANY WORK.
- DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
- PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM, AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
- VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.
- ALL EXISTING SITE TRAFFIC REGULATORY SIGNAGE TO BE INVENTORIED AND IF REMOVED FOR CONSTRUCTION SHALL BE RETURNED TO LGU.
- ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CITY OF MINNETONKA REMOVAL NOTES:

- RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0 - SW1.3

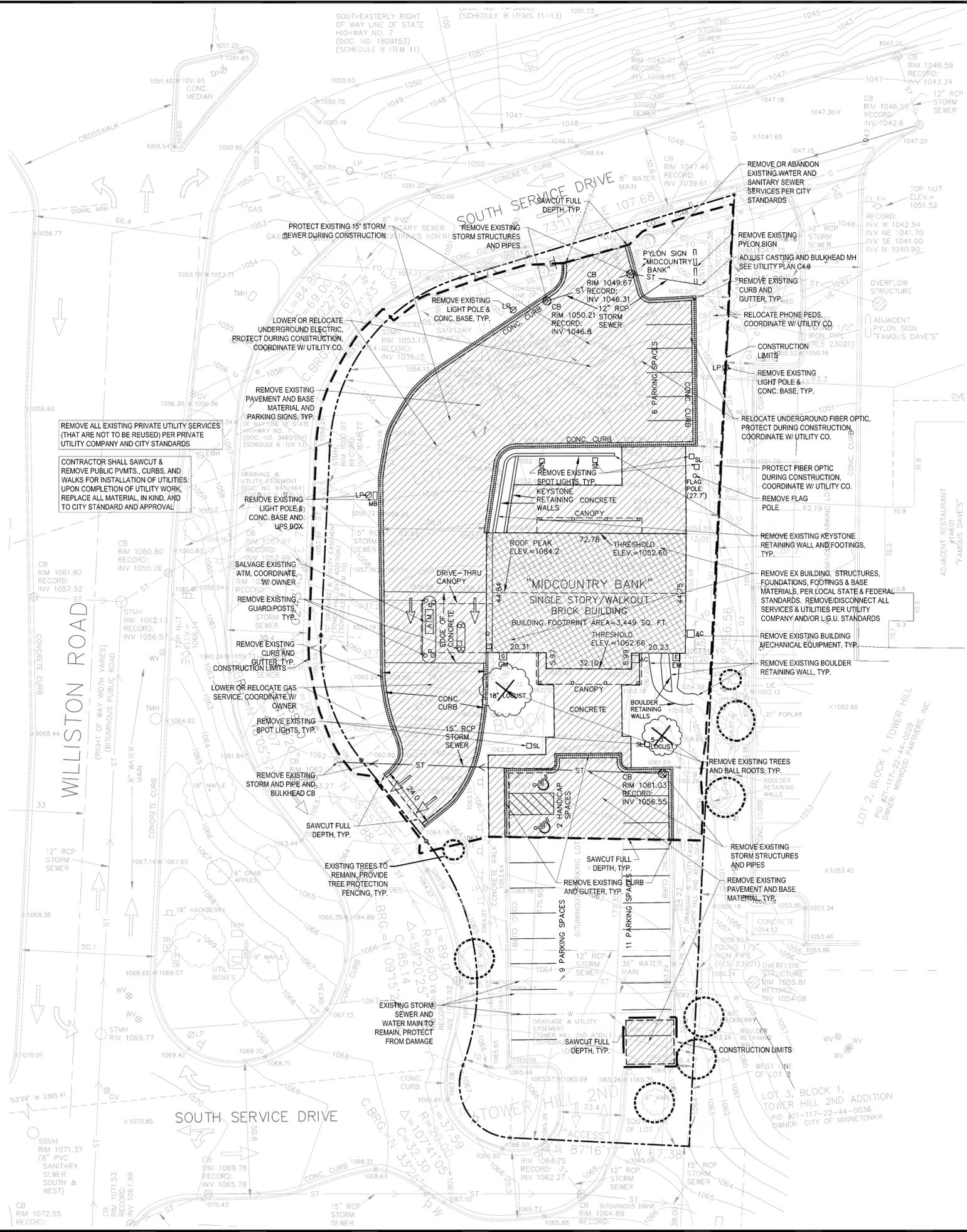
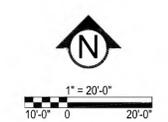


FURNISH A AND INSTALL TEMPORARY FENCE AT THE TREE'S DRIP LINE OR CONSTRUCTION LIMITS AS SHOWN ON PLAN, PRIOR TO ANY CONSTRUCTION. WHERE POSSIBLE PLACE FENCE 25' BEYOND DRIP LINE. PLACE TREE PROTECTION SIGN ON POSTS, ONE PER INDIVIDUAL TREE (FACING CONSTRUCTION ACTIVITY), OR ONE EVERY 100' LF ALONG A GROVE OR MULTI-TREE PROTECTION AREA.

**1 TREE PROTECTION
NTS**

REMOVALS LEGEND:

- 1125 --- EX. 1' CONTOUR ELEVATION INTERVAL
- [Hatched Box] REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PVMTS.
- [Cross-hatched Box] REMOVAL OF STRUCTURE INCLUDING ALL FOOTINGS AND FOUNDATIONS.
- [Dashed Box] REMOVE CURB AND GUTTER. IF IN RIGHT-OF-WAY, COORDINATE WITH LOCAL GOVERNING UNIT.
- [Circle with X] TREE PROTECTION
- [Circle with X] TREE REMOVAL - INCLUDING ROOTS AND STUMPS



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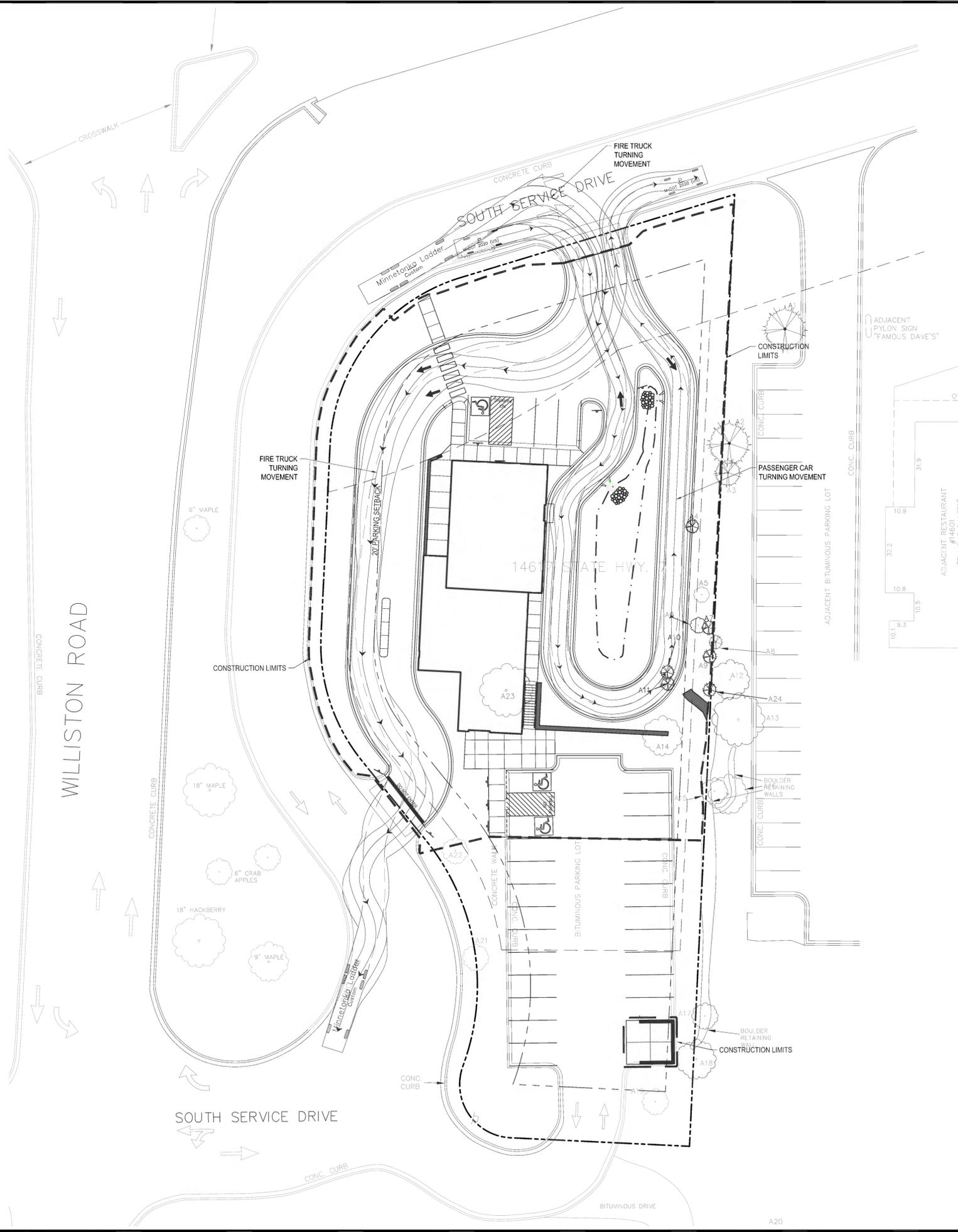
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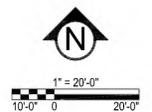
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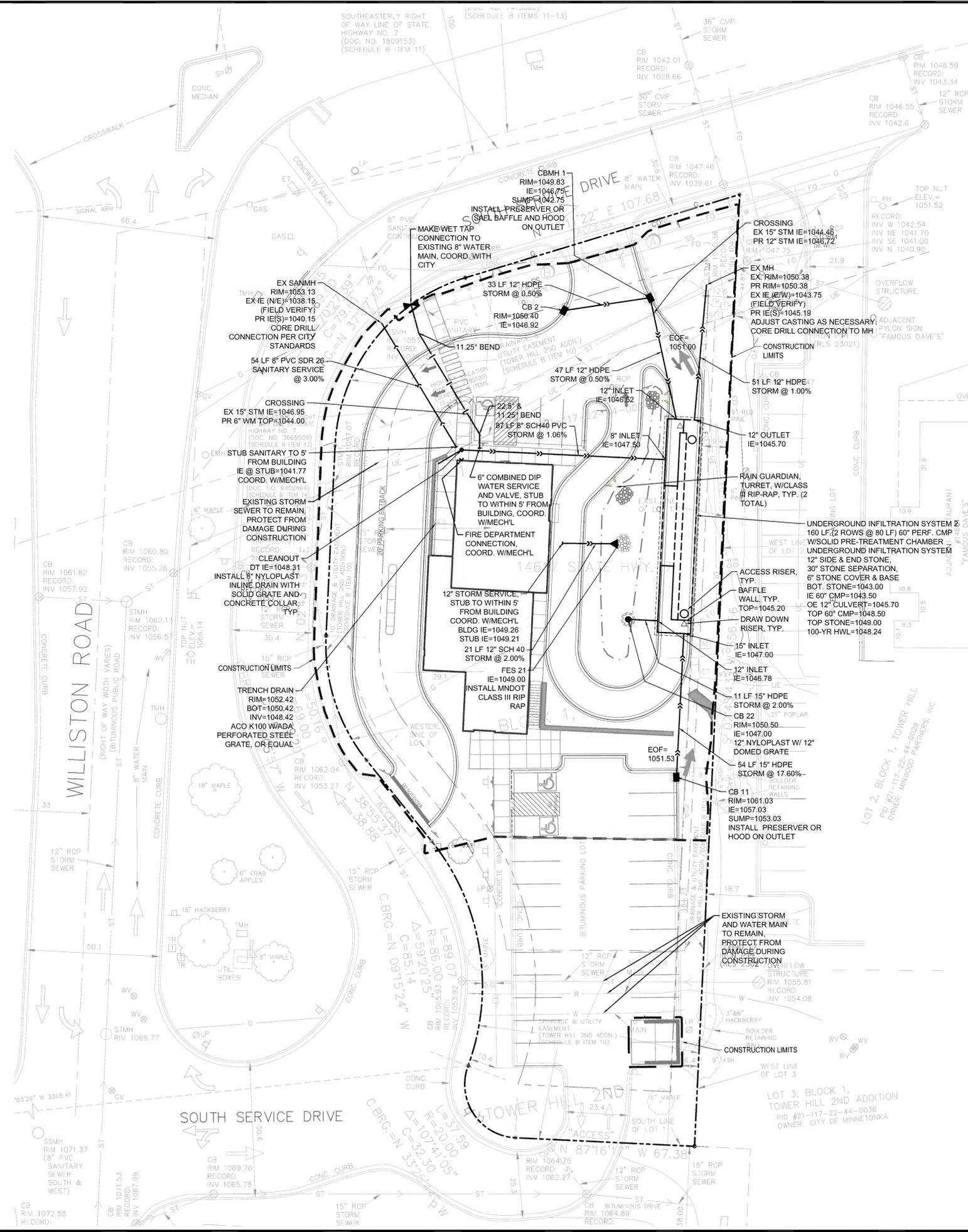
TREE MITIGATION PLAN

C2.2
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- SITE PLAN LEGEND:**
- PROPERTY LINE
 - CONSTRUCTION LIMITS
 - CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT
 - GUTTER WHERE APPLICABLE-SEE PLAN
 - TRAFFIC DIRECTIONAL ARROW PAVEMENT MARKINGS
 - SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
 HC = ACCESSIBLE SIGN
 NP = NO PARKING FIRE LANE
 ST = STOP
 CP = COMPACT CAR PARKING ONLY





GENERAL UTILITY NOTES:

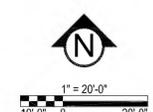
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- SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
- CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AND "SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
- CASTINGS SHALL BE SALVAGED FROM STRUCTURE REMOVALS AND RE-USED OR PLACED AT THE DIRECTION OF THE OWNER.
- ALL WATER PIPE SHALL BE CLASS 52 DUCTILE IRON PIPE (DIP) AWWA C151, ASME B16.4, AWWA C110, AWWA C153 UNLESS OTHERWISE NOTED.
- ALL SANITARY SEWER SHALL BE SDR 26 POLYVINYL CHLORIDE (PVC) ASTM D3034 & F679, OR SCH 40 ASTM D1785, 2665, ASTM F794, 1866 UNLESS OTHERWISE NOTED.
- ALL STORM SEWER PIPE SHALL BE HDPE ASTM F714 & F2306 WITH ASTM D3212 SPEC FITTINGS UNLESS OTHERWISE NOTED.
- PIPE LENGTHS SHOWN ARE FROM CENTER TO CENTER OF STRUCTURE OR TO END OF FLARED END SECTION.
- UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH ARCHITECTURAL AND MECHANICAL PLANS.
- CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.15 FEET PER DETAILS. RIM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
- A MINIMUM OF 8 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL.
- A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
- ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
- CONNECTIONS TO EXISTING STRUCTURES SHALL BE CORE-DRILLED.
- COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL DRAWINGS.
- COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
- ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROL DEVICES SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- ALL STRUCTURES, PUBLIC AND PRIVATE, SHALL BE ADJUSTED TO PROPOSED GRADES WHERE REQUIRED. THE REQUIREMENTS OF ALL OWNERS MUST BE COMPLIED WITH. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
- CONTRACTOR SHALL COORDINATE CONNECTION OF IRRIGATION SERVICE TO UTILITIES. COORDINATE THE INSTALLATION OF IRRIGATION SLEEVES NECESSARY AS TO NOT IMPACT INSTALLATION OF UTILITIES.
- CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
- ALL JOINTS AND CONNECTIONS IN STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, OR OTHER STRUCTURES.
- ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED IN ACCORDANCE WITH MN RULES, CHAPTER 4714, SECTION 1109.0.
- FOR ALL SITES LOCATED IN CLAY SOIL AREAS, DRAIN TILE MUST BE INSTALLED AT ALL LOW POINT CATCH BASINS 25' IN EACH DIRECTION. SEE PLAN AND DETAIL. INSTALL LOW POINT DRAIN TILE PER PLANS AND GEOTECHNICAL REPORT RECOMMENDATIONS AND REQUIREMENTS.

CITY OF MINNETONKA UTILITY NOTES:

- RESERVED FOR CITY SPECIFIC UTILITY NOTES.

UTILITY LEGEND:

- MANHOLE
- CATCH BASIN
- GATE VALVE AND VALVE BOX
- PROPOSED FIRE HYDRANT
- WATER MAIN
- SANITARY SEWER
- STORM SEWER



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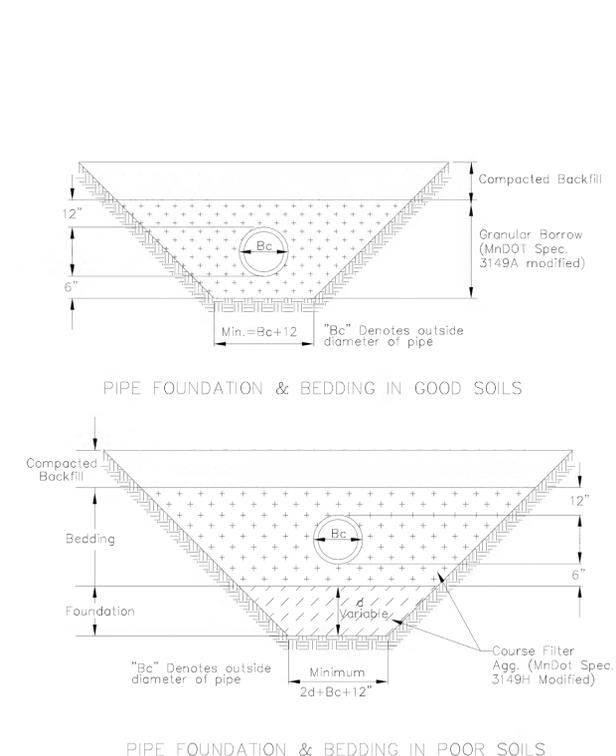
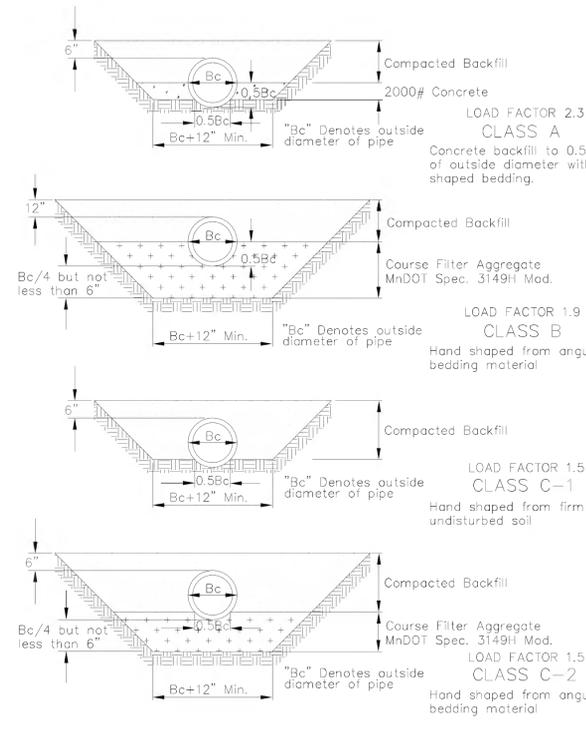
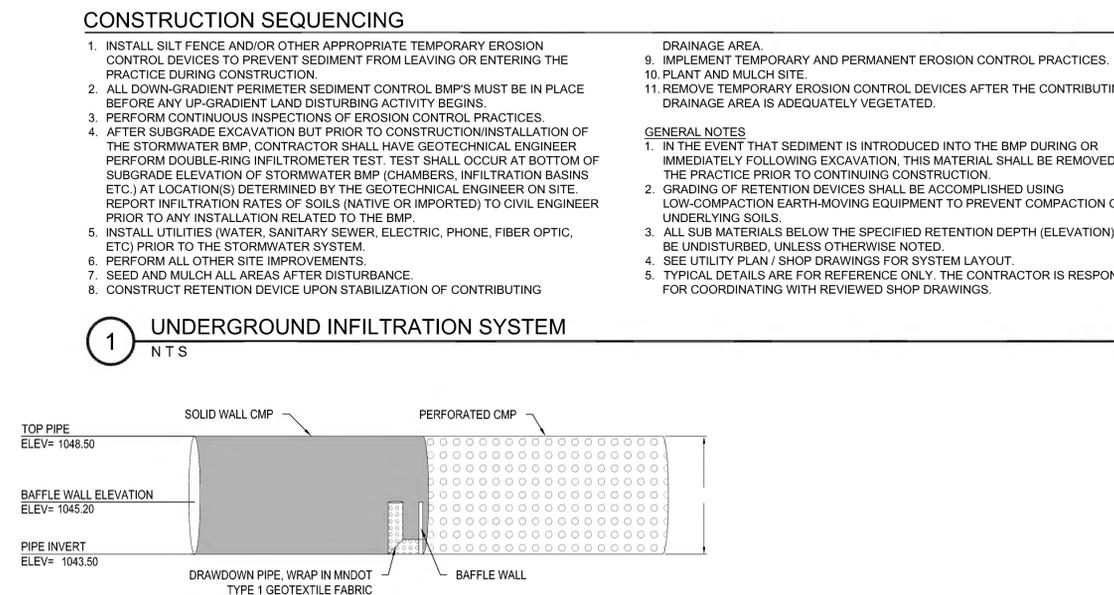
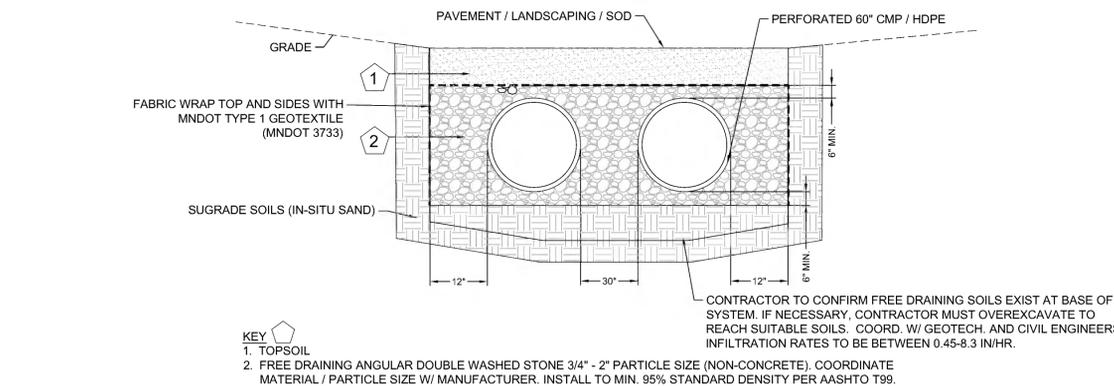
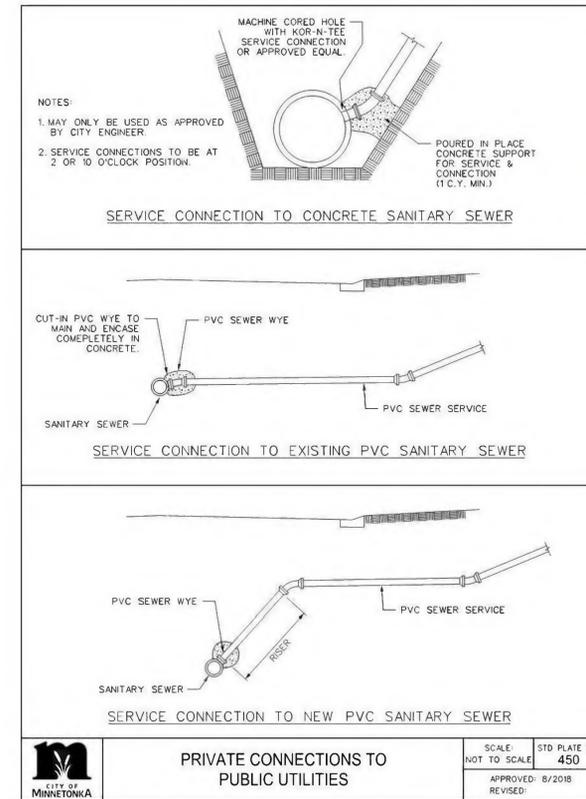
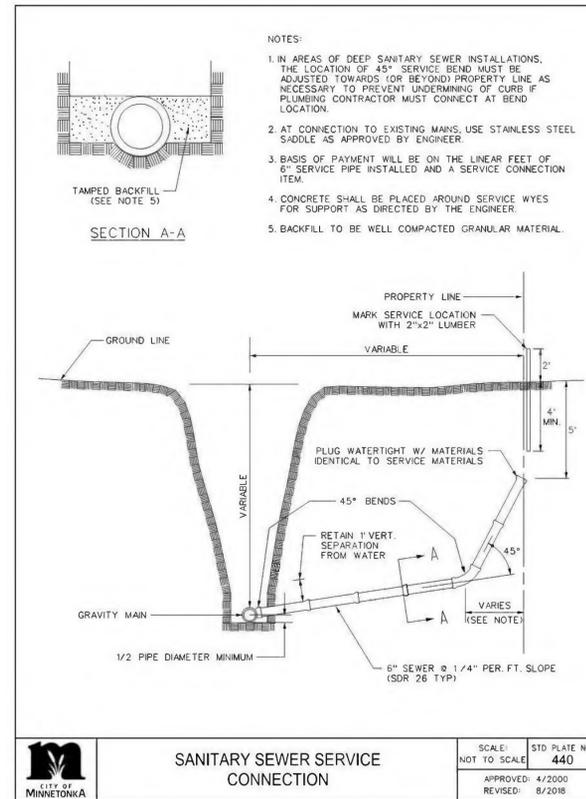
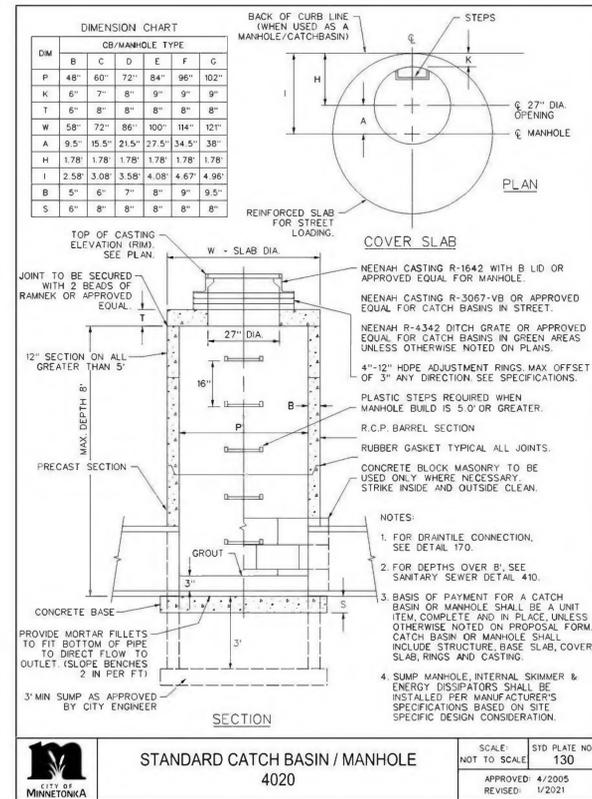
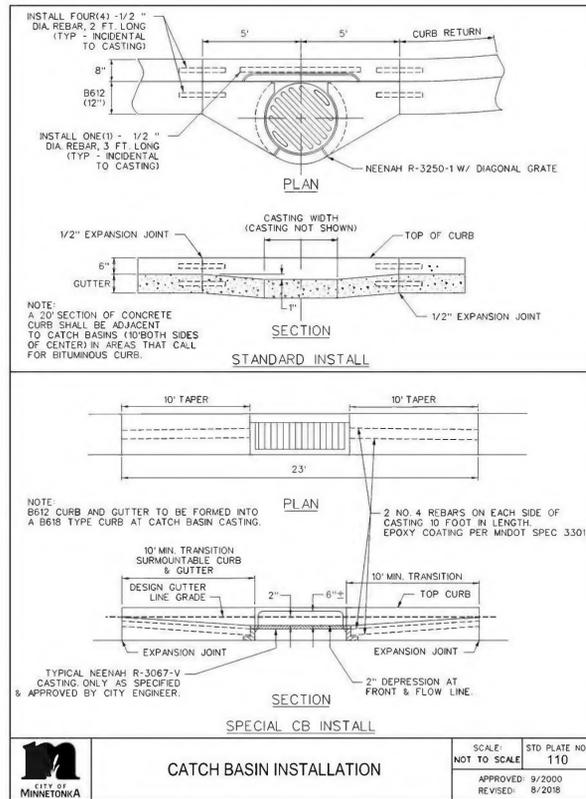
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UTILITY PLAN



CivilSite GROUP
Civil Engineering • Surveying • Landscape Architecture
5000 Glenwood Avenue
Golden Valley, MN 55422
civilsitegroup.com 612-615-0060

