

City Council Agenda Item #13A
Meeting of July 23, 2018

Brief Description

Items concerning a multi-family residential development by Dominion, at 11001 Bren Road East.

- 1) Ordinance rezoning the property from I-1, industrial, to PUD, planned unit development;
- 2) Resolution approving a master development plan and final site and building plans;
- 3) Resolution approving preliminary and final plats;
- 4) Resolution approving vacation of easements; and
- 5) Negative declaration on the need for an Environmental Impact Statement

Recommendation

Staff recommends the city council hold the public hearing and:

- 1) Adopt the ordinance and resolutions approving the rezoning, master development plan, final site and building plans, preliminary and final plats, and vacation of easements;
- 2) Approve the resolution for a negative declaration on the need for an Environmental Impact Statement

Background

In 2017, Dominion presented a concept plan for redevelopment of the 9.4-acre property at 11001 Bren Road. The plan contemplated removal of an existing office building and construction of three buildings containing a total of 454 apartment units. Two of the buildings would be dedicated to workforce housing, while the third building would be affordable, independent senior housing. The city council generally indicated support for the concept, but suggested to city staff that pedestrian connections and broader park planning in OPUS be considered.

Proposal

Dominion has submitted formal applications for the redevelopment of the site. As proposed, the existing building and parking lot would be removed and three new apartment buildings would be constructed for a total of 482 units. The westerly building (Building A) is a proposed four-story, 83-unit, workforce housing building. The southerly building (Building B) would contain 137 workforce units. This building would have the above-grade appearance of two, five-story buildings. However, the building would share one foundation and underground garage. The northerly building (Building C) would be six-stories in height and contain 262 affordable, independent senior units. Unit mix within the buildings is generally proposed as follows:

	Building A General Occupancy	Building B General Occupancy	Building C Senior Occupancy
1 Bedroom	14	41	59
2 Bedroom	45	75	149
3 Bedroom	24	21	54
TOTAL UNITS	83	137	262

All three buildings would include rooftop solar energy systems, capable of producing more than 562,000 kilowatts of energy per year. Dominion indicates that the solar energy system would cover roughly 20 percent of the residential development's total electricity cost and would result in an annual power savings of over \$50,000.

Planning Commission Review and Recommendation

The planning commission considered the redevelopment proposal on May 24, 2018. The commission report, associated plans, and meeting minutes are attached. Staff recommended approval of the proposal, finding:

1. The proposed residential use is consistent with both the past plans for OPUS and the future goals for the area.
2. The use of PUD zoning is appropriate, as it would promote a public benefit recognized by the ordinance. Specifically, the Dominion proposal would result in the provision of 220 workforce housing units and 262 affordable, independent senior units.
3. The proposed buildings have been attractively designed. The building articulation and variety of materials – including glass, smooth fiber cement panels, fiber cement siding, woodgrain aluminum panels, and masonry – would provide visual interest from both onsite and offsite views.
4. The level of site impact would be reasonable for full redevelopment of a site.

At the commission meeting, a public hearing was opened to take comment. No one appeared to speak. Following the public hearing, the commission discussed and expressed general support for the proposal. On a 6-0 vote, the commission recommended that the city council approve the redevelopment.

City Council Review

In addition to those land use items previously considered by the planning commission, the city council must consider two additional land use related items.

- **Preliminary and Final Plats.** Dominion proposes to divide the larger development site into two lots. The workforce housing buildings would be located on one lot and the senior building on the other. Staff finds the plats to be reasonable, as the subdivision is required simply for financing purposes; a plat is not required by city code.

Generally, the city requires payment of park dedication fees prior to release of a final plat for recording. In the case of Dominion, park dedication fees would be collected prior to issuance of the required grading permit. At \$5,000 per dwelling unit, park dedication for the project equates to \$2,410,000. As a condition of approval, the city may choose to credit verified costs for construction of a pedestrian underpass beneath Bren Road East and other pedestrian improvements. The details for cost verification and crediting will be outlined in the contracts for private development.

- **Vacation of Easement.** Dominion requests that a utility easement and small portion of roadway easement be vacated. Staff finds both vacations to be reasonable. As part of the platting of the site a new utility easement would be dedicated over a relocated sanitary main and the pre-existing easement would become obsolete. The roadway easement to be vacated is a roughly 1,400 sq.ft. remnant area not required for roadway purposes at this time.

Environmental Assessment Worksheet and Comment Period

An Environmental Assessment Worksheet (EAW) is a document prepared by a Responsible Government Unit (RGU), in this case the city, to generally evaluate the potential environmental impact of a proposed development. An EAW includes a series of 20 questions related to land use, geology, water resources, wildlife, emissions, and traffic among other things. If the answers to these questions suggest that a project will result in a significant environmental impact, the RGU may declare that an Environmental Impact Statement (EIS) is required. An EIS is a more in-depth environmental review.

While the preparation of some EAWs is discretionary, state law mandates an EAW for residential development containing over 375 attached living units. As the Dominion project would contain 482 units, a mandatory EAW was completed. The EAW, which is attached for reference, concluded that the proposal would not result in a significant environmental impact. The EAW was distributed for review by various state and local agencies and organizations. The comments received were generally consistent with those of city staff – as outlined in the planning commission staff report – and would be addressed by the conditions of approval contained in the staff-drafted resolutions. Two questions were received related to whether a more in-depth traffic impact study should be undertaken to understand the redevelopment’s potential impact on regional systems - Highways 169 and 62. Staff does not believe that further analysis is necessary, given that: (1) traffic in OPUS has been studied in depth over the last decade; (2) that the proposed redevelopment would be consistent with the city’s Opus Overlay District ordinance – which regulates development based on p.m. peak hour trips; and (3) the traffic generated by the development would generally be “reverse traffic” for the area, with the proposed development’s residents leaving OPUS in the a.m. and returning in the p.m. Though the traffic generated by the proposed residential development would be greater than that of the preexisting office development at the site, staff is confident that the traffic generated would not have significant negative impact on state roadways. A response letter was sent to those parties describing those reasons why the city does not anticipate any regional freeway system impact as a result of the proposed development.

Summary Comments

The proposed residential development would result in a considerable change to the aesthetic and activity on the west side of OPUS. From staff’s perspective this change is reasonable,

appropriate, and welcome. It would be consistent with both the past plans for OPUS and the future goals for the area.

Staff Recommendation

Staff recommends that the city council adopt the following related to the Bren Road Development, a multi-family residential development by Dominion, at 11001 Bren Road East:

- 1) Ordinance rezoning the property from I-1, industrial, to PUD, planned unit development, and adopting a master development plan;
- 2) Resolution approving final site and building plans;
- 3) Resolution approving preliminary and final plats;
- 4) Resolution approving vacation of easements;
- 5) Resolution making a negative declaration on the need for an Environmental Impact Statement.

Submitted through:

Geralyn Barone, City Manager
Julie Wischnack, AICP, Community Development Director
Loren Gordon, AICP, City Planner

Originated by:

Susan Thomas, AICP, Assistant City Planner

MINNETONKA PLANNING COMMISSION
May 24, 2018

Brief Description	Items concerning Bren Road Development, a multi-family residential development by Dominion, at 11001 Bren Road East. <ol style="list-style-type: none">1) Ordinance rezoning the property from I-1, industrial, to PUD, planned unit development;2) Master development plan;3) Site and building plan review;4) Environmental Impact Statement declaration; and5) Comprehensive Guide Plan declaration.
Recommended Commission Recommendation	Recommend the city council: <ol style="list-style-type: none">1) Adopt the ordinance and resolutions approving rezoning, master development plan and final site and building plans;2) Approve by motion a negative declaration on the need for an Environmental Impact Statement.
Recommended Planning Commission Action	Adopt a resolution declaring that proposal is consistent with the comprehensive plan

Background

In 2017, Dominion presented a concept plan for redevelopment of the 9.4-acre property at 11001 Bren Road. The plan contemplated removal of an existing office building and construction of three buildings containing a total of 454 apartment units. Two of the buildings would be dedicated to workforce housing, while the third building would be affordable, independent senior housing. The city council generally indicated support for the concept, but suggested to city staff that pedestrian connections and broader park planning in OPUS be considered.

Formal Application

Dominion has now submitted formal applications for the redevelopment of the site. The proposal, it includes a total of 482 rental units constructed in three, new buildings. The city council introduced the proposal on April 30, 2018. The council generally reiterated its concept plan comments.

Proposal Summary

The following is intended to summarize the Dominion proposal. Additional information associated with the proposal can be found in the “Supporting Information” section of this report.

- **Existing Site Conditions.**

The roughly 9.4-acres subject property is located near the west entrance to OPUS and directly west of the future Southwest Light Rail Transit (SWLRT) station. A roughly 133,000 square foot office building is centrally located on the property. The building is surrounded by parking on its south, east and west sides. Though the industrially-zoned property is considered fully developed, it does contain several noticeable natural features, including: (1) a roughly 50-foot change in elevation from the highest point on the west side of the site to the lowest point on the east side; (2) a Type-1 wetland located adjacent to Bren Road; and (3) 97 high-priority trees.

- **Proposed Buildings.**

As proposed, the existing building and parking lot would be removed and three new apartment buildings would be constructed. The westerly building (Building A) is a proposed four-story, 83-unit, workforce housing building. The southerly building (Building B) would contain 137 workforce units. This building would have the above-grade appearance of two, five-story buildings. However, the building would share one foundation and underground garage. The northerly building (Building C) would be six-stories in height and contain 262 affordable, independent senior units. Unit mix within the building is generally proposed as follows:

	Building A General Occupancy	Building B General Occupancy	Building C Senior Occupancy
1 Bedroom	14	41	59
2 Bedroom	45	75	149
3 Bedroom	24	21	54
TOTAL UNITS	83	137	262

All three buildings would include rooftop solar energy systems, capable of producing more than 562,000 kilowatts of energy per year. Dominion indicates that the solar energy system would cover roughly 20 percent of the residential development’s total electricity cost and would result in an annual power saving of over \$50,000.

- **Proposed Parking and Site Improvements.**

The proposed buildings would be served by 552 parking stalls. The majority of parking would be located within underground garage space, with surface parking generally situated between the buildings. Vehicle access to the proposed development would be via two driveways to Bren Road East. One drive would generally be located on the north side of the site and the other located on the east, across from the LRT station. Other site

improvements are also proposed, including: an internal trail system, links to the existing OPUS trail system, outdoor patio areas, an outdoor pool, a bike café, and public art.

Primary Questions and Analysis

A land use proposal is comprised of many details. In evaluating a proposal, staff first reviews these details and then aggregates them into a few primary questions or issues. The following outlines both the primary questions and staff findings associated with the proposal.

- **Is the proposed residential land use appropriate?**

Yes. The proposed residential use is consistent with both the past plans for OPUS and the future goals for the area. During its 1970s development, OPUS was envisioned to contain residential areas “convenient to the office, commercial and industrial portions ... as well as to the surrounding services, communities, mass transportation systems, parks and recreational areas.”¹ Looking to the future, the 2030 Comprehensive Guide Plan generally emphasizes accommodating a variety of housing types within the community that will appeal to a variety of residents at a variety of ages and a variety of income levels. The plan specifically notes that redevelopment within the OPUS area should include the provision of additional residential uses.

- **Is the use of PUD zoning appropriate?**

Yes. The city of Minnetonka uses PUD zoning to provide flexibility from certain ordinance regulations in order to achieve public benefits that may not otherwise be achieved. One of the specific public benefits recognized by the ordinance is the provision of affordable housing. The Dominion proposal would result in the provision of 220 workforce housing units and 262 affordable, independent senior units.

- **Is the proposed building design reasonable?**

Yes. Representatives of Dominion and city staffs spent considerable time discussing the design and façade treatments proposed for each of the three buildings. In staff’s opinion the resulting plans are not only reasonable, but very attractive. The proposed building articulation and variety of materials – including glass, smooth fiber cement panels, fiber cement siding, woodgrain aluminum panels, and masonry – would provide visual interest from both onsite and offsite views.

- **Are the proposed site impacts reasonable?**

Yes. Grading and tree impacts would occur to accommodate the proposed buildings, parking lots, and development amenities. Generally, the west half of the site would be lowered by roughly 15 feet, while the east side would be raised from 2 to 22 feet. The grading would likely result in the removal or impact to 76 high-priority trees. Staff finds this level of impact to be reasonable for full redevelopment of a site.

The construction of SWLRT will require some associated changes to Bren Road East. If approved, construction on the Dominion project would occur well in advance of SWLRT/Bren Road East work. To address this, Dominion has provided a “phased” site plan for the east side of the property. The Phase I plan includes temporary curbing and

¹ Rauenhorst Corporation, *Opus 2: Crossroads of Tomorrow, Today*. (Minneapolis, Rauenhorst Corporation), 13.

bituminous paving at the site entrance. Phase 2 would be completed in conjunction with SWLRT completion – or by a certain negotiated date in the event SWLRT is further delayed or not completed. Phase 2 includes installation of concrete curbing and driveway entrance, landscaping, and public art.

- **Are pedestrian connections to SWLRT adequately addressed?**

Generally, yes. With the assumed construction of SWLRT, the applicant and city staff have explored – at length – opportunities to connect the proposed residential development to the OPUS Station. To supplement the applicant/staff discussion, the city also commissioned WSB & Associates to review various connections.

- ✓ Option #1. Grade Separated Crossing, East Side. A grade separated crossing would be desirable from the east side of the subject property to the station area. Depending on its design, an overpass crossing would be cumbersome and costly. Moreover, anecdotally, pedestrians are less likely to go “up” to cross a roadway than they are to go “down.” Unfortunately, an underpass crossing is not viable for two reasons: (1) the existing high water table; and (2) challenges related to surface water drainage around the LRT station. Staff does not believe a grade separated crossing on the east would be appropriate at this time.
- ✓ Option #2. Grade Separated Crossing, North Side. While an underpass on the east side of the site has significant challenges, an underpass on the north side may be a feasible option. With a roughly 285-foot connection to the existing trail system, pedestrians could use existing trail system and underpasses to reach the LRT station. While certainly providing a viable route, many pedestrians may seek a more direct connection to the station. This connection is also discussed in the “Supporting Information” section of this report.
- ✓ Options #3. Improved At Grade Crossing. There is an existing, at grade crossing just south of the subject property. Improvements at this crossing could enhance the safety of what would be a nearly direct connection to LRT. Improvements could include a rectangular rapid flashing beacon (RRFB) or a high-intensity activated crosswalk beacon (HAWK). Both systems utilize pedestrian-activated lights. While an RRFB requires traffic to yield to pedestrians, a HAWK requires traffic to stop.



² (May 2009) *Rectangular Rapid Flashing Beacon*. Retrieved from: https://safety.fhwa.dot.gov/intersection/conventional/unsignalized/tech_sum/fhwasa09009/
³ (September 28, 2010) *Double-Red Signal Reduces Crashes at Crosswalks*. Retrieved from: <http://www.iihs.org/iihs/sr/statusreport/article/45/10/4>

It is staff's opinion that, at this time, an improved at-grade crossing with the installation of strategically installed fencing on the east side of Bren Road East is the most appropriate option. WSB will be providing a recommendation as to which option – RRFB or HAWK – should be installed.

- **Should an Environmental Impact Statement (EIS) be required?**

No. An Environmental Assessment Worksheet (EAW) is a document prepared by a Responsible Government Unit (RGU) to generally evaluate the potential environmental impact of a proposed development. An EAW includes a series of 20 questions related to land use, geology, water resources, wildlife, emissions, and traffic among other things. If the answers to these questions suggest that a project will result in a significant environmental impact, the RGU may declare that an Environmental Impact Statement (EIS) is required. An EIS is a more in-depth environmental review.

While the preparation of some EAWs is discretionary, state law mandates an EAW for residential development containing over 375 attached living units. As the Dominion project would contain 482 units, a mandatory EAW was completed. The EAW, which is attached for reference, concluded that the proposal would *not* result in a significant environmental impact. By law, the city must make an official "declaration" on the need for an EIS. Staff recommends that the city make a negative declaration for the Dominion project.

Summary Comments

The proposed residential development would result in a considerable change to the aesthetic and activity on the west side of OPUS. From staff's perspective this change is reasonable, appropriate, and welcome. It would be consistent with both the past plans for OPUS and the future goals for the area.

Staff Recommendation

Staff recommends that the commission recommend the city council adopt the following related to the Bren Road Development, a multi-family residential development by Dominion, at 11001 Bren Road East:

- 1) Ordinance rezoning the property from I-1, industrial, to PUD, planned unit development, and adopting a master development plan; and
- 2) Resolution approving final site and building plans.
- 3) A motion making a negative declaration on the need for an Environmental Impact Statement.

Staff further recommends the planning commission adopt the resolution declaring that proposal consistent with the comprehensive plan.

Originator: Susan Thomas, AICP, Assistant City Planner
Through: Loren Gordon, AICP, City Planner

Supporting Information

Surrounding Land Uses

North: charter school property, zoned industrial
South: office/industrial property, zoned industrial
East: retail/warehouse property, zoned commercial
West: cemetery and medium-density residential development

Planning

Guide Plan designation: mixed-use
Existing Zoning: I-1, Industrial

Required Actions

The proposal requires the following:

Land Use

- **Rezoning.** To facilitate the proposed development, Dominion is requesting that the property be rezoned to PUD. The planning commission makes a recommendation to the city council, which has final authority to approve or deny the rezoning.
- **Master Development Plan.** Under the zoning ordinance, a master development plan is required in conjunction with PUD zoning. The planning commission makes a recommendation to the city council, which has final authority to approve or deny the master development plan.
- **Final Site and Building Plans.** By city code, site and building plan review is required in conjunction with PUD zoning. The planning commission makes a recommendation to the city council, which has final authority to approve or deny the final site and building plans.
- **Preliminary and Final Plats.** The city would not require individual buildings to be located on individual lots. In fact, within a PUD, multiple buildings are allowed on one lot. Nevertheless, Dominion has indicated that, for financing purposes, Buildings A and B should be located on one lot and Building C located on a separate lot. Plat applications, which staff would consider straight-forward items, will be considered by the planning commission and city council at a future date.
- **Easement Vacation.** The site includes several existing easements, some of which will become obsolete should the Dominion proposal be approved. Vacation applications, will be considered by the council in conjunction with the preliminary and final plats.
- **Environmental Impact Statement Declaration.** Based on the number of living units proposed, an Environmental Assessment Worksheet (EAW) is required under state statute. An EAW

includes a series of 20 questions, the answers to which suggest whether a proposal will have significant environmental impact. If the Responsible Government Unit (RGU), in this case the city, determines that a project will result in significant impact, the RGU may declare that an Environmental Impact Statement (EIS) is required. An EIS is a more in-depth environmental review. The planning commission makes a recommendation to the city council, which has final authority to make a positive or negative declaration on the need to conduct an EIS.

Finance

- **Tax Increment Financing.** To assist with the production of affordable housing, Dominion is requesting that the city provide Tax Increment Financing (TIF) assistance in the amount of \$7,809,000 with a maximum term of 26 years. The Economic Development Advisory Commission (EDAC) and city council previously reviewed this request for assistance and found the request reasonable. While the actual use of TIF is not the purview of the planning commission, the planning commission is required to determine that the redevelopment of the property is consistent with the city's comprehensive plan.
- **Contract for Private Development.** The EDAC reviewed a draft Contract for Private Development at its April 19 meeting. The contract outlines the key points of the TIF request as well as expectations for the development. The council will review the final contract at its June 4 meeting. This contract is not the purview of the planning commission.

Grading

The highest point of the subject property is situated in the northwest corner of the site. The property slopes significantly downward from this point to a wetland located in northeast corner of the site. The change in grade is roughly 50 feet. Much of the area west of the existing lot is "steep" as defined by the ordinance; it has grades of 23 to 24 percent.

Significant earthwork would be necessary to accommodate the proposed buildings, parking lots, and other site improvements. A two tier retaining wall, ranging in aggregate height from roughly 10 feet to 20 feet, would be constructed parallel to the west property line. Generally, excavation would occur on the western third of the site, while fill would be placed on the eastern two-thirds. At various areas, up to 15 feet of earth would be removed and up to 22 feet of earth would be placed.

Tree Impact

The property contains a total of 202 regulated trees. While the trees in and around the site's wetland were naturally seeded, the majority of the trees on the site appear to be have been planted as part of the original landscape plan for the existing building/site. These trees are, nevertheless, regulated by the tree ordinance. As proposed:

	Existing	Removed	% Removed
High Priority	97	76	78%
Significant	105	81	77%

* By city code, a tree is considered removed if 30 percent or more of the critical root zone of is compacted, cut, filled or paved.

As the proposal is for redevelopment of property, the level of tree removal/impact would be permitted under the tree protection ordinance.

Stormwater

As proposed, stormwater runoff would be directed to several catch basins and directed via pipe to one of three stormwater facilities located under the proposed parking lot. One of the facilities would be a water reuse tank; water from this tank would be used for on-site irrigation. Two underground facilities would be more “typical” underground infiltration systems. The facilities would ultimately outlet to the public storm sewer system.

Engineering staff has reviewed the plans associated with the proposal and finds them to be generally acceptable. As a condition of approval, final plans must meet both the city’s Water Resources Management Plan standards and Nine Mile Creek Watershed District rules.

Utilities

Public water and sewer facilities are available at the site. Existing water mains are located east of the site in Bren Road East and running parallel to the south property line. An existing sanitary sewer main runs north-south through the subject property itself; there is also a sewer main within Bren Road East.

A new private watermain would be constructed from the public main at the south property line to the north. Building A and C would be connected to this new main, while Building B would take service from the existing main along the south property line.

As proposed, the sewer main on the site and its associated public easement must be relocated. The new buildings would be connected to this new line. While the proposed plans show the relocation of the main, staff are concerned with several private improvements the plans show within close proximity to the main and within the necessary easement. These improvements include patio and pool improvements. As a condition of approval, these items must be relocated outside of the easement.

Parking

Dominium representatives and city staff spent considerable time discussing the provision of parking. From Dominion’s perspective, adequate parking must be provided to meet investor goals and resident demand. From city staff’s perspective, parking should certainly be provided to meet parking demand. However, assumed demand should take into consideration the availability of light rail

transit and evolving transportation choices and options (Lyft, Uber, and the possibility of self-driving cars, etc.)

Ultimately, a plan generally satisfying both Dominion and staff goals was submitted. As proposed, parking would be constructed/supplied as follows:

	Building A	Building B	Building C
Underground	54	95	225
Surface	178		
TOTAL	552		

The parking ratio proposed would be slightly less than at other apartment buildings in the community. However, it would be consistent with Institute of Transportation Engineers suggested parking demand.

		Stalls per Bedroom	Stalls per Unit
Traditional City Code Standard		n/a	2
PROPOSED	General Occ.	0.64	1.25
	Senior Occ.	0.54	1.26
ITE	General Occ.	n/a	1.0 to 1.3*
	Senior Occ.	n/a	0.33 to 0.50
Tonka on the Creek		1.15	1.49
Carlson Island		1.03	1.55
The Ridge		.93	2
Highland Bank		1.2	1.78
** Institute of Transportation Engineers, Low/Mid-Rise Apts, within 1/3 mile of LRT station and more than 10 miles from Central Business District			

Traffic

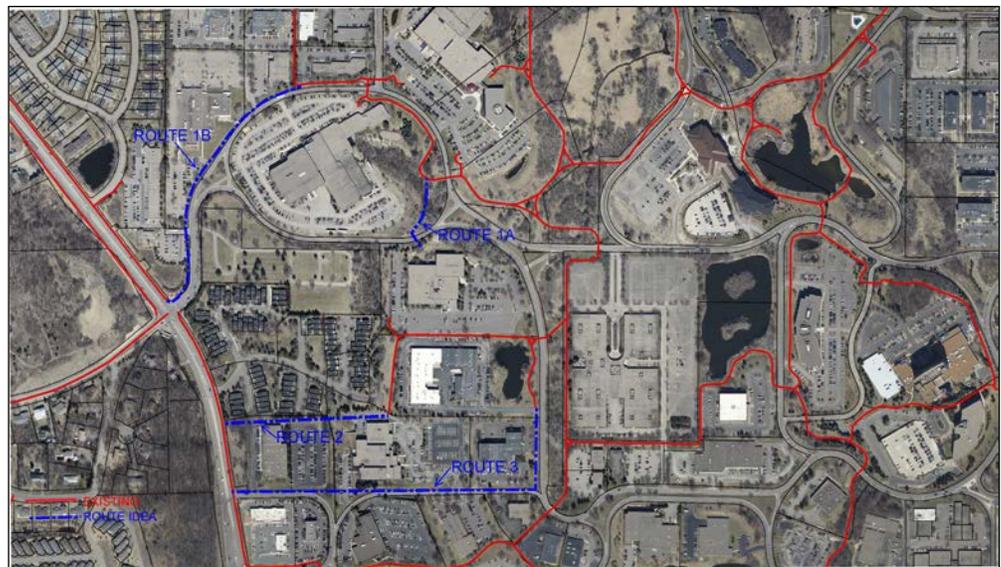
OPUS is sometimes maligned for its one-way road system, which casual visitors to the area can find confusing. However, from a traffic movement perspective, the roadway design is excellent.

As part of the EAW, daily traffic counts were taken on Bren Road East. The counts showed 2,497 vehicle trips per day on that stretch of roadway adjacent to the subject property. As designed, Bren Road East “can carry more than 10,000 vehicles a day at a very high level

of service.”⁴ The EAW notes that though the proposal would “substantially increase traffic on Bren Road East, the traffic volume would be well below capacity of this roadway.”

Pedestrian Improvements

In addition to exploring pedestrian connections to SWLRT, a city staff group – comprised of planning, engineering, legal, public works, and recreation staff – has spent time evaluating opportunities to enhance pedestrian connections in and around the area of the subject property in an effort to enhance the regional network with a connection to Shady Oak Road. Three primary routes have been identified. Aside from specific engineering factors – grade, drainage, etc. – each of these connections has positive and negative aspects and each has unknowns that would need to be explored in further detail.



In staff’s opinion it would be appropriate to construct ROUTE 1A in conjunction with the Bren Road Development project. Together with public trails existing and proposed on the perimeter of the subject property, this route would provide a grade separated north/south link for pedestrians on the west side of Bren Road East. As a condition of approval, the final plat submitted by the applicant must be drafted to accommodate the underpass and short link portion of ROUTE 1A and an estimate of associated cost submitted to the city. These costs may be deducted from the required park dedication fee. The construction and cost responsibilities would be outlined through a Developer Agreement (a contract-type document signed by Dominion and the city), which is also included as a condition of approval.

Staff will continue to look at the viability of other connections to Shady Oak Road independent of this development proposal.

⁴ Dominion EAW, 19.

Setbacks, Etc.

The PUD ordinance contains no specific development standards relating to setbacks, lot coverage, etc. However, the following chart outlines these items for informational purposes:

	Measurement
Setbacks*	
North property line	35 ft
South property line	35 ft
East property line	40 ft
West property line	35 ft
Height	
Building A	43 ft
Building B	63 ft
Building C	81 ft
Miscellaneous	
Impervious Surface	60.7%
Rounded down to nearest 5 ft	

SBP Standards

The proposal would meet the site and building standards as outlined in City Code §300.27 Subd.5:

1. Consistency with the elements and objectives of the city's development guides, including the comprehensive plan and water resources management plan.

Finding: The proposed high-density residential development is consistent with the general housing goals of the 2030 Comprehensive Guide Plan and the specific Plan's specific goal to provide additional housing in the OPUS area. Further, the proposal has been reviewed by city planning, engineering, and natural resources staff and found to be generally consistent with the city's development guides, include the water resources management plan.

2. Consistency with this ordinance.

Finding: The proposal is consistent with the zoning ordinance.

3. Preservation of the site in its natural state to the extent practicable by keeping tree and soil removal and designing grade changes to

be in keeping with the general appearance of neighboring developed or developing properties.

Finding: The proposal would result in tree and soil removal. The subject property is a developed site, with the only “natural” area being Type-1 wetland on the northeast corner. While the proposal would result in tree and soil removal, the wetland area would not be disturbed.

4. Creation of harmonious relationship of buildings and open spaces with natural site features and with existing and future buildings having a visual relationship to the development.

Finding: The proposal would result in a harmonious relationship of buildings, with open space generally located at the perimeter of the site.

5. Creation of a function and harmonious design for structures and site features, with special attention to the following:
 - An internal sense of order for the buildings and uses on the site and provision of a desirable environment for occupants, visitors, and the general community.
 - The amount and location of open space and landscaping.
 - Materials, textures, colors and details of construction as an expression of the design concept and compatibly of the same with the adjacent and neighboring structures and uses.
 - Vehicular and pedestrian circulation, including walkways, interior drives and parking in terms of location and number of access points to the public streets, width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic and arrangement and amount of parking.

Finding: The proposal would result in a unique and attractively-designed neighborhood.

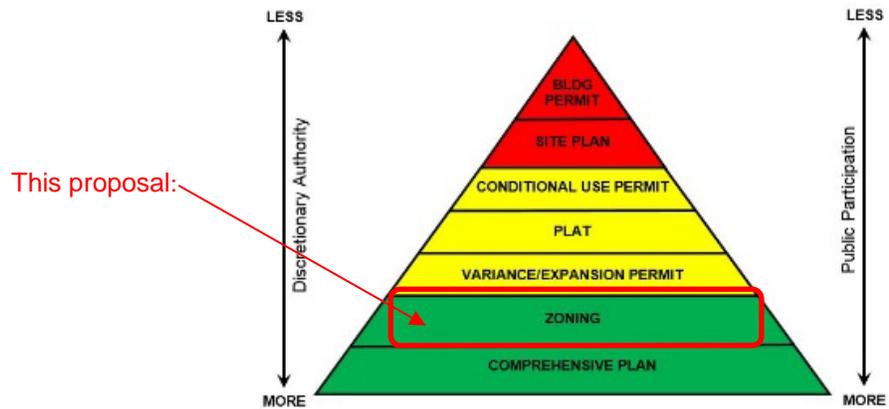
6. Promotion of energy conservation through design, location, orientation and elevation of structures, the use and location of glass in structures, and the use of landscape materials and site grading.

Finding: The proposal includes installation of rooftop solar energy systems, capable of producing more than 562,000 kilowatts of energy per year new. As new construction, the building code would require use of additional energy saving features within the buildings themselves.

7. Protection of adjacent and neighboring properties through reasonable provision for surface water drainage, sound and site buffers, preservation of views, light and air and those aspects of design not adequately covered by other regulations which may have substantial effects on neighboring land uses.

Finding: The proposal would visually and physically alter the property and the immediate area. However, this change would occur with any redevelopment of the site, which the city has long anticipated.

Pyramid of Discretion



Motion Options

The planning commission has three options:

1. Concur with the staff recommendation. In this case a motion should be made recommending the city council adopt the resolution approving the request.
2. Disagree with staff's recommendation. In this case, a motion should be made recommending the city council deny the request. This motion must include a statement as to why denial is recommended.
3. Table the requests. In this case, a motion should be made to table the item. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant, or both.

Voting Requirement

The planning commission will make a recommendation to the city council. The city council's final approval requires an affirmative vote of four members.

Neighborhood Comments

The city sent notices to 132 property owners and has received no written comments to date.

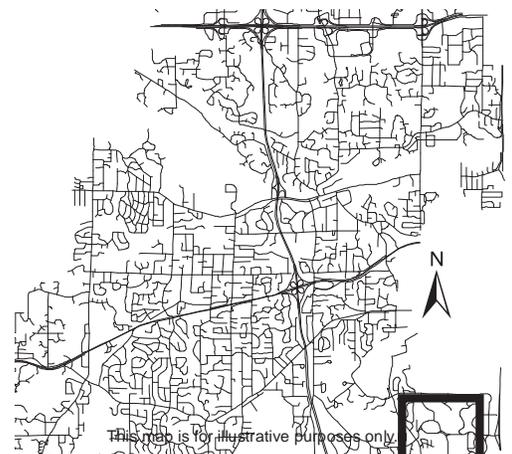
Deadline for Action

July 23, 2018



Location Map

Project: Dominion
Address: 11001 Bren Rd E





Bren Road Development





Bren Road Development Written Statement

The Bren Road Development is a proposed multi-family development that will include 482 units of housing for senior and general occupancy population. The site is currently zoned as I-1 Industrial, occupied by Digi International Inc., and is being used as a commercial- office space. The proposed zoning of the project is a PUD (Planned Unit Development) and the intended use of the project is multi-family housing.



Building Data								
	Building A		Building B		Building C		Total	
	Quantity	Percentage	Quantity	Percentage	Quantity	Percentage	Quantity	Percentage
1 Bedroom	14	16.87%	41	29.93%	59	22.52%	114	23.65%
2 Bedroom	45	54.22%	75	54.74%	149	56.87%	269	55.81%
3 Bedroom	24	28.92%	21	15.33%	54	20.61%	99	20.54%
Total Units	83		137		262		482	
Total Bedrooms	176		254		519		949	

Parking Data				
	Building A	Building B	Building C	Total
Garage Stalls	54	95	225	374
Surface Stalls				178
				552

BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

PROPOSED SITE



DESCRIPTION OF PROPERTY SURVEYED
(Per Schedule A of the herein referenced Title Commitment)

Lots 1 and 2, Block 1, Opus 2 Eighth Addition.

Hennepin County, Minnesota
Tennens Property

TITLE COMMITMENT EXCEPTIONS

(Per Schedule B, Part II of the herein referenced Title Commitment)

The property depicted on this survey and the easements of record shown hereon are the same as the property and the easements described in the Commitment for Title Insurance issued by Commercial Partners Title, LLC, as agent for Old Republic National Title Insurance Company, File No. 53041, effective date May 19, 2017. The numbers below correspond to those in the title commitment.