SRa
ARCHITECTURE | INTERIORS

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CIVIL DETAILS

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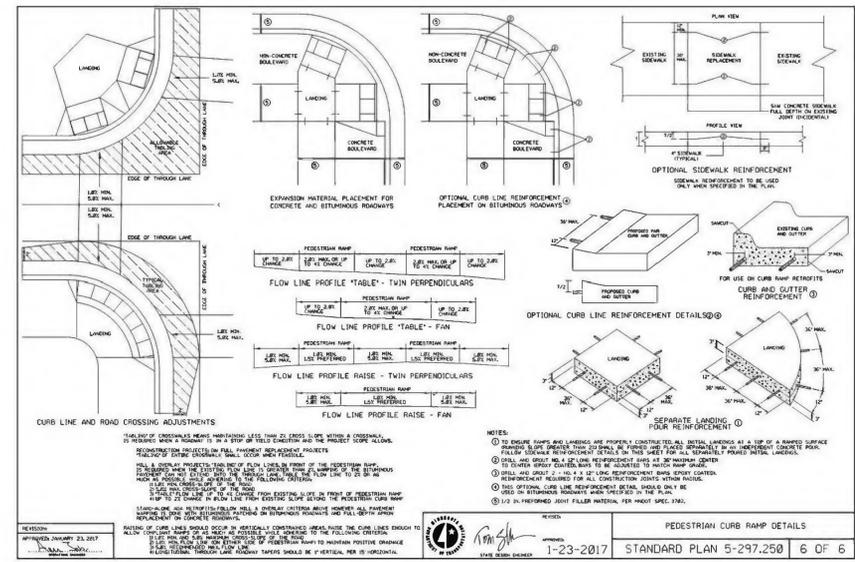
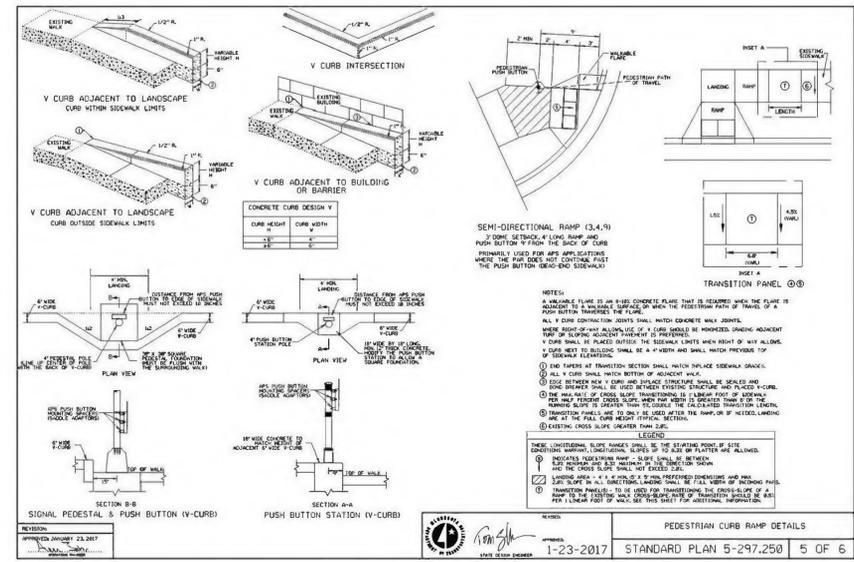
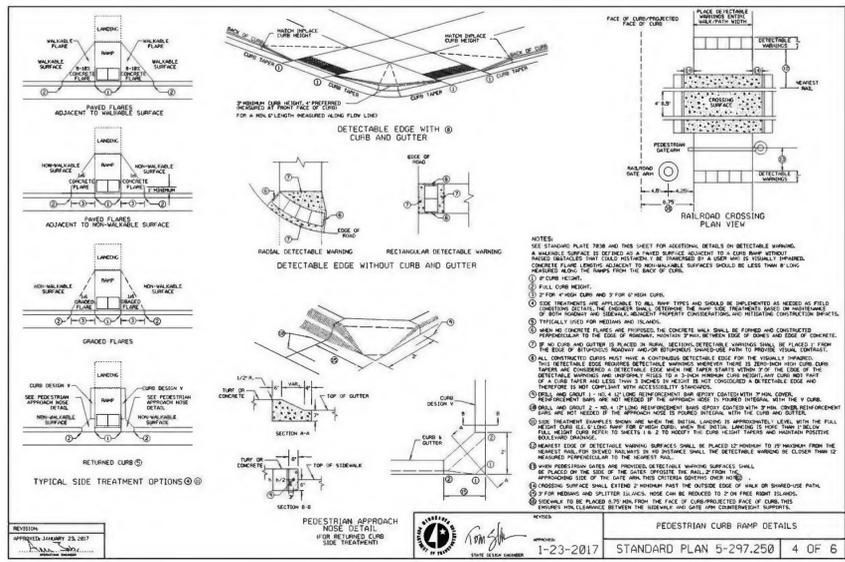
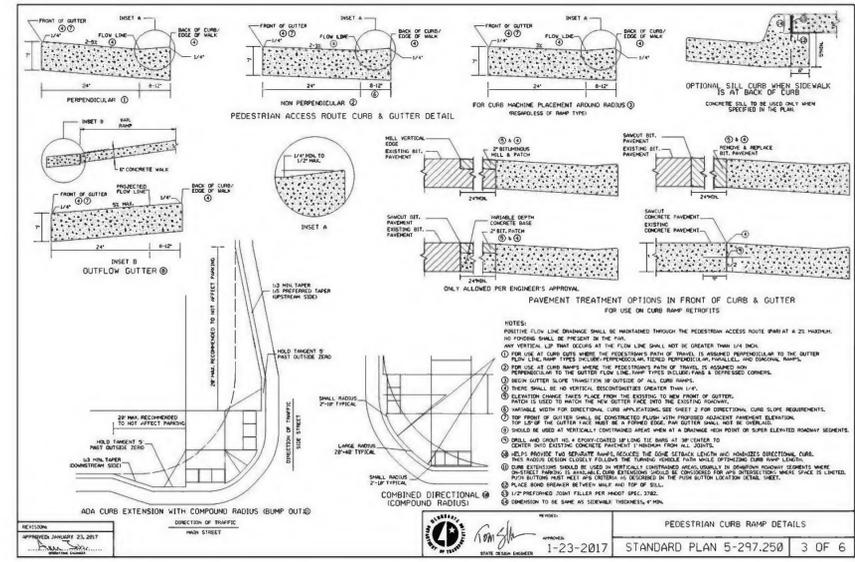
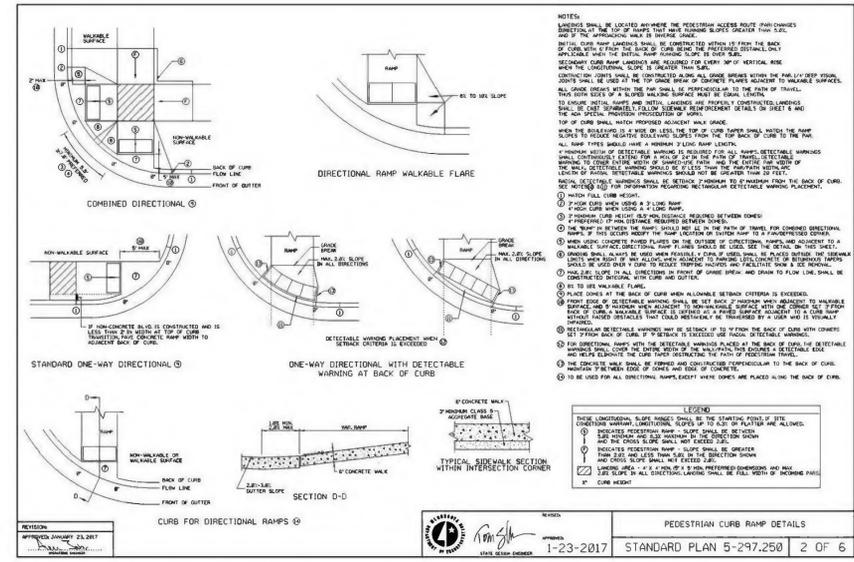
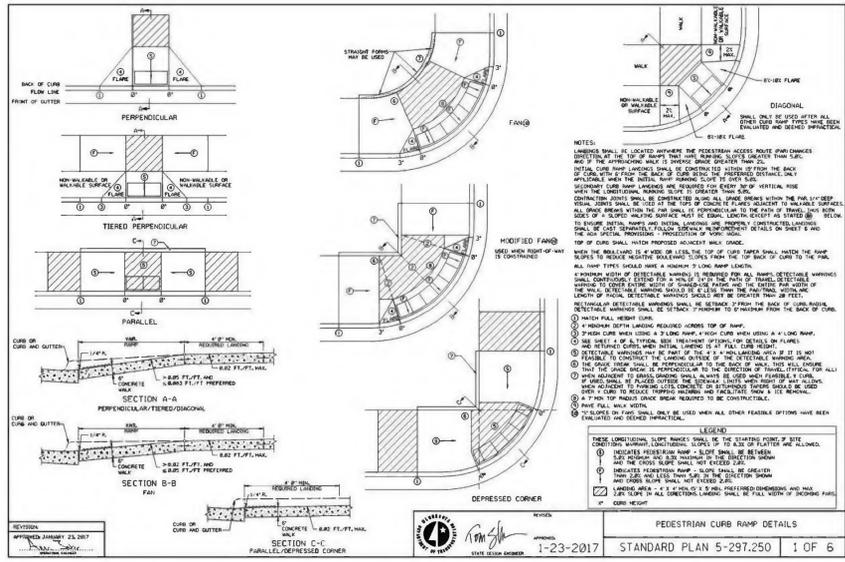
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THE CONTRACTOR AND ALL SUBCONTRACTORS INVOLVED WITH A CONSTRUCTION ACTIVITY THAT DISTURBS SITE SOIL OR WHO IMPLEMENT A POLLUTANT CONTROL MEASURE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) MUST COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) GENERAL PERMIT (DATED AUGUST 1, 2018 # MNR10001) AND ANY LOCAL GOVERNING AGENCY HAVING JURISDICTION CONCERNING EROSION AND SEDIMENTATION CONTROL.

STORMWATER DISCHARGE DESIGN REQUIREMENTS

SWPPP

THE NATURE OF THIS PROJECT WILL BE CONSISTENT WITH WHAT IS REPRESENTED IN THIS SET OF CONSTRUCTION PLANS AND SPECIFICATIONS. SEE THE SWPPP PLAN SHEETS AND SWPPP NARRATIVE (ATTACHMENT A CONSTRUCTION SWPPP TEMPLATE) FOR ADDITIONAL SITE SPECIFIC SWPPP INFORMATION. THE PLANS SHOW LOCATIONS AND TYPES OF ALL TEMPORARY AND PERMANENT EROSION PREVENTION AND SEDIMENT CONTROL BMP'S. STANDARD DETAILS ARE ATTACHED TO THIS SWPPP DOCUMENT.

THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES IS AS FOLLOWS:

1. INSTALL STABILIZED ROCK CONSTRUCTION ENTRANCE
2. INSTALLATION OF SILT FENCE AROUND SITE
3. INSTALL ORANGE CONSTRUCTION FENCING AROUND INFILTRATION AREAS
4. INSTALL INLET PROTECTION AT ALL ADJACENT AND DOWNSTREAM CATCH BASINS
5. CLEAR AND GRUB FOR TEMPORARY SEDIMENT BASIN / POND INSTALL
6. CONSTRUCT TEMPORARY SEDIMENT BASIN / POND (SECTION 14)
7. CLEAR AND GRUB REMAINDER OF SITE
8. STRIP AND STOCKPILE TOPSOIL
9. ROUGH GRADING OF SITE
10. STABILIZE DENUDED AREAS AND STOCKPILES
11. INSTALL SANITARY SEWER, WATER MAIN STORM SEWER AND SERVICES
12. INSTALL SILT FENCE / INLET PROTECTION AROUND CBS
13. INSTALL STREET SECTION
14. INSTALL CURB AND GUTTER
15. BITUMINOUS ON STREETS
16. FINAL GRADE BOULEVARD, INSTALL SEED AND MULCH
17. REMOVE ACCUMULATED SEDIMENT FROM BASIN / POND
18. FINAL GRADE POND / INFILTRATION BASINS (DO NOT COMPACT SOILS IN INFILTRATION AREAS.)
19. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED BY EITHER SEED OR SOD/LANDSCAPING, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.

RECORDS RETENTION:

THE SWPPP (ORIGINAL OR COPIES) INCLUDING, ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION BY THE PERMITTEE WHO HAS OPERATIONAL CONTROL OF THAT PORTION OF THE SITE. THE SWPPP CAN BE KEPT IN EITHER THE FIELD OFFICE OR IN AN ON SITE VEHICLE DURING NORMAL WORKING HOURS.

ALL OWNER(S) MUST KEEP THE SWPPP, ALONG WITH THE FOLLOWING ADDITIONAL RECORDS, ON FILE FOR THREE (3) YEARS AFTER SUBMITTAL OF THE NOT AS OUTLINED IN SECTION 4. THIS DOES NOT INCLUDE ANY RECORDS AFTER SUBMITTAL OF THE NOT.

1. THE FINAL SWPPP
2. ANY OTHER STORMWATER RELATED PERMITS REQUIRED FOR THE PROJECT;
3. RECORDS OF ALL INSPECTION AND MAINTENANCE CONDUCTED DURING CONSTRUCTION (SEE SECTION 11, INSPECTIONS AND MAINTENANCE);
4. ALL PERMIT OPERATION AND MAINTENANCE AGREEMENTS THAT HAVE BEEN IMPLEMENTED, INCLUDING ALL RIGHT OF WAY, CONTRACTS, COVENANTS AND OTHER BINDING REQUIREMENTS REGARDING PERPETUAL MAINTENANCE, AND
5. ALL REQUIRED CALCULATIONS FOR DESIGN OF THE TEMPORARY AND PERMANENT STORMWATER MANAGEMENT SYSTEMS.

SWPPP IMPLEMENTATION RESPONSIBILITIES:

1. THE OWNER AND CONTRACTOR ARE PERMITTEE(S) AS IDENTIFIED BY THE NPDES PERMIT.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ON-SITE IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE CONTRACTOR'S SUBCONTRACTORS.
3. CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWN/LEGIBLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMP'S TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMP'S AND IMPLEMENTATION OF THE SWPPP
4. CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMP'S IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT. ONE OF THESE INDIVIDUAL(S) MUST BE AVAILABLE FOR AN ON-SITE INSPECTION WITHIN 72 HOURS UPON REQUEST BY MPCA. CONTRACTOR SHALL PROVIDE TRAINING DOCUMENTATION FOR THESE INDIVIDUAL(S) AS REQUIRED BY THE NPDES PERMIT. THIS TRAINING DOCUMENTATION SHALL BE RECORDED IN OR WITH THE SWPPP BEFORE THE START OF CONSTRUCTION OR AS SOON AS THE PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED. DOCUMENTATION SHALL INCLUDE:
 - 4.1. NAMES OF THE PERSONNEL ASSOCIATED WITH THE PROJECT THAT ARE REQUIRED TO BE TRAINED PER SECTION 21 OF THE PERMIT.
 - 4.2. DATES OF TRAINING AND NAME OF INSTRUCTOR AND ENTITY PROVIDING TRAINING.
 - 4.3. CONTENT OF TRAINING COURSE OR WORKSHOP INCLUDING THE NUMBER OF HOURS OF TRAINING.
5. FOLLOWING FINAL STABILIZATION AND THE TERMINATION OF COVERAGE FOR THE NPDES PERMIT, THE OWNER IS EXPECTED TO FURNISH LONG TERM OPERATION AND MAINTENANCE (O & M) OF THE PERMANENT STORM WATER MANAGEMENT SYSTEM.

CONSTRUCTION ACTIVITY REQUIREMENTS

SWPPP AMENDMENTS (SECTION 6):

1. ONE OF THE INDIVIDUALS DESCRIBED IN ITEM 21.2.A OR ITEM 21.2.B OR ANOTHER QUALIFIED INDIVIDUAL MUST COMPLETE ALL SWPPP CHANGES. CHANGES INVOLVING THE USE OF A LESS STRINGENT BMP MUST INCLUDE A JUSTIFICATION DESCRIBING HOW THE REPLACEMENT BMP IS EFFECTIVE FOR THE SITE CHARACTERISTICS.
2. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMP'S AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS HAVING A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER.
3. PERMITTEES MUST AMEND THE SWPPP TO INCLUDE ADDITIONAL OR MODIFIED BMP'S AS NECESSARY TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER INSPECTIONS OR INVESTIGATIONS BY THE SITE OWNER OR OPERATOR, USEPA OR MPCA OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING THE DISCHARGE OF POLLUTANTS TO SURFACE WATERS OR GROUNDWATER OR THE DISCHARGES ARE CAUSING WATER QUALITY STANDARD EXCEEDANCES (E.G., NUISANCE CONDITIONS AS DEFINED IN MINN. R. 7050.0210, SUBP. 2) OR THE SWPPP IS NOT CONSISTENT WITH THE OBJECTIVES OF A USEPA APPROVED TMDL.

BMP SELECTION AND INSTALLATION (SECTION 7):

1. PERMITTEES MUST SELECT, INSTALL, AND MAINTAIN THE BMP'S IDENTIFIED IN THE SWPPP AND IN THIS PERMIT IN AN APPROPRIATE AND FUNCTIONAL MANNER AND IN ACCORDANCE WITH RELEVANT MANUFACTURER SPECIFICATIONS AND ACCEPTED ENGINEERING PRACTICES.

EROSION PREVENTION (SECTION 8):

1. BEFORE WORK BEGINS, PERMITTEES MUST DELINEATE THE LOCATION OF AREAS NOT TO BE DISTURBED.
2. PERMITTEES MUST MINIMIZE THE NEED FOR DISTURBANCE OF PORTIONS OF THE PROJECT WITH STEEP SLOPES. WHEN STEEP SLOPES MUST BE DISTURBED, PERMITTEES MUST USE TECHNIQUES SUCH AS PHASING AND STABILIZATION PRACTICES DESIGNED FOR STEEP SLOPES (E.G., SLOPE DRAINING AND TERRACING).
3. PERMITTEES MUST STABILIZE ALL EXPOSED SOIL AREAS, INCLUDING STOCKPILES. STABILIZATION MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION WHEN CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. STABILIZATION MUST BE COMPLETED NO LATER THAN 14 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY HAS CEASED. STABILIZATION IS NOT REQUIRED ON CONSTRUCTED BASE COMPONENTS OF ROADS, PARKING LOTS AND SIMILAR SURFACES. STABILIZATION IS NOT REQUIRED ON TEMPORARY STOCKPILES WITHOUT SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS (E.G., CLEAN AGGREGATE STOCKPILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES) BUT PERMITTEES MUST PROVIDE SEDIMENT CONTROLS AT THE BASE OF THE STOCKPILE.
4. FOR PUBLIC WATERS THAT THE MINNESOTA DNR HAS PROMULGATED 'WORK IN WATER RESTRICTIONS' DURING SPECIFIED FISH SPAWNING TIME FRAMES, PERMITTEES MUST COMPLETE STABILIZATION OF ALL EXPOSED SOIL AREAS WITHIN 200 FEET OF THE WATERS EDGE, AND THAT DRAIN TO THESE WATERS, WITHIN 24 HOURS DURING THE RESTRICTION PERIOD.
5. PERMITTEES MUST STABILIZE THE NORMAL WETTED PERIMETER OF THE LAST 200 LINEAR FEET OF TEMPORARY OR PERMANENT DRAINAGE DITCHES OR SWALES THAT DRAIN WATER FROM THE SITE WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER OR PROPERTY EDGE. PERMITTEES MUST COMPLETE STABILIZATION OF REMAINING PORTIONS OF TEMPORARY OR PERMANENT DITCHES OR SWALES WITHIN 14 CALENDAR DAYS AFTER CONNECTING TO A SURFACE WATER OR PROPERTY EDGE AND CONSTRUCTION IN THAT PORTION OF THE DITCH TEMPORARILY OR PERMANENTLY CEASES.
6. TEMPORARY OR PERMANENT DITCHES OR SWALES BEING USED AS A SEDIMENT CONTAINMENT SYSTEM DURING CONSTRUCTION (WITH PROPERLY DESIGNED ROCK-DITCH CHECKS, BIO RILLS, SILT DIKES, ETC.) DO NOT NEED TO BE STABILIZED. PERMITTEES MUST STABILIZE THESE AREAS WITHIN 24 HOURS AFTER THEIR USE AS A SEDIMENT CONTAINMENT SYSTEM CEASES.
7. PERMITTEES MUST NOT USE MULCH, HYDROMULCH, TACKIFIER, POLYACRYLAMIDE OR SIMILAR EROSION PREVENTION PRACTICES WITHIN ANY PORTION OF THE NORMAL WETTED PERIMETER OF A TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE SECTION WITH A CONTINUOUS SLOPE OF GREATER THAN 2 PERCENT.
8. PERMITTEES MUST PROVIDE TEMPORARY OR PERMANENT ENERGY DISSIPATION AT ALL PIPE OUTLETS WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER OR PERMANENT STORMWATER TREATMENT SYSTEM.
9. PERMITTEES MUST NOT DISTURB MORE LAND (I.E., PHASING) THAN CAN BE EFFECTIVELY INSPECTED AND MAINTAINED IN ACCORDANCE WITH SECTION 11.

SEDIMENT CONTROL (SECTION 9):

1. PERMITTEES MUST ESTABLISH SEDIMENT CONTROL BMP'S ON ALL DOWNGRADIENT PERIMETERS OF THE SITE AND DOWNGRADIENT AREAS OF THE SITE THAT DRAIN TO ANY SURFACE WATER, INCLUDING CURB AND GUTTER SYSTEMS. PERMITTEES MUST LOCATE SEDIMENT CONTROL PRACTICES UPGRADIENT OF ANY BUFFER ZONES. PERMITTEES MUST INSTALL SEDIMENT CONTROL PRACTICES BEFORE ANY UPGRADIENT LAND-DISTURBING ACTIVITIES BEGIN AND MUST KEEP THE SEDIMENT CONTROL PRACTICES IN PLACE UNTIL THEY ESTABLISH PERMANENT COVER.
2. IF DOWNGRADIENT SEDIMENT CONTROLS ARE OVERLOADED, BASED ON FREQUENT FAILURE OR EXCESSIVE MAINTENANCE REQUIREMENTS, PERMITTEES MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMP'S TO ELIMINATE THE OVERLOADING AND AMEND THE SWPPP TO IDENTIFY THESE ADDITIONAL PRACTICES AS REQUIRED IN ITEM 6.3.

3. TEMPORARY OR PERMANENT DRAINAGE DITCHES AND SEDIMENT BASINS DESIGNED AS PART OF A SEDIMENT CONTAINMENT SYSTEM (E.G., DITCHES WITH ROCK-CHECK DAMS) REQUIRE SEDIMENT CONTROL PRACTICES ONLY AS APPROPRIATE FOR SITE CONDITIONS.
4. A FLOATING SILT CURTAIN PLACED IN THE WATER IS NOT A SEDIMENT CONTROL BMP TO SATISFY ITEM 9.2 EXCEPT WHEN WORKING OPERATIONS (E.G., CONCRETE STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE CONSTRUCTION ACTIVITY. PERMITTEES MUST PREVENT LIQUID AND SOLID WASHOUT WASTES FROM CONTACTING THE GROUND AND MUST DESIGN THE CONTAINMENT SO IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. PERMITTEES MUST PROPERLY DISPOSE LIQUID AND SOLID WASTES IN COMPLIANCE WITH MPCA RULES. PERMITTEES MUST INSTALL A SIGN INDICATING THE LOCATION OF THE WASHOUT FACILITY.
5. PERMITTEES MUST RE-INSTALL ALL SEDIMENT CONTROL PRACTICES ADJUSTED OR REMOVED TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS LEAVING OR GRADING, OR PASSAGE OF VEHICLES, IMMEDIATELY AFTER THE SHORT-TERM ACTIVITY IS COMPLETED. PERMITTEES MUST RE-INSTALL SEDIMENT CONTROL PRACTICES BEFORE THE NEXT PRECIPITATION EVENT EVEN IF THE SHORT-TERM ACTIVITY IS NOT COMPLETE.
6. PERMITTEES MUST PROTECT ALL STORM DRAIN INLETS USING APPROPRIATE BMP'S DURING CONSTRUCTION UNTIL THEY ESTABLISH PERMANENT COVER ON ALL AREAS WITH POTENTIAL FOR DISCHARGING TO THE INLET.
7. PERMITTEES MAY REMOVE INLET PROTECTION TO SET UP A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (E.G., STREET FLOODING/FREEZING) IS IDENTIFIED BY THE PERMITTEES OR THE JURISDICTIONAL AGENCY (E.G., CITY/COUNTY/TOWNSHIP/MINNESOTA DEPARTMENT OF TRANSPORTATION ENGINEER). PERMITTEES MUST DOCUMENT THE NEED FOR REMOVAL IN THE SWPPP.
8. PERMITTEES MUST PROVIDE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AT THE BASE OF STOCKPILES ON THE DOWNGRADIENT PERIMETER.
9. PERMITTEES MUST LOCATE STOCKPILES OUTSIDE OF NATURAL BUFFERS OR SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.
10. PERMITTEES MUST INSTALL A VEHICLE TRACKING BMP TO MINIMIZE THE TRACK OUT OF SEDIMENT FROM THE CONSTRUCTION SITE OR ONTO PAVED ROADS WITHIN THE SITE.
11. PERMITTEES MUST USE STREET SWEEPING IF VEHICLE TRACKING BMP'S ARE NOT ADEQUATE TO PREVENT SEDIMENT TRACKING ONTO THE STREET.
12. PERMITTEES MUST INSTALL TEMPORARY SEDIMENT BASINS AS REQUIRED IN SECTION 14.
13. IN ANY AREAS OF THE SITE WHERE FINAL VEGETATIVE STABILIZATION WILL OCCUR, PERMITTEES MUST RESTRICT VEHICLE AND EQUIPMENT USE TO MINIMIZE SOIL COMPACTION.
14. PERMITTEES MUST PRESERVE TOPSOIL ON THE SITE, UNLESS INFEASIBLE.
15. PERMITTEES MUST DIRECT DISCHARGES FROM BMP'S TO VEGETATED AREAS UNLESS INFEASIBLE.
16. PERMITTEES MUST PRESERVE A 5 FOOT NATURAL BUFFER OR, IF A BUFFER IS INFEASIBLE ON THE SITE, PROVIDE REDUNDANT (DOUBLE) PERIMETER SEDIMENT CONTROLS WHEN A SURFACE WATER IS LOCATED WITHIN 50 FEET OF THE PROJECT'S EARTH DISTURBANCES AND STORMWATER FLOWS TO THE SURFACE WATER. PERMITTEES MUST INSTALL PERIMETER SEDIMENT CONTROLS AT LEAST 5 FEET APART UNLESS LIMITED BY LACK OF AVAILABLE SPACE. NATURAL BUFFERS ARE NOT REQUIRED ADJACENT TO ROAD DITCHES, JUDICIAL DITCHES, COUNTY DITCHES, STORMWATER CONVEYANCE CHANNELS, STORM DRAIN INLETS, AND SEDIMENT BASINS. IF PRESERVING THE BUFFER IS INFEASIBLE, PERMITTEES MUST DOCUMENT THE REASONS IN THE SWPPP. SHEET PILING IS A REDUNDANT PERIMETER CONTROL IF INSTALLED IN A MANNER THAT RETAINS ALL STORMWATER.
17. PERMITTEES MUST USE POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, DOSING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR SUPPLIER. THE PERMITTEES MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION AND MUST DIRECT TREATED STORMWATER TO A SEDIMENT CONTROL SYSTEM FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.

DEWATERING AND BASIN DRAINING (SECTION 10):

1. PERMITTEES MUST DISCHARGE TURBID OR SEDIMENT-LADEN WATERS RELATED TO DEWATERING OR BASIN DRAINING (E.G., PUMPED DISCHARGES, TRENCH/DITCH CUTS FOR DRAINAGE) TO A TEMPORARY OR PERMANENT SEDIMENT BASIN ON THE PROJECT SITE UNLESS INFEASIBLE. PERMITTEES MUST DEWATER TO SURFACE WATERS IF THEY VISUALLY CHECK TO ENSURE ADEQUATE TREATMENT HAS BEEN OBTAINED AND NUISANCE CONDITIONS (SEE MINN. R. 7050.0210, SUBP. 2) WILL NOT RESULT FROM THE DISCHARGE. IF PERMITTEES CANNOT DISCHARGE THE WATER TO A SEDIMENTATION BASIN PRIOR TO ENTERING A SURFACE WATER, PERMITTEES MUST TREAT IT WITH APPROPRIATE BMP'S SUCH THAT THE DISCHARGE DOES NOT ADVERSELY AFFECT THE SURFACE WATER OR DOWNSTREAM PROPERTIES.
2. IF PERMITTEES MUST DISCHARGE WATER CONTAINING OIL OR GREASE, THEY MUST USE AN OIL-WATER SEPARATOR OR SUITABLE FILTRATION DEVICE (E.G., CARTRIDGE FILTERS, ABSORBENTS PADS) PRIOR TO DISCHARGE.
3. PERMITTEES MUST DISCHARGE ALL WATER FROM DEWATERING OR BASIN-DRAINING ACTIVITIES IN A MANNER THAT DOES NOT CAUSE EROSION OR SCOUR IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS OR INUNDATION OF WETLANDS IN THE IMMEDIATE VICINITY OF DISCHARGE POINTS THAT CAUSES SIGNIFICANT ADVERSE IMPACT TO THE WETLAND.
4. IF PERMITTEES USE FILTERS WITH BACKWASH WATER, THEY MUST HALL THE BACKWASH WATER AWAY FOR DISPOSAL, RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.