- do not require comment.
- Subject to an easement for sanitary sewer purposes in favor of the City of Minnetonka as contained in CR Book 73, Page 3995823.
Partially vacated by Resolution No. 81-6541 adopted April 20, 1981, filed May 6, 1981, as Document No. 1423875. [Partially vacated easements. Shown hereon as drainage and utility easements per the plat of OPUS 2 EIGHTH ADDITION.]
- Together with the right of the owner of that part of Lots 1 and 2, Block 1, Opus 2 Eighth Addition embraced within Outlots D, G and F, The Townships of Shady Oak to an easement for road purposes over Fendale Drive as provided in Document No. 1086026 (See Order Document No. 1292381), as shown by recital on the Certificate of Title. [Undefined area on property. Fendale Drive is shown on available maps, west of the property. Not Shown hereon.]
- Subject to a 30 foot sanitary sewer easement in favor of the City of Minnetonka as described in Parcel No. 25 in instrument filed January 12, 1973, as CR Document No. 3995823 (Now as to part of Lot 1), as shown by recital on the Certificate of Title.
Partially vacated by Resolution No. 81-6541 adopted April 20, 1981, filed May 6, 1981, as Document No. 1423875. [Partially vacated easements. Shown hereon as drainage and utility easements per the plat of OPUS 2 EIGHTH ADDITION.]
- Subject to a reservation unto Clover Drive, Inc., and its successors and assigns, of an easement 2.00 feet in width for concrete edging purposes over, under and across that portion of said Lots 1 and 2 being adjacent to the public right-of-way designated as Ben Road West and Ben Road East on the recorded plat of Opus 2 Eighth Addition, as shown by recital on the Certificate of Title. [Along the Easterly and northerly property lines Shown hereon.]
- Easements for utilities and drainage as shown on the recorded plat of Opus 2 Eighth Addition. [Located along the south, easterly and northerly property lines. Shown hereon.]
- Easement for public right-of-way purposes, in favor of the City of Minnetonka, a municipal corporation, as created in document dated May 27, 1978, filed August 30, 1978, as Document No. 1188817. [Located in the Northeast side of the property. Shown hereon.]
- Permanent easement reserved in Declaration of Industrial Standards and Protective Covenants dated April 7, 1981, filed April 8, 1981, as Document No. 1420987. Assigned as shown by Assignment dated September 6, 1983, filed April 3, 1984, as Document No. 1570485. [Easements defined per plat]
- Easement for storm sewer purposes, in favor of the City of Minnetonka, a Minnesota municipal corporation, as created in document dated October 12, 1982, filed February 23, 1983, as Document No. 1502290. [Located at the southwest corner of the property. Shown hereon.]

ALTA/NSPS OPTIONAL TABLE A NOTES
(The following items refer to Table A optional survey notices and specifications)

- Monuments placed (or a reference monument or witness to the corner) at all major corners of the boundary of the property, unless already marked or referenced by existing monuments or witnesses to the corner are shown hereon.
- The address, if disclosed in documents provided to or obtained by the surveyor, or observed while conducting the fieldwork is 11001 Bren Road East, Minnetonka, MN 55343, 02432, effective date of November 4, 2016.
- This property is contained in Zone X (areas determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map No. 27053C0343F, Community Panel No. 02432.
- The Gross land area is 409,223 +/- square feet or 9.39 +/- acres.
- (a) Any current zoning classification, setback requirements, height and floor space area restrictions, and parking requirements, shown hereon, are per a report or letter provided to the surveyor by the City of Minnetonka dated 7/13/2017, for the subject property as follows:
Zone H1, Industrial.
Setbacks: Front 35 feet from local neighborhood or collector streets or 50 feet from railroads and major collector streets or arterial roadways, Side & Rear 70 feet Zone R1, 50 feet from Zone R4.
Height: thirty five (35) feet.
Floor Area Ratio: Lot coverage shall not exceed eighty five (85) percent of lot area.
- (a) Exterior dimensions of all buildings are shown at ground level.
- Substantial features observed in the process of conducting fieldwork, are shown hereon.
- Stripping of clearly identifiable parking spaces on surface parking areas and lots are shown hereon. The number and type of clearly identifiable parking stalls on this site are as follows: 427 Regular - 9 Disabled = 436 Total Parking Stalls.
- We have shown underground utilities on and/or serving the surveyed property per Gopher State One-Call Ticket Nos. 171592945, 171592952 and 171841569. The following utilities and municipalities were notified:
CITY OF MINNETONKA (952)988-8400 COMCAST (800)762-0592 CENTURYLINK (855)742-6062
CENTER POINT ENERGY (409)641-9371 SPRINT/LONG DISTANCE (800)521-0579 LEVELS/COMMUNICATIONS (877)366-8344
XCEL ENERGY (800)848-7558 ZAYO BANDWIDTH (888)267-1063
- Utility operators do not consistently respond to locate requests through the Gopher State One Call service for surveying purposes such as this. Those utility operators that do respond, often will not locate utilities from their main line to the customer's structure or facility. They consider those utilities ***** (redactions) that are outside their jurisdiction. These ***** (redactions) on the surveyed property or adjoining properties, may not be located since most operators will not mark such "private" utilities. A private utility locator may be contacted to investigate these utilities further, if requested by the client.
- The locations of underground utility lines shown hereon is an approximation based on available maps, unless otherwise noted on the survey.
- Maps provided by those notified above, either along with a field location or in lieu of such a location, are very often inaccurate or inconclusive. EXTREME CAUTION MUST BE EXERCISED BEFORE AN EXCAVATION TAKES PLACE ON OR NEAR THIS SITE. BEFORE DIGGING, YOU ARE REQUIRED BY LAW TO NOTIFY GOPHER STATE ONE CALL AT LEAST 48 HOURS IN ADVANCE AT 811 or (651) 454-0002.
- The names of the adjoining owners, as shown hereon, are based on information obtained from current tax records.
- We are not aware of any evidence of recent earth moving work, building construction or building additions observed in the process of conducting our field work.
- We are not aware of any proposed changes in street right of way lines or evidence of recent street or sidewalk construction or repairs observed in the process of conducting our field work.
- The location of wetland delineation markers, which have been determined by a qualified specialist, are shown hereon.
- Plottable offsite (i.e., appurtenant) easements or servitudes disclosed in documents provided to or obtained by the surveyor are shown hereon.

SURVEY REPORT

- The Surveyor was not provided utility easement documents for the subject property except for those shown on the Survey.
- The bearings for this survey are based on the Hennepin County Coordinate System NAD 83 (1988 Adjust).
- Benchmark: MnDOT name HEART, in Minnetonka, 1.0 mile west along trunk highway 62 from the junction of trunk highway 62 and trunk highway 169 in Eden Prairie, at trunk highway 62 mile point 104.75, 45.0 feet north of the westbound trunk highway 62 fog line, 76.3 feet south of the ramp from shady oak road to westbound trunk highway 62, 1.5 feet south of the witness post. Elevation = 962.095 (NAVD88)
Site Benchmark: Top nut of fire hydrant located south of the entrance to the site on the west side of Bren Road. Elevation = 897.41 (NAVD88)
- Curb and guard rail falls on the property along Bren Road E.
- Bituminous path falls on the property along the south line.

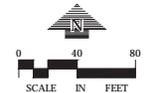
CERTIFICATION

To Minnetonka Leased Housing Associates II, L.L.P.; Digi International Inc. Commercial Partners Title, LLC; and Old Republic Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1 - 4, 6(a), 7(a), 8, 9, 11, 13, 16, 17, 18 and 19 of Table A thereof. The field work was completed on July 06, 2017.

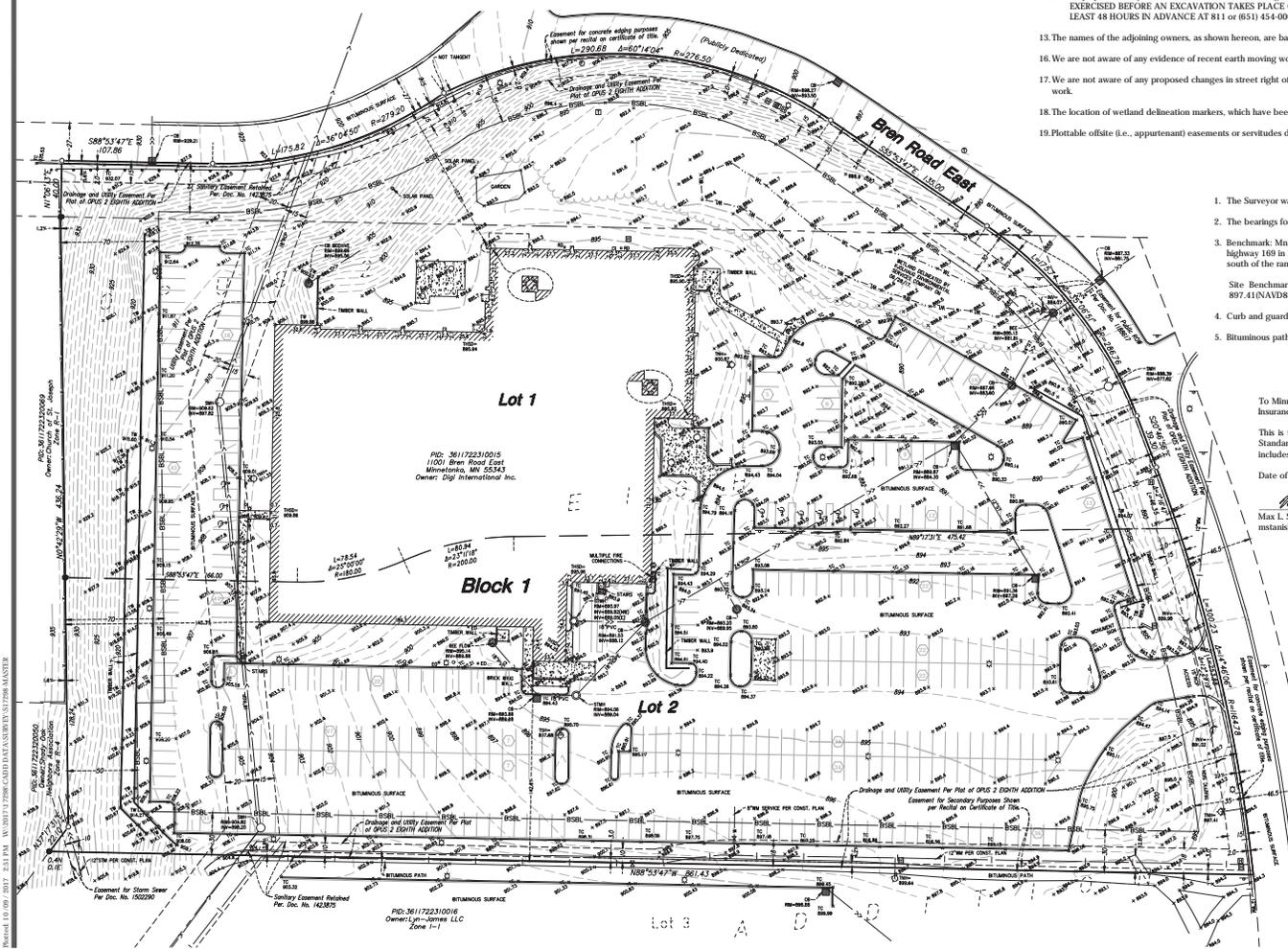
Date of Plat or Map: October 9, 2017

Max I. Stansibowski
Max I. Stansibowski, PLS Minnesota License No. 49898
mstansibowski@loucksinc.com



SURVEY LEGEND

AS ASH	◇ HYDRANT	— STORM SEWER
BA BASSWOOD	⊠ GATE VALVE	— SANITARY SEWER
BO BOXELDER	⊠ POWER POLE	— WATERMAN
CO COTTONWOOD	⊠ LIGHT POLE	— CULVERT
EL ELM	⊠ YARD LIGHT	— UNDERGROUND CABLE TV
FR MISC FRUIT	— GUY WIRE	— UNDERGROUND ELECTRIC
LO LOCUST	— SIGN	— UNDERGROUND FIBER OPTIC
PI PINE	⊠ SPOT ELEVATION	— UNDERGROUND GAS
SP SPRUCE	⊠ ELECTRIC TRANSFORMER	— UNDERGROUND TELEPHONE
TR TREE (GEN)	⊠ TELEPHONE PEDESTAL	— UNDERGROUND UTILITY
⊠ CATCH BASIN	⊠ ELECTRIC MANHOLE	— OVERHEAD UTILITY
⊠ STORM MANHOLE	⊠ GAS VALVE	— FORCE MAIN
⊠ SANITARY MANHOLE	⊠ TELEPHONE MANHOLE	— CHAIN LINK FENCE
⊠ FIBER MANHOLE	⊠ ELECTRIC METER	— CONCRETE CURB
⊠ GUARDPOST	⊠ GAS METER	⊠ CONCRETE
⊠ ROOF DRAIN	⊠ DENOTES 1/2 INCH X 14 INCH IRON MONUMENT SET, MARKED "S 49898"	⊠ CONDUIT
⊠ FIRE CONNECTION	⊠ DENOTES IRON MONUMENT FOUND	⊠ GUARDRAIL
⊠ ELECTRIC OUTLET	⊠ DENOTES NAIL MONUMENT FOUND	⊠ BUILDING SETBACK
⊠ HAND HOLE		
⊠ POST INDICATOR VALVE		



11001 BREN ROAD EAST

MINNETONKA, MN 55343

DOMINUM DEVELOPMENT AND ACQUISITIONS, LLC
2905 NORTHWEST BOULEVARD SUITE 150 FARMOUTH, MN 55441

LOUCKS
PLANNING CIVIL ENGINEERING LAND SURVEYING LANDSCAPE ARCHITECTURE ENVIRONMENTAL
7200 Hennock Lane, Suite 300 Maple Grove, MN 55868 763.424.5505 www.loucksinc.com

CADD QUALIFICATION

CADD files prepared by the Consultant for the project are a representation of the Consultant's professional services for the work on this project. The Consultant warrants that the CADD files are accurate and complete for the project. The Consultant's approval does not constitute a warranty of the accuracy or completeness of the CADD files. The Consultant shall not be responsible for any errors or omissions in the CADD files that are the result of the user's misuse of the software or the user's failure to update the files or to use the files in accordance with the user's manual.

SUBMITTAL/REVISIONS

07.14.17	SURVEY ISSUED
07.24.17	REVISED UTILITY ESM, PER COMMENTS
07.27.17	REVISED PER COMMENTS

PROFESSIONAL SIGNATURE

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
Max I. Stansibowski
License No. 49898
Date: 07.14.17

QUALITY CONTROL

Loucks Project No. 17298
Project Lead: MLS
Drawn By: NES
Checked By: MLS
Field Crew: DJP/REP

VICINITY MAP



ALTA/NSPS LAND TITLE SURVEY

1 OF 1

11001 BREN ROAD EAST

MINNETONKA, MN 55343

DOMINUM DEVELOPMENT AND ACQUISITIONS, LLC.

2905 NORTHWEST BOULEVARD SUITE 150 FLEMING, MN 55441

LOUCKS

PLANNING CIVIL ENGINEERING LAND SURVEYING LANDSCAPE ARCHITECTURE ENVIRONMENTAL

7200 Hennock Lane, Suite 300 Maple Grove, MN 55369 763.424.5505 www.loucksinc.com

CADD QUALIFICATION

CADD files prepared by the Consultant for the project are reviewed by the Consulting professional services for accuracy and completeness. The Consultant is not responsible for the accuracy of the project files unless otherwise stated on the contract. With the Consultant's approval, users may be permitted to create copies of the CADD files for their own use. The Consultant, its affiliates, or licensors do not warrant the accuracy, completeness, or reliability of the project files and the user shall be responsible for the accuracy, completeness, and reliability of the project files.

SUBMITTAL REVISIONS

DATE	REVISION
07/14/17	SURVEY ISSUED
07/20/17	TREE HEIGHTS
07/27/17	ADDITIONAL TREES

PROFESSIONAL SIGNATURE

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Signature: *Stephen Nicholson*
 License No. 49088
 Date: 07-14-17

QUALITY CONTROL

Loecks Project No. 17298
 Project Lead: MLS
 Drawn By: NBS
 Checked By: MSL
 Field Crew: DJP/REP

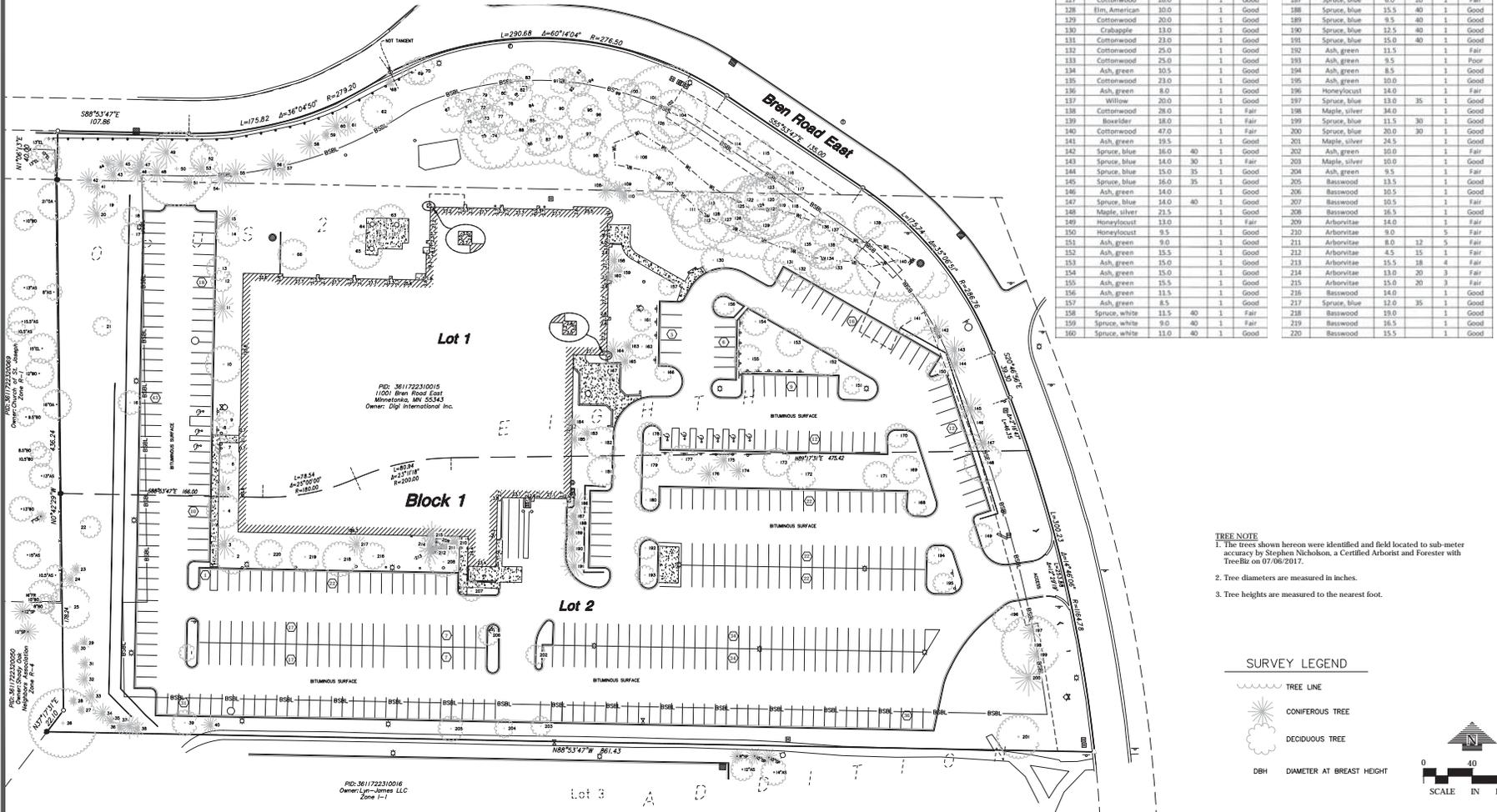
VICINITY MAP



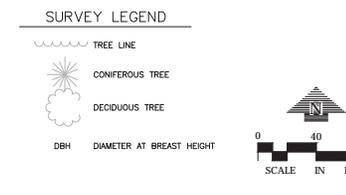
TREE INVENTORY SURVEY

1 OF 1

Tree #	Species	DBH	Height	Stems	Condition
1	Ash, green	8.0	1	Poor	
2	Basewood	19.5	1	Good	
3	Spruce, blue	15.5	35	1	Good
4	Ash, green	19.0	1	Good	
5	Spruce, blue	13.0	35	1	Good
6	Crabapple	9.5	1	Good	
7	Crabapple	9.0	1	Good	
8	Crabapple	9.0	1	Good	
9	Crabapple	9.0	1	Good	
10	Ash, green	20.0	1	Good	
11	Spruce, blue	16.0	30	1	Good
12	Maple, sugar	11.0	1	Fair	
13	Maple, sugar	12.5	1	Good	
14	Maple, sugar	13.0	1	Good	
15	Spruce, blue	14.0	30	1	Good
16	Maple, Norway	12.5	1	Good	
17	Maple, sugar	11.5	1	Good	
18	Crabapple	8.5	1	Good	
19	Basewood	14.0	1	Good	
20	Spruce, blue	14.0	30	1	Good
21	Honeylocust	9.5	1	Good	
22	Ash, green	11.0	1	Good	
23	Spruce, white	6.5	18	1	Fair
24	Spruce, white	8.0	25	1	Good
25	Cottonwood	24.0	1	Good	



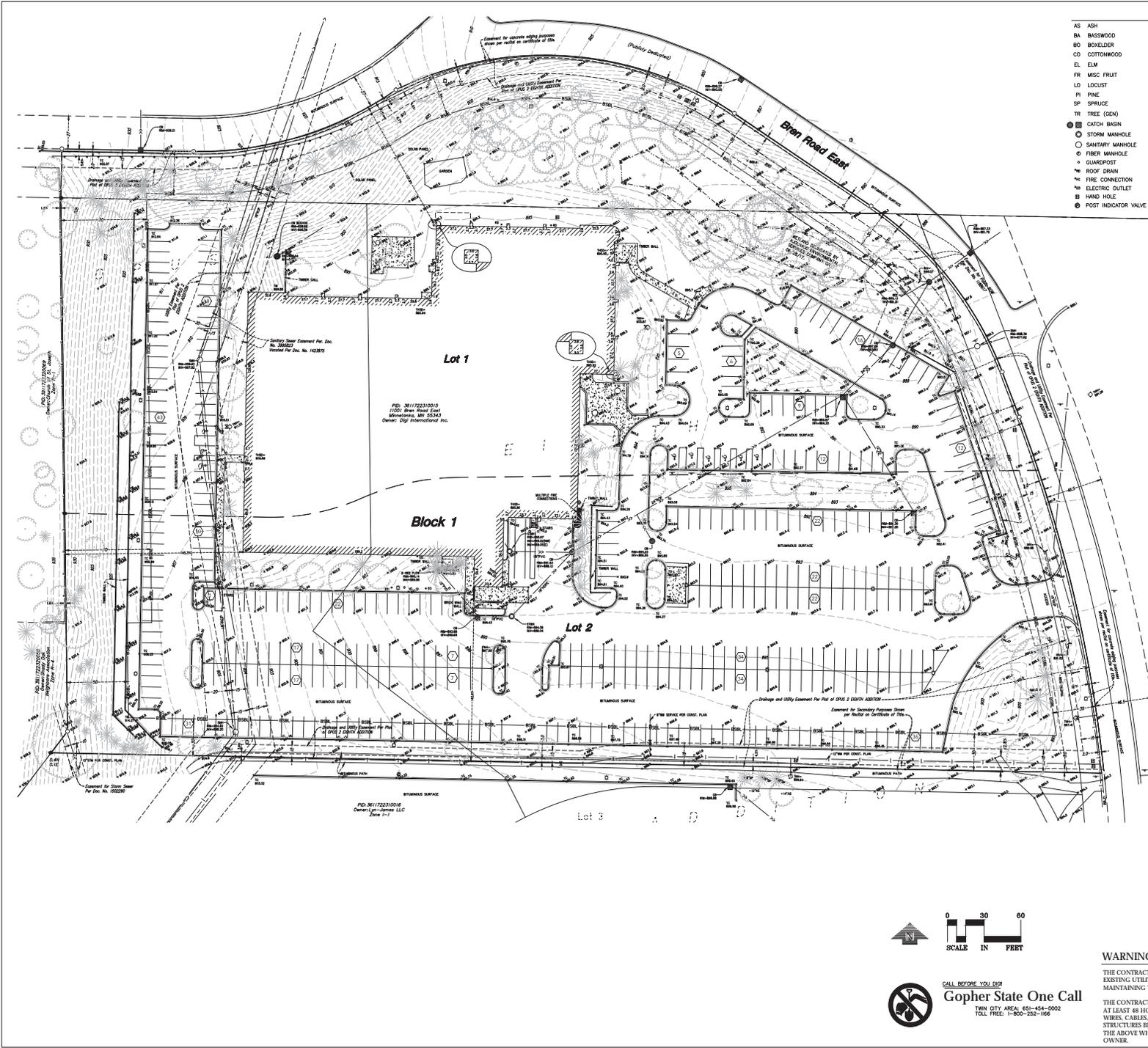
TREE NOTE
 1. The trees shown hereon are identified and field located to sub-meter accuracy by Stephen Nicholson, a Certified Arborist and Forester with TreeBe Inc. on 07/06/2017.
 2. Tree diameters are measured in inches.
 3. Tree heights are measured to the nearest foot.



07/14/17 07:21:2018 1034.MXD USER:J17298 CADD(DRAWING) Job: 17298.dwg Plot: 17298.dwg

PID: 3611722310016
 Owner: Lym-James LLC
 Zone: 1A1

PID: 3611722310015
 11001 Bren Road East
 Minnetonka, MN 55343
 Owner: Digi International Inc.



SURVEY LEGEND

AS ASH	○ HYDRANT	—●— STORM SEWER
BA BASSWOOD	□ GATE VALVE	—●— SANITARY SEWER
BO BOXELDER	□ POWER POLE	—●— WATERMAIN
CO COTTONWOOD	□ LIGHT POLE	—●— EOLVERT
EL ELM	○ YARD LIGHT	—●— UNDERGROUND CABLE TV
FR MISC FRUIT	—●— GUY WIRE	—●— UNDERGROUND ELECTRIC
LO LOCUST	—●— SIGN	—●— UNDERGROUND FIBER OPTIC
FR PINE	● SPOT ELEVATION	—●— UNDERGROUND GAS
SP SPRUCE	● ELECTRIC TRANSFORMER	—●— UNDERGROUND TELEPHONE
TR TREE (GEN)	□ TELEPHONE PEDESTAL	—●— UNDERGROUND UTILITY
● CATCH BASIN	□ ELECTRIC MANHOLE	—●— OVERHEAD UTILITY
● STORM MANHOLE	□ GAS VALVE	—●— FORCE MAIN
○ SANITARY MANHOLE	□ TELEPHONE MANHOLE	—●— CHAIN LINK FENCE
○ GUANOPOST	□ ELECTRIC METER	—●— CONCRETE CURB
● ROOF DRAIN	○ GAS METER	—●— CONCRETE
● FIRE CONNECTION	□ SPOUTS, 1/2" INCH X 1/4"	—●— GUARDRAIL
● ELECTRIC OUTLET	□ 2" X 2" WOOD ENCLOSURE	—●— GROUND
● HAND HOLE	□ 3" X 3" MARKED "L3" 4" DIA"	—●— CONTOUR
● POST INDICATOR VALVE	● DENOTES IRON MONUMENT FOUND	—●— CONTOUR
	● DENOTES NAIL MONUMENT FOUND	—●— CONTOUR
		—●— CONTOUR

BKV GROUP
 Architecture
 Interior Design
 Landscape Architecture
 Engineering

Boarman
 Kroos
 Vogel
 Group
 Inc.

222 North Second Street
 Minneapolis, MN 55401
 Telephone: 612-339-3752
 Facsimile: 612-339-6212
 www.bkvgroup.com
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 ENVIRONMENTAL

7300 Humboldt Lane, Suite 300
 Maple Grove, MN 55369
 763.424.5595
 www.louisinc.com

LOUIS PROJECT NO. 172886A

PROJECT TITLE
 BREN ROAD
 MASTER SITE
 DEVELOPMENT

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

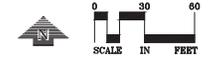
NOT FOR CONSTRUCTION

CERTIFICATION
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Ditch, PE
 49933 XX000000
 License Number Date
 DATE 03/30/2018
 DRAWN BY JED
 CHECKED BY J.P.D.
 COMMISSION NUMBER 1305123

SHEET TITLE
 EXISTING CONDITIONS

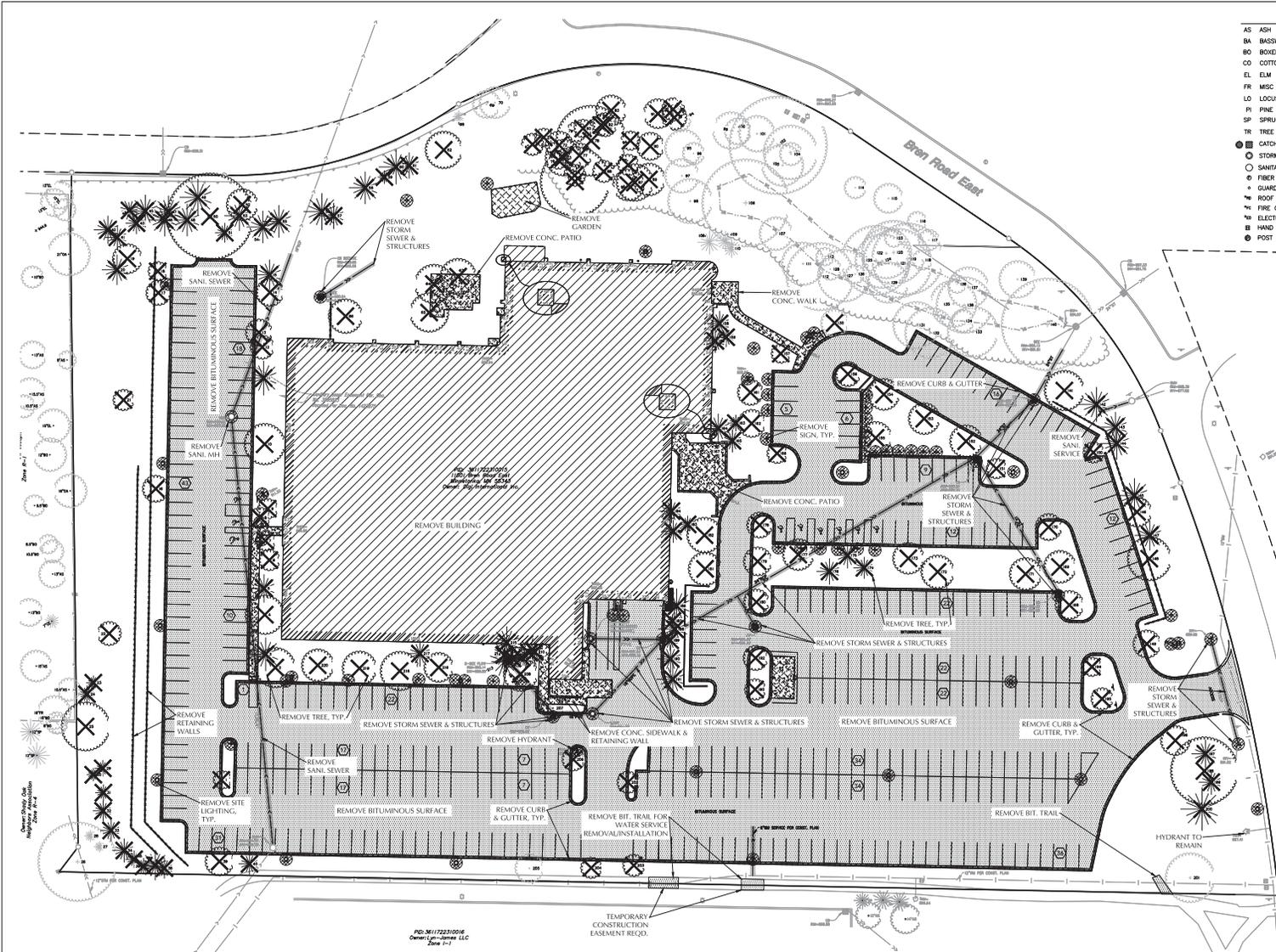
SHEET NUMBER
 C1-1



CALL BEFORE YOU DIG
 Gopher State One Call
 TWIN CITY AREA: 651-454-0002
 TOLL FREE: 1-800-252-1666

WARNING:
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.
 THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