INSPECTIONS AND MAINTENANCE (SECTION 11):

1. PERMITTEES MUST ENSURE A TRAINED PERSON, AS IDENTIFIED IN ITEM 21.2.B, WILL INSPECT THE ENTIRE CONSTRUCTION SITE AT LEAST ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 1/2 INCH IN 24 HOURS.
2. PERMITTEES MUST INSPECT AND MAINTAIN ALL PERMANENT STORMWATER TREATMENT BMP'S.
3. PERMITTEES MUST INSPECT ALL EROSION PREVENTION AND SEDIMENT CONTROL BMP'S AND POLLUTION PREVENTION MANAGEMENT MEASURES TO ENSURE INTEGRITY AND EFFECTIVENESS. PERMITTEES MUST REPAIR, REPLACE OR SUPPLEMENT ALL NONFUNCTIONAL BMP'S WITH FUNCTIONAL BMP'S BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY UNLESS ANOTHER TIME FRAME IS SPECIFIED IN ITEM 11.5 OR 11.8. PERMITTEES MAY TAKE ADDITIONAL TIME IF FIELD CONDITIONS PREVENT ACCESS TO THE AREA.
4. DURING EACH INSPECTION, PERMITTEES MUST INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS BUT NOT CURB AND GUTTER SYSTEMS. FOR EVIDENCE OF EROSION AND SEDIMENT DEPOSITION, PERMITTEES MUST REMOVE ALL DEBRIS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS AND RE-STABILIZE THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED SOIL. PERMITTEES MUST COMPLETE REMOVAL AND STABILIZATION WITHIN SEVEN (7) CALENDAR DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS. PERMITTEES MUST USE ALL REASONABLE EFFORTS TO OBTAIN ACCESS. IF PRECLUDED, REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN SEVEN (7) DAYS OF OBTAINING ACCESS. PERMITTEES ARE RESPONSIBLE FOR CONTACTING ALL LOCAL, REGIONAL, STATE AND FEDERAL AUTHORITIES AND RECEIVING ANY APPLICABLE PERMITS, PRIOR TO CONDUCTING ANY WORK IN SURFACE WATERS.
5. PERMITTEES MUST INSPECT CONSTRUCTION SITE VEHICLE EXIT LOCATIONS, STREETS AND CURB AND GUTTER SYSTEMS WITHIN AND ADJACENT TO THE PROJECT FOR SEDIMENTATION FROM EROSION OR TRACKED SEDIMENT FROM VEHICLES. PERMITTEES MUST REMOVE SEDIMENT FROM ALL PAVED SURFACES WITHIN ONE (1) CALENDAR DAY OF DISCOVERY OR, IF APPLICABLE, WITHIN A SHORTER TIME TO AVOID A SAFETY HAZARD TO USERS OF PUBLIC STREETS.
6. PERMITTEES MUST REPAIR, REPLACE OR SUPPLEMENT ALL PERIMETER CONTROL DEVICES WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/2 OF THE HEIGHT OF THE DEVICE.
7. PERMITTEES MUST DRAIN TEMPORARY AND PERMANENT SEDIMENTATION BASINS AND REMOVE THE SEDIMENT WHEN THE DEPTH OF SEDIMENT COLLECTED IN THE BASIN REACHES 1/2 THE STORAGE VOLUME.
8. PERMITTEES MUST ENSURE THAT AT LEAST ONE INDIVIDUAL PRESENT ON THE SITE (OR AVAILABLE TO THE PROJECT SITE IN THREE (3) CALENDAR DAYS) IS TRAINED IN THE JOB DUTIES DESCRIBED IN ITEM 21.2.B.
9. PERMITTEES MAY ADJUST THE SWPPP SCHEDULE DESCRIBED IN ITEM 11.2 AS FOLLOWS:
 - a. IF INSPECTIONS AND PERMANENT COVER CAN BE REDUCED TO ONCE PER MONTH, EVEN IF CONSTRUCTION ACTIVITY CONTINUES ON OTHER PORTIONS OF THE SITE, OR
 - b. WHERE SITES HAVE PERMANENT COVER ON ALL EXPOSED SOIL, AND NO CONSTRUCTION ACTIVITY IS OCCURRING ANYWHERE ON THE SITE, INSPECTIONS CAN BE REDUCED TO ONCE PER MONTH AND, AFTER 12 MONTHS, MAY BE SUSPENDED COMPLETELY UNTIL CONSTRUCTION ACTIVITY RESUMES. THE MPCA MAY REQUIRE INSPECTIONS TO RESUME IF CONDITIONS WARRANT, OR
 - c. WHERE CONSTRUCTION ACTIVITY HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS, INSPECTIONS MAY BE SUSPENDED. INSPECTIONS MUST RESUME WITHIN 24 HOURS OF RUNOFF OCCURRING, OR UPON RESUMING CONSTRUCTION, WHICHEVER COMES FIRST.
10. PERMITTEES MUST RECORD ALL INSPECTIONS AND MAINTENANCE ACTIVITIES WITHIN 24 HOURS OF BEING CONDUCTED AND THESE RECORDS MUST BE RETAINED WITH THE SWPPP. THESE RECORDS MUST INCLUDE:
 - a. DATE AND TIME OF INSPECTIONS; AND
 - b. NAME OF PERSONS CONDUCTING INSPECTIONS; AND
 - c. ACCURATE FINDINGS OF INSPECTIONS, INCLUDING THE SPECIFIC LOCATION WHERE CORRECTIVE ACTIONS ARE NEEDED; AND
 - d. CORRECTIVE ACTIONS TAKEN (INCLUDING DATES, TIMES, AND PARTY COMPLETING MAINTENANCE ACTIVITIES); AND
 - e. DATE OF ALL RAINFALL EVENTS GREATER THAN 1/2 INCHES IN 24 HOURS, AND THE AMOUNT OF RAINFALL FOR EACH EVENT. PERMITTEES MUST OBTAIN RAINFALL AMOUNTS BY EITHER A PROPERLY MAINTAINED RAIN GAUGE INSTALLED ON-SITE, A WEATHER STATION THAT IS WITHIN ONE (1) MILE OF YOUR LOCATION, OR A WEATHER REPORTING SYSTEM THAT PROVIDES SITE SPECIFIC RAINFALL DATA FROM RADAR SUMMARIES; AND
 - f. IF PERMITTEES OBSERVE A DISCHARGE DURING THE INSPECTION, THEY MUST RECORD AND SHOULD PHOTOGRAPH AND DESCRIBE THE LOCATION OF THE DISCHARGE (I.E., COLOR, ODOR, SETTLED OR SUSPENDED SOLIDS, OIL SHEEN, AND OTHER OBVIOUS INDICATORS OF POLLUTANTS); AND
 - g. ANY AMENDMENTS TO THE SWPPP PROPOSED AS A RESULT OF THE INSPECTION MUST BE DOCUMENTED AS REQUIRED IN SECTION 6 WITHIN SEVEN (7) CALENDAR DAYS.

POLLUTION PREVENTION MANAGEMENT (SECTION 12):

1. PERMITTEES MUST PLACE BUILDING PRODUCTS AND LANDSCAPE MATERIALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER. PERMITTEES ARE NOT REQUIRED TO COVER OR PROTECT PRODUCTS WHICH ARE EITHER NOT A SOURCE OF CONTAMINATION TO STORMWATER OR ARE DESIGNED TO BE EXPOSED TO STORMWATER.
2. PERMITTEES MUST PLACE PESTICIDES, FERTILIZERS AND TREATMENT CHEMICALS UNDER COVER (E.G., PLASTIC SHEETING OR TEMPORARY ROOFS) OR PROTECT THEM BY SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
3. PERMITTEES MUST STORE HAZARDOUS MATERIALS AND TOXIC WASTE, INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING COMPOUNDS, AND ACIDS) IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. STORAGE AND DISPOSAL OF HAZARDOUS WASTE MATERIALS MUST BE IN COMPLIANCE WITH MINN. R. CH. 7045 INCLUDING SECONDARY CONTAINMENT AS APPLICABLE.
4. PERMITTEES MUST PROPERLY STORE, COLLECT AND DISPOSE SOLID WASTE IN COMPLIANCE WITH MINN. R. CH. 7035.
5. PERMITTEES MUST POSITION PORTABLE TOILETS SO THEY ARE SECURE AND WILL NOT TIP OR BE KNOCKED OVER. PERMITTEES MUST PROPERLY DISPOSE SANITARY WASTE IN ACCORDANCE WITH MINN. R. CH. 7041.
6. PERMITTEES MUST TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR LEAKED CHEMICALS, INCLUDING FUEL, FROM ANY AREA WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED INCLUDING THE USE OF DRIP PANS OR ABSORBENTS UNLESS INFEASIBLE. PERMITTEES MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. PERMITTEES MUST REPORT AND CLEAN UP SPILLS IMMEDIATELY AS REQUIRED BY MINN. STAT. 115.001, USING DRY CLEAN UP MEASURES WHERE POSSIBLE.
7. PERMITTEES MUST LIMIT VEHICLE EXTERIOR WASHING AND EQUIPMENT TO A DEFINED AREA OF THE SITE. PERMITTEES MUST

CONTAIN RUNOFF FROM THE WASHING AREA IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTROLS AND MUST DISPOSE WASTE FROM THE WASHING ACTIVITY PROPERLY. PERMITTEES MUST PROPERLY USE AND STORE SOAPS, DETERGENTS, OR SOLVENTS.

8. PERMITTEES MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (E.G., CONCRETE STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE CONSTRUCTION ACTIVITY. PERMITTEES MUST PREVENT LIQUID AND SOLID WASHOUT WASTES FROM CONTACTING THE GROUND AND MUST DESIGN THE CONTAINMENT SO IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. PERMITTEES MUST PROPERLY DISPOSE LIQUID AND SOLID WASTES IN COMPLIANCE WITH MPCA RULES. PERMITTEES MUST INSTALL A SIGN INDICATING THE LOCATION OF THE WASHOUT FACILITY.

PERMIT TERMINATION (SECTION 4 AND SECTION 13):

1. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ALL TERMINATION CONDITIONS LISTED IN SECTION 13 ARE COMPLETE.
2. PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER SELLING OR OTHERWISE LEGALLY TRANSFERRING THE ENTIRE SITE, INCLUDING PERMIT RESPONSIBILITY FOR ROADS (E.G., STREET SWEEPING) AND STORMWATER INFRASTRUCTURE FINAL CLEAN OUT, OR TRANSFERRING PORTIONS OF A SITE TO ANOTHER PARTY. THE PERMITTEES' COVERAGE UNDER THIS PERMIT TERMINATES AT MIDNIGHT ON THE SUBMISSION DATE OF THE NOT.
3. PERMITTEES MUST COMPLETE ALL CONSTRUCTION ACTIVITY AND MUST INSTALL PERMANENT COVER OVER ALL AREAS PRIOR TO SUBMITTING THE NOT. VEGETATIVE COVER MUST CONSIST OF A UNIFORM PERENNIAL VEGETATION WITH A DENSITY OF 70 PERCENT OF ITS EXPECTED FINAL GROWTH. VEGETATION IS NOT REQUIRED WHERE THE FUNCTION OF A SPECIFIC AREA DICTATES NO VEGETATION, SUCH AS IMPERVIOUS SURFACES OR THE BASE OF A SAND FILTER.
4. PERMITTEES MUST CLEAN THE PERMANENT STORMWATER TREATMENT SYSTEM IF ANY ACCUMULATED SEDIMENT AND MUDS ENSURE THE SYSTEM MEETS ALL APPLICABLE REQUIREMENTS IN SECTION 15 THROUGH 19 AND IS OPERATING AS DESIGNED.
5. PERMITTEES MUST REMOVE ALL SEDIMENT FROM CONVEYANCE SYSTEMS PRIOR TO SUBMITTING THE NOT.
6. PERMITTEES MUST REMOVE ALL TEMPORARY SYNTHETIC EROSION PREVENTION AND SEDIMENT CONTROL BMP'S PRIOR TO SUBMITTING THE NOT. PERMITTEES MAY LEAVE BMP'S DESIGNED TO DECOMPOSE ON-SITE IN PLACE.
7. FOR RESIDENTIAL CONSTRUCTION ONLY, PERMIT COVERAGE TERMINATES ON INDIVIDUAL LOTS IF THE STRUCTURES ARE FINISHED AND TEMPORARY EROSION PREVENTION AND DOWNSTREAM PERIMETER CONTROL IS COMPLETE. THE RESIDENCE SELLS TO THE HOMEOWNER, AND THE PERMITTEE DISTRIBUTES THE MPCA'S 'HOMEOWNER FACT SHEET' TO THE HOMEOWNER.
8. FOR CONSTRUCTION PROJECTS ON AGRICULTURAL LAND (E.G., PIPELINES ACROSS CROPLAND), PERMITTEES MUST RETURN THE DISTURBED LAND TO ITS PRECONSTRUCTION AGRICULTURAL USE PRIOR TO SUBMITTING THE NOT.

SEED NOTES:

ALL SEED MIXES AND APPLICATION SHALL BE IN ACCORDANCE WITH THE MNDOT SEEDING MANUAL.

GENERAL RECOMMENDATIONS:

THE CONTRACTOR IS RESPONSIBLE TO SALVAGE AND PRESERVE EXISTING TOPSOIL NECESSARY FOR FINAL STABILIZATION AND TO ALSO MINIMIZE COMPACTION IN ALL LANDSCAPE AREAS. IMMEDIATELY BEFORE SEEDING THE SOIL SHALL BE TILLED TO A MINIMUM DEPTH OF 3 INCHES.

TEMPORARY EROSION CONTROL SEEDING, MULCHING & BLANKET.

SEED

- TEMPORARY SEED SHALL BE MNDOT SEED MIX 21-112 (WINTER WHEAT COVER CROP) FOR WINTER AND 21-111 (OATS COVER CROP) FOR SPRING/SUMMER APPLICATIONS. BOTH SEED MIXES SHALL BE APPLIED AT A SEEDING RATE OF 100 LBS/ACRE.

MULCH

- IMMEDIATELY AFTER SEEDING, WITHIN 24 HOURS, MNDOT TYPE 1 MULCH SHOULD BE APPLIED TO PROTECT AND ENHANCE SEED GERMINATION. MULCH SHALL BE APPLIED AT 90% COVERAGE (2 TONS PER ACRE OF STRAW MULCH)

SLOPES

- 3:1 (HORIZ/VERT.) OR FLATTER MUCH SHALL BE COVERED WITH MULCH
- SLOPES STEEPER THAN 3:1 OR DITCH BOTTOMS SHALL BE COVERED WITH EROSION CONTROL BLANKET.
- SEE PLAN FOR MORE DETAILED DITCH AND STEEP SLOPE EROSION CONTROL TREATMENTS.

TRAINING SECTION 21

DESIGN ENGINEER: DAVID J. KNAEBLE P.E.
TRAINING COURSE: DESIGN OF SWPPP
TRAINING ENTITY: UNIVERSITY OF MINNESOTA
INSTRUCTOR: JOHN CHAPMAN
DATES OF TRAINING COURSE: 8/22/2012- 8/23/2012
TOTAL TRAINING HOURS: 12
DATE OF RECERTIFICATION: 4/22/22
EXPIRATION: 5/31/2025

OWNER INFORMATION

MIDCOUNTRY BANK FSB
7825 WASHINGTON AVE S, SUITE 900
BLOOMINGTON, MN 55439
CONTACT: CHRIS VOSBEEK
952-400-2821

AREAS AND QUANTITIES:

SITE AREA CALCULATIONS			
	EXISTING CONDITION	PROPOSED CONDITION	
BUILDING COVERAGE	3,449 SF	8.5%	3,597 SF 8.9%
ALL PAVEMENTS	22,672 SF	56.0%	22,596 SF 55.8%
ALL NON-PAVEMENTS	14,369 SF	35.5%	14,297 SF 35.3%
TOTAL SITE AREA	40,490 SF	100.0%	40,490 SF 100.0%

IMPERVIOUS SURFACE		
	EXISTING CONDITION	PROPOSED CONDITION
	26,121 SF	64.5%
	26,193 SF	64.7%
DIFFERENCE (EX. VS PROP.)	72 SF	0.2%

EROSION CONTROL QUANTITIES		
DISTURBED AREA	31,635 SF	0.73 AC
SILT FENCE/BIO-ROLL	±1100 LF	
EROSION CONTROL BLANKET	3,200 SF	
INLET PROTECTION DEVICES	±10 EA	

NOTE: QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION.

SWPPP CONTACT PERSON

CONTRACTOR:

SWPPP INSPECTOR TRAINING:
ALL SWPPP INSPECTIONS MUST BE PERFORMED BY A PERSON THAT MEETS THE TRAINING REQUIREMENTS OF THE NPDES CONSTRUCTION SITE PERMIT.
TRAINING CREDENTIALS SHALL BE PROVIDED BY THE CONTRACTOR AND KEPT ON SITE WITH THE SWPPP

PARTY RESPONSIBLE FOR LONG TERM OPERATION AND MAINTENANCE OF PERMANENT STORM WATER MANAGEMENT SYSTEM

PERMANENT STORMWATER MANAGEMENT IS NOT REQUIRED AS PART OF THIS PROJECT TO MEET NPDES PERMIT REQUIREMENTS. THE PROPERTY OWNER IS RESPONSIBLE FOR THE LONG TERM OPERATION AND MAINTENANCE OF THE PROPOSED STORMWATER SYSTEM.

SWPPP ATTACHMENTS (ONLY APPLICABLE IF SITE IS 1 ACRE OR GREATER):

NA

SUPPLEMENTARY SITE SPECIFIC EROSION CONTROL NOTES:

THESE NOTES SUPERCEDE ANY GENERAL SWPPP NOTES.

THIS PROJECT IS LESS THAN 1.0 ACRES SO AN NPDES PERMIT IS NOT REQUIRED.

PROJECT NARRATIVE:

PROJECT IS A REDEVELOPMENT OF AN EXISTING COMMERCIAL SITE INTO A NEW COMMERCIAL BANK BUILDING. SITE AND LANDSCAPE IMPROVEMENTS WILL OCCUR.

SPECIAL TMDL BMP REQUIREMENTS SITE SPECIFIC (IF REQUIRED):

NOT REQUIRED

PERMANENT STABILIZATION NOTES SITE SPECIFIC:

- PERMANENT SEED MIX
- FOR THIS PROJECT ALL AREAS THAT ARE NOT TO BE SOODED OR LANDSCAPED SHALL RECEIVE A NATIVE PERMANENT SEED MIX.
 - AREAS IN BUFFERS AND ADJACENT TO OR IN WET AREAS MNDOT SEED MIX 33-261 (STORMWATER SOUTH AND WEST) AT 35 LBS PER ACRE.
 - DRY AREAS MNDOT SEED MIX 35-221 (DRY PRAIRIE GENERAL) AT 40 LBS PER ACRE.
 - MAINTENANCE SHALL BE IN ACCORDANCE TO THE MNDOT SEEDING MANUAL.



MIDCOUNTRY BANK
14617 STATE HIGHWAY 7, MINNETONKA, MN 55345

MIDCOUNTRY BANK FSB
7825 WASHINGTON AVE S, SUITE 900, BLOOMINGTON, MN 55439

PROJECT

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

David J. Knaeble
DATE 07/21/23 LICENSE NO. 48776

ISSUE/SUBMITTAL SUMMARY

DATE DESCRIPTION

07/21/23 CITY SUBMITTAL



Memorandum

SRF No. 16654

To: Bria Raines, Planner
City of Minnetonka

From: Tom Sachi, PE, Project Manager
Ashley Sherry, EIT, Traffic Engineer I

Date: September 15, 2023

Subject: MidCountry Bank Redevelopment Traffic Summary; Minnetonka, Minnesota

Introduction

As requested, SRF has completed a traffic study for the proposed redevelopment of the MidCountry Bank located at 14617 Highway 7 in the City of Minnetonka, MN. The project site is located in the southeast quadrant of the Minnesota Trunk Highway 7 (TH 7) and Williston Road intersection. The following information provides the assumptions, analysis, and recommendations offered for consideration.

Existing Conditions

Data Collection

Weekday a.m. and p.m. peak period vehicular turning movement and pedestrian/bicyclist counts were collected by SRF during the week of March 20, 2023, at the following study intersections:

- TH 7 and Williston Road
- South Service Road and Williston Road
- South Service Road and Woodhill Road
- South Service Road and North Access
- South Service Road and Northeast Access
- South Service Road and Southeast Access

Proposed Development

The proposed residential development is located along South Service Road, at the site of the existing MidCountry Bank. The existing bank located on the project site is proposed to be redeveloped into a 2,200 square foot (SF) building of the current MidCountry Bank and a 1,700 square foot (SF) restaurant space. The development includes 27 parking stalls. Note, a tenant for the restaurant space is not currently known at this time and could range from a coffee shop to a fast food type of restaurant. Both the bank and restaurant land uses are expected to have drive-thru window, with one (1) on each side of the proposed building. The current three (3) access driveways are expected to remain. The development is anticipated to be constructed by year 2025.

Traffic Forecasts

Background Traffic Growth

To account for general background growth in the area, an annual growth rate of one-half (0.5) percent was applied to the existing peak hour traffic volumes to develop year 2025 background forecasts.

Proposed Development Trip Generation

To account for traffic impacts associated with the proposed development, a trip generation estimate for the proposed development was completed for the a.m., midday and p.m. peak hours and a daily basis. The estimate was developed using the *ITE Trip Generation Manual, 11th Edition*. Results of the trip generation estimate indicate that the proposed development is expected to generate approximately 146 a.m. peak hour, 86 midday peak hour, 56 p.m. peak hour, and 907 daily new trips. The trips associated with MidCountry Bank are already on site and expected to remain the same. However, a majority of the restaurant trips are expected to be made up of vehicles already travelling within the study area (i.e., pass-by/diverted-link trips). Therefore appropriate reductions were applied and the proposed development could potentially generate 14 weekday a.m. peak hour, 40 weekday midday peak hour, 26 p.m. peak hour, and 454 weekday daily trips that are new to the study area.

Operations Analysis

To determine how the study intersections and site access locations will operate under existing and year 2025 build conditions, an intersection capacity analysis was completed using Synchro/SimTraffic software.

Results of the existing capacity analysis indicate that all study intersections currently operate at an acceptable overall LOS D or better during the a.m. and p.m. peak hour, and an acceptable overall LOS C or better during the midday peak hour, with the existing geometric layout and traffic controls. While acceptable, the TH 7 and Williston Road intersection operates at a LOS D during the a.m. and p.m. peak hours, it is expected to operate near capacity during these timeframes. Northbound and southbound delays currently reach over a minute during the a.m. and p.m. peaks, however, this is expected due to the signal timing along a busy state highway where priority is given to the eastbound/westbound directions. Although delays were reaching a minute, queues in the northbound direction along Williston Road were not observed to back up to the South Service Road. No other significant delay or queueing issues were observed during the peak hours.

Results of the analysis indicate that all study intersections are expected to continue to operate at an acceptable overall LOS D or better during the a.m. and p.m. peak hour, and an acceptable overall LOS C or better during the midday peak hour, with the existing geometric layout and traffic controls. At the TH 7 and Williston Road intersection, it is expected that the delays during the a.m. peak hour will increase by five (5) seconds on average per vehicle due to the increase in trips associated with the proposed development, particularly due to the increase in westbound left-turning trips.

While this increase in delay pushes the intersection near the LOS D/E threshold where a breakdown in operations could be expected to occur, the intersection will operate at a LOS D, which is acceptable. The resulting northbound queues along Williston Road approaching TH 7 are expected to reach to the South Service Road/Lloyds Drive intersection approximately 10 to 15 percent of the peak hour.

These increased delays and queues at the TH 7 and Williston Road intersection along with the increase in turning maneuvers at the Williston Road and South Service Road/Lloyds Drive intersection are expected to result in side-street delays increasing by approximately 24 seconds for the eastbound direction (Lloyds Drive) and 11 seconds for the South Service Road. This will directly impact motorists exiting the neighborhood west of Williston Road. While still an acceptable level of service, eastbound left-turning vehicles exiting Lloyds Drive will have a significant increase in delay.

The increase in delays during the midday and p.m. peak hour are expected to range between one (1) to three (3) seconds when a fast-food restaurant was assumed on site. Minimal increases in delay are expected at the side-street during the midday and p.m. peak hours, with every unsignalized side-street approach still operating at a LOS B or better.

Although there is expected to be an increase in delays and queueing during the a.m. peak hour, no geometric or traffic control changes are needed to accommodate the proposed development from an intersection capacity perspective. To minimize the increase in delays and queues within the study area, particularly along Williston Road and Lloyds Drive, consider exploring restaurant tenants that are not expected to peak during the a.m. peak hour.

Parking Assessment

A detailed parking analysis was completed to determine the impacts of the proposed redevelopment. Parking generation estimates for both a proposed coffee shop and fast-food restaurant were developed using the *ITE Parking Generation Manual, Fifth Edition*.

It is expected that if the development included a coffee shop, there would be an average peak parking demand between nine (9) to 14 stalls, with an 85th percentile demand between 17 to 22 stalls. This would allow for a surplus between five (5) to 18 stalls on site.

If the development included a fast food restaurant, it is expected that there would be an average peak parking demand between four (4) to 22 stalls, with an 85th percentile demand between seven (7) to 30 stalls. This would indicate the site would be expected to be over capacity during the 12:00 p.m. time period if parking generates at an 85th percentile rate, with a deficit of three (3) stalls. However, there would be a surplus of five (5) stalls if it generates at an average rate. This potential minor deficit could result in those guests using the drive-through instead of parking.

Based on the City Code requirements, 22 parking stalls would be required for this site. This would result in a five (5) space surplus for the site.

Site Plan Review

A detailed review of the two (2) proposed drive-thrus was completed to identify if the provided queueing space will be sufficient to accommodate the proposed development. Both the proposed bank and restaurant land uses are expected to have a drive-thru on site. Note, the bank is expected to replace the existing bank on site. Therefore, SRF completed drive-thru queueing observations at the existing site to determine the average and maximum queues. From the observations, the average queue for the bank drive-thru was one (1) vehicle, and the maximum observed queue was two (2) vehicles. The bank drive-thru is expected to provide queueing space for up to six (6) vehicles, indicating that the bank drive-thru will be sufficient and not block any internal site circulation.

Additionally, a drive-thru review was completed for the restaurant land use. A review of both a coffee-shop and fast-food restaurant was completed given the unknown tenant of the space. According to the *Drive-Through Queue Generation Study* completed by Counting Cars in 2012, maximum queues at a coffee shop can extend to 16 vehicles, with an 85th percentile queue of 14 vehicles and average queue of 10 vehicles. SRF has also completed similar queueing studies for coffee-shops in the Twin Cities and found these estimates to be similar to current travel behavior. This would be expected to occur during the morning hours. For a fast-food restaurant, maximum queues can reach 13 vehicles with 85th percentile queues of 12 vehicles and average queues of nine (9) vehicles. This would be expected during the lunch period.

The proposed drive-thru space is expected to accommodate 16 vehicles before backing into the northern access on site. Given the available queueing space, there is not expected to be queueing issues on site. Note, a 16th queued vehicle may begin to prohibit egress operations on site out of the north access if vehicles are not queued tightly. This queue should continue to be monitored to determine if queues impact egress operations. It would be expected that egress operations would be impacted for less than one (1) minute, and likely during the a.m. peak hour when a coffee shop would peak. If the restaurant space is a fast-food restaurant, queues would not expect to impact egress operations. Note, vehicles entering the drive-thru would be expected to head immediately left when entering the site, which could conflict with vehicles exiting the site on the right-hand side of the roadway at the north access. While operational impacts are likely minimal, there is a potential for safety/crash issues with this criss-cross behavior that should be monitored.

Overall Site Plan

A review of the proposed site plan was completed to identify any issues and recommend potential improvements for consideration with regard to sight distance, parking, access, circulation and drive-thru queueing space, and pedestrian facilities. In general, the following should be considered (and shown in the attached graphic).

1. Incorporate traffic controls, signing, and striping based on guidelines established in the *Manual on Uniform Traffic Control Devices* (MUTCD).
2. Special consideration should be made to limit any sight distance impacts from future structures, landscaping, and signing, particularly along South Service Road.

3. Place Do Not Enter signs on the west side of the site at the bank drive-thru exit area.
4. The steps from the southern parking lot to the restaurant should be adequately maintained in the winter for safety reasons.
5. Vehicles exiting the site at the western access may have slightly obstructed sight distance due to the curvature of the South Service Road. Note, this access currently exists and no crashes were reported. If traffic is limited to bank drive-through users, there is expected to be minimal conflicts.
6. There is adequate sidewalk connections from the parking areas to the building on site.
7. It is recommended to construct a sidewalk connection and crossing from the northwest portion of the site to the sidewalk in the southeast quadrant of the TH 7 and Williston Road intersection.
8. Based on the minimal queues expected for the bank drive-thru and the expected minimal internal circulation through the area, it is recommended that the western bypass around the bank drive thru not be constructed, initially.
 - a. Vehicles using the restaurant drive thru would be expected to use the north site access. If the bypass is removed, ensure the handicapped parking stall on the north part of the site has adequate room to back out so the vehicle can proceed to the north site access.
 - b. If significant safety issues do arise with the restaurant drive-thru ingress/egress maneuvers and queueing, there is potential to need the western bypass lane, as shown in the site plan, and forcing all exiting drive-thru traffic to the western site access and make the north access enter only to minimize conflicts.
 - c. Install “Bank Drive-Thru Only” signs to the single lane drive-thru approach.

Summary and Conclusions

The following study conclusions and recommendations are offered for consideration:

- 1) Results of the existing capacity analysis indicate that all study intersections currently operate at an acceptable overall LOS D or better during the a.m. and p.m. peak hour, and an acceptable overall LOS C or better during the midday peak hour, with the existing geometric layout and traffic controls. While acceptable, the TH 7 and Williston Road intersection operates at a LOS D during the a.m. and p.m. peak hours, it is expected to operate near capacity during these timeframes.
 - a. Northbound and southbound delays currently reach over a minute during the a.m. and p.m. peaks.
 - b. Although delays were reaching a minute, queues in the northbound direction along Williston Road were not observed to back up to the South Service Road.
- 2) The proposed development consists of a 2,200 square foot (SF) building of the current MidCountry Bank and a 1,700 square foot (SF) restaurant space. The proposed development includes 27 parking stalls.
 - a. A tenant for the restaurant space is not currently known at this time and could range from a coffee shop to a fast food type of restaurant.

- b. Both the bank and restaurant land uses are expected to have drive-thru windows, with one (1) on each side of the proposed building.
 - c. The current three (3) access driveways are expected to remain.
 - 3) Results of the trip generation estimate indicate that the proposed development is expected to generate approximately 146 a.m. peak hour, 86 midday peak hour, 56 p.m. peak hour, and 907 daily new trips.
 - a. When accounting for pass-by trips, the proposed development could potentially 14 weekday a.m. peak hour, 40 weekday midday peak hour, 26 p.m. peak hour, and 454 weekday daily trips that are new to the study area.
 - 4) Results of the analysis indicate that all study intersections are expected to continue to operate at an acceptable overall LOS D or better during the a.m. and p.m. peak hour, and an acceptable overall LOS C or better during the midday peak hour, with the existing geometric layout and traffic controls.
 - a. At the TH 7 and Williston Road intersection, it is expected that the delays during the a.m. peak hour will increase by five (5) seconds on average per vehicle due to the increase in trips associated with the proposed development, particularly due to the increase in westbound left-turning trips.
 - b. While this increase in delay pushes the intersection near the LOS D/E threshold where a breakdown in operations could be expected to occur, the intersection will operate at a LOS D, which is acceptable.
 - c. The resulting northbound queues along Williston Road approaching TH 7 are expected to reach to the South Service Road/Lloyds Drive intersection approximately 10 to 15 percent of the peak hour.
 - d. These increased delays and queues at the TH 7 and Williston Road intersection along with the increase in turning maneuvers at the Williston Road and South Service Road/Lloyds Drive intersection are expected to result in side-street delays increasing by approximately 24 seconds for the eastbound direction (Lloyds Drive) and 12 seconds for the South Service Road.
 - i. This will have a significant impact on eastbound left-turning motorists from Lloyds Drive.
 - e. Although there is expected to be an increase in delays and queueing during the a.m. peak hour, no geometric or traffic control changes are needed to accommodate the proposed development from an intersection capacity perspective.
 - f. To minimize the increase in delays and queues within the study area, particularly along Williston Road and Lloyds Drive, consider exploring restaurant tenants that are not expected to peak during the a.m. peak hour.
 - 5) It is expected that if the development included a coffee shop, there would be an average peak parking demand between nine (9) to 14 stalls, with an 85th percentile demand between 17 to 22 stalls. This would allow for a surplus between five (5) to 18 stalls on site.

- 6) If the development included a fast food restaurant, it is expected that there would be an average peak parking demand between four (4) to 22 stalls, with an 85th percentile demand between seven (7) to 30 stalls.
 - a. This would indicate the site would be expected to be over capacity during the 12:00 p.m. time period if parking generates at an 85th percentile rate, with a deficit of three (3) stalls. This potential minor deficit could result in those guests using the drive-through instead of parking.
 - b. There would be a surplus of five (5) stalls if it generates at an average rate.
- 7) Based on the City Code requirements, 22 parking stalls would be required for this site. This would result in a five (5) space surplus for the site.
- 8) From the observations, the average queue for the bank drive-thru was one (1) vehicle, and the maximum observed queue was two (2) vehicles. The bank drive-thru is expected to provide queueing space for up to six (6) vehicles, indicating that the bank drive-thru will be sufficient and not block any internal site circulation.
- 9) The proposed restaurant drive-thru space is expected to accommodate 16 vehicles.
 - a. Based on research, the potential maximum queue would be expected to be 16 vehicles for a coffee shop and 13 vehicles for a restaurant.
 - b. No significant queueing impacts are expected, however, the 16th queued vehicle may begin to prohibit egress operations on site out of the north access if vehicles are not queued tightly.
 - c. This queue should continue to be monitored to determine if queues impact egress operations. It would be expected that egress operations would be impacted for less than one (1) minute, and likely during the a.m. peak hour when a coffee shop would peak. If the restaurant space is a fast-food restaurant, queues would not expect to impact egress operations.
- 10) Vehicles entering the drive-thru would be expected to head immediately left when entering the site, which could conflict with vehicles exiting the site on the right-hand side of the roadway at the north access. While operational impacts are likely minimal, there is a potential for safety/crash issues with this criss-cross behavior that should be monitored.
- 11) Detailed Site Plan improvements are listed in the Site Plan Review section and shown in the attached graphic.



Possible safety issues from the criss-cross ingress/egress operations

Install "Bank Drive-Thru Only" Signs

Bypass lane not initially needed from an operations perspective. If safety/operational issues arise from the restaurant drive-thru, consider construction in the future.



RET. WALL - SEE CIVIL
PATIO SPACE - SEE CIVIL

TENANT
1700 SF

BANK
2200 SF

RET. WALL - SEE CIVIL

6449 CITY WEST PARKWAY
SUITE 300
EDEN PRAIRIE, MINNESOTA 55344
PH: 952-996-9662
WWW.SRA-MN.COM

PRELIMINARY
NOT FOR
CONSTRUCTION

ISSUE #	DATE	DESCRIPTION
	08/11/2023	VARIANCE APPLICATION
	08/11/2023	CONDITIONAL USE PERMIT APPLICATION
	08/11/2023	SITE AND BUILDING PLAN REVIEW

MIDCOUNTRY BANK

SITE PLAN & FL



Site Plan Recommendations

Minnetonka MidCounty Bank Redevelopment Traffic Study
City of Minnetonka

02316654
September 2023

Figure 1

Ordinance No. 2023-XXX

An ordinance rezoning the property at 14617 Highway 7 from B-1, office business, to B-2, limited business district.

The City Of Minnetonka Ordains:

Section 1.

1.01 The property at 14617 Highway 7 is hereby rezoned from B-1, office business, to B-2, limited business district.

1.02 The property is legally described as:

Lot 001, Block 001, Tower Hill 2nd Addition, Hennepin County, Minnesota.

Section 2.

2.01 This ordinance is based on the following findings:

1. The rezoning to B-2 would be consistent with the zoning of the adjacent property.
2. The proposed rezoning is consistent with the implementation of the comprehensive plan and would support the redevelopment of the site.
3. The proposal would not negatively impact the public health, safety, or general welfare.

2.02 This ordinance is subject to the following conditions:

1. The site must be developed and maintained in substantial conformance with the following plans:
 - Site Plan, dated July 21, 2023
 - Exterior Renderings, Aug. 11, 2023
 - Landscaping Plan, dated July 21, 2023
 - Grading Plans, dated July 21, 2023
 - Utility Plans, dated July 21, 2023

2. The development must further comply with all conditions outlined in City Council Resolution No. 2023-XXX, adopted by the Minnetonka City Council on Oct. 30, 2023.

Section 3. This ordinance is effective immediately.

Adopted by the city council of the City of Minnetonka, Minnesota, on Oct. 30, 2023.

Brad Wiersum, Mayor

Attest:

Becky Koosman, City Clerk

Action on this ordinance:

Date of introduction: Oct. 16, 2023

Date of adoption:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Ordinance adopted.

Date of publication:

I certify that the foregoing is a true and correct copy of an ordinance adopted by the city council of the City of Minnetonka, Minnesota at a regular meeting held on Oct. 30, 2023.

Becky Koosman, City Clerk

Resolution No. 2023-XXX

Resolution approving a conditional use permit, and final site and building plans, with variances, for redevelopment of the property at 14617 State Highway 7

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 The subject property is located at 14617 Highway 7. It is legally described as:

Lot 001, Block 001, Tower Hill 2nd Addition, Hennepin County, Minnesota.

1.02 Sperides Reiners Architects Inc., on behalf of MidCountry Acquisition Corp., proposes to redevelop the MidCountry Bank site. The proposal includes the removal of the existing building, construction of a larger building, and introducing a restaurant with a drive-thru window to the site.

1.03 By City Code §300.18 Subd. 4(d) uses with a drive-up window are conditionally permitted uses in the B-2 district.

1.04 By City Code §300.18 Subd. 4(f) uses with a drive-up window are conditionally permitted uses in the B-2 district.

1.05 By City Code §300.27, a site and building plan review is required when a commercial site is redeveloped, expanded, or intensified.

1.06 The proposal requires a conditional use permit for the financial use with a drive-up window, a fast-food restaurant with a drive-up window, and approval of final site and building plans, with the following variances:

- A parking setback variance from 20 feet to 10.3 feet from the west property line;
- A building setback variance from 50 feet to 35.1 feet from the westerly ROW; and
- A tree protection variance to exceed code-defined high-priority and significant tree removal rates.

1.07 On Oct. 26, 2023, the planning commission held a hearing on the proposal. The applicant was provided the opportunity to present information to the commission. The commission considered all of the comments received and the staff report, which are incorporated by reference into this resolution. The commission recommended that the city council approve the proposal.

Section 2. Standards.

2.01 City Code §300.21 Subd. 2 outlines the following general standards for conditional uses:

1. The use is consistent with the intent of this ordinance;
2. The use is consistent with the goals, policies and objectives of the comprehensive plan;
3. The use does not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements;
4. The use is consistent with the city's water resources management plan;
5. The use is in compliance with the performance standards specified in section 300.28 of this ordinance; and
6. The use does not have an undue adverse impact on public health, safety or welfare.

2.02 City Code 300.21 Subd.4(d) outlines the following specific conditions for use with a drive-up window in the B-2 district:

1. Drive-up windows and stacking areas must not be located adjacent to any residential parcel;
2. Stacking areas must provide for a minimum of six cars per aisle; and
3. Public address system must not be audible from any residential parcel; and
4. Must be subordinate to and associated with a permitted use located within a building on the site.

2.03 City Code 300.21 Subd.4(f) outlines the following specific conditions for fast-food restaurants with a drive-thru window in the B-2 district:

1. Shall be located only on sites having direct access to minor arterial streets or service roads;
2. Public address systems shall not be audible from any residential parcel;

3. Stacking for a minimum of six cars per aisle shall be provided within applicable parking lot setbacks;
4. Shall not be permitted when traffic studies indicate significant impacts on the levels of service as defined by the Institute of Traffic Engineers of adjacent streets and intersections; and
5. Building shall be set back at least 100 feet and screened from any adjacent property designated in the comprehensive plan for residential use.

2.04 By City Code §300.27, Subd. 5, the city will consider compliance with a variety of general standards when evaluating the site and building plans. Those standards are incorporated by reference into this resolution.

2.05 By City Code §300.07, Subd. 1, a variance may be granted from the requirements of the zoning ordinance when: (1) the variance is in harmony with the general purposes and intent of this ordinance; (2) when the variance is consistent with the comprehensive plan; and (3) when the applicant establishes that there are practical difficulties in complying with the ordinance. Practical difficulties mean: (1) the proposed use is reasonable; (2) the need for a variance is caused by circumstances unique to the property, not created by the property owner, and not solely based on economic considerations; and (3) the proposed use would not alter the essential character of the surrounding area.

Section 3. Findings.

3.01 The proposal generally meets the general conditional use permit standards outlined in City Code §300.21 Subd.2.

1. The use is consistent with the intent of this ordinance;
2. The use is consistent with the goals, policies and objectives of the comprehensive plan;
3. The use would not have an undue adverse impact on governmental facilities, utilities, services or existing or proposed improvements;
4. The use is consistent with the city's water resources management plan;
5. But for the setback and tree removal variances, the use would comply with the performance standards specified in section 300.28 of the ordinance; and
6. The use would not have an undue adverse impact on public health, safety or welfare.

3.02 The proposed financial institution with a drive-up window would meet the conditional use permit standards outlined in City Code 300.21 Subd.4(d) for a

use with a drive-up window in the B-2 district:

1. The drive-up windows are not adjacent to residential properties.
2. The proposed site plan illustrates a stacking area for at least six vehicles for either drive-thru lane.
3. As a condition of this resolution, public address systems must not be audible from any residential parcel.
4. The drive-up window is associated with a financial institution that is a principal use.

3.03 The proposal would meet the conditional use permit standards outlined in City Code 300.21 Subd.4(f) for fast food restaurants with a drive-up window.

1. The subject property is accessed via the Hwy 7 service road.
2. As a condition of this resolution, public address systems must not be audible from any residential parcel.
3. The proposed site plan illustrates a stacking area for at least six vehicles for either drive-thru.
4. A city-commissioned traffic study does not anticipate the proposed redevelopment would significantly impact levels of service beyond the current conditions.
5. The proposed building would be located over 100 feet from the closest residential building and is separated by a right-of-way.

3.04 The proposal would meet site and building plan standards outlined in the City Code §300.27, Subd.5.

1. The proposal has been reviewed by city planning, engineering, and natural resources staff and found to be generally consistent with the city's development guides, including the water resources management plan.
2. Apart from the variance, the proposal would comply with the standards of the ordinance. As outlined in section 3.05 of this resolution, the variance standard is met.
3. The proposal would require a variance to the tree protection ordinance for the removal of significant and high-priority trees beyond permitted by city code. As outlined in section 3.05 of this resolution, the variance standard is met.

4. The proposed redevelopment would incorporate more landscaping on the site than currently exists. The landscaping plan proposed trees, shrubs, grasses, and perennials, increasing the number of natural features.
5. The proposed redevelopment would create an additional use on an underutilized site. The site plan would create an appropriate physical order to the site, allowing a minimum of six vehicles stacking on-site for each drive-thru.
6. As a new construction, the proposed restaurant would include energy efficiency/conservation components required by the state building code.
7. The proposal would visually and physically alter the property and the immediate area. However, it is not anticipated to negatively impact adjacent or neighboring properties.

3.05 The proposed restaurant would meet the variance standard as outlined in City Code §300.07, Subd. 1.

1. Intent of the Ordinance.
 - a) The intent of the ordinance, as it relates to the parking lot setback from adjacent uses, is to ensure safety and a visual buffer. The proposed parking lot would be located adjacent to an existing parking lot in the east; there are no safety or visual implications. Along the westerly right-of-way, there is no sidewalk that would be impacted by the reduced parking or building setback.
 - b) The intent of the ordinance, as it relates to the tree protection ordinance, is to ensure tree preservation by reasonably limiting the removal of trees. The tree protection ordinance makes no allowance for the removal of trees caused by the removal of buildings. In the case of the subject property, the majority of site trees are located in proximity to the existing building and parking lot. They were likely planted as landscaping at the time the building was originally constructed. Redevelopment of the site would not be possible meeting the tree ordinance. Precluding redevelopment to ensure the preservation of previous landscaping would be unreasonable.
2. Consistent with the Comprehensive Plan. The 2040 Comprehensive Plan designation is for an office use. The subject property is an existing and continued use as a financial institution, which is consistent with the 2040 Comprehensive Plan.
3. Practical Difficulties. There are practical difficulties in meeting the standards of the ordinance.
 - a) Reasonableness.

- 1) The setback variances would improve the site, which has not undergone alterations in over 20 years. The reduced setback will allow for secondary use on the property and relocation of the building further from the easterly property line. Although a setback is required to the 50-foot front yard setback, the building, in general, is further set back from all property lines that the existing building.
 - 2) The request for the removal of four significant trees and four high-priority trees is reasonable. The practical difficulty is that the majority of trees on the site are within 20 feet of the existing structure in the basic tree removal area, making the tree protection ordinance difficult to meet. These trees would be significantly impacted by the demolition of the existing structure if not removed.
- b) Unique Circumstance:
- 1) When the frontage road was constructed in 2000, the property was reduced in size, reducing the setbacks to the existing structure and parking lot and creating nonconforming setbacks. The setback variances are necessary in order to reuse the existing parking lots and accesses. This is a unique circumstance not common to all similarly zoned properties.
 - 2) The majority of site trees are located in proximity to the existing building and parking lot. They were likely planted as landscaping at the time the building was originally constructed. Redevelopment of the site would not be possible meeting the tree ordinance. This is a unique circumstance not common to all similarly zoned properties.
- c) Character of Locality. The redevelopment of the site is not anticipated to impact adjacent or neighboring properties negatively.

Section 4. City Council Action.

4.01 The city council approves the conditional use permit, with variances, based on the above findings. Approval is subject to the following conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, except as modified by the conditions below:
 - Site Plan, dated July 21, 2023
 - Exterior Renderings, Aug. 11, 2023

- Landscaping Plan, dated July 21, 2023
 - Grading Plans, dated July 21, 2023
 - Utility Plans, dated July 21, 2023
2. A site development permit is required. This permit will cover the demolition, grading, and installation of sewer, water, and stormwater facilities:
- a) Unless authorized by city staff, no site work – including tree removal – may begin until a complete site development permit application has been submitted, reviewed by staff, and approved.
 - b) The following must be submitted for the site development permit to be considered complete.
 - 1) Electronic plans and specifications submitted through the city's electronic permit and plan review system.
 - 2) Final site, grading, drainage, utility, landscape, and tree mitigation plans, and a stormwater pollution prevention plan (SWPPP) for staff approval.
 - a. Final site plan must:
 - Remove the bypass lane in the westerly parking lot setback.
 - b. Final utility plan.
 - Illustrate removal of any unused water service piping back to the main with the curb stop turned off.
 - Note that any unused sanitary sewer piping must be removed back to the main with the wye cut out and sleeved.
 - c. Final landscaping plan must:
 - Meet minimum landscaping and mitigation requirements as outlined in the ordinance, including species beneficial to pollinators.
 - Include information relating to species, sizes, and quantities.
 - d. A tree mitigation plan. The plan must meet mitigation requirements as outlined in the

ordinance. However, at the sole discretion of staff, mitigation may be decreased. Based on the submitted plans, the mitigation requirements would be unknown based upon submitted plans.

e. Final stormwater management plan. The plan is required for the entire site's impervious surface and must demonstrate conformance with the following criteria:

- Rate. Limit peak runoff flow rates to that of existing conditions from the 2-, 10-, and 100-year events at all points where stormwater leaves the site.
- Volume. Provide for on-site retention of 1.1 inches of runoff from the entire site's impervious surface.
- Quality. Provide for all runoff to be treated to at least 60 percent total phosphorus annual removal efficiency and 90 percent total suspended solid annual removal efficiency.

In addition:

- A geotechnical report is required to determine if stormwater infiltration is feasible. If feasible, volume retention via infiltration is required.
- Provide evidence that any underground systems would be able to support 83,000 pounds and 10,800 pounds per square foot outrigger load.
- Underground facilities must be inspected by a third party during installation to ensure that pressure requirements are adequately met.
- Two inches of vertical freeboard is required between the lowest floor of the building and the high water level of the underground stormwater chamber.
- The stormwater basin must be designed to accommodate the potential future addition of a bypass lane.

- 3) A utility exhibit. The exhibit must show only property lines, buildings, sewer, and water stormwater facilities. The exhibit must clearly note which lines are private and which are public.
- c) Prior to the issuance of a site development permit
- 1) The resolution must be recorded with Hennepin County.
 - 2) Submit the following documents the following documents are in a city-approved format:
 - A stormwater maintenance agreement.
 - A salt and chloride management plan.
 - A construction management plan.
 - 3) Submit individual letters of credit or cash escrow for 125% of a bid cost or 150% of an estimated cost to construct streets and utility improvements, comply with the grading permit, wetland restoration, tree mitigation requirements, and to restore the site. One itemized letter of credit is permissible if approved by staff. The city will not fully release the letters of credit or cash escrow until (1) as-built drawings have been submitted; (2) a letter certifying that the streets and utilities have been completed according to the plans approved by the city has been submitted; (3) vegetated ground cover has been established; and (4) required landscaping or vegetation has survived one full growing season.
 - 4) Submit cash escrow in the amount of \$5,000. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document, the builder and property owner will acknowledge:
 - The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.
 - 5) Hold a preconstruction meeting with site contractors and

city planning, engineering, public works, and natural resources staff. The meeting may not be held until all items required under 4.01 Subd.3(b) and Subd.3(c) of this resolution have been submitted, reviewed by staff, and approved.

- 6) Install a temporary rock driveway, erosion control, tree and wetland protection fencing and any other measures identified on the SWPPP for staff inspection. These items must be maintained throughout the course of construction.
 - 7) A right-of-way permit and inspection are required for connections to public utilities.
3. A building permit is required. The permit application and supporting plans and documents must be submitted through the city's online permit review system. Unless authorized by appropriate staff, no site work may begin until the permit has been approved.
- a) Prior to the issuance of the building permit:
 - 1) Submit a construction management plan if the builder is not the same entity doing grading work on the site. The construction management plan submitted at the time of grading permit may fulfill this requirement.
 - 2) Submit a cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document, the builder and property owner will acknowledge:
 - The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.
 - If the builder is the same entity doing grading work on the site, the cash escrow submitted at the time of grading permit may fulfill this requirement.
 - 3) Submit electronic CAD files or certified as-built drawings for public infrastructure in microstation or DXF and PDF format.

- 4) Submit a truck-turning exhibit. The exhibit must use the templates for the city's largest fire truck and illustrate that the truck can maneuver through the site.
4. The drive-thru canopy must not be structurally attached to the principal building or anchored below the ground.
5. Construct a sidewalk connection between the site and the existing trail facilities that provide access to the southeast quadrant of TH 7.
6. A solid opaque trash enclosure must be constructed to match the principal structure.
7. The trash enclosure must be located outside of the drainage and utility easement.
8. Install "Do Not Enter" signs at the bank drive-thru exit. The city may require additional signage, striping needs, and curbed medians in the event that site circulation becomes problematic.
9. Public address systems must not be audible from any residential parcel.
10. An emergency responder radio coverage system may be required within the building per Appendix P of the MN State Fire Code.
11. The property owner is responsible for replacing any required landscaping that dies.
12. The city council may reasonably add or revise conditions to address any future unforeseen problems.
13. Any change to the approved use that results in a significant increase in a significant change in character would require a revised conditional use permit.
14. Construction must begin by November 13, 2024, unless the city council grants a time extension.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Oct. 30, 2023.

Brad Wiersum, Mayor

Attest:

Becky Koosman, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a meeting held on Oct. 30, 2023.

Becky Koosman, City Clerk

MINNETONKA PLANNING COMMISSION
Oct. 26, 2023

Brief Description Preliminary and final plat approval for PRIORY WOODS, a three-lot subdivision at 4633 Sparrow Road.

Recommendation Recommend the city council adopt the resolution approving the plats.

Proposal

The applicant is proposing to subdivide the property at 4633 Sparrow Road into three conforming, single-family residential lots. This proposal includes a preliminary plat and final plat application.

No variances are required for this proposal.

Existing Property

The existing lot is located at the northeast corner of the intersection of Sparrow Road and Priory Lane. The subject property is 1.83 acres and improved with a single-family home and bituminous driveway off of Priory Lane. The existing home was constructed in 1928, prior to the establishment of the city zoning ordinance.

Approximately half of the subject property has topography between 20 to 30 percent slope and a minimal area between proposed Lots 1 and 2 with a steep slope.¹

For more information, see the “Slope Analysis” section of the report.

Proposed Subdivision

The applicant is requesting to subdivide the existing property into three residential lots between 0.56 to 0.7 acres each. The applicant proposes to remove the existing single-family home and a moderate amount of trees to accommodate the three new single-family homes.

The three lots would meet minimum lot area requirements for properties zoned R-1, as outlined in the subdivision ordinance.

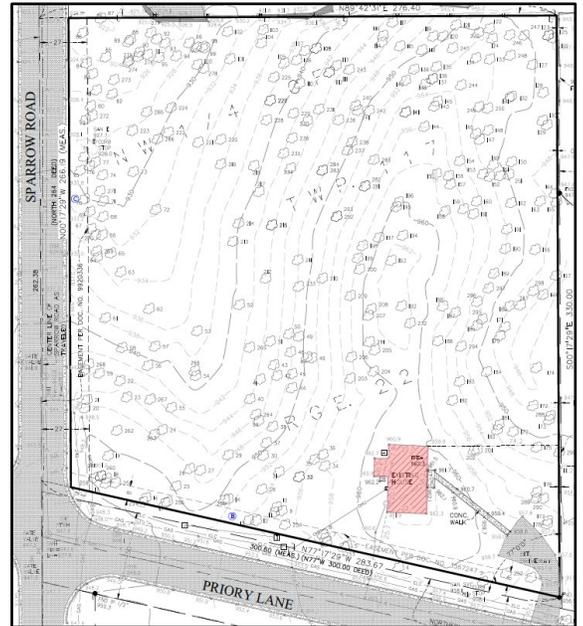


Image 1 - Existing Conditions

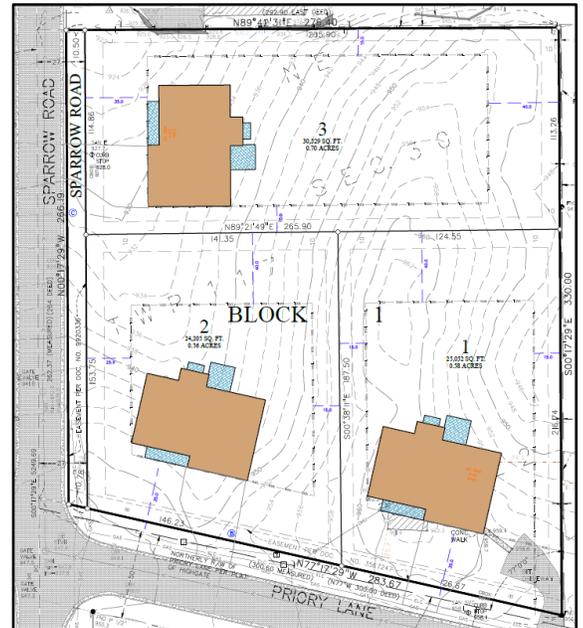


Image 2 - Proposed Subdivision

¹ Per City Code 300.02 Subd. 139 – A steep slope is defined as grade of at least 30 percent or more.

For the subdivision lot standards, see the “Subdivision” section of the report.

Primary Questions and Analysis

A land use proposal is comprised of many details. In evaluating a proposal, staff first reviews these details and then aggregates them into a few primary questions or issues. The following outlines both the primary questions associated with the proposed subdivision and the staff's findings.

- **Are the proposed lots reasonable?**

Yes. In Minnetonka, the minimum lot size for newly created R-1 lots is roughly half an acre; the existing lot size is 1.85 acres. The proposal is reasonable, as it would create two additional lots, all meeting or exceeding minimum R-1 lot standards. The city's subdivision ordinance outlines minimum area and dimensional standards for single-family residential lots. For the subdivision standards, see the “Subdivision” section of this report.

- **Would the proposal meet the tree ordinance?**

Yes. As proposed, the applicant would remove 41 high-priority trees on the site, which is equal to the 35 percent maximum that can be removed by ordinance. Additionally, the applicant has proposed to remove or would be significantly impacting the critical root zones of 67 significant trees on the property (46.8 percent), which is below the 50 percent allowed by the ordinance. If approved, the applicant may be required to mitigate for some of the trees removed during home construction. However, mitigation requirements would be determined based on plans submitted with the building permit.

Staff Recommendation

Recommend the city council adopt the resolution approving the preliminary and final plat for PRIORY WOODS.

Originator: Bria Raines, Planner
Through: Loren Gordon, AICP, City Planner

Supporting Information

- Project No.** 23018.23a
- Property** 4633 Sparrow Road
- Applicant** Zehnder Homes Inc.
- Surrounding Land Uses** The properties to the north, south, east, and west are all zoned R-1, guided for low-density residential and improved with detached, single-family residential homes.
- Planning** Guide Plan designation: Low-density residential
Zoning: R-1
- Slope Analysis** As required by the subdivision application process, generalized home footprints, locations, and grading plans have been submitted by the applicant. The plans show minimal grading to accommodate the three new home footprints and the infiltration basins.
- The existing areas over 30 percent slope would not be altered by the grading proposal.
- Subdivision** As required by the subdivision ordinance, the subdivided parcels must meet the lot design standards. The subdivision standards are as follows:

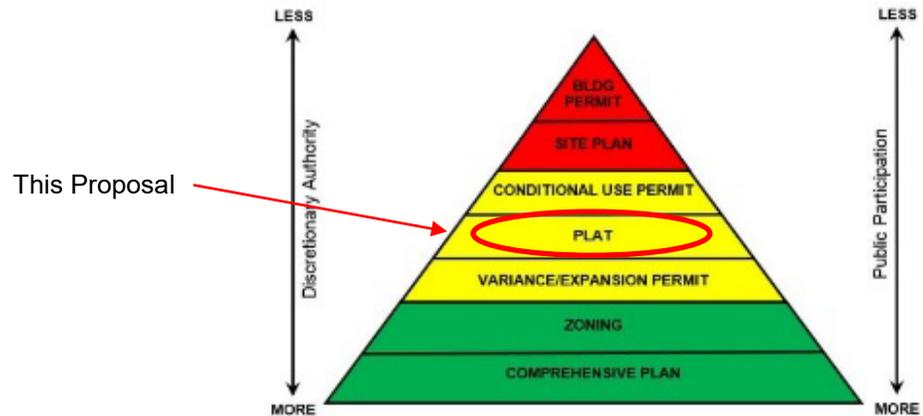
	Required by ordinance	Lot 1	Lot 2	Lot 3
Total area	22,000 sf	25,052 sf	24,205 sf	30,329 sf
Buildable area	3,500 sf	~ 11,850 sf	~ 9,770 sf	~16,025 sf
Width at ROW	80 ft	125 ft	145 ft	115 ft
Width at setback	110 ft	125 ft	145 ft	115 ft
Lot depth	125 ft	190 ft	155 ft	265 ft

*Numbers rounded to the nearest 5 ft.

The proposal meets the subdivision ordinance.

- Natural Resources** Best management practices must be followed during the course of site preparation and construction activities. This would include the installation and maintenance of a temporary rock driveway, erosion control, and tree protection fencing. As a condition of approval, the applicant must submit a construction management plan detailing these management practices.
- Neighborhood Comments** The city sent notices to 38 area property owners and received five comments to date. See comments in attachments.

Pyramid of Discretion



Motion Options

The planning commission has three options:

1. Concur with staff's recommendation. In this case, a motion should be made recommending the city council adopt the resolution approving the subdivision.
2. Disagree with staff's recommendation. In this case, a motion should be made recommending the city council deny the request. The motion must include a statement as to how ordinance standards are not met and, therefore, why denial is recommended.
3. Table the request. In this case, a motion should be made to table the request. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant or both.

Voting Requirement

The planning commission will make a recommendation to the city council on the applicant's proposal. A recommendation for approval requires an affirmative vote of a simple majority. The city council's final approval requires affirmative votes of four members.

Deadline for Decision

Dec. 18, 2023



Location Map

Project: Priors Woods
Address: 4633 Sparrow Road



“Priory Woods” – Development Narrative

Developer Introduction:

ZEHNDER HOMES, INC. –

Eric Zehnder
10300 10th Ave N
Plymouth, Minnesota 55441
Telephone: 651-303-5747
Email: ericzehnder@zehnderhomes.com

Zehnder Homes is proposing to develop a single parcel in the city of Minnetonka, Minnesota that will create a community of 3 detached single-family homes on +/- 1.87 acres of land. The site is located in the NE corner of the intersection of Sparrow Road and Priory Lane. There will not be any new public infrastructure needed for this development. The land will be subdivided into three lots, with two homes having driveways off of Priory Lane, and one home with a driveway on Sparrow Road. The development will be named “Priory Woods” and is a conforming, single-family, R1 development. Care has been taken to minimize grading and the impact to existing trees. The percentage of removed trees is within the allowed percentages. This proposed residential development will consist of single-family style homes.