BKV/BS-3042

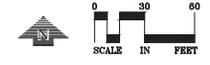


SURVEY LEGEND

AS ASH	○ HYDRANT	— STORM SEWER
BA BASSWOOD	□ GATE VALVE	— SANITARY SEWER
BO BOXELDER	□ POWER POLE	— WATERMAIN
CO COTTONWOOD	□ LIGHT POLE	— EOLVERT
EL ELM	○ YARD LIGHT	— UNDERGROUND CABLE TV
FR MISC FRUIT	— GUY WIRE	— UNDERGROUND ELECTRIC
LO LOCUST	— SIGN	— UNDERGROUND FIBER OPTIC
FR PINE	• SPOT ELEVATION	— UNDERGROUND GAS
SP SPRUCE	□ ELECTRIC TRANSFORMER	— UNDERGROUND TELEPHONE
TR TREE (GEN)	□ TELEPHONE PEDESTAL	— UNDERGROUND UTILITY
● CATCH BASIN	○ ELECTRIC MANHOLE	— OVERHEAD UTILITY
○ STORM MANHOLE	○ GAS VALVE	— FORCE MAIN
○ SANITARY MANHOLE	○ TELEPHONE MANHOLE	— CHAIN LINK FENCE
□ GUANOPOST	○ ELECTRIC METER	— CONCRETE CURB
□ ROOF DRAIN	○ GAS METER	— CONCRETE
□ FIRE CONNECTION	□ SPOTTES 1/2" DIA. X 14"	— GROUND
□ ELECTRIC OUTLET	□ 3/4" DIA. DEPARTMENT	— GUARDRAIL
□ HAND HOLE	□ 3/4" DIA. 1.5" DIA. 4"	— GUARDRAIL
□ POST INDICATOR VALVE	□ INDICATES IRON MONUMENT FOUND	— CONCRETE
	□ INDICATES NAIL MONUMENT FOUND	— BSBL - BUILDING SETBACK

DEMOLITION LEGEND

	REMOVE EXISTING BITUMINOUS SURFACE
	REMOVE EXISTING CONCRETE PAVING, PATIOS, ETC.
	REMOVE EXISTING BUILDINGS & ACCESSORIES
	REMOVE EXISTING CURB & GUTTER, RETAINING WALLS, WOOD FENCE, BILLBOARDS, ETC.
	REMOVE EXISTING UTILITIES
	REMOVE EXISTING MANHOLES, POWER POLES, LIGHT POLES, BOLLARDS, PARKING METERS, SIGNS, ETC.
	REMOVE EXISTING TREES
	REMOVE EXISTING GRAVEL SURFACE



CALL BEFORE YOU DIG
Gopher State One Call
 TWIN CITY AREA: 651-454-0002
 TOLL FREE: 1-800-252-1666

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BKV GROUP
 Architecture
 Interior Design
 Landscape Architecture
 Engineering
 Boarman
 Kroos
 Vogel
 Group
 Inc.
 222 North Second Street
 Minneapolis, MN 55401
 Telephone: 612-339-3752
 Facsimile: 612-339-6212
 www.bkvgroup.com
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 ENVIRONMENTAL
 7300 Humboldt Lane, Suite 300
 Maple Grove, MN 55369
 763.424.5505
 www.louisbks.com
 LOUIS PROJECT NO. 17288.0A

PROJECT TITLE
BREN ROAD MASTER SITE DEVELOPMENT

OWNER/DEVELOPER

ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

NOT FOR CONSTRUCTION

CERTIFICATION
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Dsch, PE	XXXXXX
49933	XXXXXX
License Number	Date
DATE	03/30/2018
DRAWN BY	PEJ
CHECKED BY	PJD
COMMISSION NUMBER	1303123

SHEET TITLE
DEMOLITION PLAN

SHEET NUMBER
C1-2

CONSULTANTS

LOUCKS
 PLANNING
 CIVIL ENGINEERING
 LAND SURVEYING
 LANDSCAPE ARCHITECTURE
 ENVIRONMENTAL
 7300 Humboldt Lane, Suite 300
 Maple Grove, MN 55369
 763.424.5505
 www.loucks.com

LOUCKS PROJECT NO. 17288.0A

PROJECT TITLE
**BREN ROAD
 MASTER SITE
 DEVELOPMENT**

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

**NOT FOR
 CONSTRUCTION**

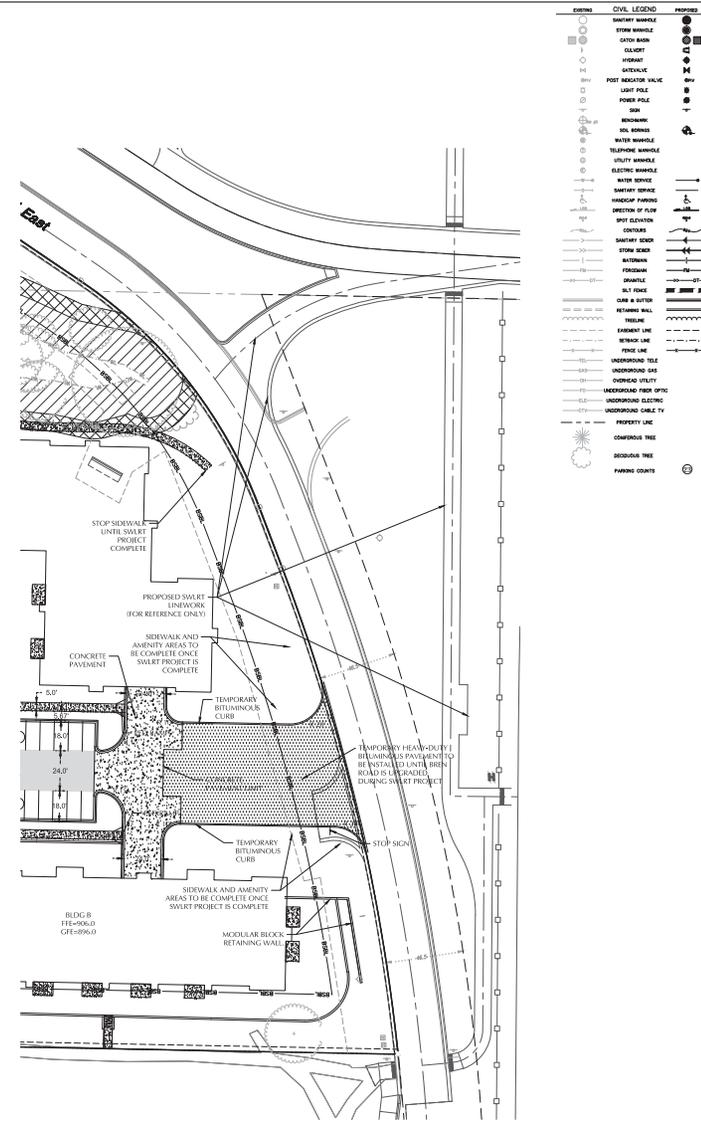
CERTIFICATION
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Dish, PE	
49933	XXXXXXXX
License Number	Date
DATE	03/30/2018
DRAWN BY	SKT
CHECKED BY	PJO
COMMISSION NUMBER	1303123
SHEET TITLE	

**TEMPORARY
 SITE
 PLAN**

SHEET NUMBER

C2-2

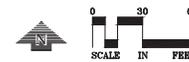


SITE NOTES

- ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN FOR THE DETAIL SHEETS AND STATE/LOCAL PAVING/TOR REQUIREMENTS.
- ACCESSIBLE PARKING AND ACCESSIBLE ROUTES SHALL BE PROVIDED PER CURRENT ADA STANDARDS AND LOCAL/STATE REQUIREMENTS.
- ALL CURB DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF WALL UNLESS OTHERWISE NOTED.
- TYPICAL FULL-SIZED PARKING STALL IS 8.5' X 18' WITH A 24' WIDE TWO-WAY DRIVE UNLESS OTHERWISE NOTED.
- ALL CURB RADIUS SHALL BE 3.0' UNLESS OTHERWISE NOTED.
- BITUMINOUS IMPREGNATED FIBER BOARD TO BE PLACED AT FULL DEPTH OF CONCRETE ADJACENT TO EXISTING STRUCTURES AND BEHIND CURB ADJACENT TO DRIVEWAYS AND SIDEWALKS.
- SEE SITE ELECTRICAL PLAN FOR SITE LIGHTING.
- BITUMINOUS PARKING STALLS TO BE LIGHT DUTY BITUMINOUS. DRIVE ISLES TO BE HEAVY DUTY BITUMINOUS.

SITE PLAN LEGEND

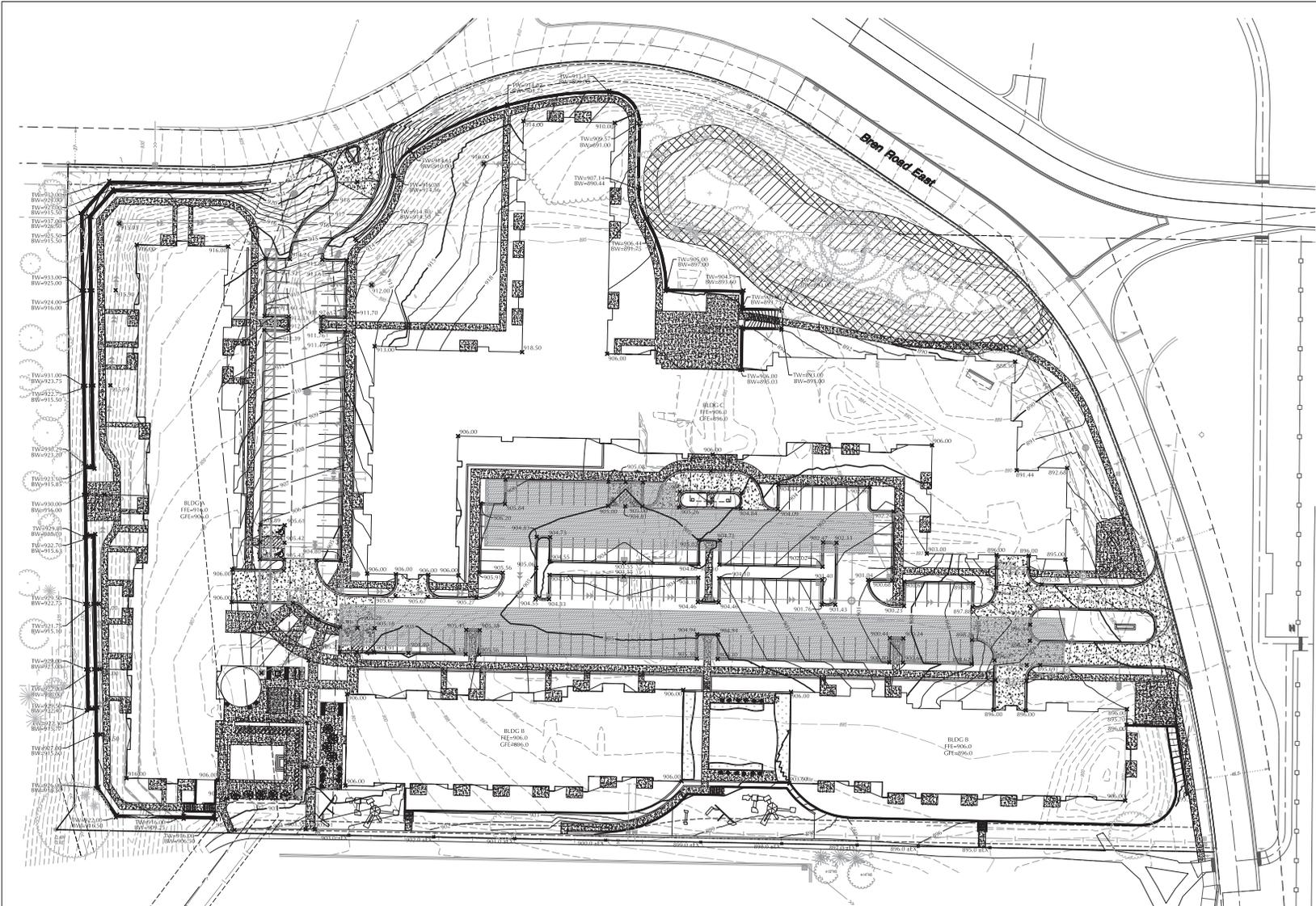
- CONCRETE SIDEWALK
- HEAVY DUTY BITUMINOUS PAVEMENT
- CONCRETE PAVEMENT
- TEMPORARY HEAVY DUTY BITUMINOUS PAVEMENT



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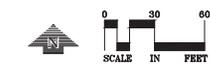
WARNING:

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EXISTING	CIVIL LEGEND	PROPOSED
○	SAFETY MANHOLE	○
○	STORM MANHOLE	○
○	CATCH BASIN	○
○	VALVE	○
○	POLE	○
○	POST SIGNOR VALVE	○
○	LIGHT POLE	○
○	POWER POLE	○
○	SDS	○
○	RECURRING	○
○	WATER MANHOLE	○
○	TELEPHONE MANHOLE	○
○	UTILITY MANHOLE	○
○	ELECTRIC MANHOLE	○
○	WATER SERVICE	○
○	SEWER SERVICE	○
○	MANHOLE FINISH	○
○	DIRECTION OF FLOW	○
○	NEW GUTTER	○
○	CONTOUR	○
○	SAFETY BENCH	○
○	STONE BENCH	○
○	POSSIBLE	○
○	SHRUBS	○
○	SHRUBS	○
○	RETAINING WALL	○
○	TRAILING	○
○	ROADWAY	○
○	UTILITY LINE	○
○	FIELD LINE	○
○	UNDERGROUND TILE	○
○	UNDERGROUND GAS	○
○	CORNER UTILITY	○
○	UNDERGROUND FIBER OPTIC	○
○	UNDERGROUND ELECTRIC	○
○	UNDERGROUND CABLE TV	○
○	PROPERTY LINE	○
○	CONSPICUOUS TREE	○
○	DEODOROUS TREE	○
○	PARKING COURTS	○

- GRADING, DRAINAGE & EROSION CONTROL NOTES**
- SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES, GUTTERFLOW LINE, FACE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 - CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.10 FEET. RM ELEVATIONS SHOWN ON PLANS DO NOT REFLECT SUMPED ELEVATIONS.
 - ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 4 INCHES OF TOP SOIL, AND SEED/MULCH OR SOIL. THESE AREAS SHALL BE WATERED/MAINTAINED BY THE CONTRACTOR UNTIL VEGETATION IS ESTABLISHED.
 - FOR SITE RETAINING WALLS "TW" EQUALS SURFACE GRADE AT TOP FACE OF WALL (NOT TOP OF WALL, "OW" EQUALS SURFACE GRADE AT WALL GRADE TRANSITION, AND "BM" EQUALS SURFACE GRADE AT BOTTOM FACE OF WALL (NOT BOTTOM OF BURIED WALL CORNERS).
 - STREETS MUST BE CLEANED AND SWEEP WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDELE FOR WEEKENDS AND HOLIDAYS. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
 - DUST MUST BE ADEQUATELY CONTROLLED.
 - SEE SWPPP FOR ADDITIONAL EROSION CONTROL NOTES AND REQUIREMENTS.
 - SEE UTILITY PLANS FOR WATER, STORM AND SANITARY SEWER INFORMATION.
 - SEE SITE PLAN FOR CURB AND BITUMINOUS TAPER LOCATIONS.
 - SEE LANDSCAPE PLAN FOR FINAL SITE STABILIZATION.



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 www.soucks.com

SOUCKS PROJECT NO. 17288.0A

PROJECT TITLE
 BRENN ROAD
 MASTER SITE
 DEVELOPMENT



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

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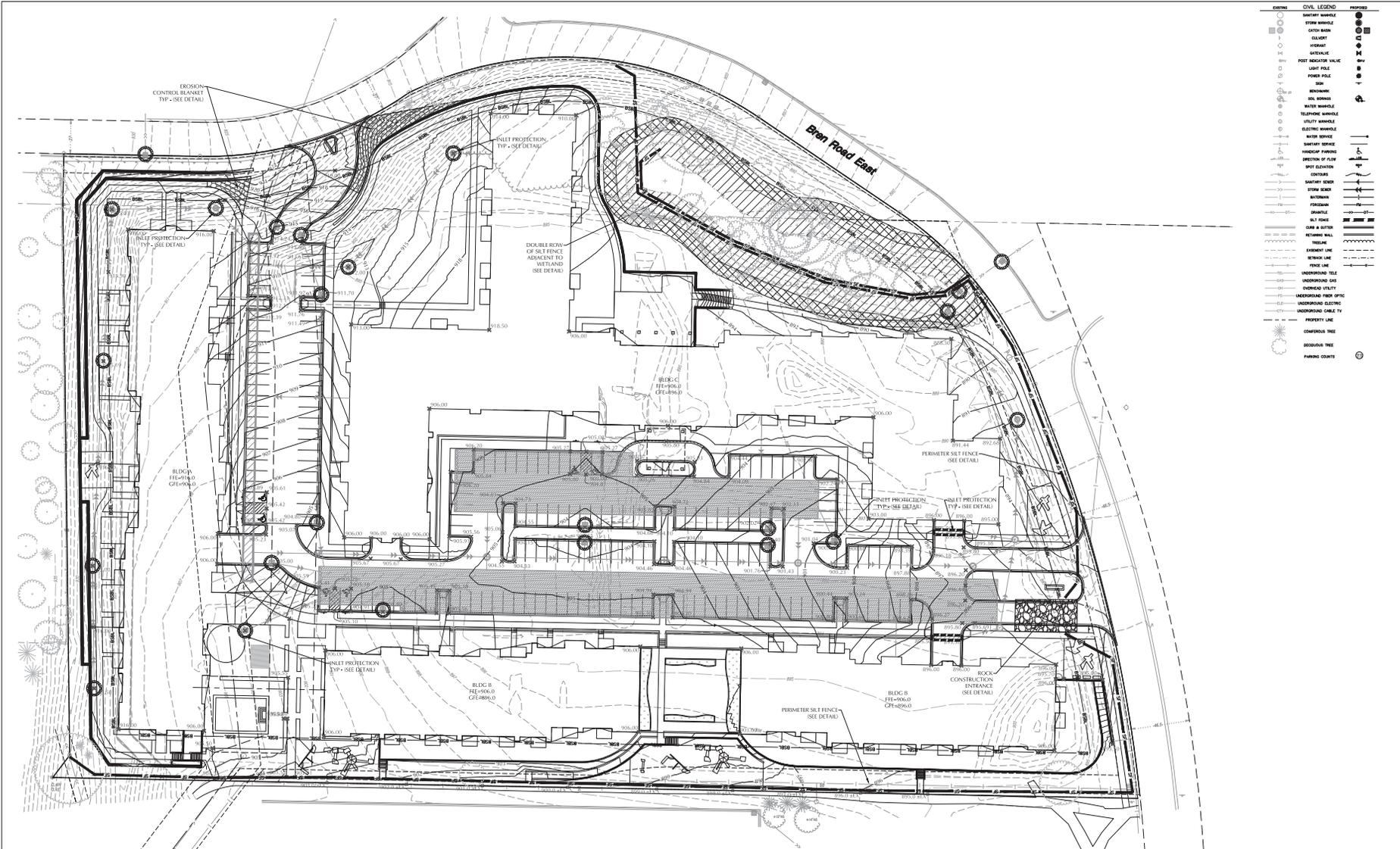
CERTIFICATION
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Ditch, PE	XXXXXX
49933	XXXXXX
License Number	Date
DATE	03/30/2018
DRAWN BY	BJT
CHECKED BY	PJO
COMMISSION NUMBER	1303123
SHEET TITLE	

GRADING PLAN

SHEET NUMBER
C3-1

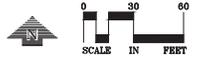
BKV/BS-2042



EXISTING	CIVIL LEGEND	PROPOSED
○	SAFETY MANHOLE	○
○	STORM MANHOLE	○
○	CATCH BASIN	○
○	COLLECTOR	○
○	INVERT	○
○	GATE VALVE	○
○	POST REDUCOR VALVE	○
○	LIGHT POLE	○
○	SOIL	○
○	WOODWAY	○
○	SOIL BRUSH	○
○	WATER MANHOLE	○
○	TELEPHONE MANHOLE	○
○	UTILITY MANHOLE	○
○	ELECTRIC MANHOLE	○
○	WATER SERVICE	○
○	SEWER SERVICE	○
○	WASTEWATER PIPING	○
○	DIRECTION OF FLOW	○
○	NEW GUTTER	○
○	CONTOUR	○
○	SAFETY BENCH	○
○	STONE BENCH	○
○	BATHING	○
○	POSSIBLE	○
○	UNPAVED	○
○	ALT. FENCE	○
○	NEW W/ GUTTER	○
○	RETAINING WALL	○
○	TRAILING	○
○	PERMITS LINE	○
○	UTILITY LINE	○
○	FIELD LINE	○
○	UNDERGROUND TELE	○
○	UNDERGROUND GAS	○
○	CORNER UTILITY	○
○	UNDERGROUND FIBER OPTIC	○
○	UNDERGROUND ELECTRIC	○
○	UNDERGROUND CABLE TV	○
○	PROPERTY LINE	○
○	CONSPICUOUS TREE	○
○	DECIDUOUS TREE	○
○	PARKING COURTS	○

SWPPP LEGEND

——	SILT FENCE
○	INLET PROTECTION
▨	EROSION CONTROL BLANKET



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LOUCKS PROJECT NO. 17288.0A

PROJECT TITLE
BREN ROAD MASTER SITE DEVELOPMENT

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

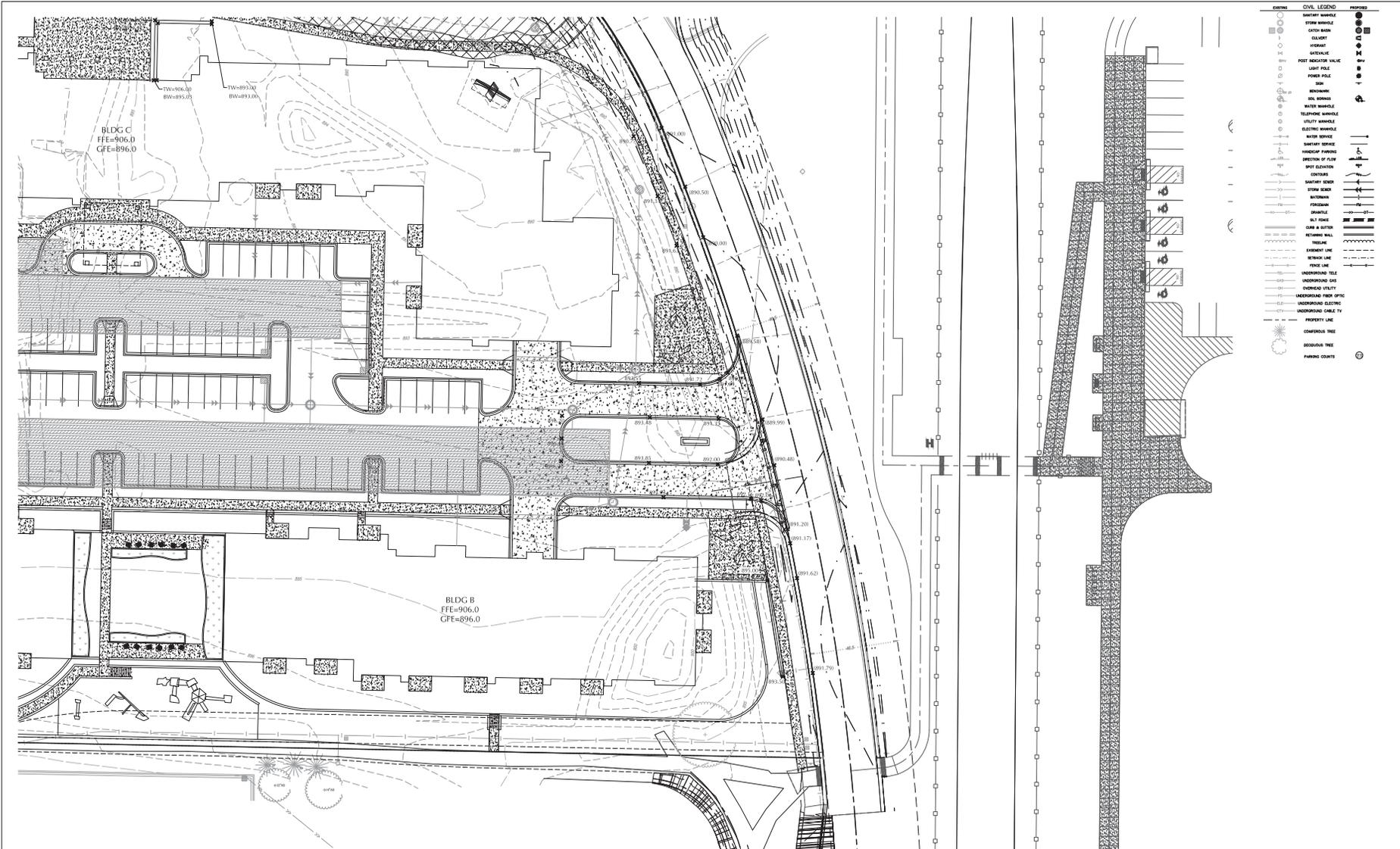
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PJ Dsch, PE	
49933	XXXXXX
License Number	Date
DATE	03/30/2018
DRAWN BY	SKT
CHECKED BY	PJO
COMMISSION NUMBER	1305123
SHEET TITLE	

SWPPP

SHEET NUMBER
C3-2

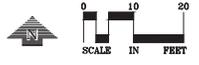


CIVIL LEGEND

EXISTING	PROPOSED
SAFETY VEHICLE	SAFETY VEHICLE
STORM MANHOLE	STORM MANHOLE
CATCH BASIN	CATCH BASIN
COLLECTOR	COLLECTOR
POCKET	POCKET
SEWER	SEWER
POST METER VALVE	POST METER VALVE
LIGHT POLE	LIGHT POLE
POWER POLE	POWER POLE
SOIL	SOIL
WATER MANHOLE	WATER MANHOLE
WATER	WATER
TELEPHONE MANHOLE	TELEPHONE MANHOLE
UTILITY MANHOLE	UTILITY MANHOLE
ELECTRIC MANHOLE	ELECTRIC MANHOLE
WATER SERVICE	WATER SERVICE
SEWER SERVICE	SEWER SERVICE
MANHOLE PAVING	MANHOLE PAVING
DIRECTION OF FLOW	DIRECTION OF FLOW
SEW	SEW
CONTOUR	CONTOUR
SAFETY SIGN	SAFETY SIGN
STONE SIGN	STONE SIGN
WALKWAY	WALKWAY
POSSIBLE	POSSIBLE
SHRUBS	SHRUBS
BILT FENCE	BILT FENCE
CONCRETE	CONCRETE
RETAINING WALL	RETAINING WALL
TRAILING	TRAILING
ROADWAY LINE	ROADWAY LINE
UTILITY LINE	UTILITY LINE
FIELD LINE	FIELD LINE
UNDERGROUND TELE	UNDERGROUND TELE
UNDERGROUND GAS	UNDERGROUND GAS
CORROSION UTILITY	CORROSION UTILITY
UNDERGROUND FIBER OPTIC	UNDERGROUND FIBER OPTIC
UNDERGROUND ELECTRIC	UNDERGROUND ELECTRIC
UNDERGROUND CABLE TV	UNDERGROUND CABLE TV
PROPERTY LINE	PROPERTY LINE
CONCRETE TREE	CONCRETE TREE
DECIDUOUS TREE	DECIDUOUS TREE
PARKING CENTS	PARKING CENTS

LEGEND

XXXX X	PROPOSED SPOT
XXXXX X	SWIRT PROPOSED SPOT
---	SWIRT CONTOUR



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LOUIS PROJECT NO. 17288.0A

PROJECT TITLE
BREN ROAD MASTER SITE DEVELOPMENT

OWNER/DEVELOPER



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COMMISSION NUMBER	1305123

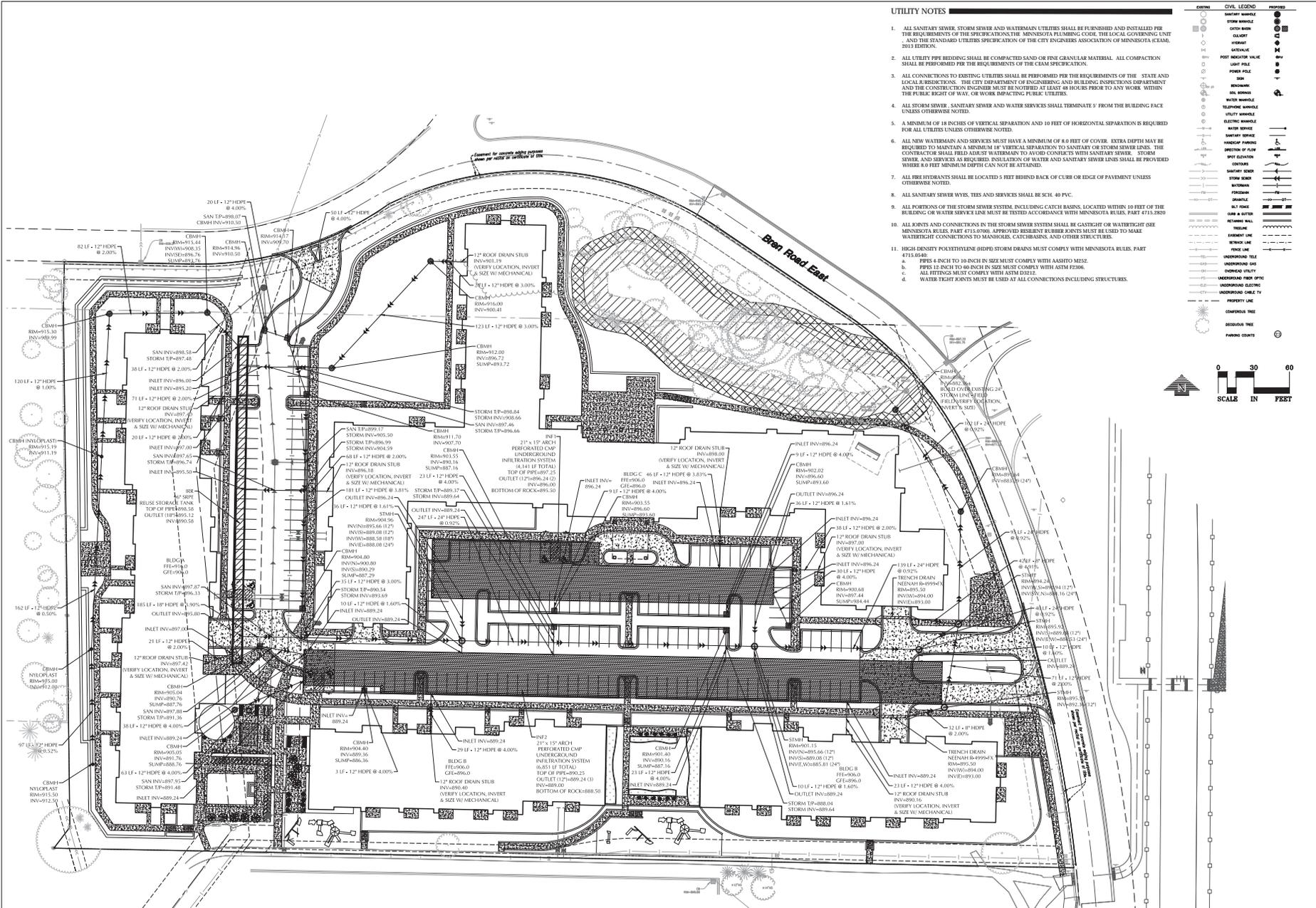
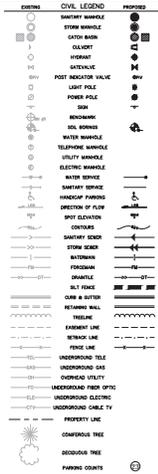
SHEET TITLE
FUTURE ENTRANCE PLAN

SHEET NUMBER
C3-4

BKV/BS-3042

UTILITY NOTES

1. ALL SANITARY SEWER, STORM SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS, THE MINNESOTA PLUMBING CODE, THE LOCAL GOVERNING UNIT AND THE STANDARD UTILITY SPECIFICATIONS OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
2. ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL COMPACTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE TEAM SPECIFICATION.
3. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
4. ALL STORM SEWER, SANITARY SEWER AND WATER SERVICES SHALL TERMINATE 5' FROM THE BUILDING FACE UNLESS OTHERWISE NOTED.
5. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
6. ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 8.0 FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONTACTS WITH SANITARY SEWER, STORM SEWER AND SERVICES AS REQUIRED. POLLUTION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE A 4 FEET MINIMUM DEPTH CAN NOT BE ATTAINED.
7. ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
8. ALL SANITARY SEWER WYS, TIES AND SERVICES SHALL BE SCH. 40 PVC.
9. ALL PORTIONS OF THE STORM SEWER SYSTEM, INCLUDING CATCH BASINS, LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED ACCORDANCE WITH MINNESOTA RULE, PART 4715.0200.
10. ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE CAST-IRON OR WATER-TIGHT GEE MINNESOTA RULES, PART 4715.0200. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATER-TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS AND OTHER STRUCTURES.
11. HIGH DENSITY POLYETHYLENE (HDPE) STORM DRAINS MUST COMPLY WITH MINNESOTA RULES, PART 4715.0540.
 - a. PIPES 18 INCH TO 36 INCH IN SIZE MUST COMPLY WITH ASTM D2688.
 - b. PIPES 18 INCH TO 36 INCH IN SIZE MUST COMPLY WITH ASTM F2916.
 - c. ALL FITTINGS MUST COMPLY WITH ASTM D2688.
 - d. WATER-TIGHT JOINTS MUST BE USED AT ALL CONNECTIONS INCLUDING STRUCTURES.



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LOUCKS PROJECT NO. 172860A

PROJECT TITLE
BREN ROAD MASTER SITE DEVELOPMENT

OWNER/DEVELOPER
DOMINIUM

ISSUE #	DATE	DESCRIPTION
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DATE	03/30/2018
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CHECKED BY	PJD
COMMISSION NUMBER	1303123
SHEET TITLE	

STORM SEWER PLAN

SHEET NUMBER
C4-2



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BKV/TS-2042

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763.424.5585
www.loucks.com

LOUCKS PROJECT NO. 17288.0A

PROJECT TITLE

**BREN ROAD
MASTER SITE
DEVELOPMENT**

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

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CONSTRUCTION**

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49933	XXXXXXXX
License Number	Date
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CHECKED BY	PJO
COMMISSION NUMBER	1303.123

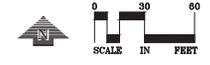
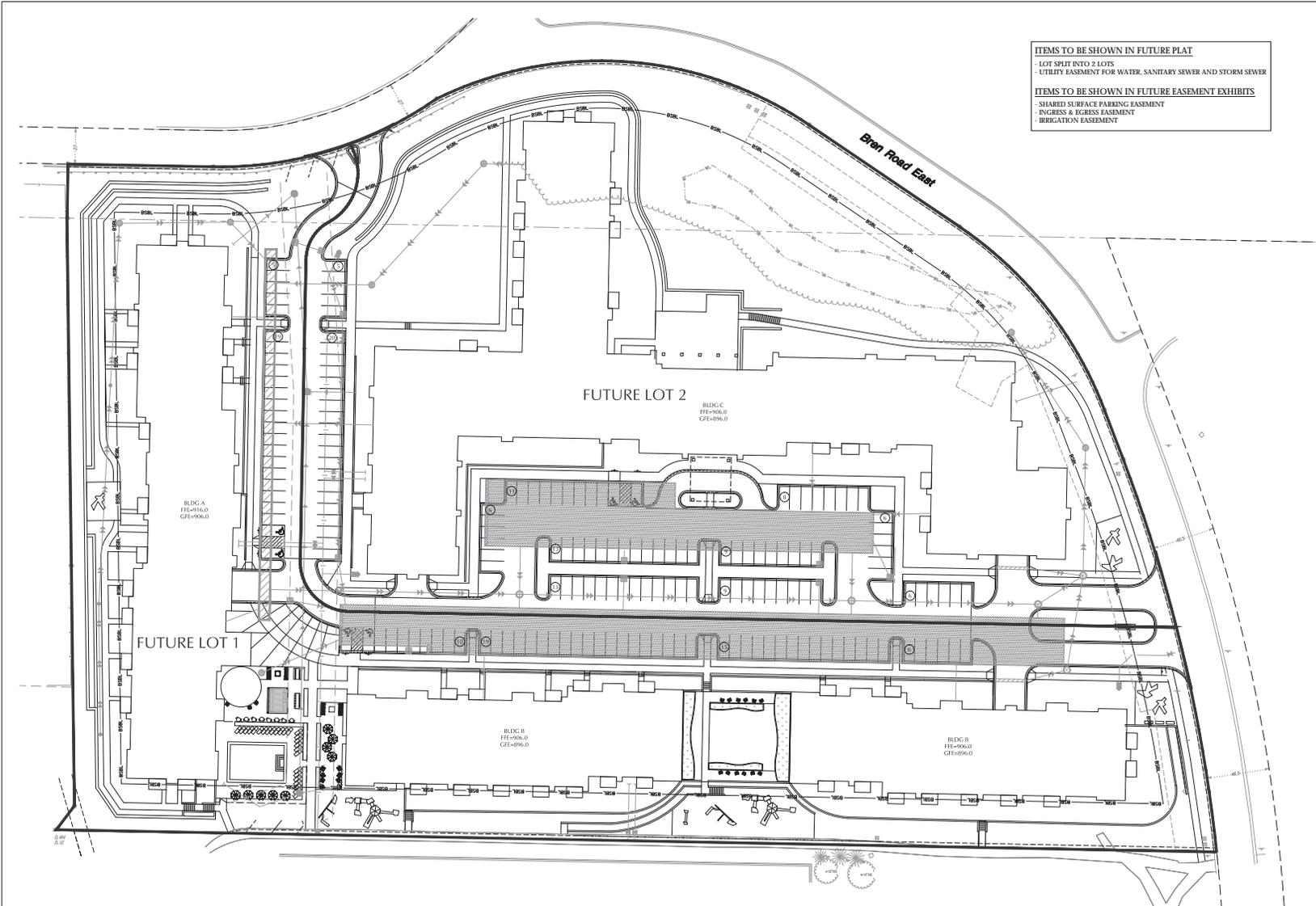
SHEET TITLE

**LOT SPLIT
EXHIBIT**

SHEET NUMBER

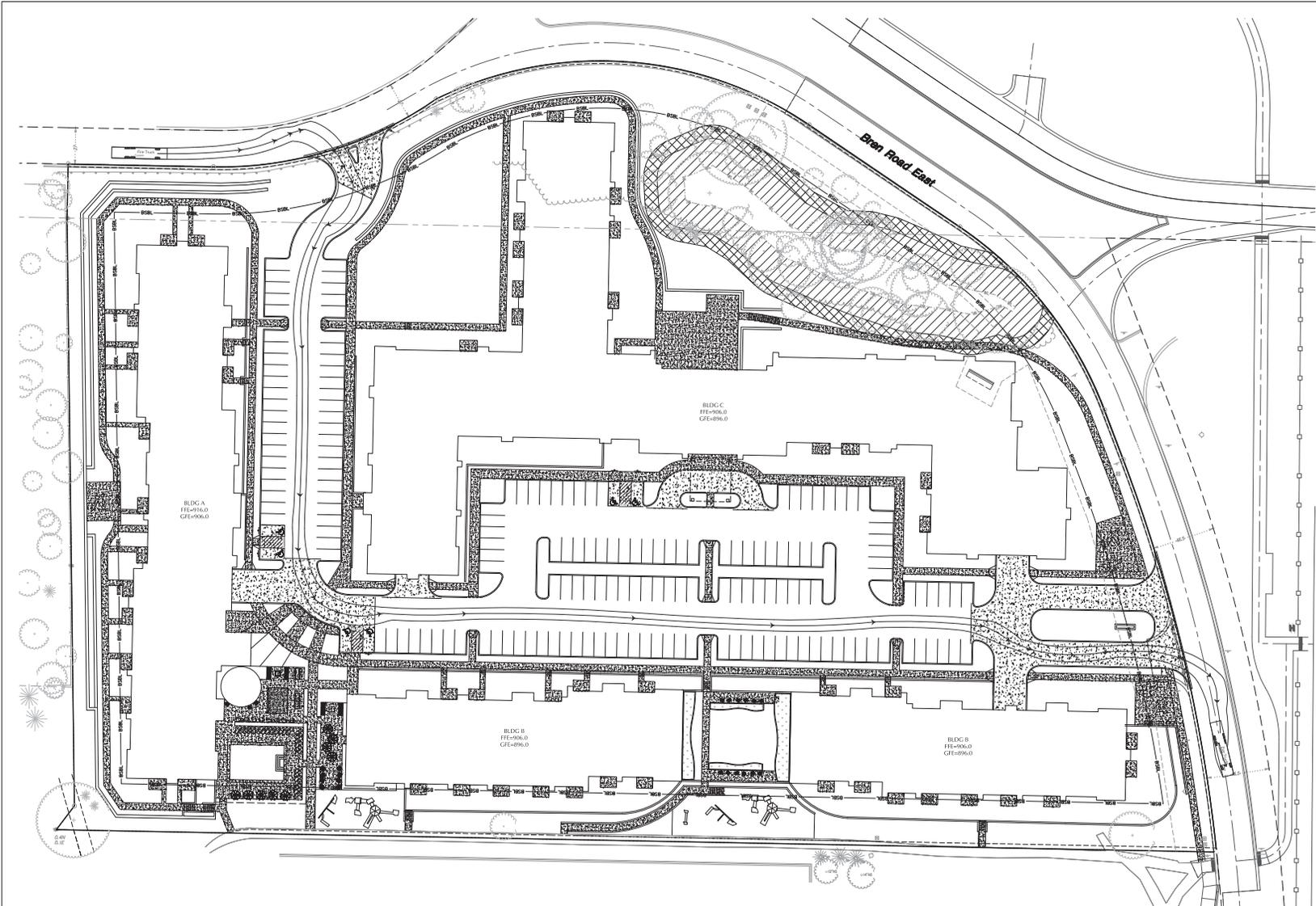
EXH-1

- ITEMS TO BE SHOWN IN FUTURE PLAT**
- LOT SPLIT INTO 2 LOTS
 - UTILITY EASEMENT FOR WATER, SANITARY SEWER AND STORM SEWER
- ITEMS TO BE SHOWN IN FUTURE EASEMENT EXHIBITS**
- SHARED SURFACE PARKING EASEMENT
 - INGRESS & EGRESS EASEMENT
 - IRRIGATION EASEMENT



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EXISTING	CIVIL LEGEND	PROPOSED
○	SAFETY MANHOLE	○
○	STORM MANHOLE	○
○	CATCH BASIN	○
○	COLLECTOR	○
○	POCKET	○
○	SEWER	○
○	POST METER VALVE	○
○	LIGHT POLE	○
○	POWER POLE	○
○	SDS	○
○	WATER MANHOLE	○
○	TELEPHONE MANHOLE	○
○	UTILITY MANHOLE	○
○	ELECTRIC MANHOLE	○
○	WATER SERVICE	○
○	SEWER SERVICE	○
○	WATERMETER PAVING	○
○	DIRECTION OF FLOW	○
○	NEW GUTTER	○
○	CONTOUR	○
○	SAFETY ISLAND	○
○	STONE ISLAND	○
○	BATHING	○
○	FOOTCURE	○
○	SMALLER	○
○	BILT FENCE	○
○	CONCRETE CURB	○
○	RETAINING WALL	○
○	TRAILING	○
○	ROADWAY LINE	○
○	UTILITY LINE	○
○	FENCE LINE	○
○	UNDERGROUND TELE	○
○	UNDERGROUND GAS	○
○	CONCRETE UTILITY	○
○	UNDERGROUND FIBER OPTIC	○
○	UNDERGROUND ELECTRIC	○
○	UNDERGROUND CABLE TV	○
○	PROPERTY LINE	○
○	CONTOUR TREE	○
○	DEVELOPER TREE	○
○	PARKING COURTS	○

BKV GROUP
 Architecture
 Interior Design
 Landscape Architecture
 Engineering

Boarman
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222 North Second Street
 Minneapolis, MN 55401
 Telephone: 612-339-3752
 Facsimile: 612-339-6212
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CONSULTANTS
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 ENVIRONMENTAL
 7300 Humboldt Lane, Suite 300
 Maple Grove, MN 55369
 763.424.5585
 www.loucks.com
 LOUCKS PROJECT NO. 17288.0A

PROJECT TITLE
 BRENN ROAD
 MASTER SITE
 DEVELOPMENT

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/09/2018	CITY SUBMITTAL

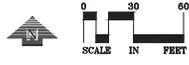
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CERTIFICATION
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Ditch, PE	49933	XXXXXX
License Number		Date
DATE	03/30/2018	
DRAWN BY	SKT	
CHECKED BY	PJD	
COMMISSION NUMBER	1303 123	

SHEET TITLE
 TRUCK
 TURNING PLAN

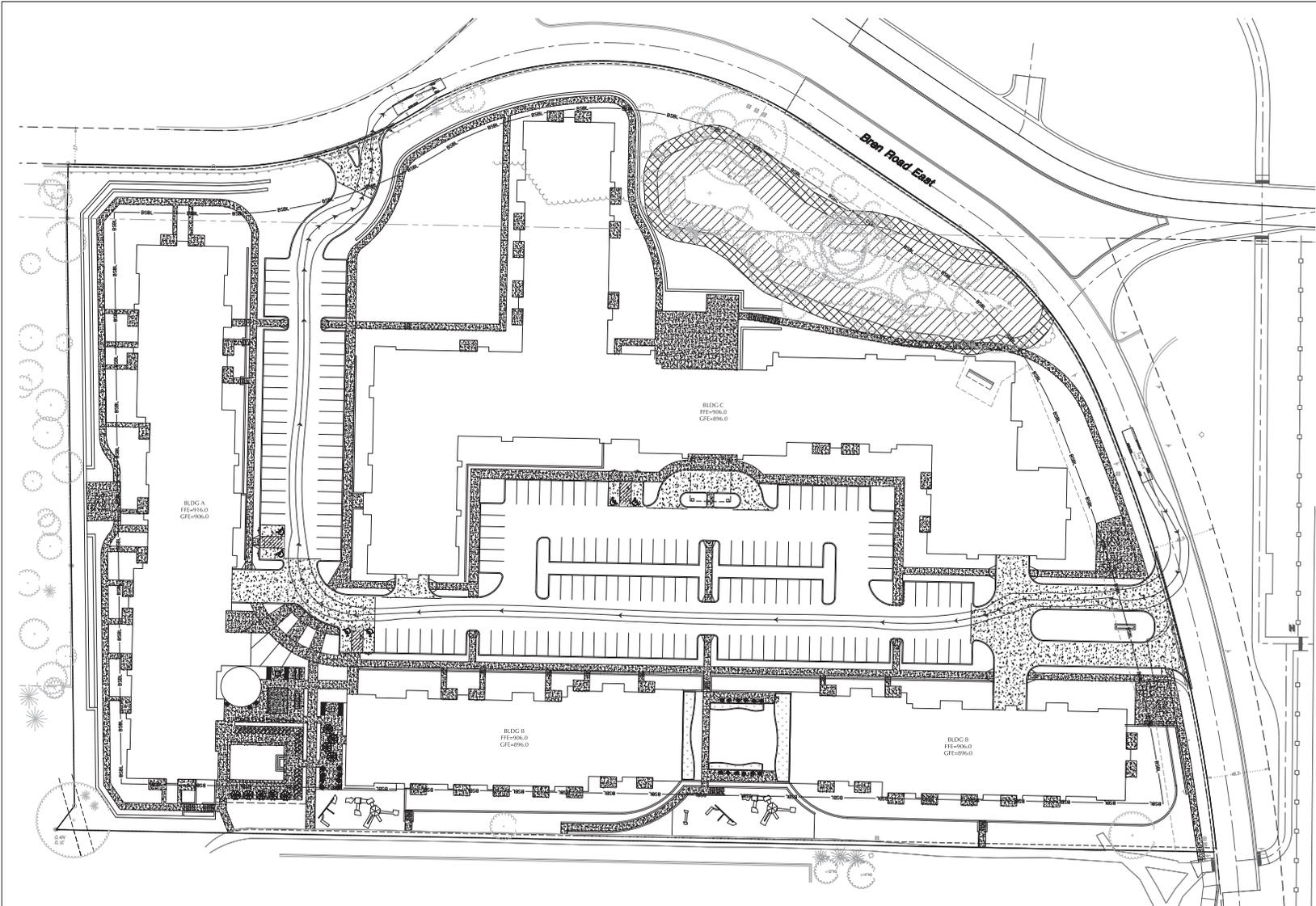
SHEET NUMBER
 EXH-2



CALL BEFORE YOU DIG
Gopher State One Call
 TWIN CITY AREA: 651-454-0002
 TOLL FREE: 1-800-252-1666

WARNING:
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.
 THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

BKV/TS-3042



EXISTING	CIVIL LEGEND	PROPOSED
○	SAFETY WALK	○
○	STONE WALK	○
○	CATCH BASIN	○
○	COLLECT	○
○	POCKET	○
○	STAIRCASE	○
○	POST SIGNOR VALVE	○
○	LIGHT POLE	○
○	POW. POLE	○
○	SOIL	○
○	RECURRING	○
○	TEL. WALK	○
○	WATER WALK	○
○	UTILITY WALK	○
○	ELECTRIC WALK	○
○	WATER SERVICE	○
○	SEWER SERVICE	○
○	MANHOLE FILING	○
○	DIRECTION OF FLOW	○
○	NEW GARDENS	○
○	CONTOUR	○
○	SAFETY SIGN	○
○	STONE SIGN	○
○	BATHING	○
○	FOODS	○
○	SMALL	○
○	BILT POLE	○
○	NEW WALK	○
○	RETURN WALL	○
○	TRAIL	○
○	ROADWAY	○
○	RETIC. LINE	○
○	FENCE LINE	○
○	UNDERGROUND TELE	○
○	UNDERGROUND GAS	○
○	CORNER UTILITY	○
○	UNDERGROUND FIBER OPTIC	○
○	UNDERGROUND CABLE TV	○
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LOUCKS PROJECT NO. 17288.0A

PROJECT TITLE
**BREN ROAD
 MASTER SITE
 DEVELOPMENT**

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
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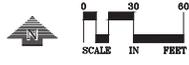
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PJ Dish, PE	49933	XXXXXX
License Number		Date
DATE	03/30/2018	
DRAWN BY	SKT	
CHECKED BY	PJD	
COMMISSION NUMBER	1303 123	

SHEET TITLE
**TRUCK
 TURNING PLAN**

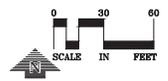
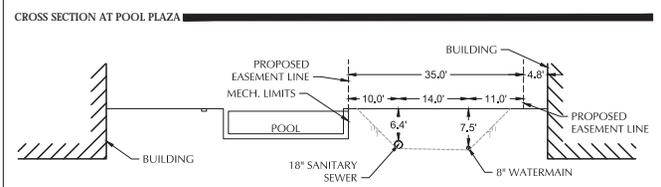
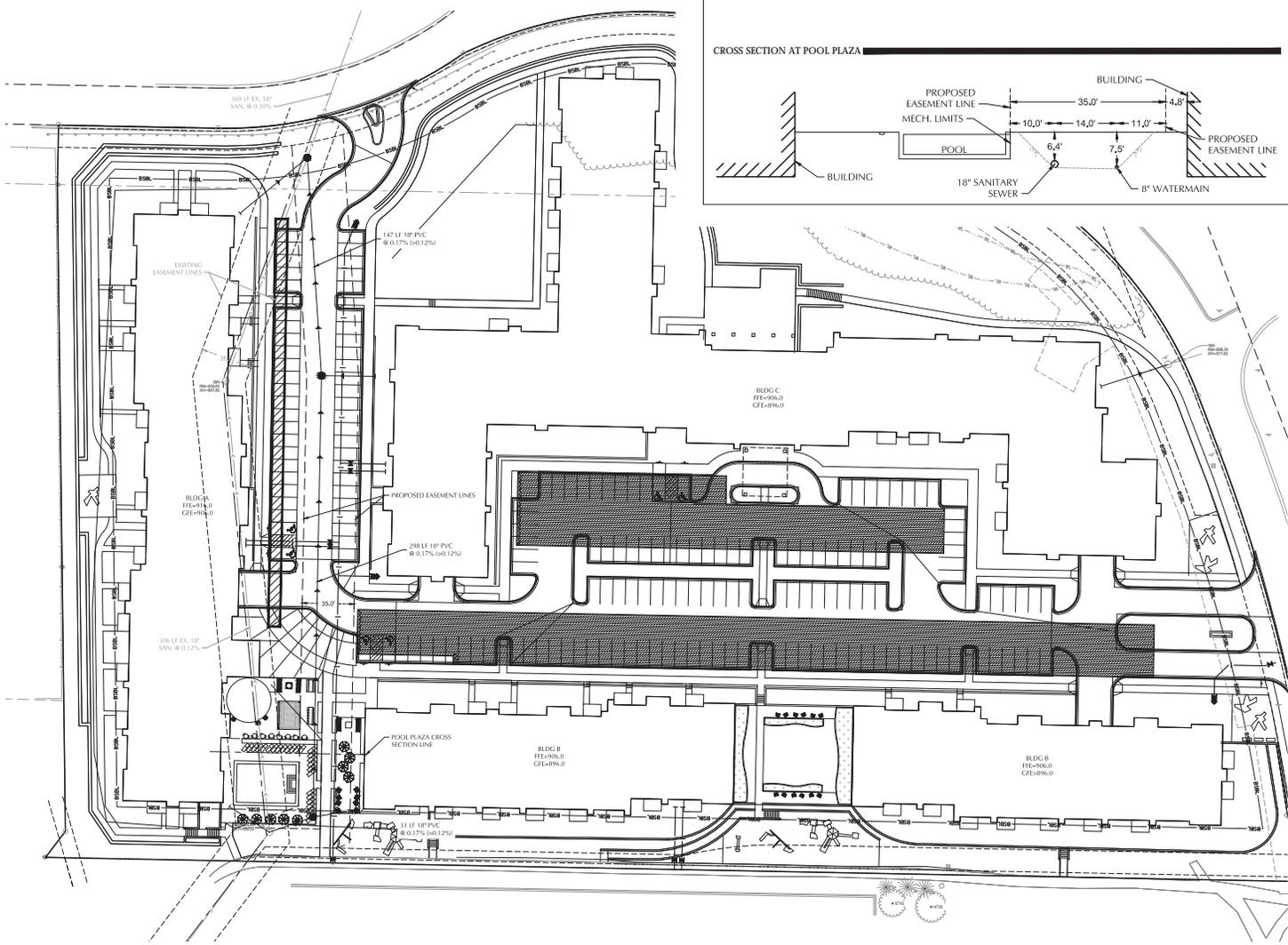
SHEET NUMBER
EXH-3



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 LOUCKS PROJECT NO. 17298.0A

PROJECT TITLE

**BREN ROAD
 MASTER SITE
 DEVELOPMENT**

OWNER/DEVELOPER



ISSUE #	DATE	DESCRIPTION
1	04/06/2018	CITY SUBMITTAL

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I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

PJ Dish, PE	
49933	XXXXXXXX
License Number	Date
DATE	03/26/2018
DRAWN BY	SKT
CHECKED BY	P.J.D.
COMMISSION NUMBER	1303 523

SHEET TITLE

**SANITARY
 SEWER EXHIBIT**

SHEET NUMBER

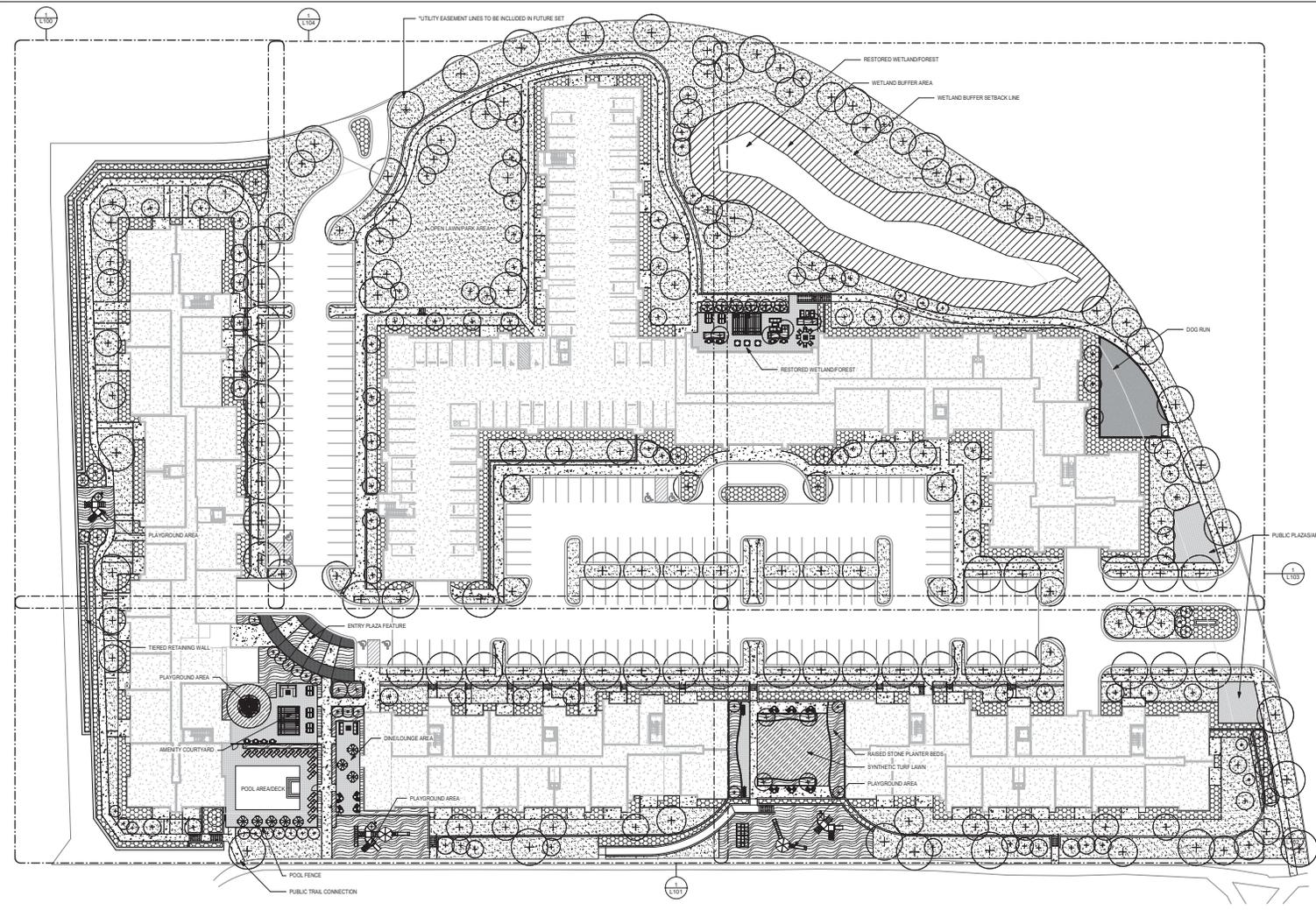
EXH-4

BKV/BS-3042

CONSULTANTS

PROJECT TITLE
**BREN ROAD
 MASTER SITE
 DEVELOPMENT**

ISSUE #	DATE	DESCRIPTION
		ISSUED
		CITY SUBMITTAL



GENERAL NOTES

- LANDSCAPE CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.
- LANDSCAPE CONTRACTOR SHALL VERIFY PLAN LAYOUT AND DIMENSIONS SHOWN AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR THE INTENT OF THE PROJECT.
- LANDSCAPE CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND/OR MATERIALS SUPPLIED.
- LANDSCAPE CONTRACTOR SHALL PROTECT EXISTING ROADS, CURB/GUTTERS, TRAILS, TREES, LINES AND SITE ELEMENTS DURING CONSTRUCTION OPERATIONS. DAMAGE TO SAME SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- LANDSCAPE CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF UNDERGROUND AND ABOVE-GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION PRIOR TO CONSTRUCTION.
- UNDERGROUND UTILITIES SHALL BE INSTALLED SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF ANY EXISTING TREES TO REMAIN.
- EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER ELEMENTS ARE BASED UPON INFORMATION SUPPLIED TO THE LANDSCAPE ARCHITECT BY OTHERS. LANDSCAPE CONTRACTOR SHALL VERIFY AND CORRECT PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT.
- ALIGNMENT AND GRADES OF THE PROPOSED WALKS, TRAILS AND/OR DRIVEWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO AVOID TREE REMOVAL AND GRADING. CHANGES IN THE ALIGNMENT AND GRADES MUST BE APPROVED BY THE LANDSCAPE ARCHITECT.
- LANDSCAPE CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN THE PLANT MATERIAL SELECTIONS AND OTHER SPECIFICATIONS WHICH MOST NEGATIVELY AFFECT PLANT ESTABLISHMENT. SURVIVAL OR WARRANTY (UNDEGRADED PLANT MATERIAL SELECTIONS OR SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- LANDSCAPE CONTRACTOR SHALL PREPARE AND SUBMIT REPRODUCIBLE AS-BUILT DRAWINGS OF LANDSCAPE INSTALLATION, IRRIGATION AND SITE IMPROVEMENTS UPON COMPLETION OF CONSTRUCTION INSTALLATION AND PRIOR TO PROJECT ACCEPTANCE.
- NO PLANTS WILL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- SOO AREAS DISTURBED DUE TO GRADING SHALL BE REPLACED, UNLESS NOTED OTHERWISE.
- WHERE SOO ADJUTS PAVED SURFACES, FINISHED GRADE OF SOODESS SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, BULK CURB, ETC.
- SOO SHALL BE Laid PARALLEL TO THE CONTOUR AND SHALL HAVE STAGGERED JOINTS ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES. SOO SHALL BE STAKED SECURELY.
- PROPOSED PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARDS FOR NURSERY STOCK AND (2011) UNLESS OTHERWISE SPECIFIED. DECIDUOUS SHRUBS SHALL HAVE AT LEAST 5 CANES AT THE SPECIFIED HEIGHT. ORNAMENTAL TREES SHALL HAVE NO Y-CROTCHES AND BEGON BRANCHING NO LOWER THAN 7 FEET ABOVE THE ROOT BALL. STREET AND BOULEVARD TREES SHALL BEGON BRANCHING NO LOWER THAN 7 ABOVE FINISHED GRADE.
- LANDSCAPE CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND/OR MATERIALS SUPPLIED.
- LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR Ongoing MAINTENANCE OF NEWLY INSTALLED MATERIALS AND THE SITE OF OWNER ACCEPTANCE. ACTS OF NEGLIGENCE OR DAMAGE INCURRED BY THE CONTRACTOR AFTER THE DATE OF OWNER ACCEPTANCE SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
- LANDSCAPE CONTRACTOR SHALL WARRANT NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF THE OWNER ACCEPTANCE. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.
- PLANTING AREAS (NOT OVER STRUCTURES) RECEIVING GROUND COVER, PERENNIALS, ANNUALS, AND/OR VEGET SHALL RECEIVE A MINIMUM OF 1" DEPTH OF PLANTING SOIL, CONSISTING OF AT LEAST 45 PARTS TOPSOIL, 45 PARTS SCREENED COMPOST OR MANURE AND 10 PARTS SAND.
- ANNUAL AND PERENNIAL PLANTING BEDS TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH WITH NO WEED BARRIER.
- SHRUB BED MASSINGS TO RECEIVE 4" DEEP SHREDDED HARDWOOD MULCH WITH PERM MATT WEED BARRIER.
- STEEL EDGER TO BE USED TO CONTAIN SHRUBS, PERENNIALS AND ANNUALS WHERE PLANTING BED MEETS SOO UNLESS OTHERWISE NOTED.
- REFER TO CIVIL FOR SITE DEMOLITION INFORMATION.
- REFER TO CIVIL FOR ADDITIONAL SITE GRADING AND UTILITY INFORMATION.
- IF A DISCREPANCY EXISTS BETWEEN THE NUMBER OF PLANTS SHOWN IN THE PLANT MATERIAL SCHEDULE AND THE PLAN, THE PLANT SCHEDULE SHALL PREVAIL.
- CONTRACTOR SHALL STAKE OUT LOCATION OF ALL PROPOSED TREES FOR APPROVAL BY LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

IRRIGATION NOTES

- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A PERFORMANCE IRRIGATION PLAN AND SPECIFICATIONS AS PART OF THE PROJECT AS PER THE PLAN SHEETS. THESE SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ORDER AND/OR INSTALLATION. IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY THAT SOODESS/SHRUBS AND PLANTED AREAS ARE IRRIGATED PROPERLY INCLUDING THOSE AREAS DIRECTLY ADJACENT TO BUILDING FOUNDATION.
- SHRUB AND PERENNIAL BEDS TO BE IRRIGATED WITH DRIP IRRIGATION. SOO TO BE IRRIGATED WITH SPRAY.
- LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER WITH A WATERLOGGAIN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANT MATERIALS SPECIFIED IN THE SCHEDULES.
- LANDSCAPE CONTRACTOR SHALL INSURE THAT SOIL CONDITIONS AND COMPACTON ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AROUND THE CONSTRUCTION SITE. UNDESIRABLE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK. IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO INSURE PROPER SURFACE AND SUBSURFACE DRAINAGE IN ALL PLANTING AREAS.
- COORDINATE IRRIGATION SLEEVING LOCATIONS WITH GENERAL CONTRACTOR.
- RAIN SENSORS TO BE INCLUDED WITHIN THE IRRIGATION DESIGN.
- IRRIGATION LIMITS TO EXTEND TO STREET BACK OF CURB.

SURVEY NOTES

- ALL EXISTING CONDITIONS SHOWN ON THIS DRAWING ARE BASED ON AN OWNER-FURNISHED SURVEY. BKV GROUP OFFERS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, FOR THE ACCURACY OR RELIABILITY OF THE INDICATED EXISTING CONDITIONS.
- THE CONTRACTOR SHALL FIELD VERIFY ALL CRITICAL EXISTING CONDITIONS INCLUDING, BUT NOT LIMITED TO, EXISTING BUILDING LOCATION, UTILITY LOCATION AND INVERT ELEVATIONS, AND EXISTING SITE GRADES PRIOR TO THE START OF WORK.
- ANY OBSERVED DEVIATIONS FROM CONDITIONS INDICATED ON THE DRAWINGS SHALL BE BROUGHT TO THE ARCHITECT OR LANDSCAPE ARCHITECT'S ATTENTION IMMEDIATELY UPON DISCOVERY. NO WORK SHALL PROCEED IN THE AREAS OF ANY DISCOVERED DEVIATIONS UNTIL THE DISCREPANCIES ARE RESOLVED.