DESCRIPTION OF PROPERTY SURVEYED

That part of the Northwest Quarter of Northeast Quarter of Section 30, Township 117 North, Range 22 West of the Fifth Principal Meridian, described as follows:

Commencing 280.5 feet South of the Northwest corner of the Northwest Quarter of the Northeast Quarter of Section 30, Township 117, Range 22; thence East 292.9 feet; thence South 330 feet; thence North 77 degrees West for a distance of 300 feet; thence North along the center of the road for a distance of 264 feet to the point of beginning, Hennepin County, Minnesota, except road.

Abstract Property

STANDARD NOTES

- Site Address: 4633 Sparrow Road, Minnetonka MN 55345
- Flood Zone Information: X (area determined to be outside of the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0317E effective date of 11/4/2016.
- Parcel Area Information:

Gross Area:	82,393 s.f.	-	1.89 acres
R/W Area:	2,808 s.f.	-	0.06 acres
Net Area:	79,585 s.f.	-	1.83 acres

*We do not affirmatively insure the quantity of acreage set forth in the description
- Benchmark: Elevations are based on Hennepin County Control Point Name: Jeff which has an elevation of: 922.21 feet (NAVD83).
- Zoning Information: The current Zoning for the subject property is R1 (Low Density Residential District) per the City of Minnetonka's zoning map found on the city GIS website 8/1/2023. The setback, height, and floor space area restrictions for said zoning designation are as follows:

Principal Structure Setbacks - Street(s): 35 feet from right of way, corner lot reduce one side by 10 feet
 Side: Sum shall not be less than 30 feet, minimum 10 feet
 Rear: 40 feet or 20% of lot depth whichever is less
 Height: 35 feet

*Please note that the zoning information shown herein may have been amended through a city process. We recommend that a zoning letter be obtained from the Zoning Administrator for the current restrictions for this site. All setback information and hardcover data for planning and design must be verified by all parties involved in the design and planning process prior to any planning or construction.

We have not received the current zoning classification and building setback requirements from the insurer.

- Utilities: We have shown the location of utilities to the best of our ability based on observed evidence together with evidence from the following sources: plans obtained from utility companies, plans provided by client, markings by utility companies and other appropriate sources. We have used this information to develop a view of the underground utilities for this site. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Also, please note that Seasonal conditions may inhibit our ability to visibly observe all the utilities located on the subject property. A Gopher State One Call was submitted for this survey. Please reference Ticket No. 231944203 for a list of utility operators in this area.

SURVEY REPORT

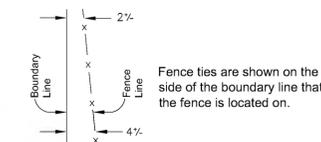
This map and report was prepared with the benefit of a Commitment for Title Insurance issued by CHB Title, LLC issuing agent for Old Republic National Title Insurance Company, File No. 59187, dated June 19, 2023.

- We note the following with regards to Schedule B of the herein referenced Title Commitment:
 - Item no. 1-8, 11 & 12 are not survey related
 - Item no. 9 - Terms and conditions of right of way for a driveway over the South 8 feet of the subject property as contained in that Warranty Deed, filed October 7, 1965, as Document No. 3567247. (B)
 - Item no. 10 - Terms and conditions of easement to the City of Minnetonka for right of way purposes, filed March 13, 2013, as Document No. 9920336. (C)
- Observations/Comments noted hereon per field survey such as (but not limited to): access, occupation, and easements and/or servitudes:
 - There appears to be a gap between the east line of the described subject property and the west line of Block 1, HIGHGATE, Hennepin County, Minnesota. As well as a gap between the south line of the described subject property and the northerly right of way of Priory Lane per the recorded plat of HIGHGATE. There also appears to be a gap between the north line of the described subject property and the south line of the described property to the north.



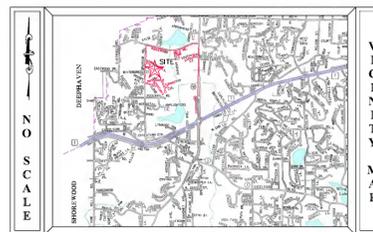
Bearings are based on the Hennepin County Coordinate System (NAD 83 - 1986 adj.)

SCALE IN FEET



SURVEY LEGEND

● CAST IRON MONUMENT	⊕ PIEZOMETER	W/E WALKOUT ELEVATION
○ IRON PIPE MONUMENT SET	⊖ POWER POLE	FF/E FIRST FLOOR ELEVATION
● DRILL HOLE FOUND	< GUY WIRE	GF/E GARAGE FLOOR ELEVATION
⊠ ROOF DRAIN	⊠ ROOF DRAIN	TOF TOP OF FOUNDATION ELEV.
⊠ CHISELED "X" MONUMENT SET	⊠ LIFT STATION	LOE LOWEST OPENING ELEV.
⊠ CHISELED "X" MONUMENT FOUND	⊕ SANITARY MANHOLE	CONCRETE
⊠ REBAR MONUMENT FOUND	⊕ SANITARY CLEANOUT	BITUMINOUS
⊠ PK NAIL MONUMENT SET	⊕ STORM MANHOLE	--- BUILDING SETBACK LINE
⊠ PK NAIL MONUMENT FOUND	⊕ STORM DRAIN	--- CTV --- CABLE TV
⊠ PK NAIL W/ ALUMINUM DISC	⊕ CATCH BASIN	--- CONCRETE CURB
⊠ SURVEY CONTROL POINT	⊕ FLARED END SECTION	--- CONTOUR EXISTING
⊠ A/C UNIT	⊕ TREE CONIFEROUS	--- CONTOUR PROPOSED
⊠ CABLE TV PEDESTAL	⊕ TREE DECIDUOUS	--- GUARD RAIL
⊠ ELECTRIC TRANSFORMER	⊕ TREE CONIFEROUS REMOVED	--- DT --- DRAIN TILE
⊠ ELECTRIC MANHOLE	⊕ TREE DECIDUOUS REMOVED	--- ELC --- ELECTRIC UNDERGROUND
⊕ ELECTRIC METER	⊕ TELEPHONE MANHOLE	--- F --- FENCE
⊕ TELEPHONE OUTLET	⊕ TELEPHONE PEDESTAL	--- FO --- FIBER OPTIC UNDERGROUND
⊕ YARD LIGHT	⊕ UTILITY MANHOLE	--- G --- GAS UNDERGROUND
⊕ LIGHT POLE	⊕ UTILITY PEDESTAL	--- OHU --- OVERHEAD UTILITY
⊕ FIBER OPTIC MANHOLE	⊕ UTILITY VAULT	--- T --- TREE LINE
⊕ FIRE DEPT. HOOK UP	⊕ WATERMAIN MANHOLE	--- > --- SANITARY SEWER
⊕ FLAG POLE	⊕ WATER METER	--- S --- STORM SEWER
⊕ FUEL PUMP	⊕ WATER SPIGOT	--- TEL --- TELEPHONE UNDERGROUND
⊕ FUEL TANK	⊕ WELL	--- TW --- RETAINING WALL
⊕ PROPANE TANK	⊕ MONITORING WELL	--- UTL --- UTILITY UNDERGROUND
⊕ GAS METER	⊕ CURB STOP	--- I --- WATERMAIN
⊕ GAS MANHOLE	⊕ GATE VALVE	--- T --- TRAFFIC SIGNAL
⊕ GENERATOR	⊕ HYDRANT	--- R --- RAILROAD TRACKS
⊕ GUARD POST	⊕ IRRIGATION VALVE	--- RS --- RAILROAD SIGNAL
⊕ HAND HOLE	⊕ POST INDICATOR VALVE	--- RS --- RAILROAD SWITCH
⊕ MAIL BOX	⊕ SIGN	--- S --- SATELLITE DISH
	⊕ SOIL BORING	--- W --- WETLAND BUFFER SIGN

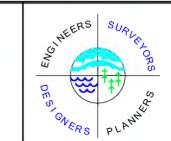


FIELD CREW	NO.	BY	DATE	REVISION
DM / AK				
DRAWN				
CMT				
CHECKED				
DATE				
7/24/2023				

USE (INCLUDING COPYING, DISTRIBUTION, AND/OR CONVEYANCE OF INFORMATION) OF THIS PRODUCT IS STRICTLY PROHIBITED WITHOUT SATHRE-BERGQUIST, INC.'S EXPRESS WRITTEN AUTHORIZATION. USE WITHOUT SAID AUTHORIZATION CONSTITUTES AN ILLEGITIMATE USE AND SHALL THEREBY INDEMNIFY SATHRE-BERGQUIST, INC. OF ALL RESPONSIBILITY. SATHRE-BERGQUIST, INC. RESERVES THE RIGHT TO HOLD ANY ILLEGITIMATE USER OR PARTY LEGALLY RESPONSIBLE FOR DAMAGES OR LOSSES RESULTING FROM ILLEGITIMATE USE.

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
 Dated this 15th day of August, 2023.

 Daniel L. Schmidt, PLS
 Minnesota License No. 26147
 schmidt@sathre.com



SATHRE-BERGQUIST, INC.
 14000 25TH AVENUE NORTH, SUITE 120
 PLYMOUTH MN 55447 (952) 476-6000
 WWW.SATHRE.COM

TWP:30-RGE.117-SEC.22
 Hennepin County
MINNETONKA, MINNESOTA

CERTIFICATE OF SURVEY
 PREPARED FOR:
ZEHNDER HOMES INC.

FILE NO.
 99595-151
1
1

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Abstract Property

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- 2) **Flood Zone Information:** X (area determined to be outside of the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0317E effective date of 11/4/2016.
- 3) **Parcel Area Information:**

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R/W Area:	2,808 s.f.	-	0.06 acres
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- 4) **Benchmark:** Elevations are based on Hennepin County Control Point Name: Jeff which has an elevation of: 922.21 feet (NAVD88).
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Principal Structure Setbacks - Street(s): 35 feet from right of way, corner lot reduce one side by 10 feet
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 Height: 35 feet

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We have not received the current zoning classification and building setback requirements from the insurer.

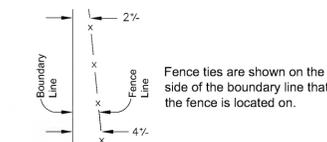
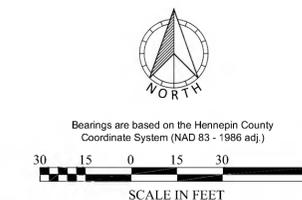
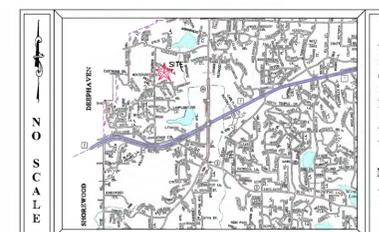
- 6) **Utilities:** We have shown the location of utilities to the best of our ability based on observed evidence together with evidence from the following sources: plans obtained from utility companies, plans provided by client, markings by utility companies and other appropriate sources. We have used this information to develop a view of the underground utilities for this site. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, the client is advised that excavation may be necessary. Also, please note that Seasonal conditions may inhibit our ability to visibly observe all the utilities located on the subject property. A Gopher State One Call was submitted for this survey. Please reference Ticket No. 231944203 for a list of utility operators in this area.

SURVEY REPORT

This map and report was prepared with the benefit of a Commitment for Title Insurance issued by CHB Title, LLC issuing agent for Old Republic National Title Insurance Company, File No. 59187, dated June 19, 2023.

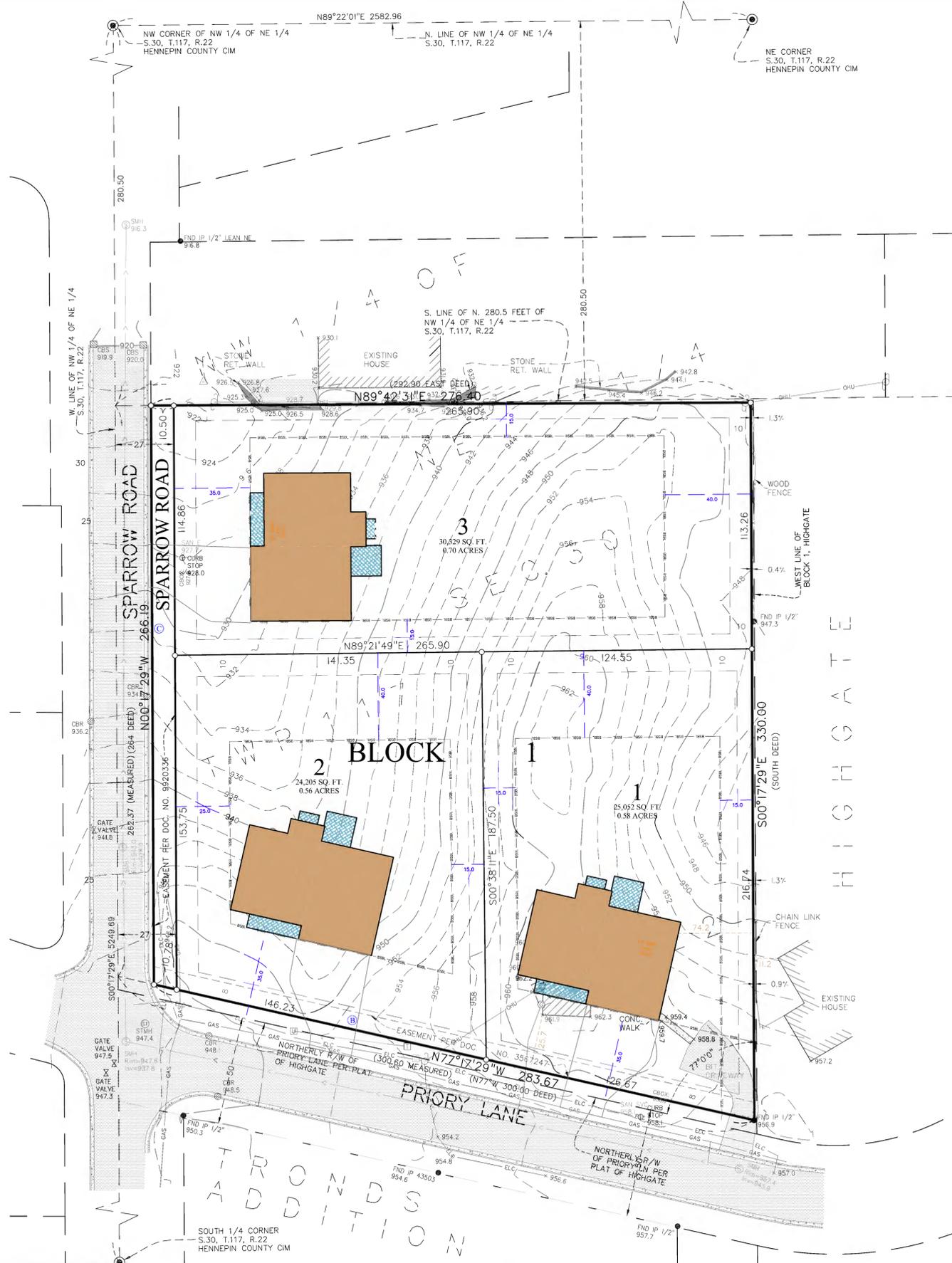
- 1) We note the following with regards to Schedule B of the herein referenced Title Commitment:
 - a) Item no. 1-8, 11 & 12 are not survey related
 - b) Item no. 9 - Terms and conditions of right of way for a driveway over the South 8 feet of the subject property as contained in that Warranty Deed, filed October 7, 1965, as Document No. 3567247. (B)
 - c) Item no. 10 - Terms and conditions of easement to the City of Minnetonka for right of way purposes, filed March 13, 2013, as Document No. 9920336. (C)
- 2) Observations/Comments noted hereon per field survey such as (but not limited to): access, occupation, and easements and/or servitudes:
 - a) There appears to be a gap between the east line of the described subject property and the west line of Block 1, HIGHGATE, Hennepin County, Minnesota. As well as a gap between the south line of the described subject property and the northerly right of way of Priory Lane per the recorded plat of HIGHGATE.
There also appears to be a gap between the north line of the described subject property and the south line the the described property to the north.

PREPARED BY	PREPARED FOR
ENGINEER SATHRE-BERGQUIST, INC. 14000 25th Ave N, Suite 120 Plymouth, MN 55447 PHONE: (952) 476-6000 FAX: (952) 476-0104 CONTACT: CHARLES WIEMERSLAGE, P.E. EMAIL: CWIEMERSLAGE@SATHRE.COM	DEVELOPER ZEHNDER HOMES INC. 14240 23RD AVE N PLYMOUTH, MN 55447 CONTACT: TOM DEHN PHONE: (651) 303-5747 EMAIL: ERICZEHNDER@ZEHNDERHOMES.COM



SURVEY LEGEND

<ul style="list-style-type: none"> ● CAST IRON MONUMENT ○ IRON PIPE MONUMENT SET ● IRON PIPE MONUMENT FOUND ● DRILL HOLE FOUND ✕ CHISELED "X" MONUMENT SET ✕ CHISELED "X" MONUMENT FOUND ✕ REBAR MONUMENT FOUND ✕ PK NAIL MONUMENT SET ▲ PK NAIL MONUMENT FOUND ○ PK NAIL W/ ALUMINUM DISC ○ SURVEY CONTROL POINT □ A/C UNIT □ CABLE TV PEDESTAL □ TREE DECIDUOUS □ TREE CONIFEROUS □ TREE CONIFEROUS REMOVED □ TREE DECIDUOUS REMOVED □ TELEPHONE MANHOLE □ TELEPHONE OUTLET □ UTILITY MANHOLE □ LIGHT POLE □ FIBER OPTIC MANHOLE □ FIRE DEPT. HOOK UP □ FLAG POLE □ FUEL PUMP □ FUEL TANK □ PROPANE TANK □ GAS METER □ GAS VALVE □ GAS MANHOLE □ GENERATOR □ GUARD POST □ HAND HOLE □ MAIL BOX 	<ul style="list-style-type: none"> ○ PIEZOMETER ○ POWER POLE ○ GUY WIRE ○ ROOF DRAIN ○ LIFT STATION ○ SANITARY MANHOLE ○ SANITARY CLEANOUT ○ STORM MANHOLE ○ STORM DRAIN ○ CATCH BASIN ○ FLARED END SECTION ○ TREE CONIFEROUS ○ TREE CONIFEROUS REMOVED ○ TREE DECIDUOUS REMOVED ○ TELEPHONE MANHOLE ○ TELEPHONE PEDESTAL ○ UTILITY MANHOLE ○ UTILITY PEDESTAL ○ UTILITY VAULT ○ WATERMAIN MANHOLE ○ WATER METER ○ WATER SPIGOT ○ WELL ○ MONITORING WELL ○ CURB STOP ○ GATE VALVE ○ HYDRANT ○ IRRIGATION VALVE ○ POST INDICATOR VALVE ○ SIGN ○ SOIL BORING 	<ul style="list-style-type: none"> ○ WOE WALKOUT ELEVATION ○ FFE FIRST FLOOR ELEVATION ○ GFE GARAGE FLOOR ELEVATION ○ TOF TOP OF FOUNDATION ELEV. ○ LOE LOWEST OPENING ELEV. ○ CONCRETE ○ BITUMINOUS ○ BUILDING SETBACK LINE ○ CTV CABLE TV ○ CONCRETE CURB ○ CONTOUR EXISTING ○ CONTOUR PROPOSED ○ GRAD RAIL ○ DRAIN TILE ○ ELC ELECTRIC UNDERGROUND ○ FENCE ○ FO FIBER OPTIC UNDERGROUND ○ GAS UNDERGROUND ○ OHU OVERHEAD UTILITY ○ TREE LINE ○ SANITARY SEWER ○ STORM SEWER ○ TEL TELEPHONE UNDERGROUND ○ RETAINING WALL ○ UTILITY UNDERGROUND ○ I WATERMAIN ○ TRAFFIC SIGNAL ○ RAILROAD TRACKS ○ RAILROAD SIGNAL ○ RAILROAD SWITCH ○ SATELLITE DISH ○ WETLAND BUFFER SIGN
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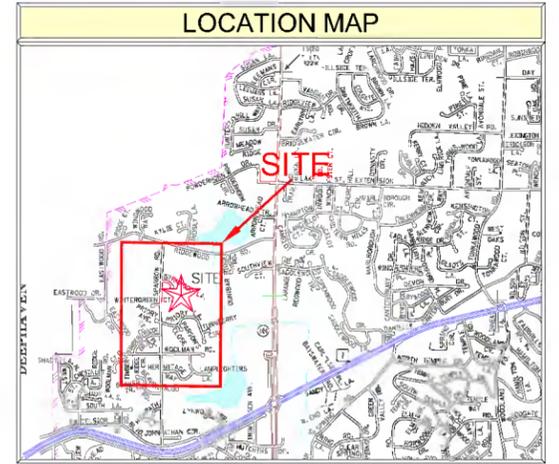
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I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
 Dated this 24th day of August, 2023.

 Daniel L. Schmidt, PLS
 schmidt@sathre.com
 Minnesota License No. 26147

 SATHRE-BERGQUIST, INC. 14000 25TH AVENUE NORTH, SUITE 120 PLYMOUTH MN 55447 (952) 476-6000 WWW.SATHRE.COM	TWP:30-RGE.117-SEC.22 Hennepin County MINNETONKA, MINNESOTA	PRELIMINARY PLAT PRIORY WOODS PREPARED FOR: ZEHNDER HOMES INC.	FILE NO. 99595-151
			1

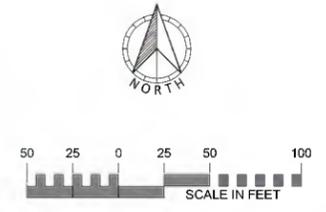


SHEET INDEX TABLE	
SHEET	Description
01	Title Sheet
02	Final Utility Plan
03	Final Grading Plan
04	Steep Slope Plan
05	Erosion Control Plan
06	Amended Soils Plan
07-08	Tree Preservation Plan
09	Construction Details

Notes:
 Lots: 3 Single Family Lots
 Front Yard Setback: 35'
 Side Yard Setback: 30' total - 10 min'
 Side Yard Setback Corner Lot: 25'
 Rear Yard Setback: 40'

R1 - Zoning
 110 Ft min at Setback line
 125 Ft Deep
 22,000 sf

PREPARED BY	PREPARED FOR
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Charles A. Wiemerslage
 CHARLES A. WIEMERSLAGE, P.E.
 Date: 09/01/23 Lic. No. 49180

ENGINEERS SURVEYORS
 DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
 14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

CITY PROJECT NO.

 MINNETONKA,
 MINNESOTA

TITLE SHEET
 PRIORY WOODS
 ZEHLER HOMES, INC.

FILE NO.
 98595-151
 01
 09

LOT 3 EXISTING SERVICE TIE CARD

Street SPARROW ROAD No. 4633 Addition
Between WINTERGREEN and PRIORY Lot PARCEL 1700 Block

Sewer Service		Water Service	
Plan No. <u>5201</u>	Sheet No. <u>8</u>	Plan No. <u>5201</u>	Sheet No. <u>8</u>
MH <u>53</u>	Distance <u>133'</u>	MH <u>53</u>	Distance <u>133'</u>
Length <u>27'</u>	Depth at P.L. <u>10'</u>	Length <u>32'</u>	Depth at P.L. <u>7'</u>

SEWER IN SAME TRENCH
5' RISER - 6' DEPTH TO MAIN

Contractor: _____ Installed by: _____ Tied by: _____ Date: _____

Contractor: CENTENNIAL Installed by: J.M. Tied by: ANTHONY K Date: 8/9/14

LOT 2 EXISTING SERVICE TIE CARD

Street SPARROW ROAD No. 4633 Addition
Between WINTERGREEN and PRIORY Lot 1700 Block

Sewer Service		Water Service	
Plan No. <u>5201</u>	Sheet No. <u>9</u>	Plan No. <u>5201</u>	Sheet No. <u>8</u>
MH <u>53</u>	Distance <u>225'</u>	MH <u>53</u>	Distance <u>225'</u>
Length <u>27'</u>	Depth at P.L. <u>10'</u>	Length <u>32'</u>	Depth at P.L. <u>7'</u>

SEWER IN SAME TRENCH
5' RISER

Contractor: _____ Installed by: _____ Tied by: _____ Date: _____

Contractor: CENTENNIAL Installed by: J.M. Tied by: ANTHONY K Date: 8/9/14

LOT 1 EXISTING SERVICE TIE CARD

Street SPARROW ROAD No. 4633 Addition
Between _____ and _____ Lot PARCEL 1700 Block

Sewer Service		Water Service	
Plan No. <u>5201</u>	Sheet No. <u>9</u>	Plan No. _____	Sheet No. _____
MH <u>53</u>	Distance <u>225'</u>	MH _____	Distance _____
Length <u>27'</u>	Depth at P.L. <u>10'</u>	Length _____	Depth at P.L. _____

SEWER IN SAME TRENCH
5' RISER

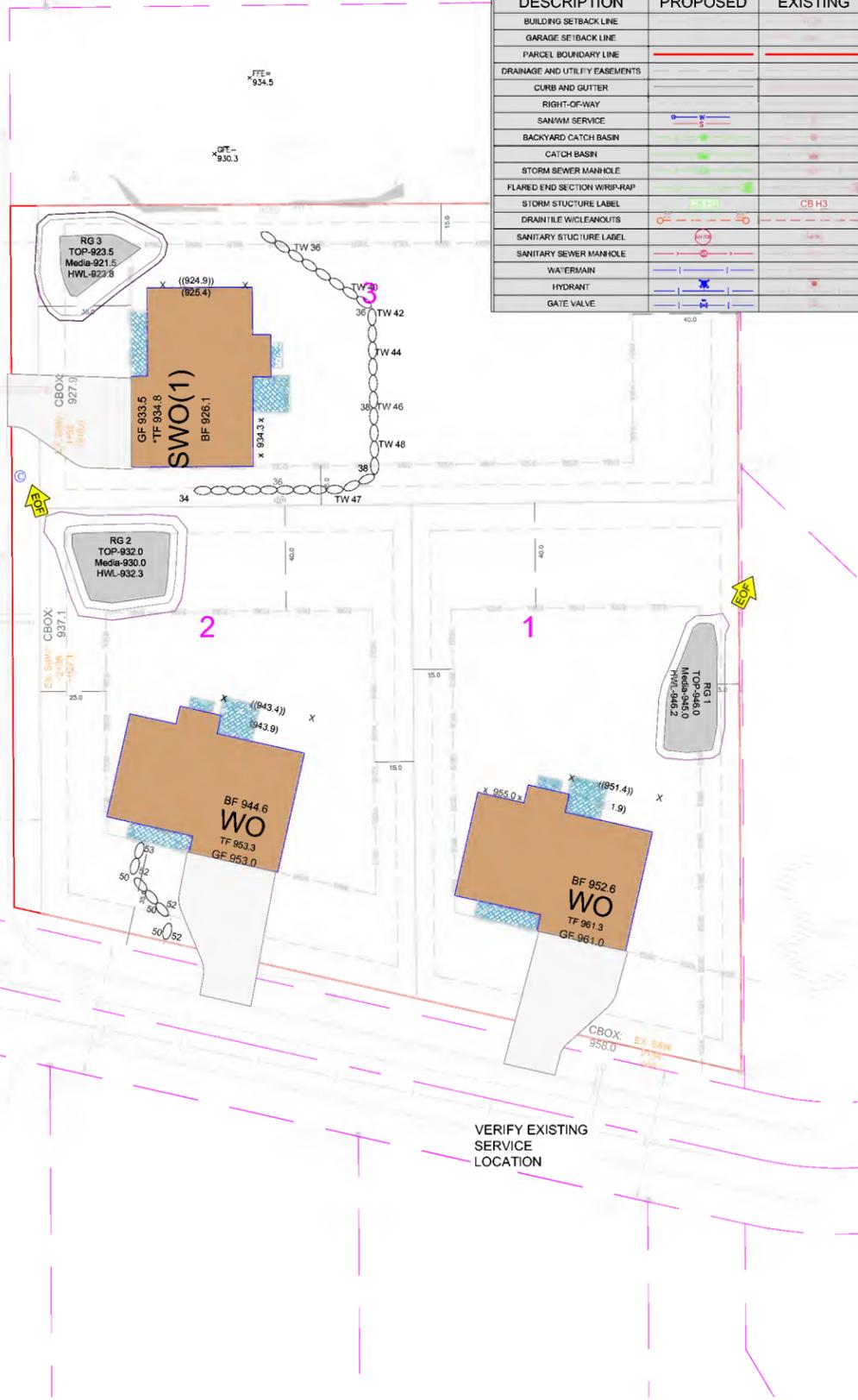
Contractor: _____ Installed by: _____ Tied by: _____ Date: _____

Contractor: CENTENNIAL Installed by: J.M. Tied by: L.S. Date: 12/77

LEGEND		
DESCRIPTION	PROPOSED	EXISTING
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
SANWM SERVICE		
BACKYARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WRAP-RAP		
STORM STRUCTURE LABEL		
DRAIN TILE W/ CLEANOUTS		
SANITARY STRUCTURE LABEL		
SANITARY SEWER MANHOLE		
WATERMAIN		
HYDRANT		
GATE VALVE		

NOTES:

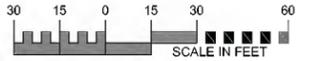
- EXISTING SERVICES TO BE REUSED AS APPROPRIATE
- VERIFY LOCATIONS & CONDITION OF ALL EXISTING SERVICES. - REMOVE AND REPLACE AS NECESSARY
- REPLACEMENT SERVICES SHALL BE
SANITARY: 6" PVC SDR26 AND RISER TO BE SDR 26.
WATER TO BE 1.5" COPPER - ONE CONTIGUOUS PIECE, NO JOINTS OR SPLICING ALLOWED IN ROW
- **REMOVE & REPLACE STREET IN KIND AS NECESSARY FOR NEW SERVICES CONNECTIONS NEEDED. EXISTING SERVICE TO BE REMOVED BACK TO MAIN - CORP STOP SHALL BE SHUT OFF AT MAIN PRIOR TO TAPPING NEW SERVICE.
- CURB BOXES TO HAVE EXTENSION RODS TO CURB STOP.
- ALL SEWER SERVICES ARE LOCATED 3' DOWNSTREAM OF WATER SERVICES.
- THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS, VERIFY EXISTING INVERT LOC. & ELEV. AND NOTIFY THE OWNER OF ANY DIFFERENCES. PRIOR TO BEGINNING CONSTRUCTION
- UNLESS OTHERWISE NOTED, OR AS SPECIFIED IN THE ABOVE NOTE, ALL MATERIALS, CONSTRUCTION TECHNIQUES AND TESTING SHALL CONFORM TO THE 1999 ED. OF THE "STANDARD UTILITIES SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION BY THE CITY ENGINEERING ASSOCIATION OF MINN." AND TO THE "STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION" MINN. DEPT. OF TRANS., 2000 EDITION, INCLUDING THE CURRENT ADDENDUM.
- WATER SERVICE TO HAVE CURB STOP & BOX INSTALLED PER DETAIL 518.
- CONNECT TO WATERMAIN WITH CORPORATION STOP AND TAPPING SADDLE PER CITY OF MINNETONKA SPECIAL PROVISIONS - SEE 2504 WATER MAIN - 2611.2D.
- CORPORATIONS MUELLER H15000 OR FORD F 600 SADDLES SMITH-BLAIR 315 OR 317



VERIFY EXISTING SERVICE LOCATION

VERIFY EXISTING SERVICE LOCATION

VERIFY EXISTING SERVICE LOCATION



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Charles A. Wiemerslage
CHARLES A. WIEMERSLAGE, P.E.
Date: 09/01/13 Lic. No. 49180

ENGINEERS SURVEYORS DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

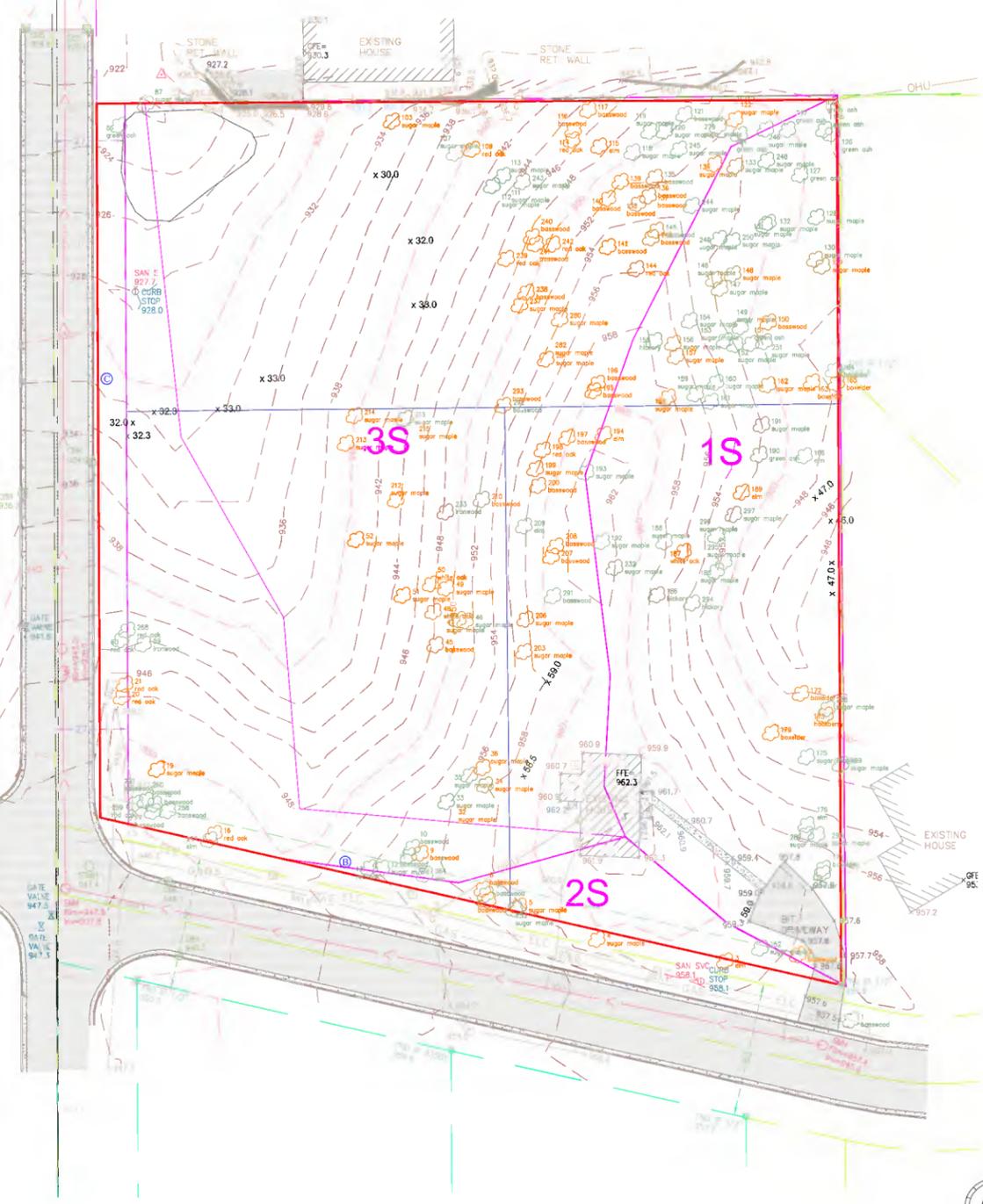
CITY PROJECT NO. _____

MINNETONKA, MINNESOTA

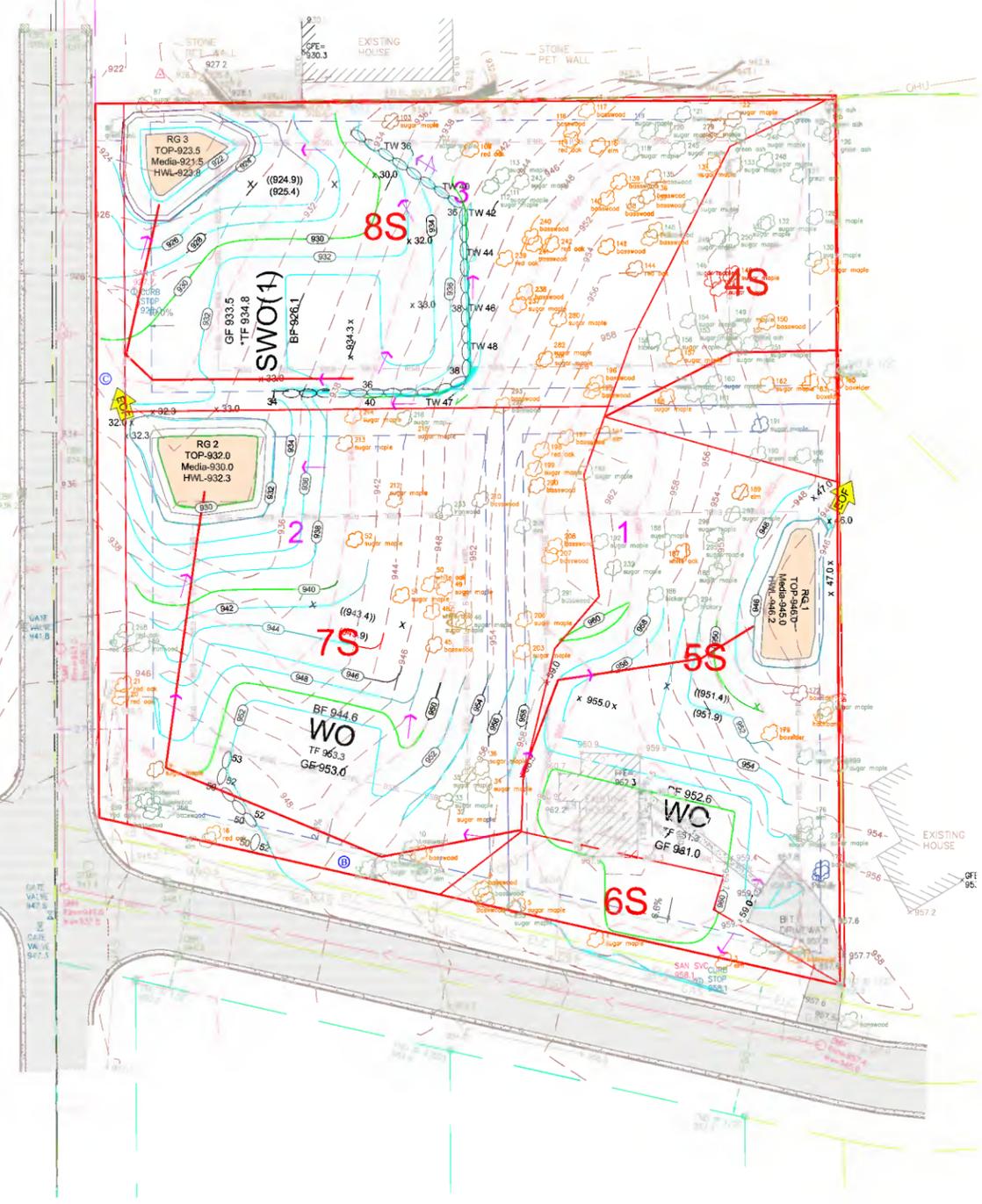
UTILITY PLAN
PRIORY WOODS
ZEHNDER HOMES, INC.

FILE NO. 99595-151
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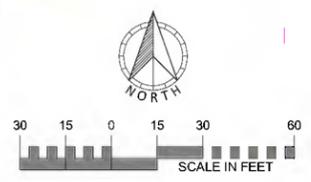
SYMBOL LEGEND		
DESCRIPTION	PROPOSED	EXISTING
MINOR CONTOUR		
MAJOR CONTOUR		
LOT LINE		
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
BACKYARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WHIP RAMP		
SANITARY SEWER MANHOLE		
HYDRANT		
GATE VALVE		
DRAIN FLOW RUNOFF ARROW		
EMERGENCY OVERTFLOW SWALE		
SILT FENCE (PRE CONSTRUCTION)		
SILT FENCE (POST CONSTRUCTION)		
SPOT ELEVATION		
TBC SPOT ELEVATION		



EXISTING DRAINAGE



PROPOSED DRAINAGE



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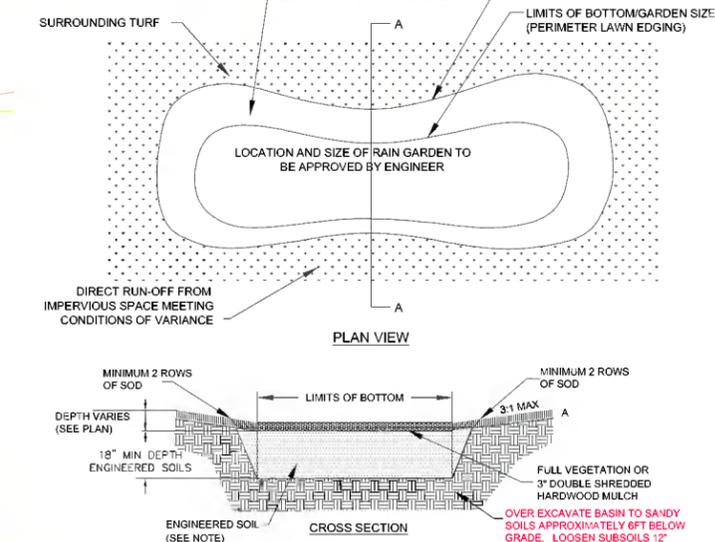
CITY PROJECT NO. _____

MINNETONKA, MINNESOTA

DRAINAGE MAP
PRIORY WOODS
ZEHNDER HOMES, INC.

FILE NO. 99595-151
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 DA

SYMBOL LEGEND		
DESCRIPTION	PROPOSED	EXISTING
MINOR CONTOUR		
MAJOR CONTOUR		
LOT LINE		
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
BACKYARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WHIRL RAMP		
SANITARY SEWER MANHOLE		
HYDRANT		
GATE VALVE		
DRAIN FLOW RUNOFF ARROW		
EMERGENCY OVERTFLOW SWALE		
SILT FENCE (PRE CONSTRUCTION)		
SILT FENCE (POST CONSTRUCTION)		
SPOT ELEVATION		
TBC SPOT ELEVATION		



- NOTES**
- FINAL GRADE AND MULCHING SHALL BE DONE BY HAND.
 - NO EQUIPMENT WILL BE ALLOWED ON THE RAIN GARDEN AFTER EXCAVATION BEGINS.
 - PERIMETER EROSION CONTROL SHALL BE INSTALLED AND REMAIN IN PLACE UNTIL TURF IS ESTABLISHED AROUND RAIN GARDEN.
 - OWNER IS RESPONSIBLE FOR NOTIFYING ENGINEER FOR INSPECTION OF RAIN GARDEN FOR
 - FINALIZING RAIN GARDEN SIZE AND LOCATION.
 - OBSERVATION OF EXCAVATION AND SCARIFYING OF SUBSOIL.
 - APPROVAL TO BACKFILL WITH ENGINEERED SOILS.
 - FINAL INSPECTION WITH MULCH AND PLANTS INSTALLED.

- GENERAL NOTES:**
- THE GRADING CONTRACTOR IS RESPONSIBLE FOR ALL STORM WATER INSPECTIONS ACCORDING TO THE MPCA STORM WATER PERMIT. THIS INCLUDES BOTH WEEKLY INSPECTIONS AND INSPECTIONS DONE AFTER A 0.5" RAIN EVENT. A COPY OF THE INSPECTION REPORT MUST BE EMAILED TO THE ENGINEER AND DEVELOPER ON A WEEKLY BASIS.
 - THE CONTRACTOR SHALL PLACE INLET PROTECTION DEVICES IN ACCORDANCE WITH THE CITY OF MINNETONKA DETAIL. FOR ALL STORM SEWER INLETS AND MAINTAIN THEM AS AN EFFECTIVE SILT CONTROL DEVICE. INLET PROTECTION SHALL BE REMOVED WHEN RESTORATION HAS BEEN ESTABLISHED.
 - ALL RETAINING WALLS WILL REQUIRE A STRUCTURAL DESIGN, A BUILDING PERMIT & A FINAL INSPECTION REPORT.
 - A 1'-2" CRUSHED ROCK ENTRANCE BERM SHALL BE PLACED AT THE SITE ENTRANCE, TO REPLACE SILT FENCE, AND MINIMIZE EROSION ON TO THE STREETS. THE ROCK BERMS SHALL BE THE WIDTH OF THE ENTRANCE AND 2 FEET HIGH WITH 4:1 SLOPES. (SEE DETAIL).
 - THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AWAY FROM THE BUILDING PAD AND STREET AREAS THROUGHOUT CONSTRUCTION.
 - THE CONTRACTOR SHALL ATTEMPT TO PREVENT SOIL MATERIALS FROM LEAVING THE SITE BY EROSION AND VEHICLE WHEEL TRACKING. HE SHALL BE RESPONSIBLE FOR CLEANING OF STREET, BOULEVARD AND UTILITY FACILITIES THAT RECEIVE ANY ERODED OR TRACKED SOIL MATERIAL OR OTHER CONSTRUCTION DEBRIS OR MATERIAL.

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- BUILDING PADS ARE 60" DEEP, UNLESS OTHERWISE NOTED. THE FRONT AND REAR BUILDING PAD LINES ARE SHOWN ON THE PLAN. THE ENGINEER SHOULD BE CONTACTED IF THE CONTRACTOR HAS ANY QUESTIONS REGARDING BUILDING PADS. REAR PAD ELEVATIONS SHOULD BE ESTABLISHED BASED OFF THE 2:1 SLOPE FROM THE 26" FLAT BENCH, OR AT MOST 50' BEHIND FRONT OF PAD.
- STAKING OFF AND MARKING OF PROPOSED INFILTRATION FACILITIES TO PREVENT SOIL COMPACTION BY HEAVY EQUIPMENT, STOCKPILING OF MATERIALS, AND TRAFFIC. IF INFILTRATION FACILITIES ARE IN PLACE DURING CONSTRUCTION ACTIVITIES, BEST PRACTICES MUST BE DEPLOYED TO PREVENT SEDIMENT AND OTHER MATERIAL FROM ENTERING THE PRACTICE(S). INFILTRATION FACILITIES MUST NOT BE EXCAVATED TO WITHIN 3FT OF FINAL GRADE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN CONSTRUCTED AND FULLY STABILIZED. ANY ACCUMULATED SEDIMENT IN AN INFILTRATION FACILITY MUST BE REMOVED IN A MANNER THAT PREVENTS COMPACTION OF THE FACILITY BOTTOM. TO PROVIDE A WELL-AERATED, HIGHLY POROUS SURFACE, THE SOILS BELOW AN INFILTRATION PRACTICE MUST BE LOOSENEED TO A MINIMUM OF 18 INCHES PRIOR TO INSTALLATION OR PLANTING.
- ALL DISTURBED AREAS MUST BE STABILIZED WITHIN 7 CALENDAR DAYS AFTER LAND-DISTURBING WORK HAS TEMPORARILY OR PERMANENTLY CEASED ON A PROPERTY THAT DRAINS TO AN IMPAIRED WATER, WITHIN 14 DAYS OTHERWISE.
- CONSTRUCTION SHOULD INCLUDE MINIMIZATION OF THE DISTURBANCE INTENSITY AND DURATION, INCLUDING PHASING OF DISTURBANCE TO MINIMIZE QUANTITY OF DISTURBED AREA AT ANY ONE TIME.
- SOILS SURFACES COMPACTED DURING CONSTRUCTION & REMAINING PERVIOUS UPON COMPLETION OF CONSTRUCTION MUST BE DECOMPACTED TO ACHIEVE:
 - A SOIL COMPACTION TESTING PRESSURE OF LESS THAN 1,400KPA OR 200PSI IN THE UPPER 12" OF SOIL OR;
 - A BULK DENSITY OF LESS THAN 1.4G PER CC OR 87 LB/FT³ IN THE UPPER 12" OF SOIL
 IN ADDITION, UTILITIES, TREE ROOTS, AND OTHER EXISTING VEGETATION MUST BE PROTECTED UNTIL FINAL REVEGETATION OR OTHER STABILIZATION OF THE SITE.
- PROTECTION OF NATURAL TOPOGRAPHY AND SOILS CONDITIONS, INCLUDED RETENTION ON SITE OF NATIVE TOPSOIL TO THE GREATEST EXTENT POSSIBLE PER RULE C SUBSECTION 3.1A)
- THE PERMITTEE WILL INSPECT ALL EROSION PREVENTION AND SEDIMENT CONTROL FACILITIES AND SOIL STABILIZATION MEASURES TO ENSURE INTEGRITY & EFFECTIVENESS. THE PERMITTEE WILL REPAIR, REPLACE OR SUPPLEMENT ALL NON FUNCTIONAL BMPs WITH FUNCTIONAL BMPs WITHIN 48HRS OF DISCOVERY & PRIOR TO THE NEXT PRECIPITATION EVENT UNLESS ADVERSE CONDITIONS PRECLUDE ACCESS TO THE RELEVANT AREA OF THE SITE. IN WHICH CASE THE REPAIR MUST BE COMPLETED AS SOON AS CONDITIONS ALLOW. WHEN ACTIVE LAND-DISTURBING ACTIVITIES ARE NOT UNDERWAY THE PERMITTEE WILL PERFORM THESE RESPONSIBILITIES AT LEAST WEEKLY UNTIL VEGETATIVE COVER IS ESTABLISHED. THE PERMITTEE WILL MAINTAIN A LOG OF ACTIVITIES UNDER THIS SECTION FOR INSPECTION BY THE DISTRICT ON REQUEST.

ENGINEERED SOIL
 TOP 6" OF RAING GARDEN SHALL BE ENGINEERED SOILS. USE ON SITE SOILS FOR THE REMAINING 12" PROVIDED THAT SAND SEEM IS INTERSECTED WHEN BASIN IS CONSTRUCTED. SHOULD SAND SEEM NOT BE FOUND THEN THE ENGINEERED SOILS LISTED BELOW SHALL BE USED.
 (DCSWCD MIX B)
 80% COARSE-WASHED SAND (MNDOT 3126)
 20% LEAF-LITTER COMPOST (ORGANIC, GRADE 2, MNDOT 3890) NO TOPSOIL OR ON-SITE SOILS MAY BE USED IN ENGINEERED SOIL MIX UNLESS APPROVED BY THE ENGINEER. 3 RING INFILTRATOR TESTING AND INFILTRATION TEST ON ENGINEERED SOILS MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

SUBSOIL TYPE	SUBSOIL TYPE	RECOM. DEPTH
A	SAND/GRAVEL	12"-16"
B	SAND WITH CLAY OR SILT	9"-12"
C	CLAY OR SILT WITH SAND	6"-9"
D	CLAYS OR SILTS	6"

NOTE:
 RAIN GARDEN VEGETATION SHALL BE GARDEN VARIETY PERENNIALS, SHRUBS, OR NATIVE PLANTS FROM A CONTAINER (PLUG TO HALF GALLON SIZE) PLACED ACCORDING TO RECOMMENDED PLANT SPACING REQUIREMENTS OR AS APPROVED BY THE ENGINEER.

- Construction Sequencing**
- Delineate the location of areas not to be disturbed before work begins.
 - Establish sediment control practices on all down gradient perimeters before any up gradient land disturbing activities begin. These practices shall remain in place until final stabilization has been established.
 - Install all perimeter sediment control devices and construction entrances. The timing of installation of sediment control practices may be adjusted in order to accommodate short-term activities, but sediment control practices must be installed before the next precipitation event even if the short-term activity is not complete.
 - Contact the City for approval of the sediment control devices.
 - Rough grade the site.
 - Install utilities.
 - Install pavements.
 - Install lawn and landscape & soil amendments.
 - Restore all disturbed areas.
 - Clean all storm sewer and conveyance systems.
 - After all disturbed areas are stabilized, obtain approval from the City and/or Watershed District.
 - Remove all temporary sediment control devices.

- CONSTRUCTION NOTES**
- INSTALL SILT FENCE AS SHOWN ON PLAN, AS REQUIRED BY THE CITY OF MINNETONKA OR DIRECTED BY THE ENGINEER.
 - INSPECT SILT FENCE, AND ROCK ENTRANCE BERM AFTER ALL RAINFALL EVENTS AS REQUIRED BY THE NPDES PERMIT.
 - WO PADS 3:1 MAX. ALL OTHER SLOPES 4:1 MAX (UNLESS NOTED)
 - RESTORATION - 1.3 ACRES
 - RESTORE ALL DISTURBED AREAS WITH AMENDED SOILS. AMENDED SOILS SHALL CONSIST OF 2" OF COMPOST FILLED INTO 6" OF TOP SOIL. COMPOST SHALL MEET MNDOT SPEC 3890 1B GRADE 1. SOIL AMENDMENTS SHALL BE COMPLETED AFTER CONSTRUCTION IS COMPLETE TO AVOID COMPACTION. (APPROXIMATELY 35,791 SF).
 - SEED POND SLOPES AND DETENTION AREAS WITH MNDOT 310 OR BWSR P8 SEED MIX AT A RATE OF 100 LBS./ACRE AND FERTILIZE WITH 20-0-10 AT 100 LBS./ACRE. SEED WETLAND BUFFER AREAS WITH MNDOT 350-MESIC PRAIRIE (36.5 PLS LBS/AC) OR BWSR 35-241 SEED MIX AND FERTILIZE WITH 20-0-10 AT 100 LBS./ACRE. (REFER TO WETLAND CREATION/BANKING PLAN FOR WETLAND SEED MIX REQUIREMENTS).
 - SEED ALL OTHER DISTURBED AREAS WITH RESIDENTIAL TURN 270 AT A RATE OF 100 LBS./ACRE AND FERTILIZE WITH 20-0-10 AT 100 LBS./ACRE. (UNLESS OTHERWISE NOTED)
 - ONLY PHOSPHOROUS FREE FERTILIZER IS TO BE USED ON SITE.
 - MULCH WITH TYPE 1 AT A RATE OF 2 TONS/ACRE AND DISC ANCHOR IMMEDIATELY AFTER PLACEMENT. USE WOODFIBER BLANKET ON ALL SLOPES 3:1 (FT) OR GREATER.
 - MAINTAIN ALL SILT FENCE UNTIL TURF HAS BEEN ESTABLISHED.
 - RESTORATION WORK WILL BE COMPLETED WITHIN 72 HOURS OF GRADING COMPLETION.
 - ALL WETLAND BUFFERS AND FULL CONSERVATION EASEMENT SHALL BE FULLY ESTABLISHED IN NATIVE VEGETATION BY A QUALIFIED RESTORATION COMPANY. NO TURF OR LAWN MAINTENANCE ACTIVITIES ARE ALLOWED WITHIN THE BUFFER AREAS. ANY EROSION BLANKET INSIDE THE CONSERVATION EASEMENT MUST BE FULLY BIODEGRADABLE SUCH AS S318D OR S328D.
 - GRADE BACK 1:1 - 3' FROM FACE OF ALL RETAINING WALLS
 - SILT FENCE - BEFORE GRADING - 1,500 LF
AFTER GRADING - 305 LF
 - TOPSOIL SHALL BE STOCKPILED AND SAVED FOR RESPREAD AFTER CONSTRUCTION IS COMPLETE. ALL DISTURBED AREAS SHALL BE RESTORED WITH TOPSOIL MEETING RPBCWD'S DEFINITION (INCLUDING AT LEAST 5% ORGANIC MATERIAL).
 - DRAIN ALL ROOF WATER TO RAIN GARDENS VIA GUTTERS & DOWNSPOUTS OR SIMILAR METHOD TO MAXIMIZE HARD COVER RUNOFF TO RAIN GARDENS.

DRAWING NAME	NO.	BY	DATE	REVISION
SPARRROW	1			
DRAWN				
CAW				
CHECKED				
CAW				
DATE				
XXXXXX				

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I HEREBY CERTIFY THAT THIS PLAN OR SPECIFICATION WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Charles A. Wiemerslage
 CHARLES A. WIEMERSLAGE, P.E.
 Date: 09/01/23 Lic. No. 49180

ENGINEERS SURVEYORS DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
 14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

CITY PROJECT NO. ---

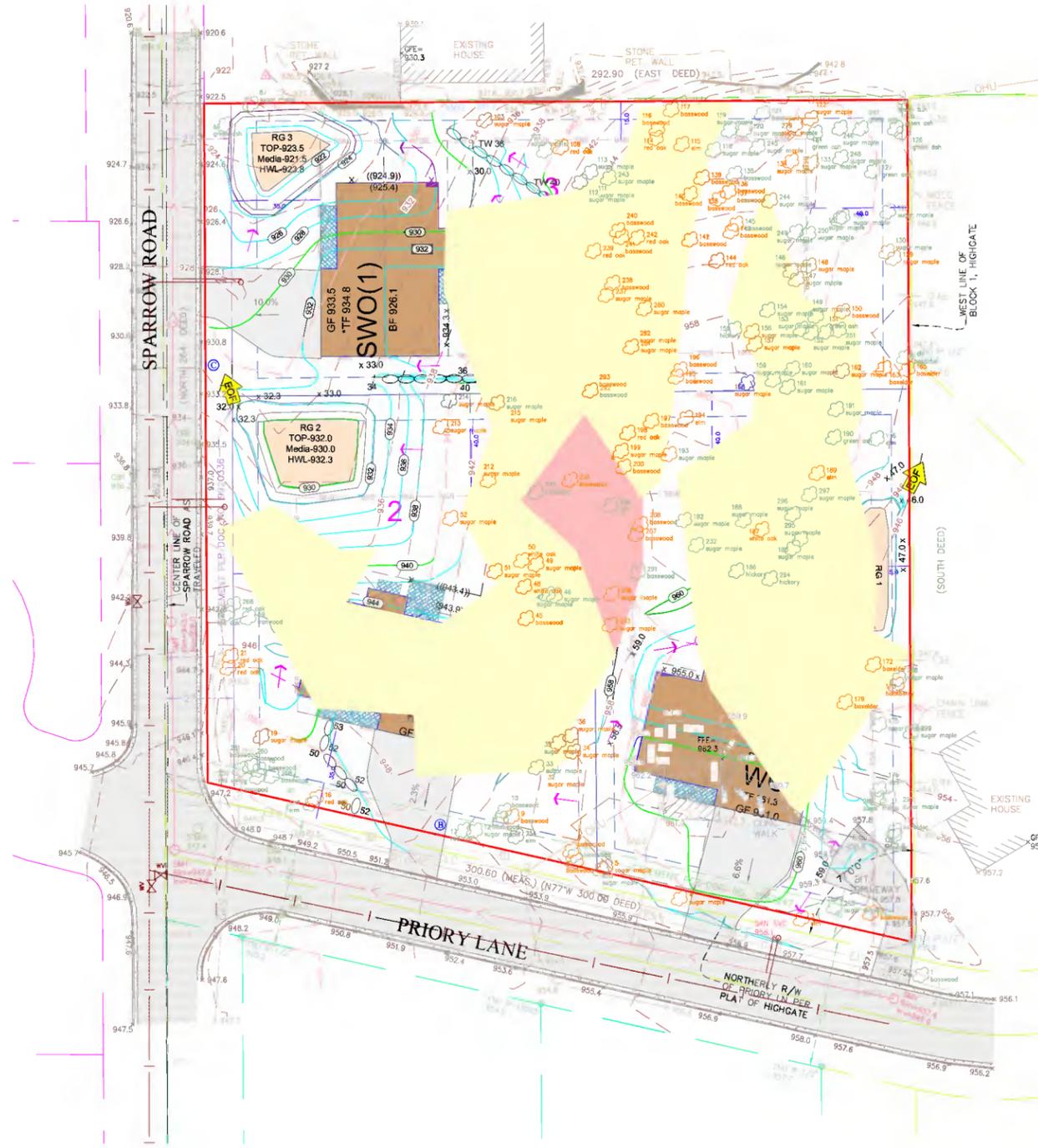
GRADING PLAN
PRIORY WOODS
ZEHNDER HOMES, INC.