IRRIGATION SUMMARY

- FLOOD SITE AREA: 3.5F / 1.6 ACRES
- LOT 1 SIZE EXISTING: 3.5F / 1.6 ACRES
- LOT 1 SIZE ADJUTS: 2.5F / 1.1 ACRES
- UNDERGROUND UTILITIES
- BUILDING FOOTPRINT (INCLUDES PAV AREA): 1.5F
- TOUR AREA (INCLUDES BUILDING FOOTPRINT): 1.5F
- JOB LANDSCAPE REG. 1.5F
- PERENNIALS AREA (ON SITE GRADES): 1.5F
- PERENNIALS AREA ON SITE (GREEN ROOF): 1.5F
- TOTAL PERENNIALS: 3.0F / 1.3 ACRES
- BIKE PARKING SPACES (ON SITE): 1 (1,000sq ft per unit)
- BIKE PARKING SPACES (IN GARAGE): 1

SITE PLAN NOTES

- NO BUILDING SIGNAGE PROPOSED AT THIS TIME.
- TRASH/RECYCLING WILL BE COLLECTED WITHIN THE BUILDING AT GROUND LEVEL. COLLECTION WILL OCCUR ON SITE WITH ENCLOSED LOADING AREAS BY SIGN.
- SNOW WILL BE REMOVED FROM SIDEWALKS. A LIMITED AMOUNT OF SNOW STORAGE IS AVAILABLE ALONG THE STREET BULKHEADS.

CERTIFICATION

NOT FOR CONSTRUCTION

DRAWN BY	RB
CHECKED BY	SH
CONSTRUCTION NUMBER	1300-24

LANDSCAPE TITLE SHEETS

SHEET NUMBER

L001

SITE - LANDSCAPE REFERENCE PLAN

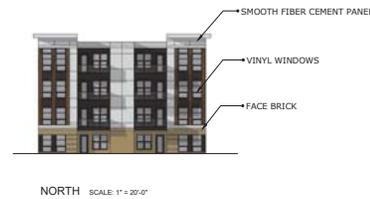
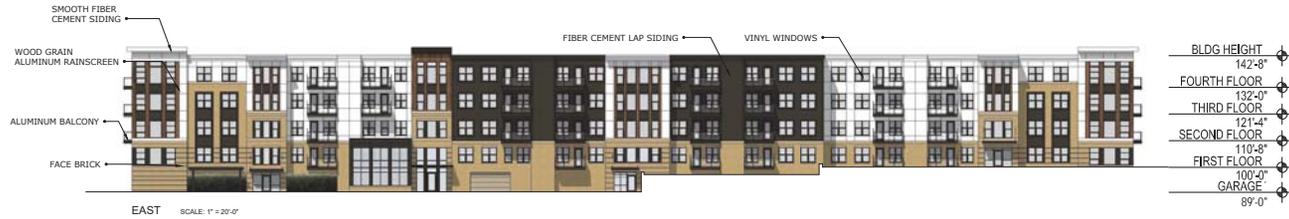
PROPOSED PLANT SCHEDULE:

QTY	SYM	COMMON NAME	SCIENTIFIC NAME	PLANTING SIZE
1	10	BLUE ANGEL HYDRANGEA	Hydrangea 'Blue Angel'	14 CONT.
1	11	SPRING BURNING BELL	Hydrangea 'Spring Burning'	14 CONT.
1	12	BLUE OAK GRASS	Hydrangea 'Blue Oak Grass'	14 CONT.
1	13	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	14	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	15	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	16	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	22	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	23	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	94	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	95	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	97	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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SHEET INDEX:

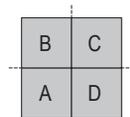
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1	11	SPRING BURNING BELL	Hydrangea 'Spring Burning'	14 CONT.
1	12	BLUE OAK GRASS	Hydrangea 'Blue Oak Grass'	14 CONT.
1	13	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	14	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	19	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	21	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	22	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	23	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
1	24	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	28	PALE PINK HYDRANGEA	Hydrangea 'Pale Pink'	14 CONT.
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1	65	PALE PINK HYDRANGEA</		

ISSUE #	DATE	DESCRIPTION
	04/02/18	CITY SUBMITTAL



**NOT FOR
CONSTRUCTION**

DRAWN BY	Author
CHECKED BY	Checker
DATE/SCALE/REVISION	Project Number

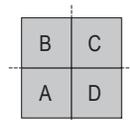
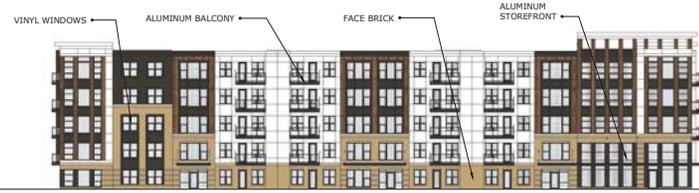


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**NOT FOR
CONSTRUCTION**

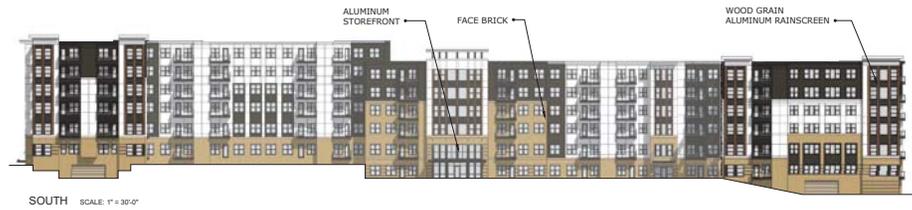
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CHECKED BY	Checker
COORDINATOR/REVISOR	Project Number

BLDG HEIGHT	153'-4"
FIFTH FLOOR	142'-8"
FOURTH FLOOR	132'-0"
THIRD FLOOR	121'-4"
SECOND FLOOR	110'-8"
FIRST FLOOR	100'-0"
GARAGE	89'-0"



**BREN ROAD
BUILDING C**

ISSUE #	DATE	DESCRIPTION
	04/02/18	CITY SUBMITTAL

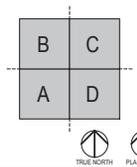


**NOT FOR
CONSTRUCTION**

DRAWN BY	Author
CHECKED BY	Checker
CONSTRUCTION NUMBER	Project Number

ELEVATIONS

A403



PROJECT TITLE
**BREN ROAD
BUILDING A**

TABLE #	DATE	DESCRIPTION
	04/20/18	CITY SUBMITTAL

CERTIFICATION

NOT FOR
CONSTRUCTION

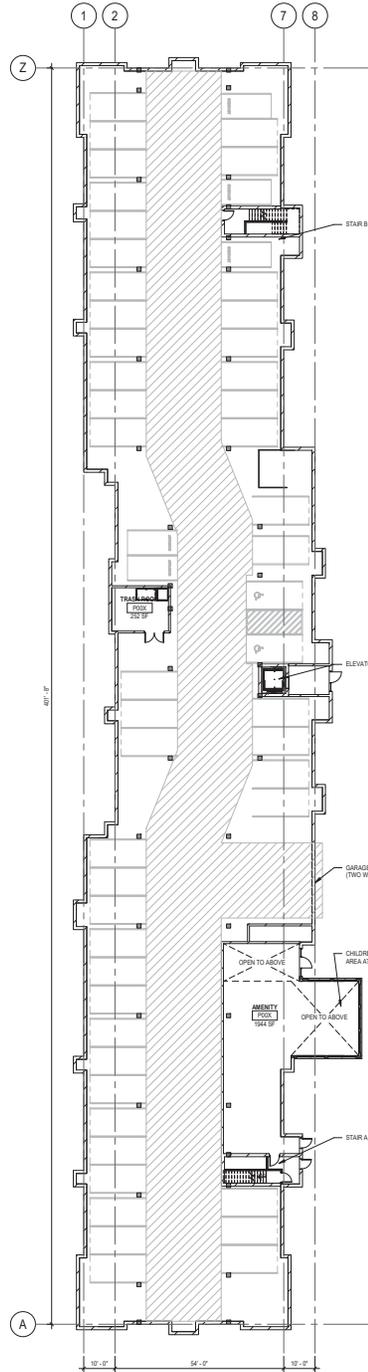
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CHECKED BY	Chad
COMMISSION NUMBER	
PROJECT NUMBER	

SHEET TITLE

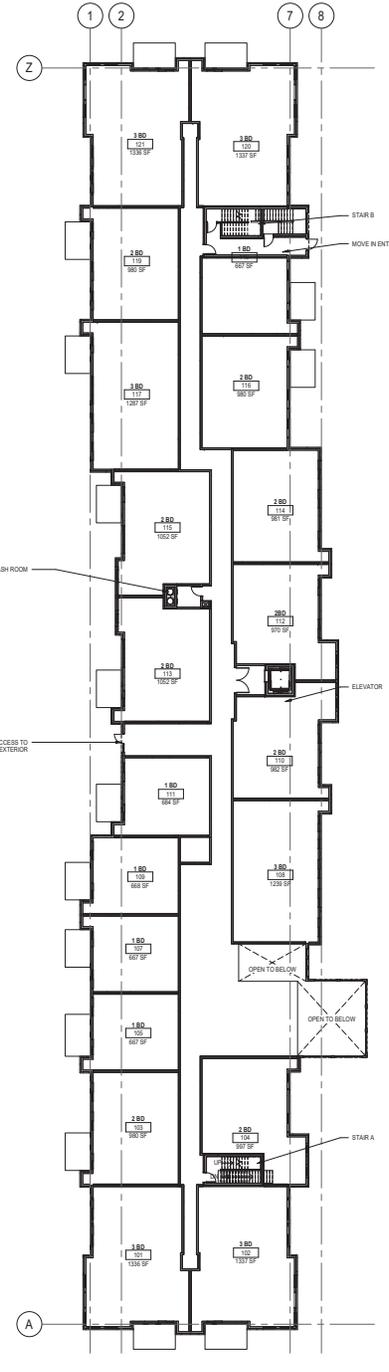
LEVELS P1 - 4
REFERENCE
PLANS

SHEET NUMBER

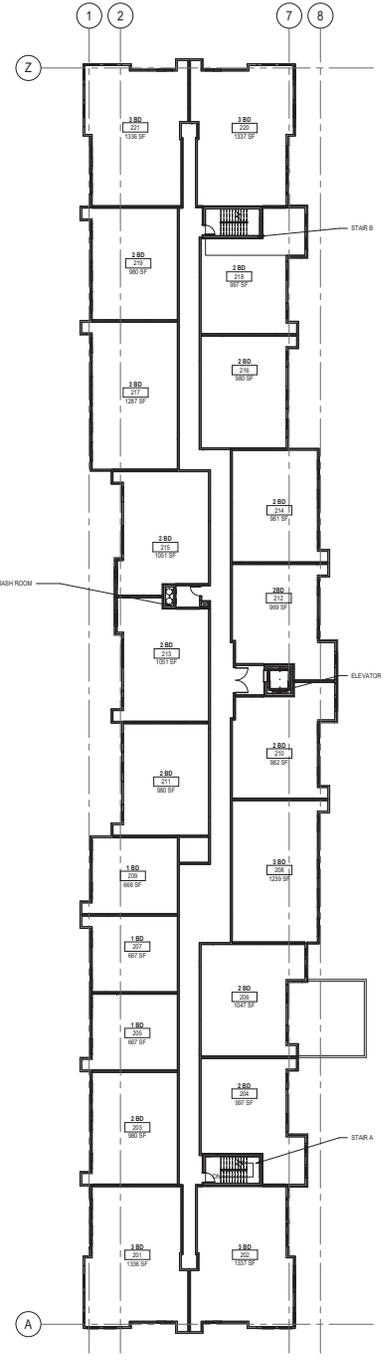
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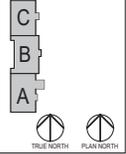
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11/16/17 11:12



2 LEVEL 1 - REFERENCE PLAN
11/16/17 11:12



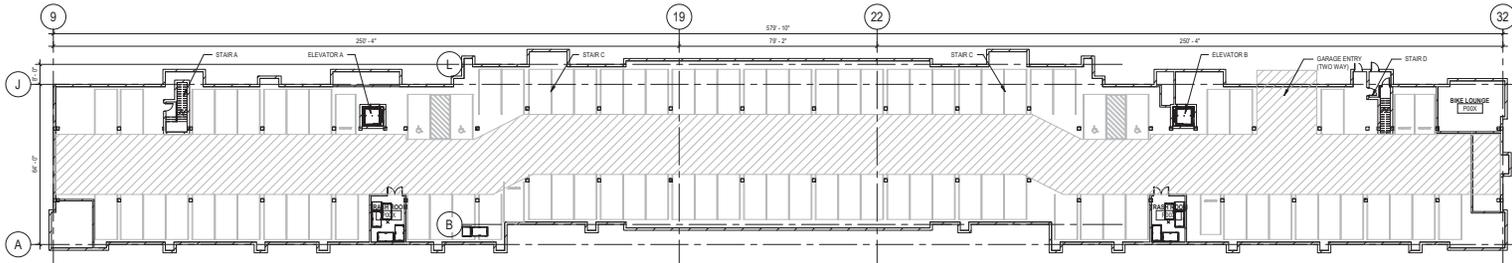
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11/16/17 11:12



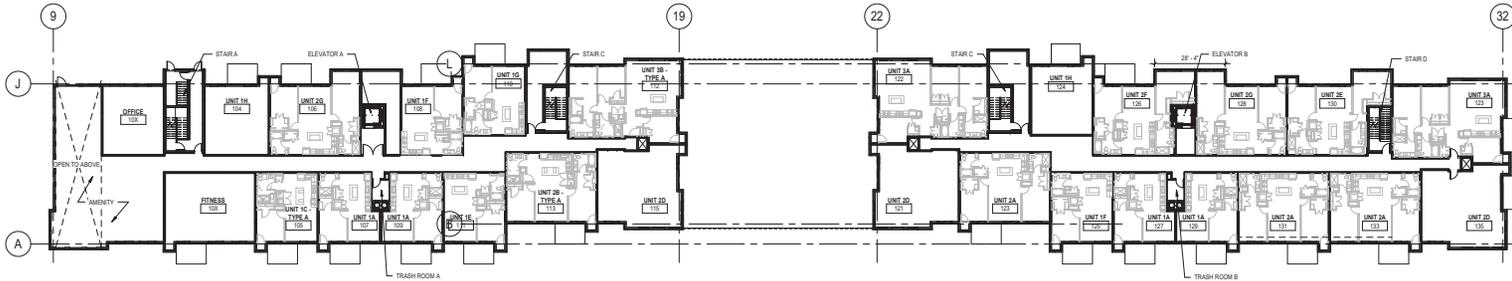
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**NOT FOR
CONSTRUCTION**

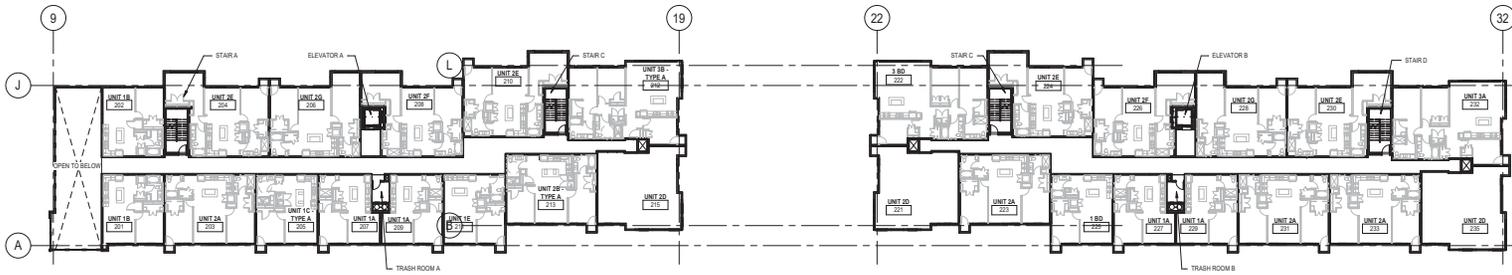
DRAWN BY	Author
CHECKED BY	Checker
COORDINATION NUMBER	Project Number



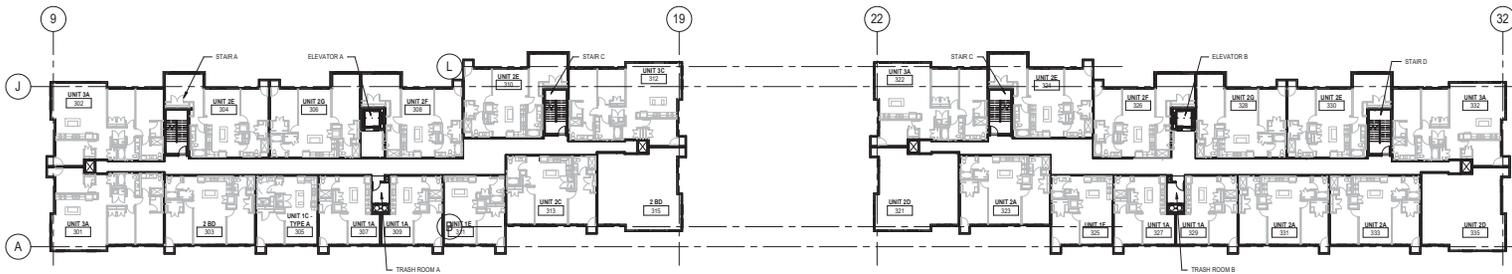
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1" = 20' 0"



2 LEVEL 1 - REFERENCE PLAN
1" = 20' 0"



3 LEVEL 2 - REFERENCE PLAN
1" = 20' 0"



4 LEVELS 3-5 - REFERENCE PLAN
1" = 20' 0"

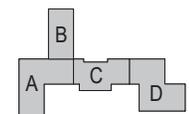
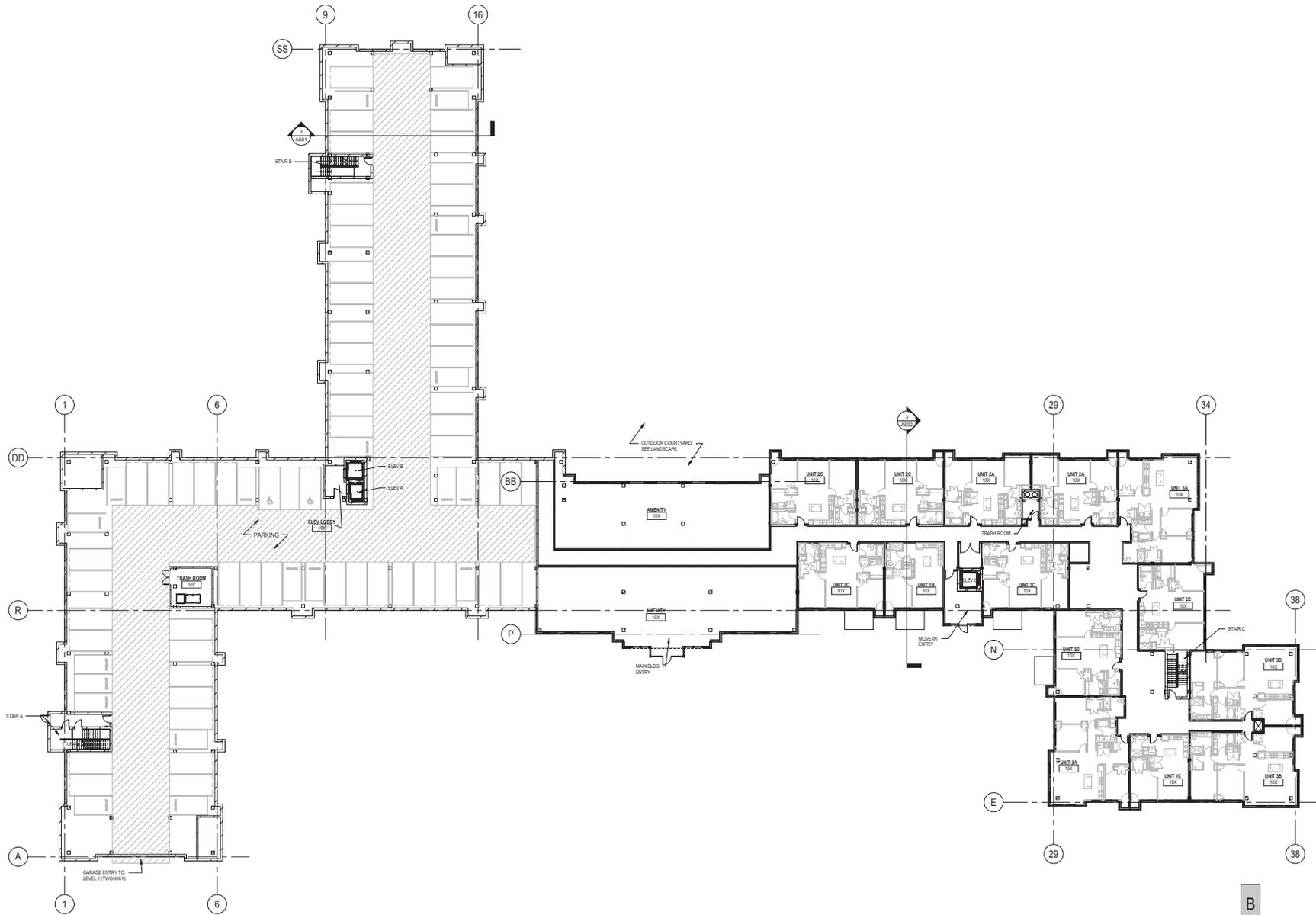
A B C D



PROJECT TITLE

**BREN ROAD
BUILDING C**

TABLE #	DATE	DESCRIPTION



CERTIFICATION

**NOT FOR
CONSTRUCTION**

DRAWN BY	Juliv
CHECKED BY	Chaker
COMMISSION NUMBER	199-136

SHEET TITLE
**LEVEL 1 -
OVERALL FLOOR
PLAN**

SHEET NUMBER

A101

PROJECT TITLE

**BREN ROAD
BUILDING C**

TABLE #	DATE	DESCRIPTION
000001A		CITY SUBMITTAL

CERTIFICATION

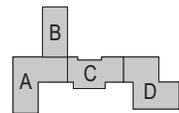
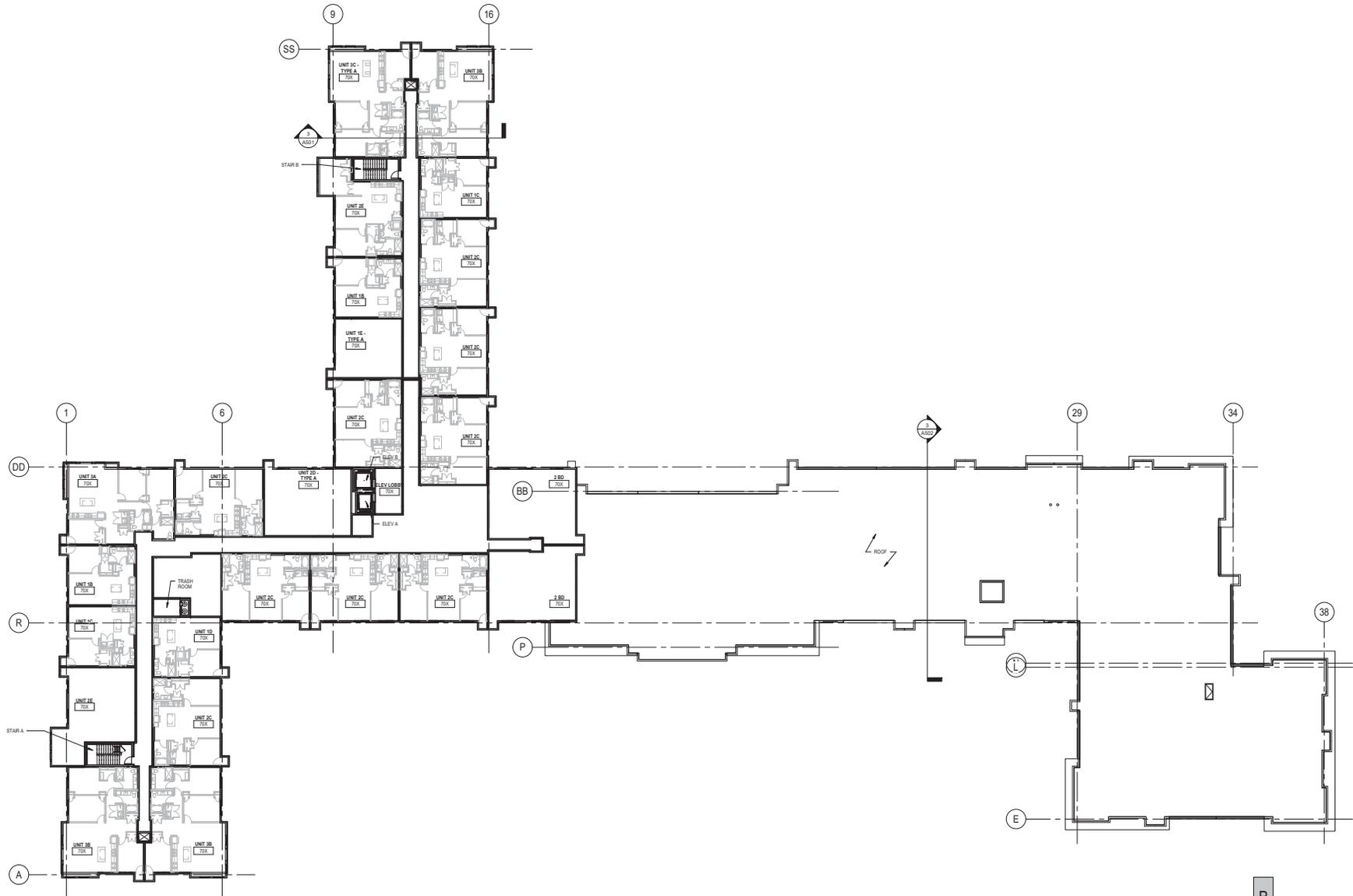
**NOT FOR
CONSTRUCTION**

DRAWN BY	Judy
CHECKED BY	Chae
COMMISSION NUMBER	199 136

SHEET TITLE
**LEVEL 7
OVERALL FLOOR
PLAN**

SHEET NUMBER

A107





BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

EXISTING SITE CONTEXT

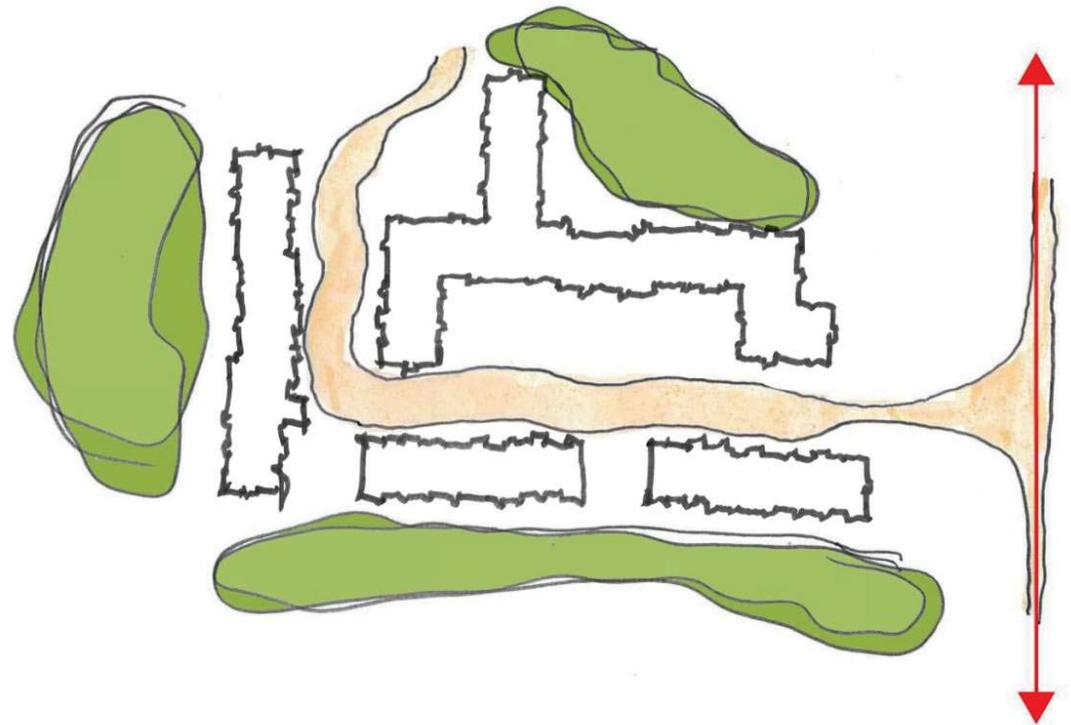
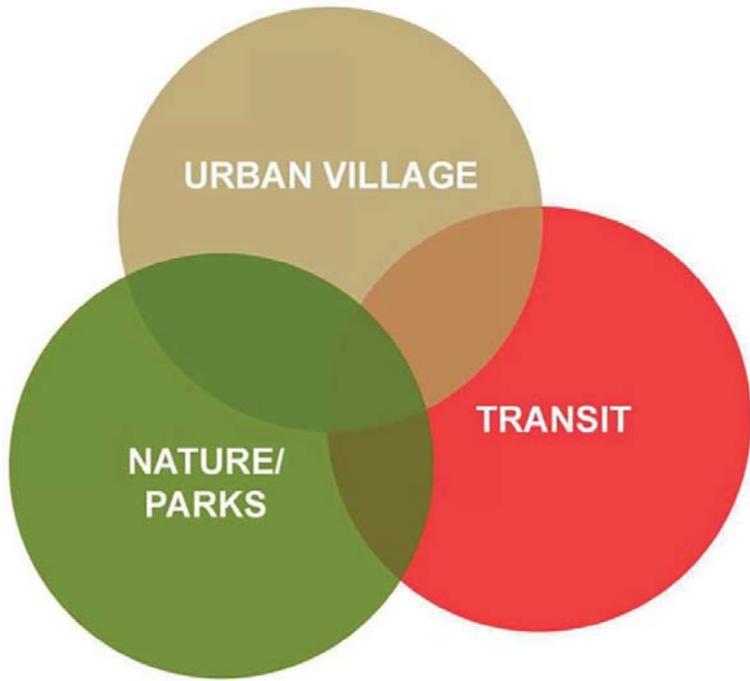




BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

EXISTING SITE INFLUENCES

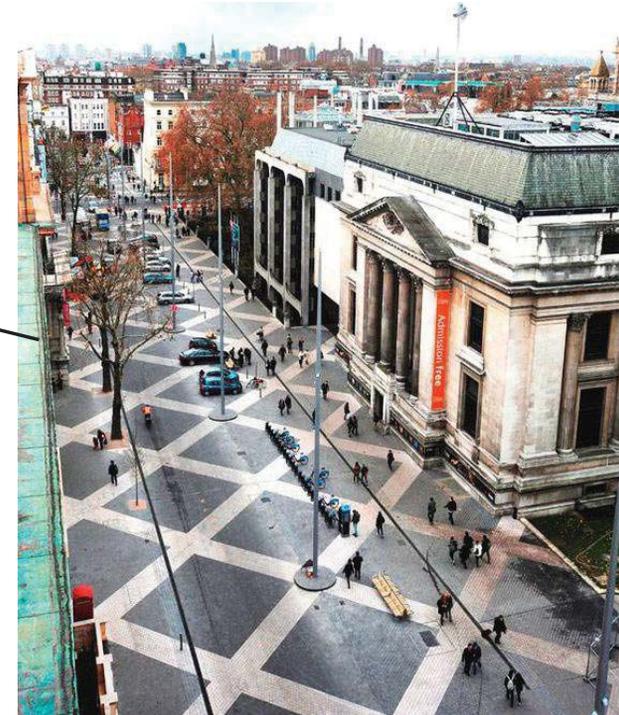
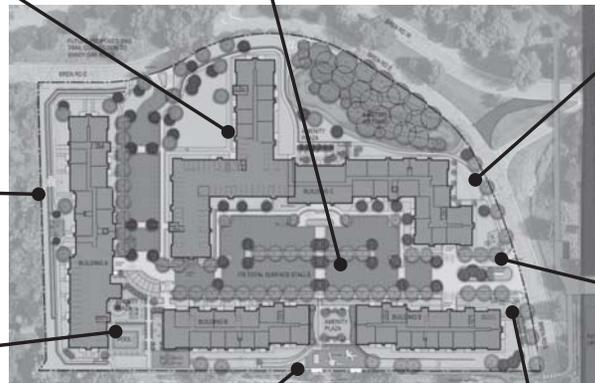




BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

CONCEPT DESIGN





BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

VISIONING IMAGES





BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

PROPOSED RENDERING





BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

PROPOSED RENDERING





- BLDG HEIGHT 153'-4"
- FIFTH FLOOR 142'-8"
- FOURTH FLOOR 132'-0"
- THIRD FLOOR 121'-4"
- SECOND FLOOR 110'-8"
- FIRST FLOOR 100'-0"
- GARAGE 89'-0"



- BLDG HEIGHT 153'-4"
- FIFTH FLOOR 142'-8"
- FOURTH FLOOR 132'-0"
- THIRD FLOOR 121'-4"
- SECOND FLOOR 110'-8"
- FIRST FLOOR 100'-0"
- GARAGE 89'-0"

BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

EXTERIOR RENDERING & ELEVATIONS - URBAN VILLAGE





BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

EXTERIOR RENDERING & ELEVATIONS - TRANSIT





BLDG HEIGHT 142'-8"
 FOURTH FLOOR 132'-0"
 THIRD FLOOR 121'-4"
 SECOND FLOOR 110'-8"
 FIRST FLOOR 100'-0"
 GARAGE 89'-0"



BLDG HEIGHT 153'-4"
 FIFTH FLOOR 142'-8"
 FOURTH FLOOR 132'-0"
 THIRD FLOOR 121'-4"
 SECOND FLOOR 110'-8"
 FIRST FLOOR 100'-0"
 GARAGE 89'-0"

BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

EXTERIOR RENDERING & ELEVATIONS - NATURE/PARKS





BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

EXTERIOR RENDERING

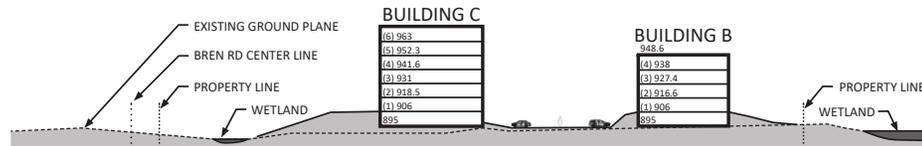




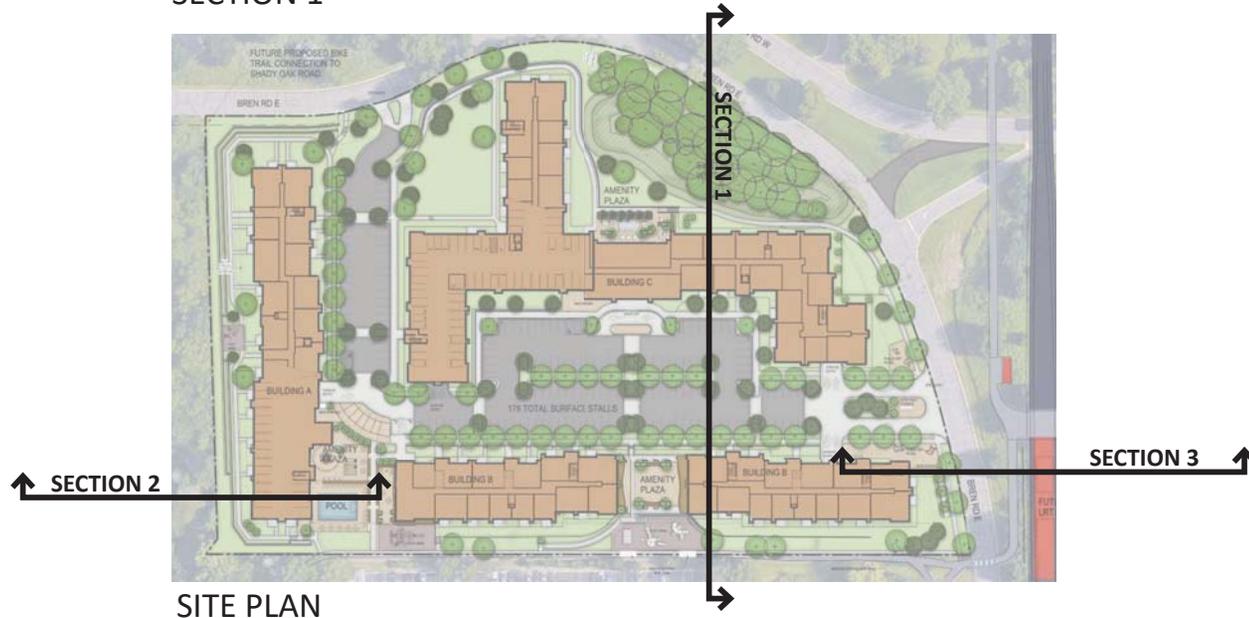
BREN ROAD DEVELOPMENT
MINNETONKA, MN
CITY SUBMITTAL - APRIL 6, 2018

EXTERIOR RENDERING

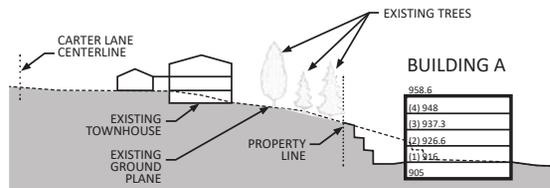




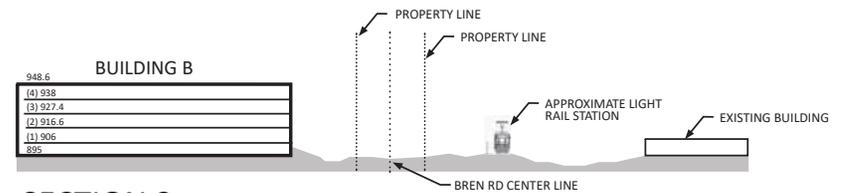
SECTION 1



SITE PLAN



SECTION 2



SECTION 3

SITE SECTIONS



DESIGN EXAMPLES- EXTERIOR
1500 NICOLLET- MINNEAPOLIS, MN



DESIGN EXAMPLES- INTERIOR
1500 NICOLLET- MINNEAPOLIS, MN



DESIGN EXAMPLES- EXTERIOR
UNION FLATS- ST PAUL, MN



DESIGN EXAMPLES- INTERIOR
UNION FLATS- ST PAUL, MN



DESIGN EXAMPLES- EXTERIOR
MILLWORK LOFTS- MINNEAPOLIS, MN





DESIGN EXAMPLES- INTERIOR
MILLWORK LOFTS- MINNEAPOLIS MN



DESIGN EXAMPLES- EXTERIOR
WEYERHAUESER- ST. PAUL, MN



DESIGN EXAMPLES- INTERIOR
WEYERHAUESER- ST. PAUL, MN



DESIGN EXAMPLES- EXTERIOR
GRAND CENTRAL FLATS- COLUMBIA HEIGHTS, MN



DESIGN EXAMPLES- INTERIOR
GRAND CENTRAL FLATS- COLUMBIA HEIGHTS, MN



DESIGN EXAMPLES- EXTERIOR
THE CAMBRIC- ST. PAUL, MN



DESIGN EXAMPLES- INTERIOR
THE CAMBRIC- ST. PAUL, MN



Parking Summary

After reviewing a number of comparable existing senior and general occupancy properties as well as new properties that are currently under construction we have come to the following conclusions. The table of this data is attached as Attachment 1.

1. The Bren Road Development project will provide considerably less parking than existing Dominion comparable properties.

For existing senior properties located in a mix of urban and suburban areas, the average total parking ratio is 1.18 parking stalls per unit. For existing general occupancy properties, the average total parking ratio is 1.85. Our proposed mix has a 1.03 ratio for the senior building and 1.28 for the general occupancy buildings which is considerably less than the existing properties.

2. The Bren Road Development project will provide less parking than pending Dominion new construction properties.

For senior new construction projects located in a mix of urban and suburban areas, the average parking ratio is 1.08 for seniors and 1.30 for general occupancy. It is important to note that two of the general occupancy properties are located in St. Paul in very urban areas where there are very limited parking options. Removing these St. Paul pending properties which are considerably more urban results in an average parking ratio of 1.13 for seniors and 1.62 for families.

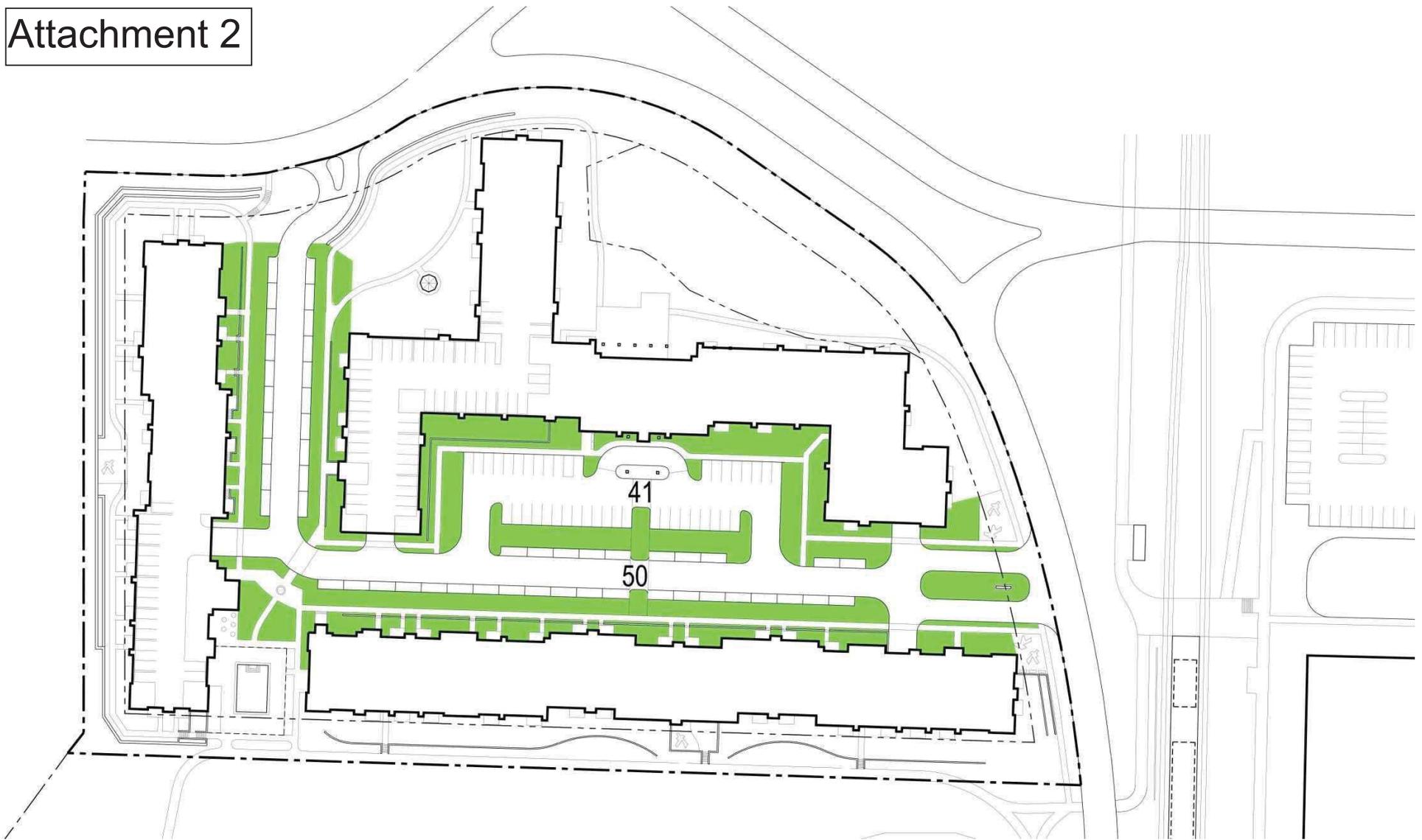
On average, 83% of garage stalls are occupied in the existing general occupancy and senior properties. It is important to note that the effective occupancy of these properties is .77 stalls/unit (83% x .92 stalls/unit). Our project is proposing .78 stalls/unit of underground parking so the demand for parking based on this data, is essentially equal to the supply. We are comfortable with this ratio because of the proximity to the future LRT stop and expected surrounding mixed-use redevelopment.

Conclusion: Overall, the parking mix we are proposing is significantly less than what we have historically provided. There will be some time between the construction completion of our project and the light rail and our future residents will not have any options if there is an insufficient amount of parking in the interim. If driverless cars and the light rail do indeed significantly reduce the parking demand in the future, BKV has come up with some plans that show where we can remove some of the existing surface parking and provide some additional green space. Attachment 2 shows the 10 and 30 year parking plans where some of the additional surface parking can be converted to green space.

Attachment 1

Dominium Parking Study											
SUBJECT PROPERTY											
Property	Address	City	Units	Surface Spots	Garage Spots	Surface Parking Ratio	Garage Parking Ratio	Total Parking Ratio	Garage Type	# Occupied-Garage	% Occupied - Garage
Building C	11001 Bren Road E	Minnetonka	262	45	225	0.17	0.86	1.03	Underground	N/A	N/A
Buildings A & B	11001 Bren Road E	Minnetonka	220	133	149	0.60	0.68	1.28	Underground	N/A	N/A
TOTALS			482	178	374	0.37	0.78	1.15			
Senior Properties											
Property	Address	City	Units	Surface Spots	Garage Spots	Surface Parking Ratio	Garage Parking Ratio	Total Parking Ratio	Garage Type	# Occupied-Garage	% Occupied - Garage
The Legends at Silver Lake Village	2500 38th Ave NE	St. Anthony	169	60	128	0.36	0.76	1.11	Underground	109	85%
River North	10940 Crooked Lake Blvd. NW	Coon Rapids	167	57	116	0.34	0.69	1.04	Underground	112	97%
The Grainwood	5119 Gateway Street SE	Prior Lake	168	49	111	0.29	0.66	0.95	Underground	111	100%
The Cavanagh	5401 51st Ave North	Crystal	130	66	94	0.51	0.72	1.23	Underground	81	86%
The Cambric	720 East 7th St	St. Paul	113	120	86	1.06	0.76	1.82	Underground	55	64%
TOTALS			149	67	109	0.45	0.73	1.18		97	89%
General Occupancy Properties											
Property	Address	City	Units	Surface Spots	Garage Spots	Surface Parking Ratio	Garage Parking Ratio	Total Parking Ratio	Garage Type	# Occupied-Garage	% Occupied - Garage
Landings at Silver Lake Village	2551 38th Ave. NE	St. Anthony	263	231	215	0.88	0.82	1.70	Underground	161	75%
808 Berry	808 Berry Street	St. Paul	267	0	405	0.00	1.52	1.52	Underground	304	75%
Stone Creek	1020 West Medicine Lake Dr.	Plymouth	125	153	140	1.22	1.12	2.34	Underground	128	91%
Bluffs at Nine Mile Creek	7475 Flying Cloud Dr.	Eden Prairie	188	118	241	N/A	1.28	1.91	Underground	196	81%
TOTALS			211	121	270	0.57	1.28	1.85		209	78%
General Occupancy Under Construction											
Property	Address	City	Units	Surface Spots	Garage Spots	Surface Parking Ratio	Garage Parking Ratio	Total Parking Ratio	Garage Type	# Occupied-Garage	% Occupied - Garage
Union Flats	787 Hampden Avenue	St. Paul	217	50	182	0.23	0.84	1.07	Underground	N/A	N/A
Grand Central Flats	4729 Grand Avenue NE	Columbia Heights	147	102	136	0.69	0.93	1.62	Underground	N/A	N/A
Millberry Apartments	778 Berry Street	St. Paul	121	42	95	0.35	0.79	1.13	Underground	N/A	N/A
TOTALS			162	64	146	0.39	0.91	1.30			
Senior Under Construction											
Property	Address	City	Units	Surface Spots	Garage Spots	Surface Parking Ratio	Garage Parking Ratio	Total Parking Ratio	Garage Type	# Occupied-Garage	% Occupied - Garage
Legends of Columbia Heights	3700 Huset Parkway	Columbia Heights	191	88	136	0.46	0.71	1.17	Underground	N/A	N/A
Legends of Apple Valley	14050 Granite Ave	Apple Valley	163	42	143	0.26	0.88	1.13	Underground	N/A	N/A
Legends at Berry	700 Emerald Street	St. Paul	240	42	175	0.18	0.73	0.90	Underground	N/A	N/A
Legends of Champlin	11635 Theatre Drive	Champlin	184	73	138	0.40	0.75	1.15	Underground	N/A	N/A
Legends of Cottage Grove	6999 E Point Douglas Rd S	Cottage Grove	184	65	129	0.35	0.70	1.05	Underground	N/A	N/A
TOTALS			192	61	146	0.32	0.76	1.08			
TOTALS - All Property Types			179	78	168	0.43	0.92	1.35		153	83%

Attachment 2



BREN ROAD DEVELOPMENT - 10 YEAR PARKING DEMAND PLAN

SCALE: 1"=80'-0"

DATE: 2-28-18



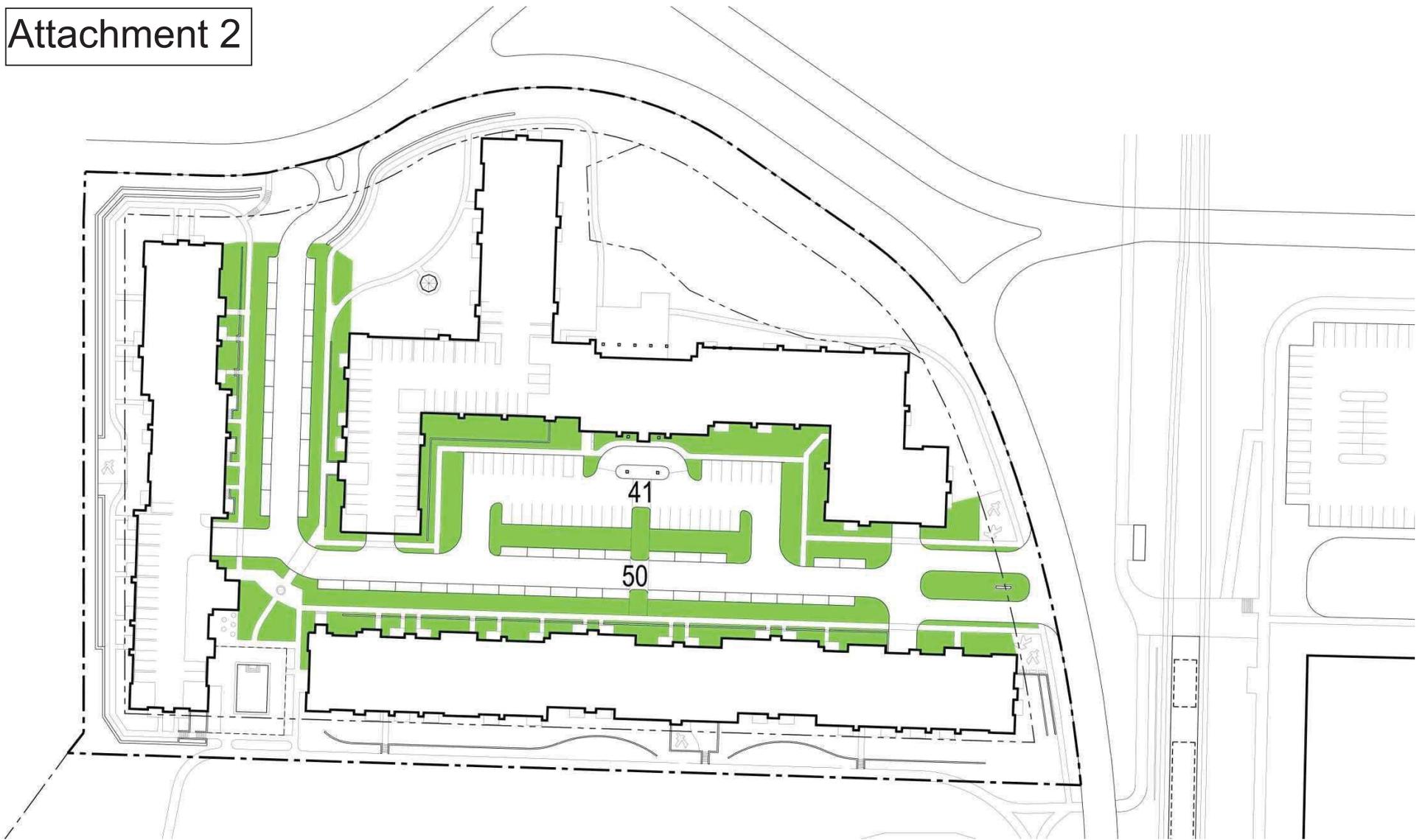


BREN ROAD DEVELOPMENT - 30 YEAR PARKING DEMAND PLAN

SCALE: 1"=80'-0"
DATE: 2-28-18



Attachment 2



BREN ROAD DEVELOPMENT - 10 YEAR PARKING DEMAND PLAN

SCALE: 1"=80'-0"

DATE: 2-28-18





BREN ROAD DEVELOPMENT - 30 YEAR PARKING DEMAND PLAN

SCALE: 1"=80'-0"
DATE: 2-28-18





Innovation Summary

Dominium is committed to providing many innovative features on the Bren Road Development project including an enhanced trail system, a green parkway entrance, public art plazas, and a bike café. In addition, this project will feature a state-of-the-art rooftop solar energy system.

Solar Energy System:

The proposed rooftop solar energy system will include over 1,500 Jinko 325-watt panels spread over all three buildings and is capable of producing more than 562,120 kWh per year. The system will be installed by Cedar Creek Energy, who have installed similar systems on past Dominium projects.

It is estimated that the system will be able to cover roughly 20% of the project's total electricity cost (including tenant unit electricity use), which results in annual power savings of over \$50,000. After accounting for the tax benefits and utility credits the project will be eligible for, the payback time is estimated to be less than seven years.

ENVIRONMENTAL ASSESSMENT WORKSHEET

This Environmental Assessment Worksheet (EAW) form and EAW Guidelines are available at the Environmental Quality Board's website at:

<http://www.eqb.state.mn.us/EnvRevGuidanceDocuments.htm>. The EAW form provides information about a project that may have the potential for significant environmental effects. The EAW Guidelines provide additional detail and resources for completing the EAW form.

Cumulative potential effects can either be addressed under each applicable EAW Item, or can be addresses collectively under EAW Item 19.

Note to reviewers: Comments must be submitted to the RGU during the 30-day comment period following notice of the EAW in the *EQB Monitor*. Comments should address the accuracy and completeness of information, potential impacts that warrant further investigation and the need for an EIS.

1. **Project title:** Dominion EAW

2. **Proposer: Dominion Management**

Contact person: Ryan J. Lunderby
Title: Vice President & Project Partner
Address: 2905 Northwest Boulevard
City, State, ZIP: Plymouth, MN 55441
Phone: (763) 354-5500
Fax:
Email: RLunderby@Dominiuminc.com

3. **RGU City of Minnetonka**

Contact person: Loren Gordon
Title: City Planner
Address: 14600 Minnetonka Boulevard
City, State, ZIP: Minnetonka, MN 55345
Phone: (952) 939-8296
Fax:
Email: lgordon@eminnetonka.com

4. **Reason for EAW Preparation:** (check one)

Required:

- EIS Scoping
 Mandatory EAW

Discretionary:

- Citizen petition
 RGU discretion
 Proposer initiated

If EAW or EIS is mandatory give EQB rule category subpart number(s) and name(s):

Residential Development
MR 4410.4300 Subpart 19.D

5. **Project Location:**

County: Hennepin
City/Township: Minnetonka
PLS Location (1/4, 1/4, Section, Township, Range): South portion of S36, T117N, R22W
Watershed (81 major watershed scale): Lower Minnesota River (33)
GPS Coordinates: Lat: 44.898491, Long: -93.415821

At a minimum attach each of the following to the EAW: See Figures 1-3

- County map showing the general location of the project;
- U.S. Geological Survey 7.5 minute, 1:24,000 scale map indicating project boundaries (photocopy acceptable); and
- Site plans showing all significant project and natural features. Pre-construction site plan and post-construction site plan.

Appendix A – Figures

Figure 1 – County Location

Figure 2 – USGS Map

Figure 3 – Project Location (aerial)

Figure 4 – Concept/Site Plan

Figure 5 – Existing Land Use

Figure 6 – Existing Zoning

Figure 7 – Parks and Trails

Figure 8 – Surficial Geology

Figure 9 – Bedrock Geology

Figure 10 – Soil Survey

Figure 11 – Surface Water Resources

Figure 12 – Well Locations

Figure 13 – Potential Contamination Areas

Figure 14 – MLCCS Land Cover

6. Project Description:

- a. **Provide the brief project summary to be published in the *EQB Monitor*, (approximately 50 words).**

Dominium proposes to redevelop an existing 9.4-acre commercial site in the City of Minnetonka to include 482 units of rental multi-family housing. The project is near the future Southwest Light Rail (SWLRT) and Opus Station and is located in the southwest corner of Bren Road East and Bren Road West (**Figure 3**).

- b. **Give a complete description of the proposed project and related new construction, including infrastructure needs. If the project is an expansion include a description of the existing facility. Emphasize: 1) construction, operation methods and features that will cause physical manipulation of the environment or will produce wastes, 2) modifications to existing equipment or industrial processes, 3) significant demolition, removal or remodeling of existing structures, and 4) timing and duration of construction activities.**

Project Description

Dominium is proposing to redevelop an existing 9.4-acre commercial site located in the southwest corner of Bren Road East and Bren Road West. The site currently houses the approximately 409,000 square foot Digi International commercial development. The site is proposed to redevelop into 482 apartment units within four buildings. Three buildings would be four stories in height and one building would be six stories in height. The units are intended to service senior and workforce housing markets and will be priced at 60% of the area's median income.

The site is located immediately west of the proposed SWLRT and adjacent to the future Opus Station. The development would include the four apartment buildings, 180 surface parking stalls, access onto Bren Road, and connect the existing trail system on the south and future trail system on the west to the Opus Light Rail Station. The proposed plan is shown on **Figure 4**.

Construction Staging/Project Schedule

Construction will consist of demolition of the current office building present on site, clearing and grubbing vegetation, grading the site, installing utilities, constructing the residential buildings, and constructing the internal roadways and parking lots. Prior to grading, erosion control and other Best Management Practices will be installed to minimize erosion and sedimentation from the site. The erosion control measures will remain in place through all phases of construction and site stabilization. The erosion control measures will be in conformance with the National Pollution Discharge Elimination System (NPDES) construction permit, Nine Mile Creek Watershed District (NMCWD) rules, and city ordinances.

The construction methods are anticipated to be conventional earthwork methods for site grading and will include scrapers, bulldozers, backhoes, and vibratory compactors. Public utilities will also be installed within trenches using this equipment. Project construction is expected to begin in 2018 and be completed in 2019.

- c. Project magnitude:

Total Project Acreage	9.4 acres
Linear project length	NA
Number and type of residential units	482 multi-family units
Commercial building area (in square feet)	NA
Industrial building area (in square feet)	NA
Institutional building area (in square feet)	NA
Other uses – specify (in square feet)	NA
Structure height(s)	4-6 stories

- d. **Explain the project purpose; if the project will be carried out by a governmental unit, explain the need for the project and identify its beneficiaries.**

The purpose of the project is to develop affordable housing to meet demand along the future SWLRT corridor. The project is being carried out by a developer.

- e. **Are future stages of this development including development on any other property planned or likely to happen?** Yes No

If yes, briefly describe future stages, relationship to present project, timeline and plans for environmental review.

- f. **Is this project a subsequent stage of an earlier project?** Yes No

If yes, briefly describe the past development, timeline and any past environmental review.

7. **Cover types:** Estimate the acreage of the site with each of the following cover types before and after development:

	Before	After		Before	After
Wetlands	0.9	0.9	Lawn/landscaping	1.8	1.3
Deep water/streams	0	0	Impervious surface	5.2	5.7
Wooded/forest	1.5	1.5	Stormwater Pond	0	0
Brush/Grassland	0	0	Other (describe)		
Cropland	0	0			
			TOTAL	9.4	9.4

8. **Permits and approvals required:** List all known local, state and federal permits, approvals, certifications and financial assistance for the project. Include modifications of any existing permits, governmental review of plans and all direct and indirect forms of public financial assistance including bond guarantees, Tax Increment Financing and infrastructure. *All of these final decisions are prohibited until all appropriate environmental review has been completed. See Minnesota Rules, Chapter 4410.3100.*

Unit of Government	Type of Application	Status
Federal		
US Army Corps of Engineers	Section 404 Permit	To Be Obtained, if needed
State		
Department of Natural Resources	Water Appropriation Permit	To Be Obtained, if needed
Pollution Control Agency	NPDES Construction Permit	To Be Obtained
Pollution Control Agency	Sanitary Sewer Extension	To Be Obtained, if needed
Pollution Control Agency	Section 401 Permit	To Be Obtained, if needed
Department of Health	Watermain Extension	To Be Obtained, if needed
Department of Health	Permit to abandon and seal private wells	To Be Obtained, if needed
Local		
City of Minnetonka	Development Application/Land Disturbance Permit	To Be Obtained
City of Minnetonka	Building Permits	To Be Obtained
City of Minnetonka	Preliminary and Final Plat Approvals	To Be Obtained
City of Minnetonka	Wetland Conservation Act Approval	To Be Obtained, if needed
Nine Mile Creek Watershed District	Grading Permit	To Be Obtained
Nine Mile Creek Watershed District	Erosion and Sediment Control	To Be Obtained
Nine Mile Creek Watershed District	Stormwater Management	To Be Obtained
Metropolitan Council	Sanitary Sewer Connection Permit	To Be Obtained

Cumulative potential effects may be considered and addressed in response to individual EAW Item Nos. 9-18, or the RGU can address all cumulative potential effects in response to EAW Item No. 19. If addressing cumulative effect under individual items, make sure to include information requested in EAW Item No. 19

9. Land use:

a. Describe:

- i. Existing land use of the site, as well as areas adjacent to and near the site, including parks, trails, prime or unique farmlands.**

The existing land use is currently Industrial. There is an existing paved trail along on the southern portion of the project area (**Figure 7**). The existing land use map is shown on **Figure 5**.

The project area is located in the southeast portion of the City. The closest major road is Shady Oak Road, located about 1,500 feet to the west. Trunk Highway 62 is located about 2,000 feet south of the project area, and Trunk Highway 169 is located about 3,700 feet east of the project area. Much of the surrounding land use is either Industrial or Office. A few parcels to the north and east are listed as Open Space. St. Margaret Cemetery is located immediately adjacent to the west and is listed as Institutional (**Figure 5**).

- ii. Plans. Describe planned land use as identified in comprehensive plan (if available) and any other applicable plan for land use, water, or resources management by a local, regional, state, or federal agency.**

The current Comprehensive Plan designates the future land use as Mixed Use. Much of the surrounding parcels are designated as Mixed Use as well. Adjacent to the project area is the future SWLRT transit extension and future Opus Station. The Opus Station area plan identifies the site and other adjacent properties in close proximity to the station as candidates for redevelopment for new housing and employment.

- iii. Zoning, including special districts or overlays such as shoreland, floodplain, wild and scenic rivers, critical area, agricultural preserves, etc.**

The area is zoned as I-1, Industrial (**Figure 6**). There are no shoreland, floodplain, critical areas, or agricultural preserve areas.

- b. Discuss the project's compatibility with nearby land uses, zoning, and plans listed in Item 9a above, concentrating on implications for environmental effects.**

The adjacent land uses are Office, Industrial, Institutional, and Open Space. Eventually, much of the surrounding area will be zoned as Mixed Use as part of the planned Opus Station Area. The adjacent zones are I-1 Industrial, R-1 Low Density Residential, and PUD Planned Unit Development. The project is compatible with the surrounding land uses. Additionally, with the planned Opus Station and SWLRT Development, these uses are compatible with the future development in the area.

- c. Identify measures incorporated into the proposed project to mitigate any potential incompatibility as discussed in Item 9b above.**

The proposed development is compatible with the adjacent land use and zoning.

10. Geology, soils and topography/land forms:

- a. **Geology - Describe the geology underlying the project area and identify and map any susceptible geologic features such as sinkholes, shallow limestone formations, unconfined/shallow aquifers, or karst conditions. Discuss any limitations of these features for the project and any effects the project could have on these features. Identify any project designs or mitigation measures to address effects to geologic features.**

The project parcels are listed as being in New Ulm Formations and sandy till (**Figure 8**). For bedrock geology, the majority of the project is located in Platteville and Glenwood Formations, with a small part of the project area in St. Peter Sandstone. The project is surrounded by these same bedrock geologies as well (**Figure 9**). The Minnesota DNR Aggregate Resources Web Map shows that no gravel pits exist on the site. The site is not listed as a Primary or Secondary Source on the MGS 7-County Metro Sand and Gravel. The Minnesota Karst Lands maps the project within the Covered Karst region, which is an area underlain by carbonate bedrock but with more than 100 feet of sediment cover.

- b. **Soils and topography - Describe the soils on the site, giving NRCS (SCS) classifications and descriptions, including limitations of soils. Describe topography, any special site conditions relating to erosion potential, soil stability or other soils limitations, such as steep slopes, highly permeable soils. Provide estimated volume and acreage of soil excavation and/or grading. Discuss impacts from project activities (distinguish between construction and operational activities) related to soils and topography. Identify measures during and after project construction to address soil limitations including stabilization, soil corrections or other measures. Erosion/sedimentation control related to stormwater runoff should be addressed in response to Item 11.b.ii.**

The soils on the Dominion site are Lester loam 10 to 22 percent slopes (L22E), Le Sueur loam 1 to 3 percent slopes (L25A), Angus loam 2 to 6 percent slopes (L37B), Angus-Moon complex 2 to 5 percent slopes (L60B), and Urban land-Udorthents wet substratum complex 0 to 2 percent slopes (U1A). The soils are well drained to somewhat poorly drained. The existing site topography is mostly flat, with very steep hills just beyond the project area to the west and north. **Figure 10** shows the soils on the site.

The volume and acreage of soil moved has been estimated based on assuming approximately 1.5 feet of material will be graded over the development area (9.4 acres). This equates to approximately 22,750 cubic yards of material being moved during grading. Development within the project area will be designed to conform with applicable state and local standards, including National Pollution Discharge Elimination System (NPDES) Construction Stormwater General Permit requirements.

NOTE: For silica sand projects, the EAW must include a hydrogeologic investigation assessing the potential groundwater and surface water effects and geologic conditions that could create an increased risk of potentially significant effects on groundwater and surface water. Descriptions of water resources and potential effects from the project in EAW Item 11 must be consistent with the geology, soils and topography/land forms and potential effects described in EAW Item 10.