MINNETONKA, MINNESOTA

FILE NO. 99595-151

03

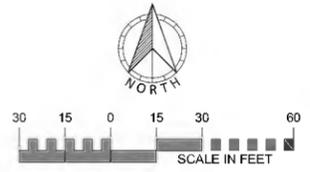
09



20% SLOPE AREA

30% SLOPE AREA

SYMBOL LEGEND		
DESCRIPTION	PROPOSED	EXISTING
MINOR CONTOUR		
MAJOR CONTOUR		
LOT LINE		
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
BACKYARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WRAP-RAP		
SANITARY SEWER MANHOLE		
HYDRANT		
GATE VALVE		
DRAIN FLOW RUNOFF ARROW		
EMERGENCY OVERFLOW SWALE		
SILT FENCE - PRE CONSTRUCTION		
SILT FENCE - POST CONSTRUCTION		
SPOT ELEVATION		
T&G SPOT ELEVATION		



EXISTING UTILITIES SHOWN ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ANY AND ALL EXISTING UTILITIES BEFORE COMMENCING WORK. HE AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES ARISING OUT OF HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL EXISTING UTILITIES.

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CAW				
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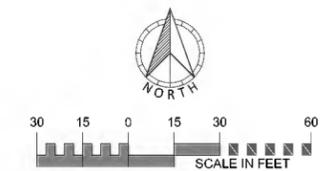
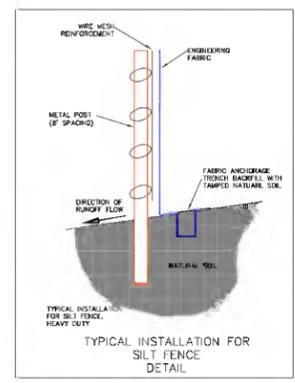
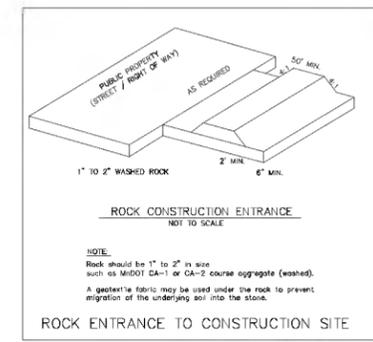
MINNETONKA, MINNESOTA

STEEP SLOPE PLAN
 PRIORY WOODS
 ZEHNDER HOMES, INC.

FILE NO. 99595-151

04

09



- ROCK ENTRANCE BERM
- SILT FENCE
- POST GRADING SILT FENCE
- BIO-ROLL
- ROCK DITCH CHECK
- CONCRETE WASHOUT
- INLET PROTECTION
- EROSION BLANKET

SYMBOL LEGEND		
DESCRIPTION	PROPOSED	EXISTING
MINOR CONTOUR		
MAJOR CONTOUR		
LOT LINE		
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
BACKWARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WRAP		
SANITARY SEWER MANHOLE		
HYDRANT		
GATE VALVE		
DRAIN FLOW RUNOFF ARROW		
EMERGENCY OVERTFLOW SWALE		
SILT FENCE (PRE CONSTRUCTION)		
SILT FENCE (POST CONSTRUCTION)		
SPOT ELEVATION		
TBC SPOT ELEVATION		

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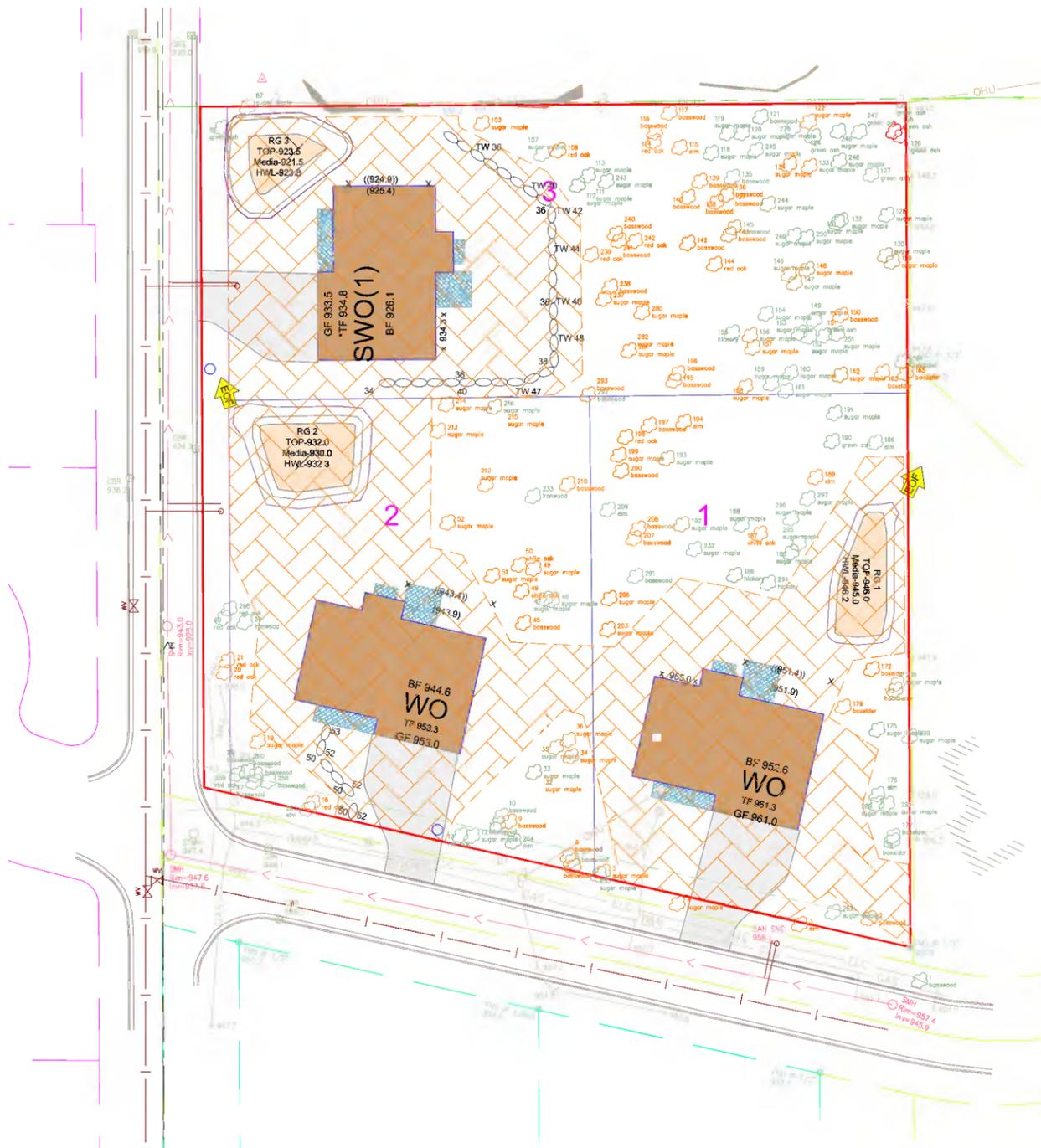
CITY PROJECT NO.

 MINNETONKA,
 MINNESOTA

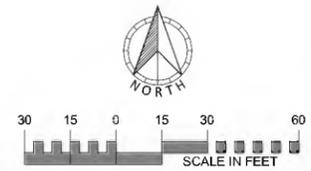
EROSION CONTROL PLAN
PRIORY WOODS
ZEHNDER HOMES, INC.

FILE NO.
 99595-151
05
09

SYMBOL LEGEND		
DESCRIPTION	PROPOSED	EXISTING
MINOR CONTOUR		
MAJOR CONTOUR		
LOT LINE		
BUILDING SETBACK LINE		
GARAGE SETBACK LINE		
PARCEL BOUNDARY LINE		
DRAINAGE AND UTILITY EASEMENTS		
CURB AND GUTTER		
RIGHT-OF-WAY		
BACKYARD CATCH BASIN		
CATCH BASIN		
STORM SEWER MANHOLE		
FLARED END SECTION WHIP/KIP		
SANITARY SEWER MANHOLE		
HYDRANT		
GATE VALVE		
DRAIN FLOW/RUNOFF ARROW		
EMERGENCY OVERTFLOW SWALE		
SILT FENCE (PRE CONSTRUCTION)		
SILT FENCE (POST CONSTRUCTION)		
SPOT ELEVATION		
TBC SPOT ELEVATION		



- SOIL AMENDMENT NOTES**
- RESTORE ALL DISTURBED AREAS SHALL BE RESTORED WITH AMENDED SOILS. AMENDED SOILS SHALL CONSIST OF 2" OF COMPOST TILLED INTO 6" OF TOP SOIL. COMPOST SHALL MEET MNDOT SPEC 3890 1B GRADE 1. SOIL AMENDMENTS SHALL BE COMPLETED AFTER CONSTRUCTION IS COMPLETE TO AVOID COMPACTION.
 - SEED ALL AMENDED AREAS WITH RESIDENTIAL TURN 270 AT A RATE OF 100 LBS./ACRE AND FERTILIZE WITH 20-0-10 AT 100 LBS./ACRE. (UNLESS OTHERWISE NOTED)
 - ONLY PHOSPHOROUS FREE FERTILIZER TO BE USE DON SITE.



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ENGINEERS SURVEYORS
 DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
 14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

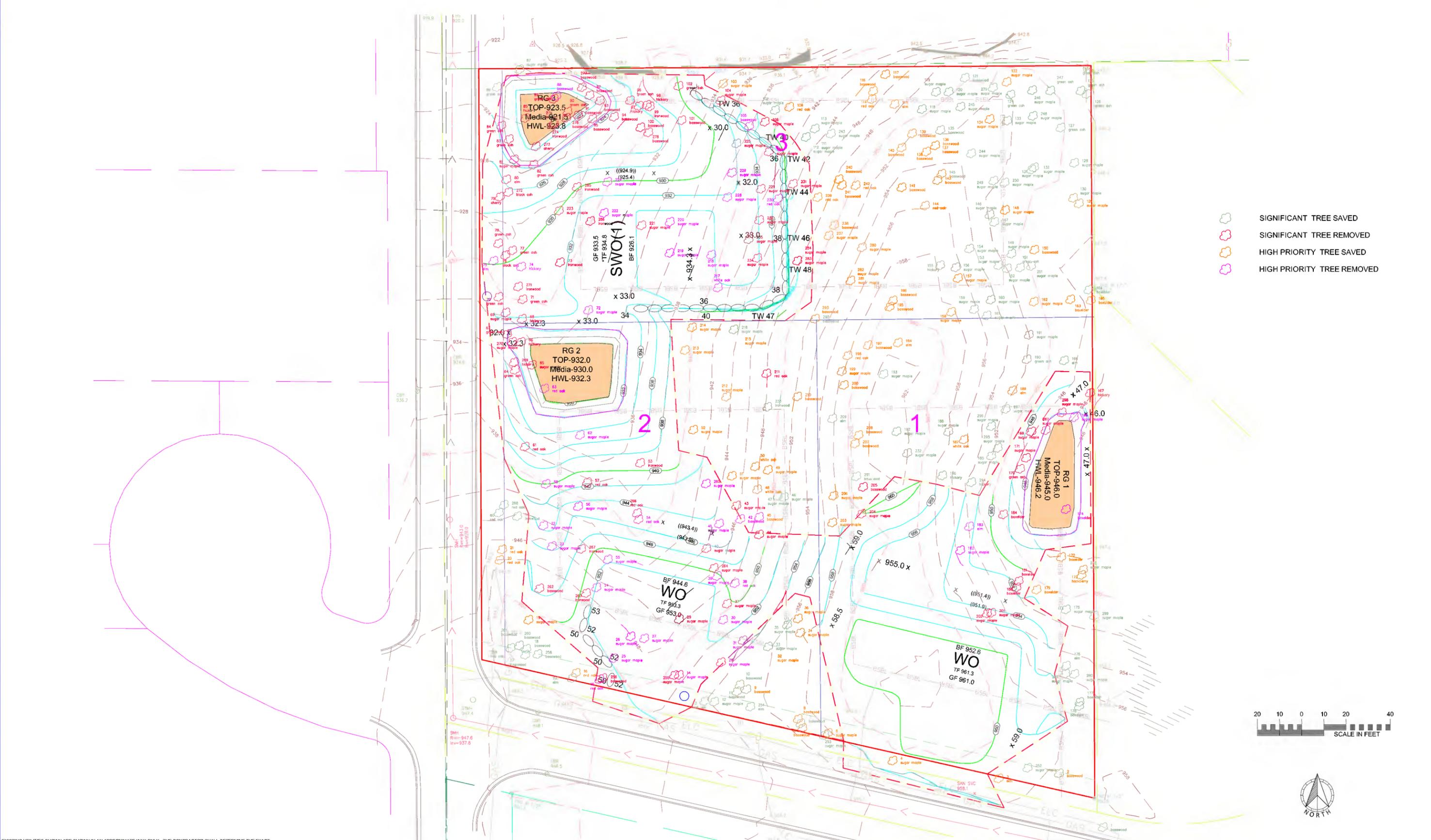
CITY PROJECT NO.

MINNETONKA,
 MINNESOTA

AMENDED SOILS PLAN
 PRIORY WOODS
 ZEHNDER HOMES, INC.

FILE NO.
 99595-151

06
 09



-  SIGNIFICANT TREE SAVED
-  SIGNIFICANT TREE REMOVED
-  HIGH PRIORITY TREE SAVED
-  HIGH PRIORITY TREE REMOVED

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 Date: 09/01/13 Lic. No. 49180

ENGINEERS SURVEYORS
 DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
 14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

CITY PROJECT NO.

 MINNETONKA,
 MINNESOTA

TREE SURVEY
PRIORY WOODS
ZEHNDER HOMES, INC.

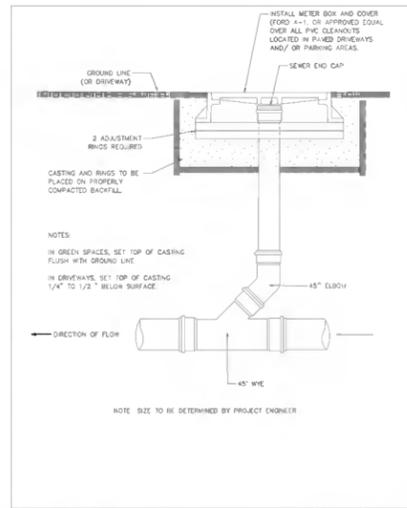
FILE NO.
 99595-151
 07
 09

Tree #	Species	Notes	DSH	HP Save	HP Remove	Sig Save	Sig Remove
1	basswood		29				
2	basswood	Cavity	27	1			
3	elm		24	1			
4	sugar maple		31	1			
5	sugar maple		12	1			
6	basswood		20	1			
7	basswood		6			1	
8	basswood		13	1			
9	basswood		19	1			
10	basswood		7.5			1	
11	basswood		7.5				1
12	sugar maple	Cavity	18				
13	red oak	No top	18				
14	sugar maple		14		1		
15	red oak		24		1		
16	red oak		24	1			
17	basswood		7			1	
18	basswood		8			1	
19	sugar maple		22	1			
20	red oak		16	1			
21	red oak		25	1			
22	sugar maple		15		1		
23	sugar maple		16		1		
24	sugar maple		14		1		
25	sugar maple		13		1		
26	sugar maple		14		1		
27	sugar maple		19		1		
28	sugar maple		15		1		
29	sugar maple		7				1
30	sugar maple		13		1		
31	sugar maple		12		1		
32	sugar maple		14	1			
33	sugar maple		7			1	
34	sugar maple		13	1			
35	sugar maple		7			1	
36	sugar maple		14	1			
37	sugar maple		9				1
38	red oak		20		1		
39	sugar maple		15.5		1		
40	sugar maple		9				1
41	sugar maple		16		1		
42	basswood		20		1		
43	sugar maple		9				1
44	sugar maple		9				1
45	basswood		17	1			
46	sugar maple		9			1	
47	sugar maple		8			1	
48	white oak		11	1			
49	sugar maple		16	1			
50	white oak		21	1			
51	sugar maple		11	1			
52	sugar maple		20	1			
53	ironwood		6				1
54	red oak		11		1		
55	sugar maple		12.5		1		
56	sugar maple		15		1		
57	red oak		8				1
58	sugar maple		12		1		
59	ironwood		6			1	
60	red oak		7			1	
61	red oak		9				1
62	sugar maple		29		1		
63	red oak		20		1		
64	green ash	Not Sig	6				
65	sugar maple		6				1
66	hickory		7				1
67	green ash	Not Sig	9				
68	hickory		7				1
69	sugar maple		7				1
70	green ash	Not Sig	11				
71	green ash	Not Sig	10				
72	sugar maple		24		1		
73	ironwood		6				1
74	hickory		10		1		
75	elm		17		1		
76	black ash	Not Sig	8				
77	green ash	Not Sig	10				
78	green ash	Not Sig	7				
79	cherry		7				1
80	elm		8				1

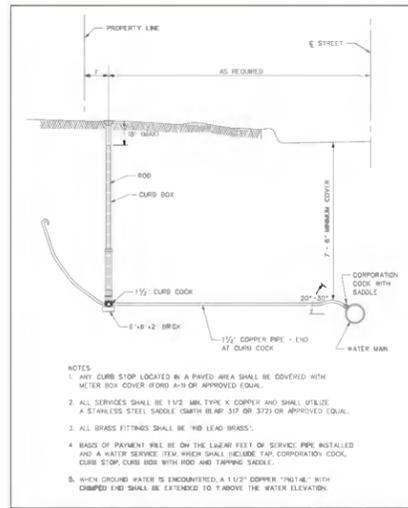
Tree #	Species	Notes	DSH	HP Save	HP Remove	Sig Save	Sig Remove
81	sugar maple		8				1
82	green ash	Dead	17				
83	green ash	Not Sig	7				
84	green ash	Not Sig	8				
85	hickory		9				1
86	green ash	Not Sig	11				
87	sugar maple		8			1	
88	basswood		11		1		
89	green ash	Not Sig	6				
90	green ash	Not Sig	8				
91	basswood		9				1
92	basswood		8			1	
93	basswood		8			1	
94	basswood		7			1	
95	basswood		8			1	
96	green ash	Not Sig	8				
97	hickory		6				1
98	hickory		8				1
99	ironwood		6				1
100	basswood		9				1
101	basswood		9				1
102	green ash	Not Sig	7				
103	sugar maple		15	1			
104	sugar maple	Canker	10				
105	basswood		10		1		
106	sugar maple		7				1
107	sugar maple		7				1
108	red oak		20	1			
109	red oak		17	1			
110	sugar maple		6				1
111	sugar maple		7			1	
112	sugar maple		9			1	
113	sugar maple		9			1	
114	red oak		19	1			
115	elm		14	1			
116	basswood		13	1			
117	basswood		17	1			
118	sugar maple		7				1
119	sugar maple		7				1
120	sugar maple		7				1
121	basswood	Dead	18				
122	sugar maple		10	1			
123	green ash	Not Sig	11				
124	green ash	Not Sig	14				
125	green ash	Not Sig	14				
126	green ash	Not Sig	15				
127	green ash	Dead	10				
128	sugar maple		9				1
129	sugar maple		11	1			
130	sugar maple		9				1
131	sugar maple	split	11				
132	sugar maple		9				1
133	sugar maple		8				1
134	sugar maple		12	1			
135	basswood	Rotten	25				
136	basswood		13	1			
137	basswood		17	1			
138	basswood		16	1			
139	basswood		24	1			
140	basswood		11	1			
141	basswood		11	1			
142	basswood		21	1			
143	basswood		18	1			
144	red oak		26	1			
145	basswood		9				1
146	sugar maple		8				1
147	sugar maple		9				1
148	sugar maple		12	1			
149	sugar maple		9				1
150	basswood		17	1			
151	green ash	Not Sig	18				
152	sugar maple		6				1
153	sugar maple		6				1
154	sugar maple		7				1
155	hickory		8				1
156	sugar maple		8				1
157	sugar maple		11	1			
158	sugar maple		10	1			
159	sugar maple		7				1
160	sugar maple		9				1
161	sugar maple		6				1

Tree #	Species	Notes	DSH	HP Save	HP Remove	Sig Save	Sig Remove
162	sugar maple		10	1			
163	boxelder		12	1			
164	boxelder		7			1	
165	boxelder		17	1			
166	elm		7			1	
167	hickory		9				1
168	sugar maple		13		1		
169	sugar maple		9				1
170	green ash	EAB	15				
171	sugar maple		7				1
172	boxelder		21	1			
173	hackberry		25	1			
174	boxelder		12		1		
175	sugar maple		6			1	
176	elm		9			1	
177	boxelder		6			1	
178	boxelder		8			1	
179	boxelder		10	1			
180	boxelder		8				1
181	boxelder		8				1
182	sugar maple		10		1		
183	elm		28		1		
184	boxelder		8				1
185	sugar maple		8			1	
186	hickory		9			1	
187	white oak		33	1			
188	sugar maple		9				1
189	elm		14	1			
190	green ash	Not Sig	17.5				
191	sugar maple		7				1
192	sugar maple		8				1
193	sugar maple		6				1
194	elm		17	1			
195	basswood		12	1			
196	basswood		25	1			
197	basswood		18	1			
198	red oak		22	1			
199	sugar maple		15	1			
200	basswood		13	1			
201	sugar maple		8				1
202	sugar maple		9				1
203	sugar maple		11	1			
204	sugar maple	Damage	18				
205	basswood	Damage	14				
206	sugar maple		20	1			
207	basswood		13	1			
208	basswood		20	1			
209	elm		8				1
210	basswood		20	1			
211	red oak	Dead	22				
212	sugar maple		14	1			
213	sugar maple		20	1			
214	sugar maple		17	1			
215	sugar maple		17	1			
216	sugar maple		9				1
217	white oak		13			1	
218	sugar maple		17			1	
219	sugar maple		19			1	
220	sugar maple		16			1	
221	sugar maple		9				1
222	sugar maple		17			1	
223	sugar maple	dead	29				
224	sugar maple		24			1	
225	sugar maple		9				1
226	sugar maple		16			1	
227	sugar maple		12			1	
228	sugar maple		12			1	
229	sugar maple		9				1
230	red oak		21			1	
231	sugar maple		7				1
232	sugar maple		6				1
233	ironwood		6				1
234	sugar maple		8				1
235	sugar maple		7				1
236	sugar maple		9				1
237	sugar maple		16	1			
238	basswood		10	1			
239	red oak		19	1			
240	basswood		14	1			
241	basswood		16	1			
242	red oak		20	1			

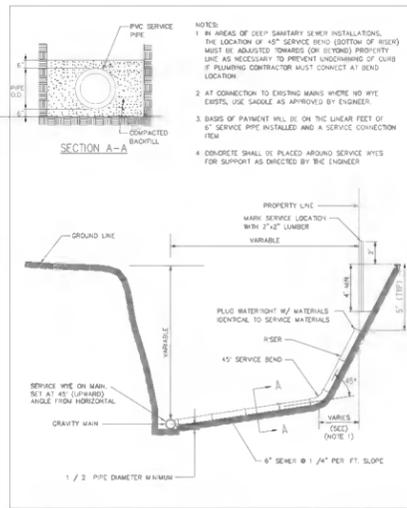
Tree #	Species	Notes	DSH	HP Save	HP Remove	Sig Save	Sig Remove
243	sugar maple		5				1
244	sugar maple		6				1
245	sugar maple		6				1
246	sugar maple		6				1
247	green ash	Not Sig	6				



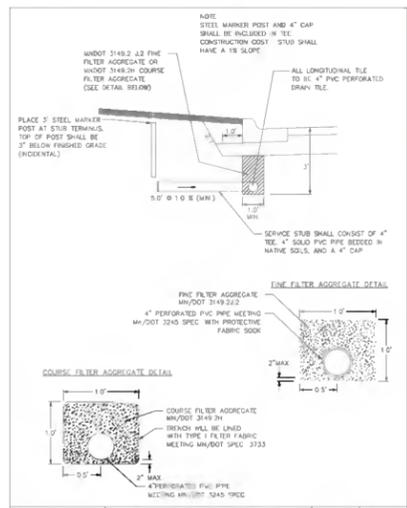
City of minnetonka
SANITARY SEWER SERVICE CLEANOUT
 SCALE: NOT TO SCALE
 STD. PLATE NO. 409
 APPROVED: 5/2000
 REVISED: 1/2017



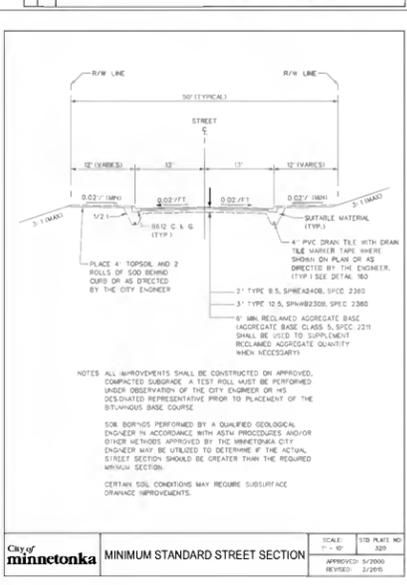
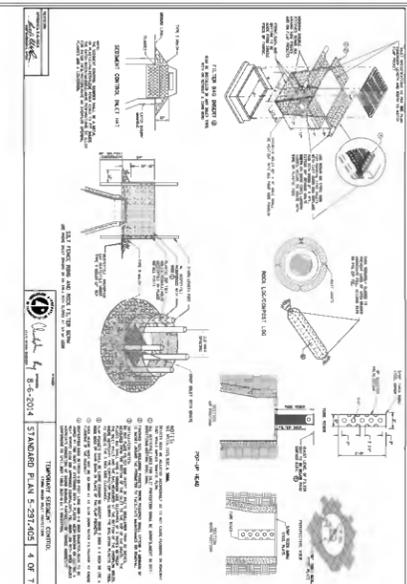
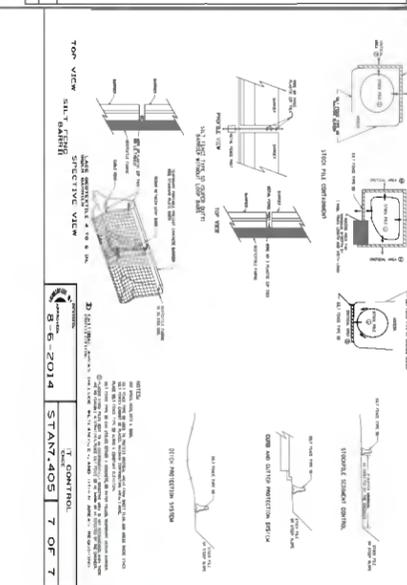
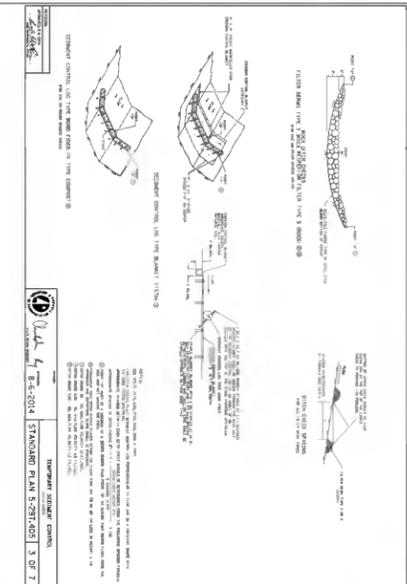
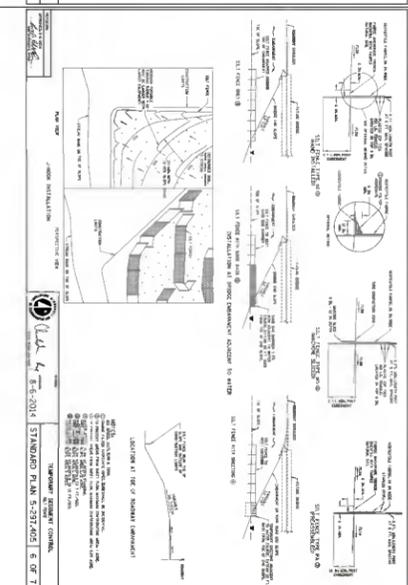
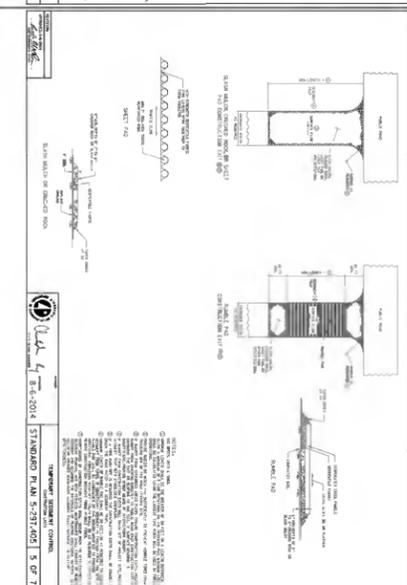
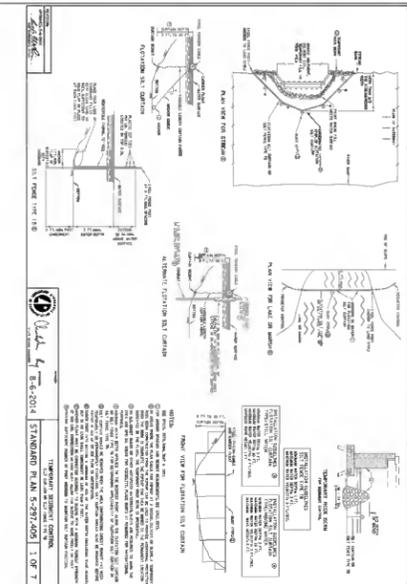
City of minnetonka
WATER SERVICE CONNECTION
 SCALE: NOT TO SCALE
 STD. PLATE NO. 488
 APPROVED: 5/2000
 REVISED: 3/2015



City of minnetonka
SANITARY SEWER SERVICE CONNECTION
 SCALE: NOT TO SCALE
 STD. PLATE NO. 488
 APPROVED: 4/2000
 REVISED: 1/2017



City of minnetonka
DRAIN TILE INSTALLATION (CONCRETE CURB)
 SCALE: NOT TO SCALE
 STD. PLATE NO. 180
 APPROVED: 2/2007
 REVISED: 1/2017



City of minnetonka
MINIMUM STANDARD STREET SECTION
 SCALE: 1" = 10'
 STD. PLATE NO. 329
 APPROVED: 5/2000
 REVISED: 3/2015

EXISTING UTILITIES SHOWN ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ANY AND ALL EXISTING UTILITIES BEFORE COMMENCING WORK. HE AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES ARISING OUT OF HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL EXISTING UTILITIES.