11. Water resources:

- a. **Describe surface water and groundwater features on or near the site in a.i. and a.ii. below.**
- i. **Surface water - lakes, streams, wetlands, intermittent channels, and county/judicial ditches. Include any special designations such as public waters, trout stream/lake, wildlife lakes, migratory waterfowl feeding/resting lake, and outstanding resource value water. Include water quality impairments or special designations listed on the current MPCA 303d Impaired Waters List that are within 1 mile of the project. Include DNR Public Waters Inventory number(s), if any.**

One wetland exists on site. It is classified as PFO1A (**Figure 11**). No lakes, streams, channels, or ditches exist on the project area. Several lakes, wetlands, and streams exist within one mile of the project site. Lone Lake (50986), Shady Oak Lake (51027, 50759) several Unnamed Lakes, and Nine Mile Creek (739) are within the one-mile buffer. Nine Mile Creek is the only impaired water within the one-mile buffer. It is listed as impaired for Chloride and Fishes Bioassessments. These impairments are considered to be construction related parameters and require additional best management practices (BMPs).

- ii. **Groundwater – aquifers, springs, seeps. Include: 1) depth to groundwater; 2) if project is within a MDH wellhead protection area; 3) identification of any onsite and/or nearby wells, including unique numbers and well logs if available. If there are no wells known on site or nearby, explain the methodology used to determine this.**

The Minnesota Well Index was reviewed and no wells exist within the project area or within the project area's 500-foot buffer. Four wells exist outside the 500-foot buffer (**Figure 12**).

The entire project area is within a Low Vulnerability portion of the Edina Drinking Water Supply Management Area (DWSMA).

- b. **Describe effects from project activities on water resources and measures to minimize or mitigate the effects in Item b.i. through Item b.iv. below.**
- i. **Wastewater - For each of the following, describe the sources, quantities and composition of all sanitary, municipal/domestic and industrial wastewater produced or treated at the site.**
- 1) **If the wastewater discharge is to a publicly owned treatment facility, identify any pretreatment measures and the ability of the facility to handle the added water and waste loadings, including any effects on, or required expansion of, municipal wastewater infrastructure.**
 - 2) **If the wastewater discharge is to a subsurface sewage treatment systems (SSTS), describe the system used, the design flow, and suitability of site conditions for such a system.**
 - 3) **If the wastewater discharge is to surface water, identify the wastewater treatment methods and identify discharge points and proposed effluent limitations to mitigate impacts. Discuss any effects to surface or groundwater from wastewater discharges.**

The City of Minnetonka is working with the developer to reconfigure the sewer system in the area to split flows between the Opus Lift Station and the gravity system. However, to analyze the most impactful scenario for wastewater infrastructure, the EAW reviews wastewater generated by the project will be collected at Minnetonka’s sanitary sewer system and conveyed to Opus Lift Station. From there, wastewater will be conveyed to Metropolitan Council Environmental Services (MCES) meter M410 and gravity interceptor 6801, and ultimately to the Blue Lake Wastewater Treatment Plant (WWTP). The Blue Lake WWTP has a treatment capacity of 32 million gallons per day (MGD). Given the plant’s treatment capacity and the estimated wastewater that will be generated at the apartment complex, it is anticipated that the wastewater generated at the apartments will not have significant impacts on the plant’s ability to effectively treat wastewater. Additionally, given the nature of the wastewater flow, domestic wastewater, specific pretreatment measures will not be required. An estimation of the wastewater generated by the Bren Road Apartments can be seen below.

Table 11a: Wastewater Flow Calculations

Bren Road Apartments – Wastewater Flow Calculations				
Unit	Unit Value (Gal/Day/Unit)	Unit Type	# of Units	Flow, GPD
1 Bedroom	110	Residential	114	12,540
2 Bedroom	220	Residential	269	59,180
3 Bedroom	330	Residential	99	32,670
Average Daily Flow (GPD)				104,390
Peaking Factor				4*
Peak Daily Flow (GPD)				417,560
Peak Design Flow (GPM)				290

**Sanitary sewer peaking factor obtained from MCES Water Resources Policy Plan. This peaking factor can be further reviewed if historical wastewater data for the City of Minnetonka becomes available.*

There is currently an office building located at the project site. This office building will be completely removed prior to the construction of the Dominion project. An estimation of the amount of wastewater generated by the office building was completed to obtain the existing wastewater flow generated on site (flow pre-apartment complex). Wastewater flow for pre-apartment complex conditions was estimated by counting the parking spaces of the office building and multiplying it by several assumptions. The assumptions were that each employee drives its own vehicle to work and that each employee generates 10 gallons of wastewater per day. In doing this, it was calculated that an average of 4,170 gallons of wastewater per day (gpd) are being generated by the office building. By taking this wastewater flow into consideration, the net average wastewater flow increase generated on site once the apartment complex is constructed will be 100,220 gpd.

Given the plant’s treatment capacity and the estimated wastewater that will be generated at the apartment complex, it is anticipated that the wastewater generated at the apartments will not have significant impacts on the plant’s ability to effectively treat wastewater. Additionally, given the nature of the wastewater flow, domestic wastewater, specific pretreatment measures will not be required.

- ii. **Stormwater - Describe the quantity and quality of stormwater runoff at the site prior to and post construction. Include the routes and receiving water bodies for runoff from the site (major downstream water bodies as well as the immediate receiving waters). Discuss any environmental effects from stormwater discharges. Describe stormwater pollution prevention plans including temporary and permanent runoff controls and potential BMP site locations to manage or treat stormwater runoff. Identify specific erosion control, sedimentation control or stabilization measures to address soil limitations during and after project construction.**

Existing Conditions

Under existing conditions the site contains an office building and parking lot with 5.2 acres of impervious surface. Existing runoff drains to catch basins in the parking lot and to the wetland south of Bren Road East. The stormwater is then discharged into the City of Minnetonka system. Runoff from 1.8 acres of the property west of the site flows to the project site and is collected in the site’s existing stormwater system. A portion of Bren Road East road runoff is also tributary to the project site. There are no existing stormwater ponds or infiltration basins on the site.

A HydroCAD Version 10.00-16 model was created to compare the existing and proposed discharge rates leaving the site. The model was based on a preliminary development design. Existing drainage patterns will be maintained post-project. The rate control comparison was made for the site improvements only and does not account for the rate control that is provided in the existing wetland in the northeast corner of the site. Site runoff will continue to outlet into the existing wetland in the northeast corner of the site. The City of Minnetonka storm sewer system conveys the runoff to the northeast.

Table 11b: Existing Peak Discharge Rates Leaving the Site

Storm	Discharge Rate (cfs)
2-Year	22.3 cfs
10-Year	39.8 cfs
100-Year	78.7 cfs

Proposed Conditions

Proposed conditions will consist of multi-family residences, a parking lot and sidewalks with 5.7 acres of impervious surface. There are 5.2 acres of impervious for existing conditions. There is an anticipated increase of 0.5 acres in proposed impervious surfaces from existing conditions.

Table 11c shows the modeled results for proposed conditions.

Table 11c: Proposed Peak Discharge Rates Leaving the Site

Storm	Discharge Rate (cfs)
2-Year	22.3 cfs
10-Year	39.8 cfs
100-Year	78.7 cfs

Relevant Regulations and Considerations

NMCWD and the City of Minnetonka regulate stormwater runoff rate, volume and treatment. The City of Minnetonka also has design standards for storm sewer conveyance systems.

Based on a review of NMCWD and City of Minnetonka rules for the type of development proposed (redevelopment of an existing parcel that disturbs over 50 percent of the existing impervious surfaces), stormwater treatment for the site must meet the following criteria:

- Runoff rate control: limit the peak runoff flow rates to that from existing conditions for the 2-, 10-, and 100-year storm events for all points where stormwater leaves the parcel
- Runoff volume control: provide on-site retention of 1-inch of runoff from all impervious surfaces. Infiltration is preferred unless site conditions prevent infiltration. Where below-ground infiltration facilities, practices or systems are proposed, pretreatment of runoff must be provided
- Water quality treatment: provide for all runoff to be treated to at least 60 percent annual removal efficiency for total phosphorus and 90 percent total annual removal efficiency from total suspended solids.

A stormwater facility will need to be designed to retain 1-inch of runoff from the site and to provide water quality treatment to meet NMCWD and the City of Minnetonka requirements. The developer plans to include underground infiltration and irrigation re-use to manage stormwater within the site.

A Stormwater Pollution Prevention Plan (SWPPP) will need to be prepared in accordance with NPDES guidelines and the City of Minnetonka's Stormwater Management criteria, and will be required to be submitted and approved prior to construction. Grading, drainage, and erosion control measures must be consistent with NMCWD's Rules and the City of Minnetonka's Surface Water Management Plan.

There will be no anticipated downstream environmental effects from the proposed project based on the project needing to meet state and local requirements.

- iii. **Water appropriation - Describe if the project proposes to appropriate surface or groundwater (including dewatering). Describe the source, quantity, duration, use**

and purpose of the water use and if a DNR water appropriation permit is required. Describe any well abandonment. If connecting to an existing municipal water supply, identify the wells to be used as a water source and any effects on, or required expansion of, municipal water infrastructure. Discuss environmental effects from water appropriation, including an assessment of the water resources available for appropriation. Identify any measures to avoid, minimize, or mitigate environmental effects from the water appropriation.

The Dominion project will be connecting to the City of Minnetonka’s existing distribution system. The existing system is made up of 16 production wells, and 260 miles of water mains. Groundwater pumped by production wells is chemically treated at 8 different plants and temporarily stored in 9 tanks (12 MG of total storage capacity). On average, the existing system delivers a daily flow of approximately 8 MG. Wells likely serving the apartment complex will be 13, and 13A due to their proximity with the project site. Specific appropriations for surface water and groundwater will not be needed since the apartment complex will be connecting to the existing distribution system.

It is important to note that the project area is located inside Edina’s Drinking Water Supply Management Area (DWSMA) (DWSMA ID# 546). In terms of vulnerability, the area surrounding the project site is classified as low vulnerability. In addition to Edina’s DWSMA, the project site borders Minnetonka’s 13 DWSMA (DWSMA ID# 215). This area of the DWSMA is also classified as low vulnerable. Due to the nature of the building to be constructed at the project site (apartment complex) and the types of vulnerabilities of both DWSMAs, it is not foreseen that the aquifer beneath the project site is in any danger from being contaminated from pollutants originated at the apartment complex.

An estimation of the water flows for the Bren Road Apartments can be seen below.

Table 11d. Water Flow Calculations

Bren Road Apartments – Water Flow Calculations					
Unit	Unit Value (Gal/Day/Person)	Unit Type	# of Units	People/Bedroom	Flow, GPD
1 Bedroom	95	Residential	114	1.3	14,079
2 Bedroom	95	Residential	269	1.3	66,443
3 Bedroom	95	Residential	99	1.3	36,680
Average Daily Flow (GPD)					117,202
Peaking Factor					3.0
Peak Daily Flow (GPD)					351,605
Peak Design Flow (GPM)					244

**A conservative peaking factor value was used when calculating peak daily flow. This peaking factor can be further reviewed if historical water data for the City of Minnetonka becomes available.*

Similar to the wastewater flow calculations, the existing office building has daily water consumption that needs to be estimated. Water usage by the office building was estimated by multiplying the number of parking spaces by a water usage assumption per employee and per parking spot. It was assumed that each employee drives its own vehicle to work and that each employee uses 12 gallons of water per day. In doing this, it was calculated that an average of 5,004 gallons of water per day are being used by the office building. By taking this water usage estimation into consideration, the net average

water usage increase on site once the apartment complex is constructed will be of 112,198 gpd. Given the water usage estimated for the project site once the apartment complex is constructed, expansion of the local distribution system will not be required at this time.

iv. **Surface Waters**

- a) **Wetlands - Describe any anticipated physical effects or alterations to wetland features such as draining, filling, permanent inundation, dredging and vegetative removal. Discuss direct and indirect environmental effects from physical modification of wetlands, including the anticipated effects that any proposed wetland alterations may have to the host watershed. Identify measures to avoid (e.g., available alternatives that were considered), minimize, or mitigate environmental effects to wetlands. Discuss whether any required compensatory wetland mitigation for unavoidable wetland impacts will occur in the same minor or major watershed, and identify those probable locations.**

A wetland delineation was completed for the project site. There is approximately 0.88 acres of wetland on site (**Figure 11**). This wetland is categorized as PFO1A. This wetland is within the Open Space of the proposed development. No wetland impacts are expected with the project. However, if as design progresses, wetland impacts are anticipated, wetland impacts will be minimized to the greatest extent possible and reviewed through the local and federal wetland permitting processes.

- b) **Other surface waters- Describe any anticipated physical effects or alterations to surface water features (lakes, streams, ponds, intermittent channels, county/judicial ditches) such as draining, filling, permanent inundation, dredging, diking, stream diversion, impoundment, aquatic plant removal and riparian alteration. Discuss direct and indirect environmental effects from physical modification of water features. Identify measures to avoid, minimize, or mitigate environmental effects to surface water features, including in-water Best Management Practices that are proposed to avoid or minimize turbidity/sedimentation while physically altering the water features. Discuss how the project will change the number or type of watercraft on any water body, including current and projected watercraft usage.**

No other surface waters exist on the project site or are anticipated to be impacted by the project.

12. Contamination/Hazardous Materials/Wastes:

- a. **Pre-project site conditions - Describe existing contamination or potential environmental hazards on or in close proximity to the project site such as soil or ground water contamination, abandoned dumps, closed landfills, existing or abandoned storage tanks, and hazardous liquid or gas pipelines. Discuss any potential environmental effects from pre-project site conditions that would be caused or exacerbated by project construction and operation. Identify measures to avoid, minimize or mitigate adverse effects from existing contamination or potential environmental hazards. Include development of a Contingency Plan or Response Action Plan.**

Publicly available data from the Minnesota Pollution Control Agency (MPCA) database were reviewed to identify verified or potentially contaminated sites that may be encountered during proposed development within the six parcels (**Figure 13**). The following databases were reviewed:

- MPCA “What’s in My Neighborhood?” website
- MPCA Storage Tank Leak Site website
- US Department of Agriculture “What’s in My Neighborhood?” website

Three listings exist within the project area, and several other listings exist within 500 feet of the project area. The listings on the project site include Multiple Listings (Site 1) and Hazardous Waste (Sites 2, 3, and 4). The Multiple Listings site consists of two Construction Stormwater Permits. Within 1,000 feet, the listings include Hazardous Waste, Industrial Stormwater, and Multiple Listings.

Inclusion on the Construction Stormwater Permit database indicates a permit is in place to limit erosion and pollution during and after construction at the site. Inclusion on the Small Quantity Hazardous Waste Generator database indicates that a site generates 1-1,000 kilograms of hazardous waste per year.

Based on this review, the potential to encounter contaminated soil and/or groundwater at the proposed project area is low. If any contaminated soil/groundwater or hazardous material is encountered, necessary steps to remediate will be taken.

- b. **Project related generation/storage of solid wastes - Describe solid wastes generated/stored during construction and/or operation of the project. Indicate method of disposal. Discuss potential environmental effects from solid waste handling, storage and disposal. Identify measures to avoid, minimize or mitigate adverse effects from the generation/storage of solid waste including source reduction and recycling.**

Development within these parcels will generate solid waste and construction debris normal to construction. Solid waste and construction debris will be disposed of in conformance with state standards. The demolition of the existing building located in the middle portion of the site will generate solid waste. This activity will be completed in conformance with state requirements and materials will be either recycled or hauled to an appropriate demolition landfill site.

The proposed development includes residential uses with no manufacturing or light industrial users planned. As a result, the waste generated should be of a similar nature to household wastes. Users will be required to recycle consistent with the city’s policies, and all recycling and solid waste disposal will be removed from the site by licensed haulers.

- c. **Project related use/storage of hazardous materials - Describe chemicals/hazardous materials used/stored during construction and/or operation of the project including method of storage. Indicate the number, location and size of any above or below ground tanks to store petroleum or other materials. Discuss potential environmental effects from accidental spill or release of hazardous materials. Identify measures to avoid, minimize or mitigate adverse effects from the use/storage of chemicals/hazardous materials including source reduction and recycling. Include development of a spill prevention plan.**

Small amounts of hazardous materials typical of a construction site (e.g., fuel oil) will be stored in approved containers. As required by the NPDES Construction Stormwater Permit, the fuel containers will be required to have secondary containment by either being bermed or stored in a truck or other facility. Fuel trucks and any other hazardous material are required to be locked when not in use to avoid vandalism.

- d. **Project related generation/storage of hazardous wastes - Describe hazardous wastes generated/stored during construction and/or operation of the project. Indicate method of disposal. Discuss potential environmental effects from hazardous waste handling, storage, and disposal. Identify measures to avoid, minimize or mitigate adverse effects from the generation/storage of hazardous waste including source reduction and recycling.**

Construction within any of the subject parcels will not involve the generation of significant amounts of hazardous wastes.

Once construction is completed, it is anticipated that the waste generated will be of similar nature to household wastes and will be disposed of similarly. There are no gas stations proposed that would include storing of hazardous materials.

13. Fish, wildlife, plant communities, and sensitive ecological resources (rare features):

- a. **Describe fish and wildlife resources as well as habitats and vegetation on or in near the site.**

Current land cover consists mostly of buildings and pavement with 91-100 percent impervious cover, and a small portion of short grasses and mixed trees with 4-10 percent impervious cover. The only notable wildlife resources on site consist of the wetland and wooded area that surrounds it, which could contain habitat for waterfowl. No suitable fish habitat exists on site. There are no designated trout streams, Wildlife Management Areas, Waterfowl Production Areas, Wildlife Refuges, Reinvest in Minnesota (RIM) easements, wild rice lakes, or Outstanding Resource Value Waters (ORVWs) within any of the parcels. The Minnesota Land Cover Classification System (MLCCS) land cover data is shown in **Figure 14**. There are no Minnesota County Biological Survey (MCBS) or Areas of Ecological Significance within or near the Dominion Development.

- b. **Describe rare features such as state-listed (endangered, threatened or special concern) species, native plant communities, Minnesota County Biological Survey Sites of Biodiversity Significance, and other sensitive ecological resources on or within close proximity to the site. Provide the license agreement number (LA-____) and/or correspondence number (ERDB #20180308) from which the data were obtained and attach the Natural Heritage letter from the DNR. Indicate if any additional habitat or species survey work has been conducted within the site and describe the results.**

A request for data was sent to the DNR on January 18, 2018. The DNR response shows no NHIS records or listings within project area or a 1-mile buffer. Additionally, the project area is categorized as a low potential zone for rusty patched bumblebees. Based on this review and a review of the site in its current developed condition, the project is anticipated to have no impact on rare or threatened species.

- c. Discuss how the identified fish, wildlife, plant communities, rare features and ecosystems may be affected by the project. Include a discussion on introduction and spread of invasive species from the project construction and operation. Separately discuss effects to known threatened and endangered species.**

The site currently contains commercial development. The planned development will result in the limited removal of vegetation and subsequent habitat, primarily in areas planned for development. The current site already contains a significant amount of impervious surface. The development is expected to occur on areas that are currently buildings, impervious surface, and landscaping. None of the site provides significant habitat to wildlife. The wetland on site is anticipated to be avoided at this time. Minor impacts that may occur will be minimized per requirements of the Wetland Conservation Act and US Corps of Engineers and vetted through the regulatory permitting process. Mitigation for wetland impacts would occur at a 2:1 ratio.

Invasive Species

The site may contain some invasive species, although no site-specific information is currently available.

The US Department of Agriculture's National Invasive Species Information Center provides information regarding Best Management Practices to prevent or mitigate invasive species establishment or movement. Guidance for implementation at all parcels can be referenced at <https://www.invasivespeciesinfo.gov/toolkit/preventionbmp.shtml>. Appropriate actions such as cleaning equipment, chipping/destroying invasive species, and limiting and securing soil disturbances will help prevent the spread of the invasive/noxious species. If necessary, herbicide application to pockets of weed growth could be implemented during and after construction, especially if soil particles are staged, or left for future phases.

- d. Identify measures that will be taken to avoid, minimize, or mitigate adverse effects to fish, wildlife, plant communities, and sensitive ecological resources.**

The site concept plan has been designed to mostly avoid the wetlands on site. The plan does not include significant park or open space development. It is expected that development will occur on areas that are currently mostly impervious surface; as such, these areas are not of significant plant or wildlife resources, or of any sensitive ecological resources.

14. Historic properties:

Describe any historic structures, archeological sites, and/or traditional cultural properties on or in close proximity to the site. Include: 1) historic designations, 2) known artifact areas, and 3) architectural features. Attach letter received from the State Historic Preservation Office (SHPO). Discuss any anticipated effects to historic properties during project construction and operation. Identify measures that will be taken to avoid, minimize, or mitigate adverse effects to historic properties.

The State Historic Preservation Office was contacted regarding historic resources in the area. The review concluded that three historic/architectural sites, Bridges 27545 and 27546, and a farmstead are located near the project area (**Appendix B**). The bridges are located eastbound and westbound on Shady Oak Road over Trunk Highway 62. The farmstead is located on Felth Road just south of

Smetana Road. The bridges are southwest of the project and the farmstead is located north of the project. No impacts to these resources are anticipated as a result of development in the project area.

15. Visual:

Describe any scenic views or vistas on or near the project site. Describe any project related visual effects such as vapor plumes or glare from intense lights. Discuss the potential visual effects from the project. Identify any measures to avoid, minimize, or mitigate visual effects.

The Dominion Development is located north of Trunk Highway 62 and west of Trunk Highway 169 and is surrounded by developed area. Development within the project area will be similar in nature to existing development in the area. Therefore, no visual impacts are anticipated. No vapor plumes or intense lighting will result from development of the subject parcels.

16. Air:

a. Stationary source emissions - Describe the type, sources, quantities and compositions of any emissions from stationary sources such as boilers or exhaust stacks. Include any hazardous air pollutants, criteria pollutants, and any greenhouse gases. Discuss effects to air quality including any sensitive receptors, human health or applicable regulatory criteria. Include a discussion of any methods used assess the project's effect on air quality and the results of that assessment. Identify pollution control equipment and other measures that will be taken to avoid, minimize, or mitigate adverse effects from stationary source emissions.

No stationary sources of emission such as boiler or stacks are anticipated with development in the area.

b. Vehicle emissions - Describe the effect of the project's traffic generation on air emissions. Discuss the project's vehicle-related emissions effect on air quality. Identify measures (e.g. traffic operational improvements, diesel idling minimization plan) that will be taken to minimize or mitigate vehicle-related emissions.

The Dominion Development project is not anticipated to significantly impact traffic in the area. Additionally, there will be less than 2,000 parking stalls for the development (545 parking stalls are planned). The project is not anticipated to impact air quality as a result of vehicle related emissions.

c. Dust and odors - Describe sources, characteristics, duration, quantities, and intensity of dust and odors generated during project construction and operation. (Fugitive dust may be discussed under item 16a). Discuss the effect of dust and odors in the vicinity of the project including nearby sensitive receptors and quality of life. Identify measures that will be taken to minimize or mitigate the effects of dust and odors.

During construction, particulate emissions will temporarily increase due to generation of fugitive dust. Construction dust control is required to be in conformance with City of Minnetonka ordinances and the NPDES Construction Stormwater permit.

The construction and operation of the proposed site redevelopment is not anticipated to involve processes that would generate odors.

17. Noise:

Describe sources, characteristics, duration, quantities, and intensity of noise generated during project construction and operation. Discuss the effect of noise in the vicinity of the project including 1) existing noise levels/sources in the area, 2) nearby sensitive receptors, 3) conformance to state noise standards, and 4) quality of life. Identify measures that will be taken to minimize or mitigate the effects of noise.

The project site is located within a suburban area and is surrounded by both Trunk Highway 62 and Trunk Highway 169 freeways, office, industrial, and institutional development. Existing noise sources consist mainly of traffic on the area freeways and roadways.

Construction noise levels and types typical of construction equipment will occur as a result of this project. Construction noise will be limited to daytime hours consistent with the City of Minnetonka's construction and noise ordinances (7 a.m. to 10 p.m. Monday-Sunday). Construction equipment will be fitted with mufflers that would be maintained throughout the construction process. The table below summarizes the peak noise levels of common types of roadway construction equipment.

Table 17a: Typical Roadway Construction Equipment Noise Levels at 50 Feet

Equipment Type	Manufacturers Sampled	Total Number of Models in Sample	Peak Noise Level	
			Range	Average
Backhoe	5	6	74-92	83
Front Loader	5	30	75-96	85
Dozer	8	41	65-95	85
Grader	3	15	72-92	84
Scraper	2	27	76-98	87
Pile Driver	N/A	N/A	95-105	101

Source: United States Environmental Protection Agency and Federal Highway Administration

There are no sensitive receptors (such as hospitals) near the site which raise special concerns for further study.

18. Transportation

a. Describe traffic-related aspects of project construction and operation. Include: 1) existing and proposed additional parking spaces, 2) estimated total average daily traffic generated, 3) estimated maximum peak hour traffic generated and time of occurrence, 4) indicate source of trip generation rates used in the estimates, and 5) availability of transit and/or other alternative transportation modes.

The site is currently developed and the proposed development would remove the existing building and parking areas. The proposed development would have 545 parking spaces. The trip generation for the proposed development is shown in the table below.

Trip Generation of Proposed Development								
482 Apartments								
Dwelling Units =	482	Daily	AM Peak Hour			PM Peak Hour		
			Total	In	Out	Total	In	Out
Trips/DU		5.44	0.36	0.26	0.74	0.44	0.61	0.39
Total Trips		2622	174	45	128	212	129	83
External to Opus	80%	2098	139	36	103	170	103	66
To Bren/TH 169 Interchange	40%	839	56	14	41	68	41	26

Trip Generation Rates are for mid-rise multi-family dwelling units from the 10th Edition of the ITE Trip Generation Manual

This site is located near the proposed Southwest LRT line and the Opus Station is less than 200 feet from the proposed site. There is currently limited transit service to the site.

- b. Discuss the effect on traffic congestion on affected roads and describe any traffic improvements necessary. The analysis must discuss the project’s impact on the regional transportation system.**

If the peak hour traffic generated exceeds 250 vehicles or the total daily trips exceeds 2,500, a traffic impact study must be prepared as part of the EAW. Use the format and procedures described in the Minnesota Department of Transportation’s Access Management Manual, Chapter 5 (available at: <http://www.dot.state.mn.us/accessmanagement/resources.html>) or a similar local guidance.

This development is located in the Opus Industrial Park. This area is governed by the Opus Overlay District which establishes trip generation limits for development in the park based on a traffic analysis of the surrounding roadway system. The site where this development is located was allocated 47 trips to the Bren Road/TH 169 Interchange assuming it would develop as commercial property. The ordinance specifically excludes residential development from the trip generation limits since the peak direction of traffic is opposite of the commercial uses in this district.

The Bren Road/TH 169 Interchange was determined to be the critical capacity constraint for traffic into and out of the park. The peak direction for traffic is inbound in the AM peak hour and outbound in the PM peak hour. As a commercial use it was assumed that the site would have about 37 inbound trips using the Bren Road/TH 169 Interchange in the AM peak hour and 37 trips outbound at the Bren Road and TH 169 Interchange in the PM peak hour. The other 10 trips allocated to this site would be in the non-peak direction. The proposed use would only have 14 trips inbound in the AM peak hour and 26 outbound in the PM peak hour at the Bren Road and TH 169 Interchange.

WSB also collected daily traffic counts on Bren Road just west of TH 169 to verify that current traffic volumes are within the thresholds assumed for the ordinance. The ordinance is based on a traffic analysis that has a Level of service “D” capacity of 3779 AM peak hour trips at this location with 2818 inbound and 961 outbound. In the PM peak hour, the study determined the Level of service capacity, which is a Level of Service D to be 3747 PM peak hour trips with 2675 outbound and 1072 inbound. The traffic counts show that there are currently 3749 AM peak hour trips at this location with 2969 inbound and 780 outbound. In the PM peak hour, there are a total of 3668 trips with 3048 outbound and 620 inbound. The proposed development would create fewer peak direction trips than the assumed use for this site and would still fall within the threshold for the capacity of this interchange.

WSB also collected daily traffic counts on Eastbound Bren Road adjacent to the site to verify that there would not be capacity issues at the site access points. This count shows there are 2497 trips per day on Bren Road East next to the site. This road is a one-way road with two lanes. This road can carry more than 10000 vehicles a day at a very high level of service. While this development would substantially increase the traffic on Bren Road East the traffic volumes will still be well below the capacity of this roadway. There is currently an at-grade crossing of Bren Road East at the southern boundary of the site. In the future, this at-grade crossing would provide access to the Southwest LRT station. Based on the volumes on Bren Road East it would be desirable to grade separate this crossing in the future. An alternative would be to maintain the at-grade crossing and provide additional enhancements such as a rectangular rapid flashing beacon (RRFB) or a High-Intensity Activated cross walk beacon (HAWK). The development plan should incorporate accommodation for future pedestrian crossings in this area.

c. Identify measures that will be taken to minimize or mitigate project related transportation effects.

Based on the information in the previous section it was concluded that there are no measures required to mitigate the projects transportation related effects. If the Southwest LRT line is developed as expected the actual trip generation for this site will likely be less than shown in the table, since it is based on surveys of similar developments in generally suburban locations with limited transit use.

19. Cumulative potential effects: (Preparers can leave this item blank if cumulative potential effects are addressed under the applicable EAW Items)

a. Describe the geographic scales and timeframes of the project related environmental effects that could combine with other environmental effects resulting in cumulative potential effects.

Development is expected to begin in 2018 and be completed in 2019. Redevelopment is occurring in some areas of the City around the area. The proposed Southwest Line Light Rail (SWLRT) extension and light rail station will be located immediately east of the proposed Dominion development. The SWLRT is expected to be in operation by 2023. An Environmental Impact Statement (EIS) has been completed for the SWLRT project.

- b. Describe any reasonably foreseeable future projects (for which a basis of expectation has been laid) that may interact with environmental effects of the proposed project within the geographic scales and timeframes identified above.**

There are no past projects whose footprints overlap with the Dominion Development project. Future projects need to be considered if the project is likely to occur and sufficient information is available to understand the possible cumulative impact. As stated, the SWLRT project and light rail station are proposed immediately east of the Dominion Development site. The EIS for the SWLRT included the potential that the rail line would spur redevelopment. Additionally, the City of Minnetonka has planned for redevelopment in this area through their Comprehensive Plan process. No other specific future projects are known at this time.

- c. Discuss the nature of the cumulative potential effects and summarize any other available information relevant to determining whether there is potential for significant environmental effects due to these cumulative effects.**

The Dominion Development project site as well as the surrounding areas are developed. Redevelopment is anticipated in the area, but there are no specific future projects known at this time. The SWLRT is proposed and cumulative potential effects have been evaluated as part of the EIS for the SWLRT project. General development in the area has been planned for in the City's Comprehensive Plan.

- 20. Other potential environmental effects:** If the project may cause any additional environmental effects not addressed by items 1 to 19, describe the effects here, discuss the how the environment will be affected, and identify measures that will be taken to minimize and mitigate these effects.

No additional environmental effects have been identified.

RGU CERTIFICATION. *(The Environmental Quality Board will only accept **SIGNED** Environmental Assessment Worksheets for public notice in the EQB Monitor.)*

I hereby certify that:

- The information contained in this document is accurate and complete to the best of my knowledge.
- The EAW describes the complete project; there are no other projects, stages or components other than those described in this document, which are related to the project as connected actions or phased actions, as defined at Minnesota Rules, parts 4410.0200, subparts 9c and 60, respectively.
- Copies of this EAW are being sent to the entire EQB distribution list.