DRAWING NAME	NO.	BY	DATE	REVISION
SPARROW	1			
DRAWN		CAW		
CHECKED		CAW		
DATE				
XXXXXX				

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I HEREBY CERTIFY THAT THIS PLAN OR SPECIFICATION WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Charles A. Wiemerslage
 CHARLES A. WIEMERSLAGE, P.E.
 Date: 09/01/23 Lic. No. 49180

ENGINEERS SURVEYORS
DESIGNERS PLANNERS

SATHRE-BERGQUIST, INC.
 14000 25TH AVE N #120 PLYMOUTH, MN. 55447 (952) 476-6000

CITY PROJECT NO. ---
MINNETONKA, MINNESOTA

CITY DETAILS
PRIORY WOODS
ZEHNDER HOMES, INC.

FILE NO.
 99595-151
09
09

FW: *NEW SUBMISSION* Planning commission and EDAC email comments

Thursday, September 28, 2023 9:03 AM

From: City of Minnetonka, MN <minnetonkamn@minnetonkamn.gov>
Sent: Thursday, September 28, 2023 8:48 AM
To: Karen Telega <ktelega@minnetonkamn.gov>; Zoning Requests <zoning@minnetonkamn.gov>
Subject: *NEW SUBMISSION* Planning commission and EDAC email comments

Planning commission and EDAC email comments

Submission #: 2729325
IP Address: [REDACTED]
Submission Date: 09/28/2023 8:47
Survey Time: 4 minutes, 2 seconds

You have a new online form submission.

Note: all answers displaying "*****" are marked as sensitive and must be viewed after your login.

Read-Only Content

Name

Stephanie Kwong

Full Address

18404 Beaverwood Hill
Minnetonka, Minnesota 55345
United States

Phone

[REDACTED]

Email

[REDACTED]

Planning commission or EDAC

Planning commission

Meeting date

October 26, 2023

Agenda item

Priory Woods proposed development

Comment

Over the past few years, the planning commission has agreed to deforest my neighborhood and it's exceptionally disappointing. We chose this neighborhood because it has woods, is not dense in housing and has wildlife including owls, deer, turkeys, foxes, etc. Lately developers are looking to cram as many houses as they can into the area. It saddens and angers me that projects are continually approved. I plead with you to stop approving the destruction of wildlife habitats and increasing the housing density along Sparrow Road.

Thank you,

City of Minnetonka, MN

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FW: *NEW SUBMISSION* Planning commission and EDAC email comments

Thursday, September 28, 2023 1:15 PM

From: City of Minnetonka, MN <minnetonkamn@minnetonkamn.gov>
Sent: Thursday, September 28, 2023 12:50 PM
To: Karen Telega <ktelega@minnetonkamn.gov>; Zoning Requests <zoning@minnetonkamn.gov>
Subject: *NEW SUBMISSION* Planning commission and EDAC email comments

Planning commission and EDAC email comments

Submission #: 2730111
IP Address: [REDACTED]
Submission Date: 09/28/2023 12:50
Survey Time: 15 minutes, 44 seconds

You have a new online form submission.

Note: all answers displaying "*****" are marked as sensitive and must be viewed after your login.

Read-Only Content

Name

Dori Estrella-Sands

Full Address

18421 Highpath Lane
Minnetonka, MN 55345
US

Phone

Email

[REDACTED]

Planning commission or EDAC

Planning commission

Meeting date

October 26, 2023

Agenda item

Priory Woods 4633 Sparrow Road

Comment

I am deeply disappointed by the City of Minnetonka's willingness to allow for several large development projects to destroy mature forests and natural habitats for wildlife (e.g. deer, turkeys, owls, etc) along Sparrow Road and Ridgewood Road. That it's happening at all is disappointing but the sheer volume of projects at one time is alarming. We moved here for the schools, nature, and related amenities and it's the protection and preservation of nature in our neighborhoods that differentiate Minnetonka from neighboring communities. My family does not support the projects underway, especially the latest Priory Woods project on Sparrow Road. We ask the city to protect our land and wildlife and stop this surge in development activity. Thank you.

Thank you,

City of Minnetonka, MN

This is an automated message generated by Granicus. Please do not reply directly to this email.

FW: *NEW SUBMISSION* Planning commission and EDAC email comments

Monday, October 2, 2023 8:16 AM

From: City of Minnetonka, MN <minnetonkamn@minnetonkamn.gov>
Sent: Sunday, October 1, 2023 2:28 PM
To: Karen Telega <ktelega@minnetonkamn.gov>; Zoning Requests <zoning@minnetonkamn.gov>
Subject: *NEW SUBMISSION* Planning commission and EDAC email comments

Planning commission and EDAC email comments

Submission #: 2735071
IP Address: [REDACTED]
Submission Date: 10/01/2023 2:28
Survey Time: 3 minutes, 45 seconds

You have a new online form submission.

Note: all answers displaying "*****" are marked as sensitive and must be viewed after your login.

Read-Only Content

Name

Justin Kwong

Full Address

18404 Beaverwood Hill
Minnetonka, MN 55345-2300
United States

Phone

[REDACTED]

Email

[REDACTED]

Planning commission or EDAC

Planning commission

Meeting date

10/26/2023

Agenda item

Priory Woods Development

Comment

I am writing to express my vehement opposition to the proposed development on Priory and Sparrow. The plans call for the near-total deforestation of a beautiful patch of woods in our neighborhood in exchange for a few cookie-cutter McMansions. There's more to property values than just being surrounded by giant \$1.3M houses. This gorgeous patch of hardwood forest, mostly sugar maples and oaks, with a few lindens and elms sprinkled in, creates an amazing oasis for wildlife that is increasingly hard to find. It's remarkably beautiful in every season, but in fall it's especially vibrant. Who needs to drive up north, when we've got the full spectrum of colors every time we stroll down the street. I'm aware that the plans allow for tree removal within the zoning limits, but it's clear that there would be nothing natural left when all is said and done. A forest depends on a mix of mature and new

growth to be sustainable over the long term. Clearing a large portion of the trees won't just shrink the forest, it will utterly destroy it. We've seen what happens, literally across the street from the proposed development site, when the natural forest floor is cleared and replaced with turf grass. While clearly not the city's doing, it's abundantly clear to anyone who goes past it what a mistake it was. Half of the trees on the land are dead or dying, with nary a wild creature in sight. When it rains, all the water just pools up in a stagnant ditch. To address this potential in the development, the plans call for not one, not two, but three rain gardens, in anticipation of the volume of runoff clearcutting the forest will create. Just let that sink in for a second. The land has no need for rain gardens now because natural woods handle rainfall just fine. And rain gardens, while lovely, are just window-dressings that require property owner investment and maintenance that can't be guaranteed over the long term, with negative impact on water quality

... following in their wake. We know that in our climate-changed reality, old ideas of what normal rainfall would be are wholly inadequate, which the prospective property owners will discover after a few 4-inch rain events like we had this past week. Our old zoning regs can't possibly keep up. Just because the current zoning rules allow developers to casually wipe out the natural habitats in an area for a tidy profit doesn't mean that we should just go along with it. Protecting wild spaces, even ones just an acre square, can have incredible benefits for wildlife (and people). This neighborhood is home to turkeys, foxes, songbirds, owls, and hawks. Without a place to seek shelter, they'll be pushed out just like in every other overdeveloped community. There are already plans to develop two other wooded areas on this street, but neither of those have such beautiful trees in such an unspoiled setting. If we wanted to live on a street utterly devoid of wildlife, we would have chosen a place on a converted farm field in Plymouth or Eden Prairie. Sprawl begets sprawl and it's unhealthy for everyone and everything, except bankers. I don't object to replacing the dwelling that exists on the parcel now; I recognize that we can't just turn back the clock on the whole neighborhood and revert it back into forest, but I urge you to limit the damage. Someone must draw a line and say that enough is enough. I have photos of the woods and the blighted trees across the street for comparison, since it is likely most people haven't actually set eyes on the spot. I will look for a way to submit them electronically, but what would be better, if you have the time, please come by and spend a few minutes in these gorgeous, precious woods. Smell the air, listen to the wind in the leaves, watch the birds. We have a chance to preserve an oasis of true majestic beauty, or we can shrug our shoulders and say, "well, that's zoning for you." We all know what the right choice is.

Thank you,
City of Minnetonka, MN

This is an automated message generated by Granicus. Please do not reply directly to this email.







RE: Priory Woods Project Questions

Wednesday, October 11, 2023 8:42 AM

Subject	RE: Priory Woods Project Questions
From	Michael Sigel
To	Bria Raines
Sent	Tuesday, October 10, 2023 4:06 PM

Hi Bria,

Sorry for the delay in sending comments.

--

I'd like to comment on the proposed Priory Woods subdivision at 4633 Sparrow Road (Sparrow Road and Priory Lane).

The proposed development is situated within the Highgate Neighborhood which has had a monument sign on the Northeast Corner of Sparrow Road and Priory Lane for the past 25+ years. This sign is quite important to the neighbors of Highgate and we're looking forward to welcoming the new neighbors once development is complete to Highgate.

I'm writing on behalf of the neighborhood to have the Highgate sign remain in its current location. I understand that this parcel is considered private property and is not required to be kept by the property owner. We respectfully request that the developer helps us preserve this important part of neighborhood history. We understand the sign may need to be temporarily taken down or the landscaping around it modified during construction.

Thank you,
Michael

—

Michael Sigel
4702 Parsons Close

Priory Woods development questions

Wednesday, October 18, 2023 9:32 AM

Subject	Priory Woods development questions
From	Scott Hultgren
To	Bria Raines
Sent	Tuesday, October 17, 2023 8:00 PM

Hi-this is Scott Hultgren, resident at 4617 Sparrow Road. I had a few questions/considerations on the development at 4633 Sparrow Road:

- Can the power line running along the property be buried during development?
- Will the buckthorn running along the north edge of the site be removed?
- Could the tree at the NW corner be considered for trimming/removal?

Thanks,
Scott Hultgren
4617 Sparrow Road

Resolution No. 2023-0XX

**Resolution approving the preliminary and final plats of
PRIORY WOODS at 4633 Sparrow Road**

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 The applicant, Zehnder Homes Inc., has requested preliminary and final plat approval for PRIORY WOODS, a three-lot subdivision.

1.02 The property is located at 4633 Sparrow Road. It is legally described as:

That part of the Northwest Quarter of Northeast Quarter of Section 30, Township 117 North, Range 22 West of the Fifth Principal Meridian, described as follows:

Commencing 280.5 feet South of the Northwest corner of the Northwest Quarter of the Northeast Quarter of Section 30, Township 117, Range 22; thence East 292.9 feet; thence South 330 feet; thence North 77 degrees West for a distance of 300 feet; thence North along the center of the road for a distance of 264 feet to the point of beginning, Hennepin County, Minnesota, except road.

1.03 On Oct. 26, 2023, the planning commission held a hearing on the proposed plats. The applicant was provided the opportunity to present information to the commission. The commission considered all of the comments received and the staff report, which are incorporated by reference into this resolution. The commission recommended that the city council grant preliminary and final plat approval.

Section 2. General Standards.

2.01 City Code §400.030 outlines general design requirements for residential subdivisions. These standards are incorporated by reference into this resolution.

Section 3. Findings.

3.01 The preliminary and final plat would meet the design standards as outlined in City

Code §400.030.

Section 4. Council Action.

4.01 The above-described preliminary plat is hereby approved, subject to the following conditions:

1. Prior to the release of the final plat for recording purposes:
 - a) Submit a revised final plat that includes:
 - 1) A minimum 10-foot wide drainage and utility easements adjacent to the public right-of-way(s) and minimum 7-foot wide drainage and utility easements along all other lot lines.
 - Consider reducing proposed ten-foot easements to 7-foot easements in locations where no utilities are proposed. Only a seven-foot wide easement is required when no utilities or pipes are proposed.
 - 2) Provide drainage and utility easements:
 - Over existing or proposed public utilities, as determined by the city engineer.
 - Over wetlands, floodplains, and stormwater management facilities, as determined by the city engineer.
 - Additionally:
 - Retaining walls are not permitted within the right of way or within drainage and utility easements.
 - b) Submit the following documents for the city attorney's review and approval. These documents must be prepared by an attorney knowledgeable in the area of real estate.
 - 1) Title evidence that is current within thirty days before the release of the final plat.
 - c) Submit the following:
 - 1) Two sets of mylars for city signatures.
 - 2) An electronic CAD file of the plat in microstation or DXF.

- 3) Park dedication fee of \$10,000. Based on the city's adopted capital improvement program, there are tennis court improvements needed at Reich Park and 1.7 miles identified of missing trail connections, both within a mile of the subject property. This park dedication fee is the result of the determination that the specific development would directly impact surrounding park and recreational facilities within the community and, therefore, necessitates access, acquisition, and improvements to those facilities.
2. Subject to staff approval, PRIORY WOODS must be developed and maintained in substantial conformance with the following plans, except as modified by the conditions below:
 - Certificate of Survey and Preliminary Plat, dated Aug. 24, 2023
 - Utility Plan, dated Sept. 1, 2023
 - Grading Plan dated Sept. 1, 2023
 3. Prior to the issuance of a building permit for the first new house within the plat, submit the following documents:
 - a) A letter from the surveyor stating that boundary and lot stakes have been installed as required by ordinance.
 - b) Proof of subdivision registration and transfer of NPDES permit, if applicable.
 - c) The final plat must be recorded at Hennepin County.
 - d) The easements and contract outlined in Sec. 4.01(1) of this resolution must be recorded.
 4. Prior to the issuance of a building permit for any of the lots within the development:
 - a) Unless authorized by city staff, no site work – including tree removal – may begin until a complete site development permit application has been submitted, reviewed by staff, and approved.
 - b) Submit the following items for staff review and approval:
 - 1) Final site, grading, drainage, utility, and tree mitigation plans, and a stormwater pollution prevention plan (SWPPP).
 - a. Final grading plan must:

- Be in substantial conformance with the plans outlined in Section 4.01(3) unless modified by Section 4.01(4) of this resolution.
 - Include no grading between Lot 1 and Lot 2 slopes. The proposed grades match the existing, resulting in unnecessary impacts to the slope.
 - Include no earthwork or grading in areas of 30 percent slope or greater.
 - Retaining walls over 4 feet in height require a separate building permit and must be designed by a licensed engineer.
- b. Tree mitigation plan. The plan must meet mitigation requirements as outlined in the ordinance. However, at the sole discretion of staff, mitigation may be decreased.
- c. Final stormwater management plan. The plan must demonstrate conformance with the following criteria:
- Rate. Limit peak runoff flow rates to that of existing conditions from the 2-, 10-, and 100-year events at all points where stormwater leaves the site.
 - Volume. Provide for onsite retention of 1.1 inches of runoff from the entire site's impervious surface.
 - Quality. Provide for all runoff to be treated to at least 60 percent total phosphorus annual removal efficiency and 90 percent total suspended solid annual removal efficiency.
- d. A construction management plan. The plan must be in a city-approved format and must outline minimum site management practices and penalties for non-compliance.
- e. A stormwater maintenance agreement in the city-approved format for review and approval of city staff.

-
- f. Any required administrative or engineering fees.
 - g. Cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document, the builder and property owner will acknowledge:
 - The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.
 - h. A driveway permit application.
- c) Install a temporary rock driveway, erosion control, tree and wetland protection fencing, and any other measures identified on the SWPPP for staff inspection. These items must be maintained throughout the course of construction. Note, staff may require chain link fencing for additional tree protection.
 - d) Submit all required hook-up fees.
6. All lots and structures within the development are subject to all R-1, low-density residential zoning standards. In addition:
- a) All lots within the development must meet all minimum access requirements as outlined in Minnesota State Fire Code Section 503. If access requirements are not met, houses must be protected with a 13D automatic fire sprinkler system or an approved alternative system.
7. Permits may be required from other agencies, including Hennepin County, Minnehaha Creek Watershed District, and the MPCA. It is the applicant's or property owner's responsibility to obtain all necessary permits.
8. During construction, the streets must be kept free of debris and sediment.
9. The property owner is responsible for replacing any required landscaping

that dies.

10. This approval will be void on Nov. 13, 2024, if: (1) the final plat has not been recorded; and (2) the city has not received and approved a written application for a time extension.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Nov. 13, 2023.

Brad Wiersum, Mayor

Attest:

Becky Koosman, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on Nov. 13, 2023.

Becky Koosman, City Clerk

Minnetonka Planning Commission Meeting

Agenda Item 9

Other Business

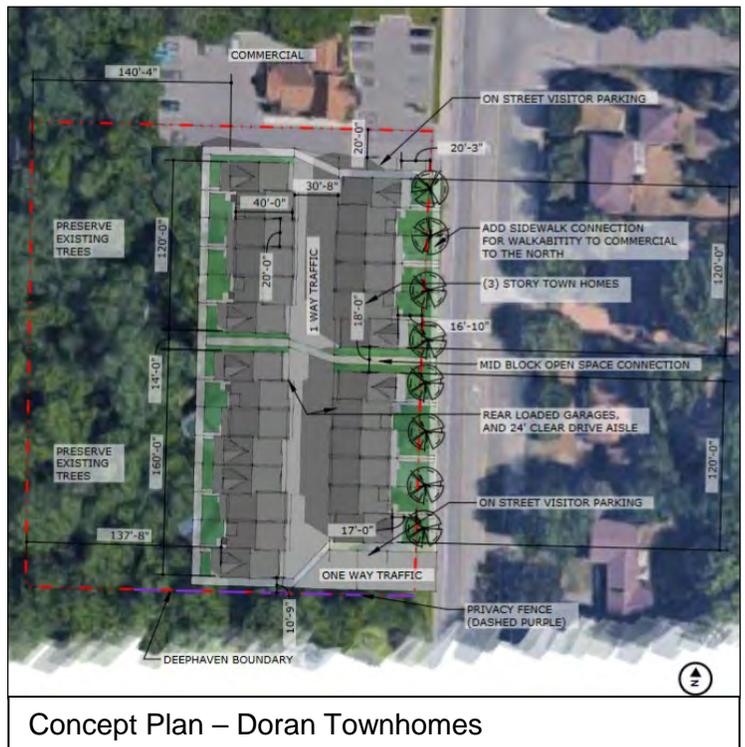
MINNETONKA PLANNING COMMISSION
Oct. 26, 2023

Brief Description	Concept plan for Doran townhome redevelopment at 3514, 3518, and 3522 County Road 101.
Action Requested	Discuss the concept plan with the applicant. No formal action is required.

Concept Plan

Doran Development has submitted a concept plan for the redevelopment of the properties at 3514, 3518, and 3522 County Road 101. The three properties comprise a site that is 2.17 acres in size. The submitted plan contemplates the removal of the existing office building, residential duplex homes and the construction of 26 rental townhome units.

The proposed concept plan anticipated the townhomes would have a vertical unit integration with three levels, including a first-level garage and living space on the second and third levels. All garage access is organized on a central alley. Vehicles would circulate through the site at the northern Co. Rd. 101 access and exit at the southern Co. Rd. 101 access. Walk-up entries are opposite the garages on each unit.



Concept Plan – Doran Townhomes



Concept Rendering – Doran Townhomes

Zoning

The site's current zoning includes an R-2 Low-Density Residential District for the two residential duplex properties and a B-1 Office Business District for the northern office property. Adjacent zoning districts include R-1 Low-Density Residential District for Minnetonka neighborhoods lying east and west of the site, B-2 Limited Business District for much of the commercial area to the north and in Deephaven to the south, R-3 Residential (20,000 SF lot size which is comparable to R-1 lots in Minnetonka).



Property Zoning

Comprehensive Plan

The site is guided for mixed-use in the comprehensive plan. This mixed-use designation would allow a range of residential and commercial uses serving as a transitional area from the Minnetonka Blvd. / Co. Rd. 101 commercial area and the adjacent residential neighborhoods in Minnetonka and Deephaven.

In a mixed-use area, uses could include residential or commercial and could be arranged vertically or horizontally. Mixed-use areas could also contain a single use which best describes the Doran proposal.



Comprehensive Plan

Planning Commission Input

The commission is asked to answer the following questions:

1. Is there merit for this concept to proceed to a formal development review?
2. If yes, why? If not, why?
3. What would make this concept better?

Originator: Loren Gordon, AICP, City Planner

ADDITIONAL INFORMATION

Concept Plan Review Process

The concept plan review process is a feedback process. It is intended to provide opportunities for the public, planning commissioners, and city councilmembers to offer feedback to potential developers. Developers may use that feedback to inform future plans and formal applications.

The concept plan review process is not a process that informs or guarantees future decisions. If a concept receives negative feedback, that does prevent a developer from submitting formal applications. Conversely, if a concept receives positive feedback that does not ensure that formal applications will be approved.

Generally, the process will include the following steps:

- **Neighborhood Meeting.** At this meeting, potential developers have an opportunity to introduce themselves and their development concept to area property owners and residents. Attendees are given the opportunity to provide feedback on the concept. This is a primary forum for public feedback.
- **Concept Plan Page.** A page is set up on minnetonkamn.gov for each concept plan. Through this page, the public can view the concept plans and provide written comments to city staff. This is a primary forum for public feedback.

Planning Commission Meeting. At this meeting, potential developers have an opportunity to introduce themselves and their development concept. This meeting is the forum for commissioner feedback. However, the public is welcome to attend the meeting to observe and make comments for the public record.

- **City Council Meeting.** At this meeting, potential developers have an opportunity to introduce themselves and their development concept. This meeting is the forum for councilmember feedback. While the public is welcome to attend the meeting to observe, no public comment will be taken.

Next Steps

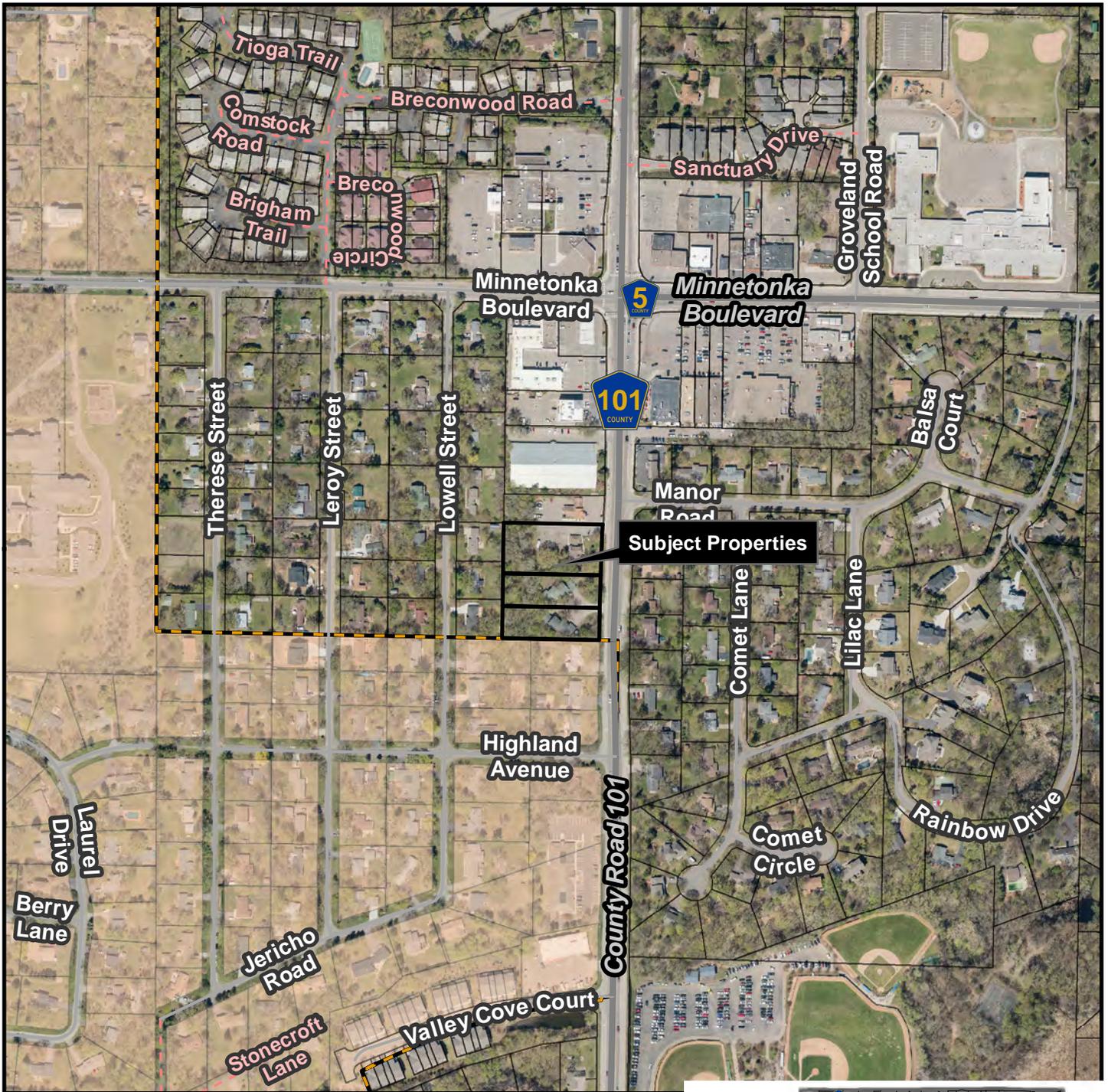
- **Formal Application.** If the developer/applicant chooses to file a formal application, notification of the application will be mailed to area property owners. Area property owners are encouraged to view plans and provide feedback via the city's website. Through recent website updates: (1) staff can provide owners with ongoing project updates, (2) owners can "follow" projects they are particularly interested in by signing up for automatic notification of project updates; (3) owners may provide project feedback on the project; and (4) and staff can review resident comments.
- **Council Introduction.** The proposal would be introduced at a city council meeting. At that time, the council would be provided another opportunity to review the issues identified during the initial concept plan review meeting and to provide direction about

any refinements or additional issues they wish to be researched and for which staff recommendations should be prepared.

- **Planning Commission Review.** The planning commission will review and subsequently make a recommendation to the city council on land use matters.
- **City Council Action.** Based on input from the planning commission, professional staff, and the general public, the city council would take final action.

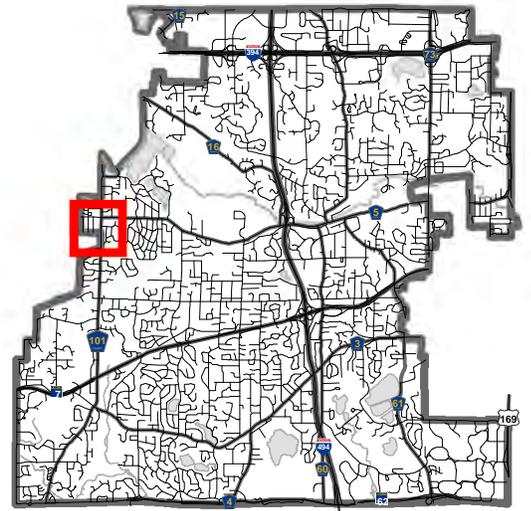
Roles and Responsibilities

- **Applicants.** Applicants are responsible for providing clear, complete, and timely information throughout the review process. They are expected to be accessible to both the city and the public and respect the integrity of the public process.
- **Public.** Neighbors and the general public will be encouraged and enabled to participate in the review process to the extent they are interested. However, effective public participation involves shared responsibilities. While the city has an obligation to provide information and feedback opportunities, interested residents are expected to accept the responsibility to educate themselves about the project and review process, provide constructive, timely, and germane feedback, and stay informed and involved throughout the entire process.
- **Planning Commission.** The planning commission hosts the primary forum for public input and provides clear and definitive recommendations to the city council. To serve in that role, the commission identifies and attempts to resolve development issues and concerns prior to the council's consideration by carefully balancing the interests of applicants, neighbors, and the general public.
- **City Council.** As the ultimate decision-maker, the city council must be in a position to equitably and consistently weigh all input from their staff, the general public, commissioners, applicants, and other advisors. Accordingly, council members traditionally keep an open mind until all the facts are received. The council ensures that residents have an opportunity to participate in the process effectively.
- **City Staff.** The city staff is neither an advocate for the public nor the applicant. Rather, staff provides professional advice and recommendations to all interested parties, including the city council, planning commission, the applicant, property owners, and residents. Staff advocates for its professional position, not a project. Staff recommendations consider neighborhood concerns but necessarily reflect professional standards, legal requirements, and broader community interests.



Location Map

Project: Doran_County Road 101
 Address: 3514, 3518, 3520, 3522, 3524 C





SCALE: 1" = 60'



EXISTING COMMERCIAL

COMMERCIAL

EXISTING SINGLE FAMILY HOMES

EXISTING SINGLE FAMILY HOMES

DEEPAVEN BOUNDARY



SCALE: 1" = 60'