Signature



Date: April 26, 2018

Title: City Planner

Appendix A

Figures

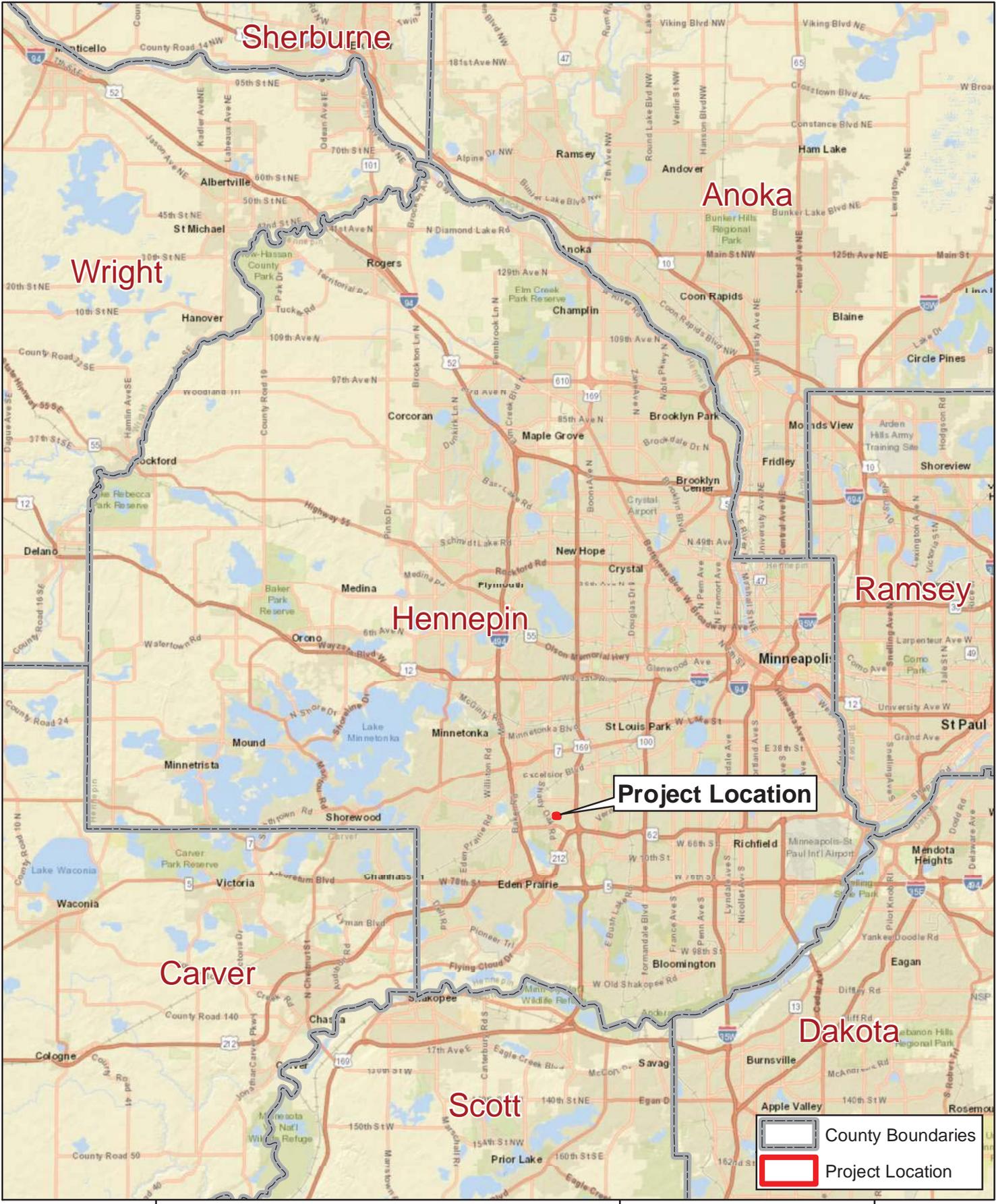


Figure 1 : Project Location
- County Location
 Dominion EAW
 Minnetonka, MN



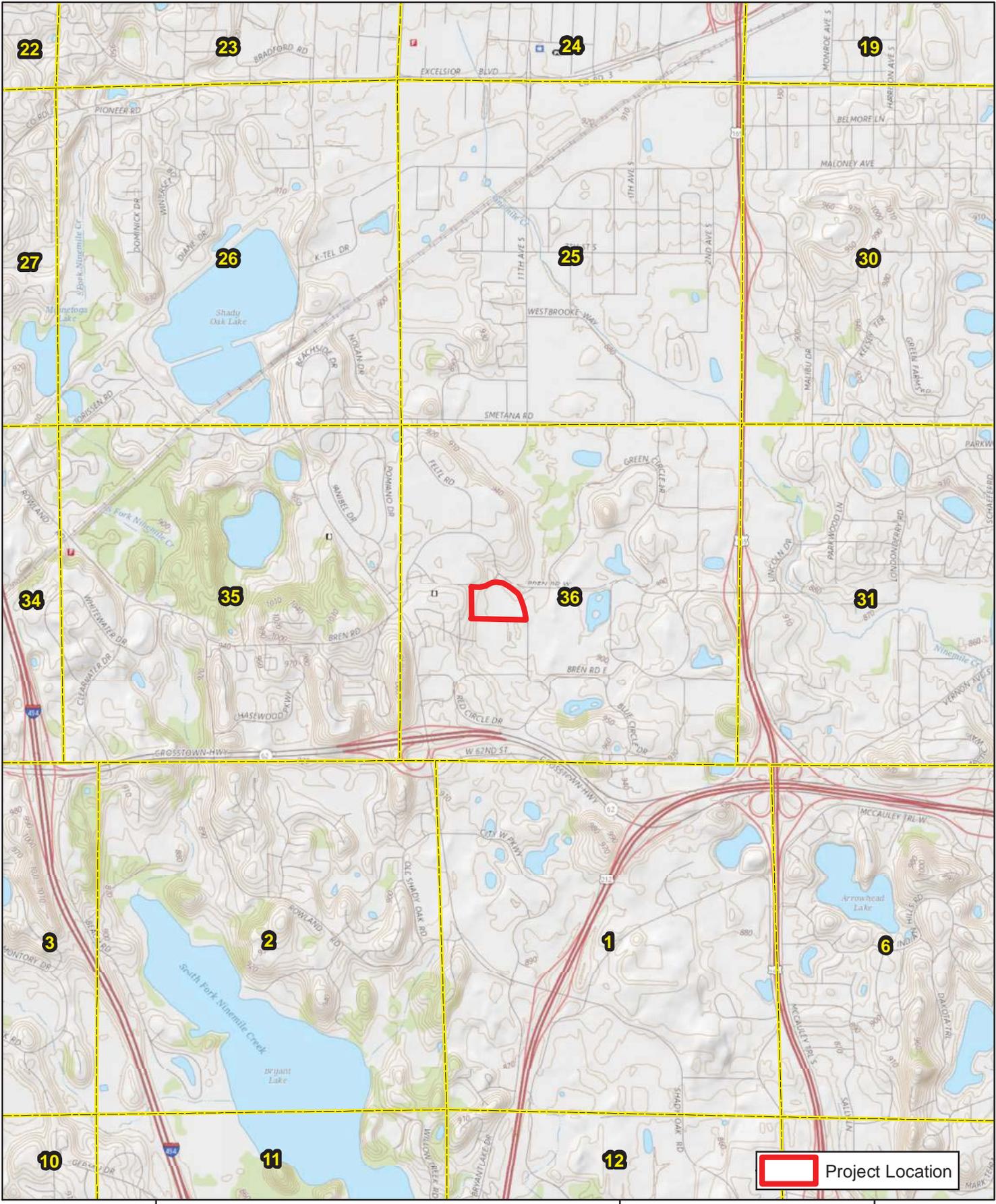


Figure 2 : Project Location
- USGS Topographic Map
 Dominion EAW
 Minnetonka, MN



0 2,000
 Feet
 1 inch = 2,000 feet



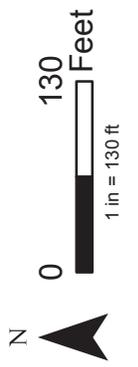


BREN ROAD DEVELOPMENT
 MINNETONKA, MN
 CITY SUBMITTAL - APRIL 6, 2018

PROPOSED SITE



Figure 4 : Concept Site Plan
 Dominium EAW
 Minnetonka, MN



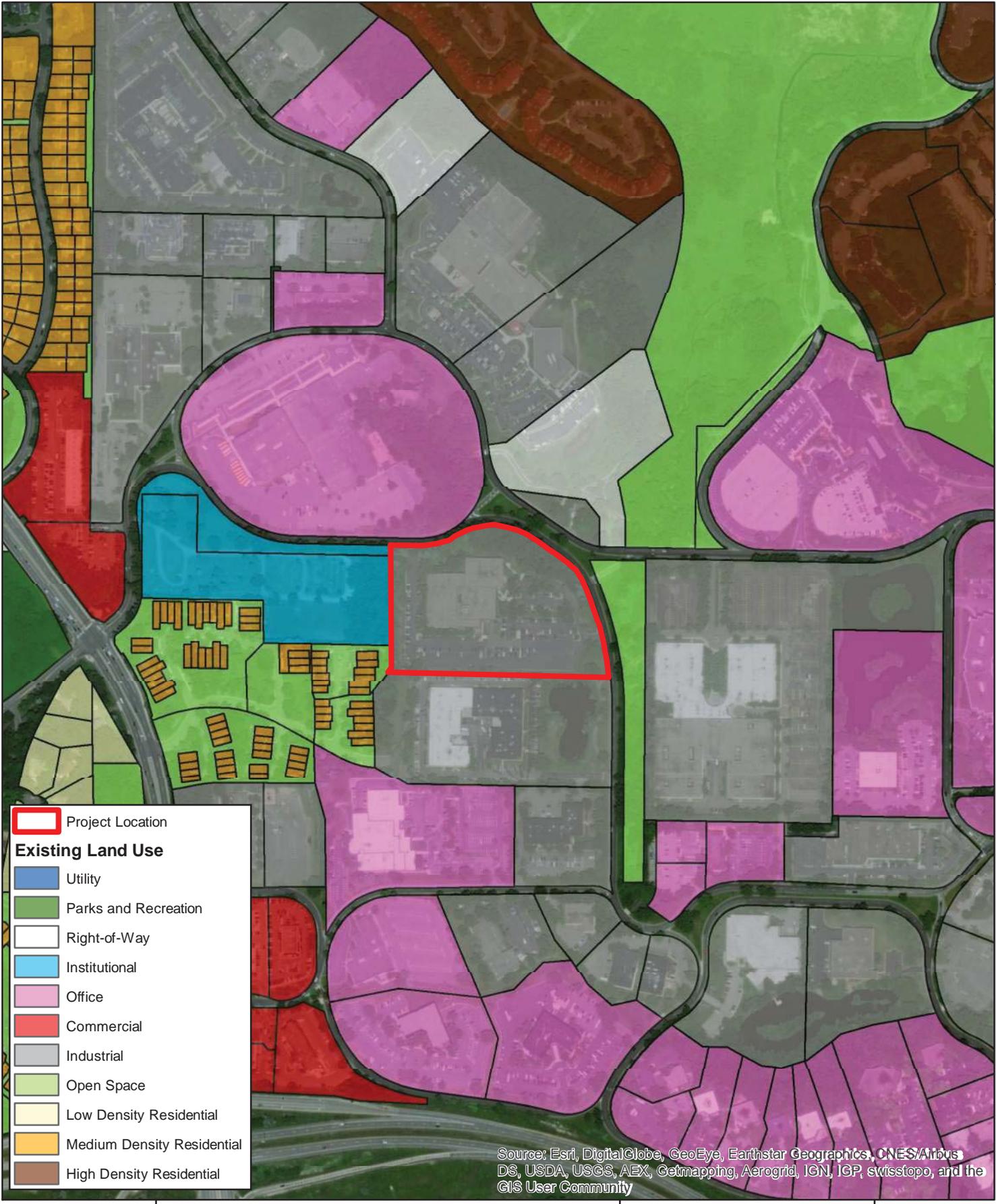
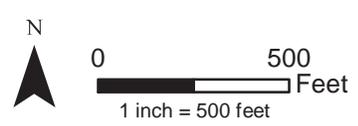


Figure 5 : Existing Land Use
Dominion EAW
Minnetonka, MN



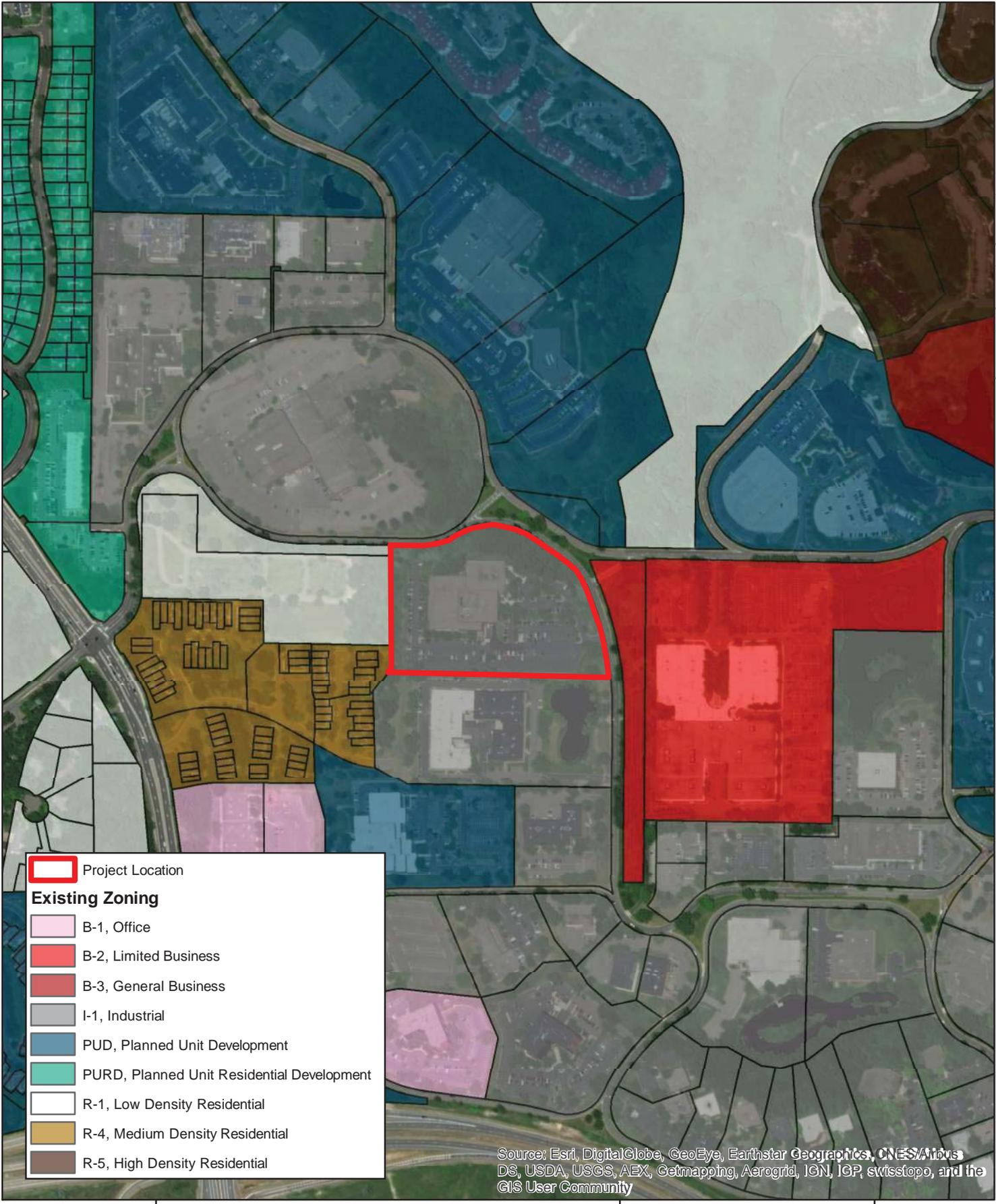


Figure 6 : Existing Zoning

Dominion EAW
Minnetonka, MN

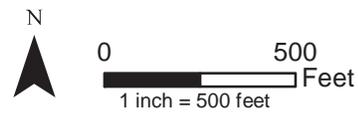


0 500 Feet
1 inch = 500 feet





Figure 7 : Parks and Trails
 Dominion EAW
 Minnetonka, MN



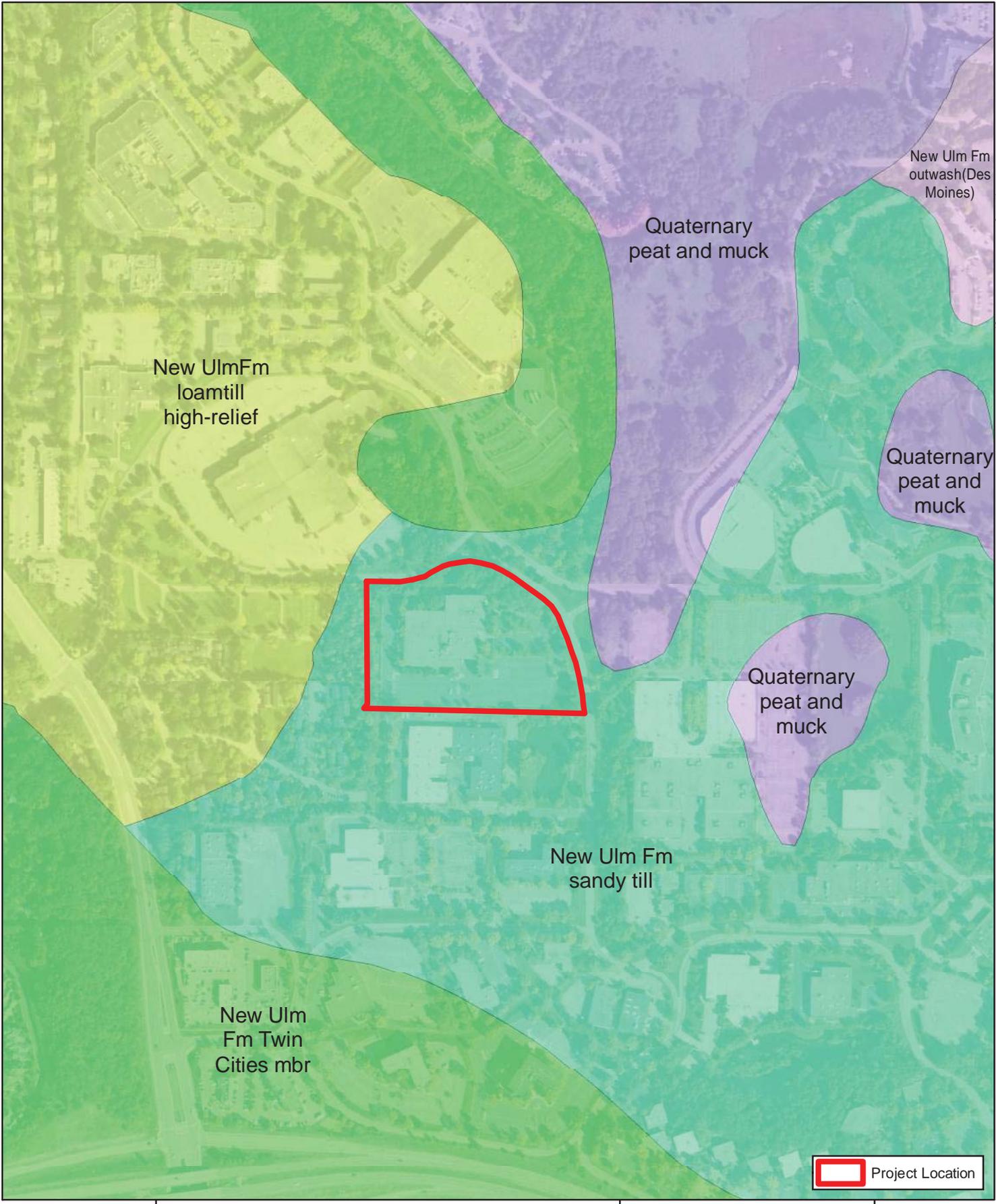


Figure 8 : Surficial Geology

Dominium EAW
Minnetonka, MN



0 500 Feet
1 inch = 500 feet



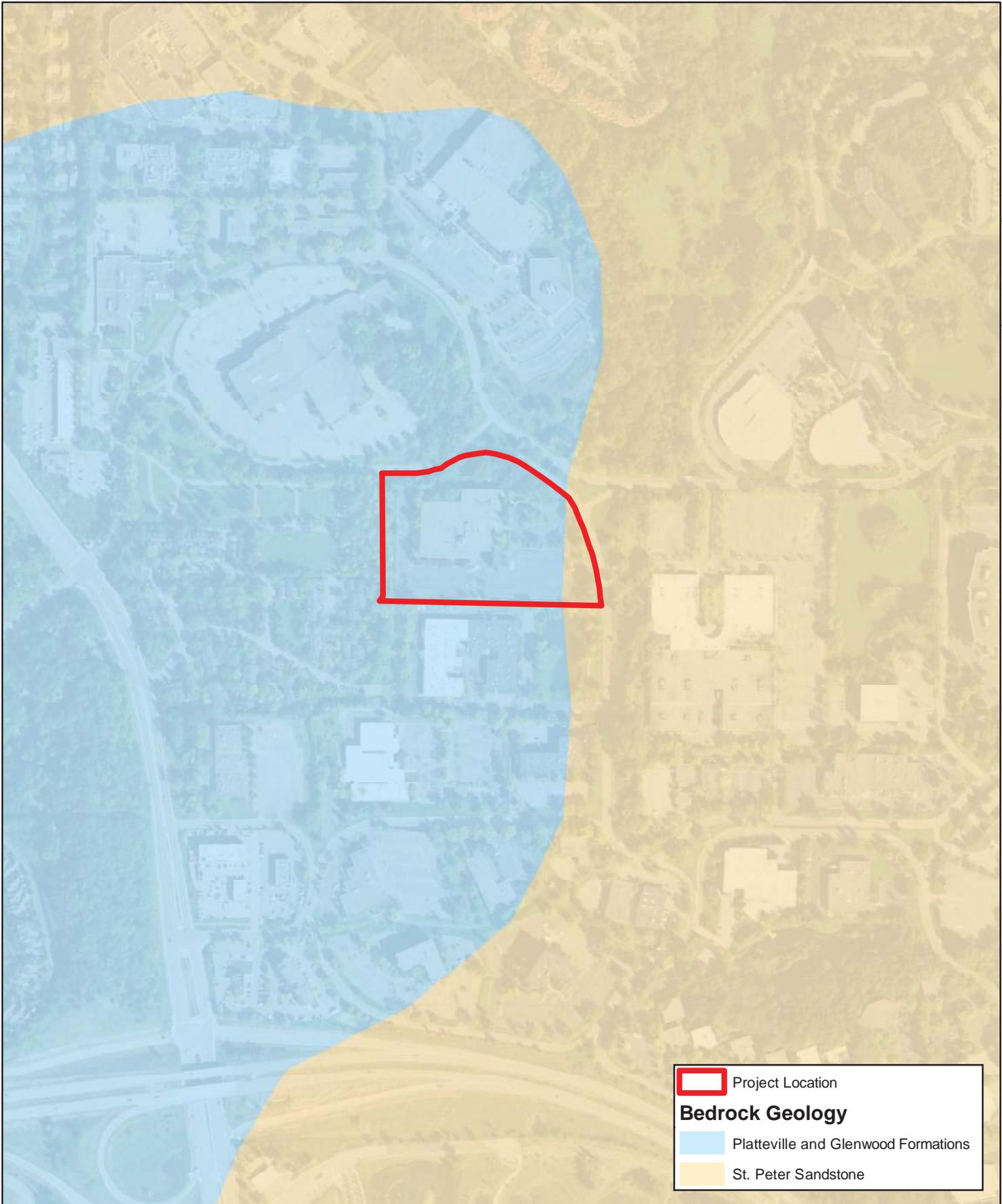


Figure 9 : Bedrock Geology

Dominium EAW
Minnetonka, MN



0 500 Feet
1 inch = 500 feet



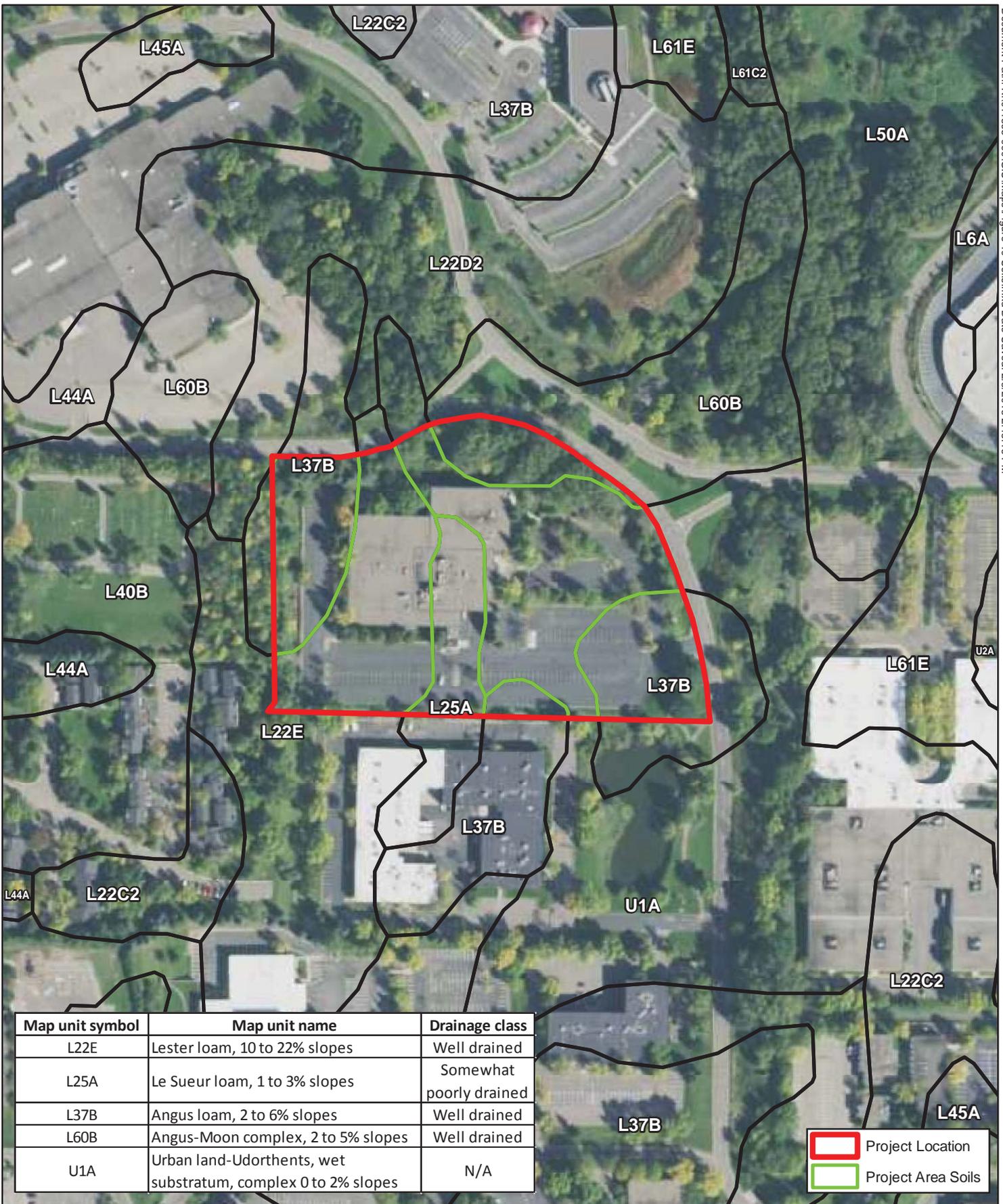
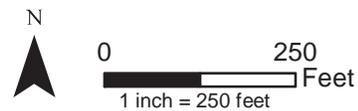


Figure 10 : Soil Survey of Hennepin County
 Dominion EAW
 Minnetonka, MN





	Project Location
	DNR Public Waters Inventory
	DNR National Wetlands Inventory
FEMA Data	
	100 Year Flood
	500 Year Flood



Figure 11 : Surface Water Resources
Dominium EAW
Minnetonka, MN

N

0 500
1 inch = 500 feet Feet



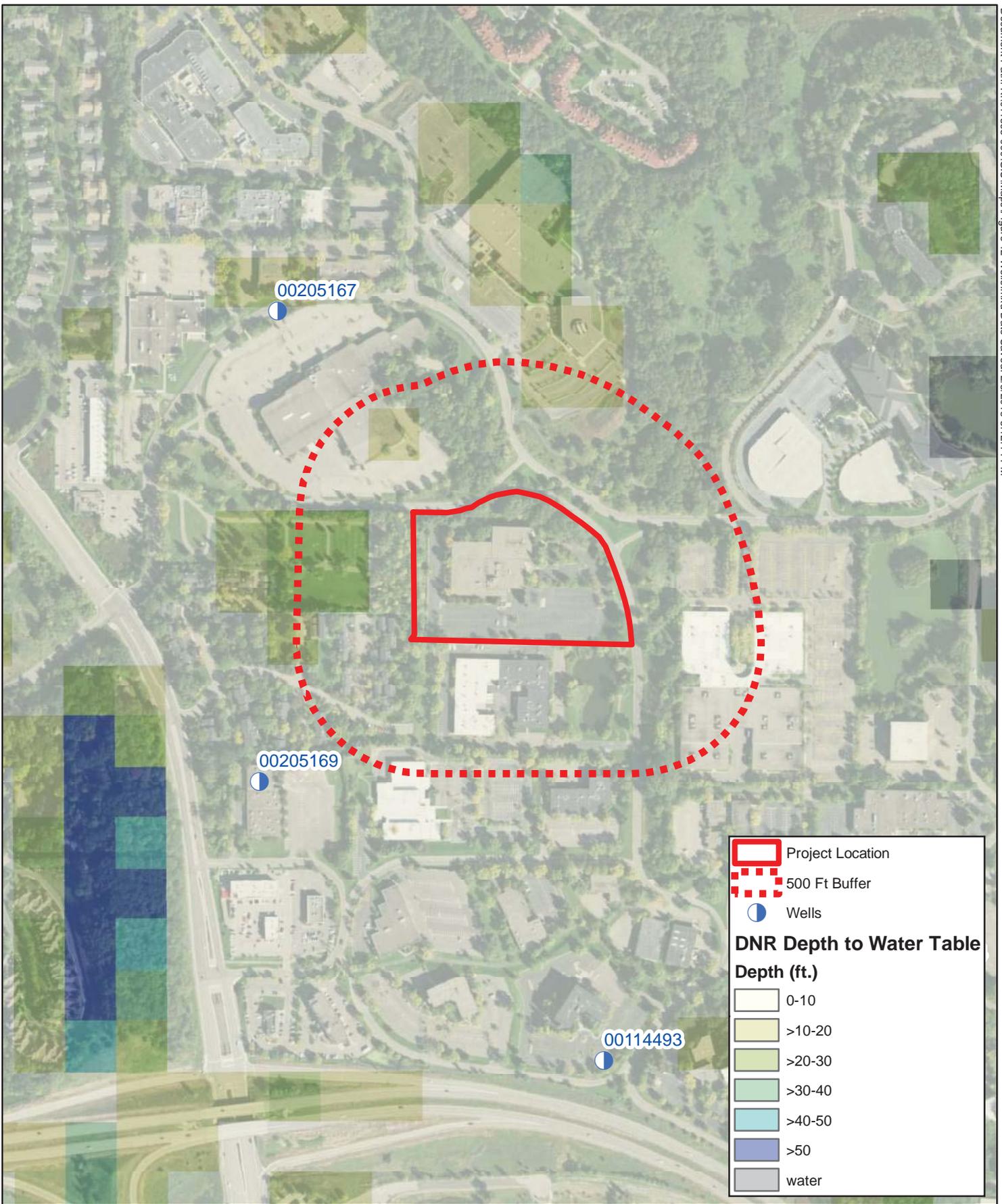


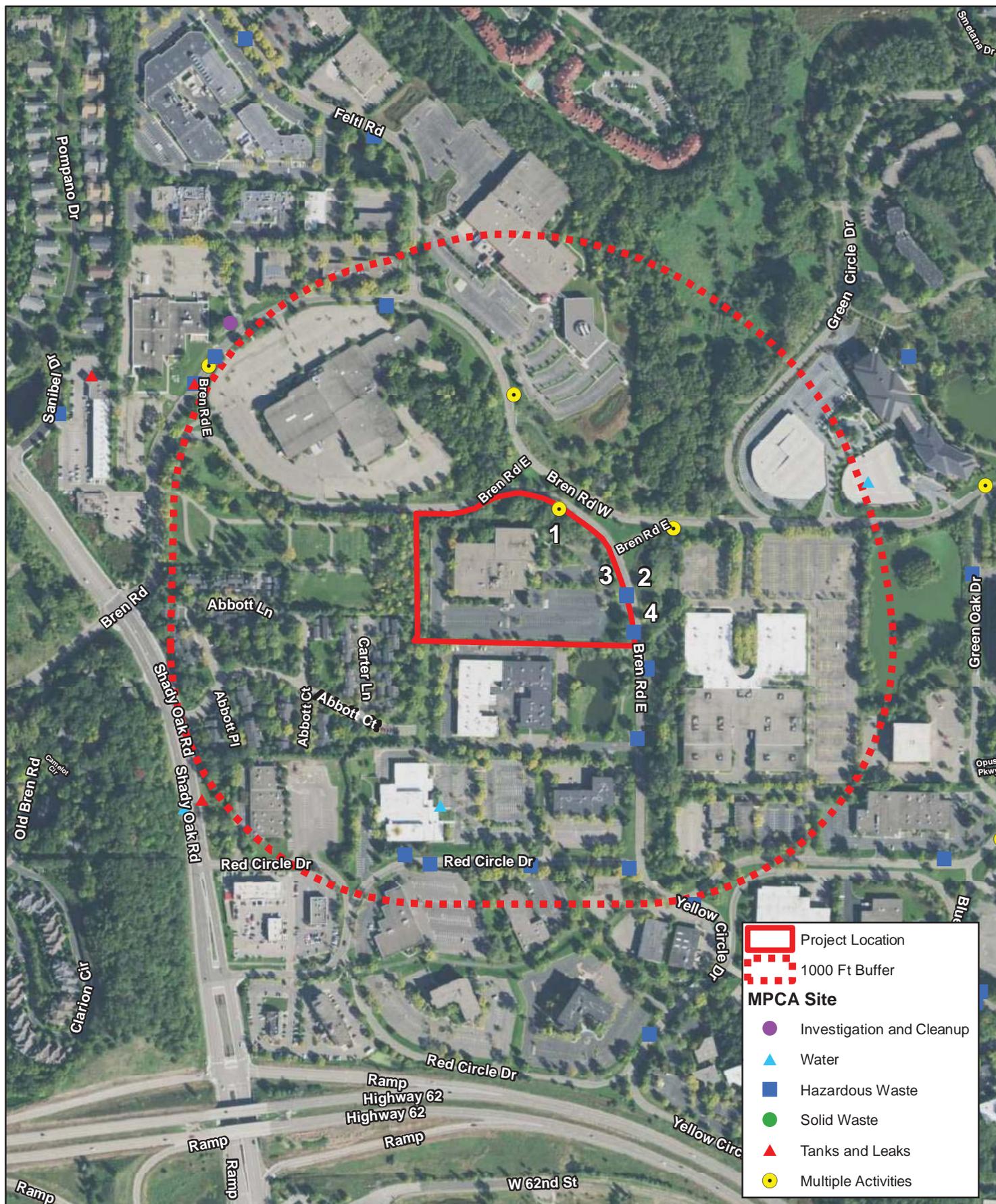
Figure 12 : Well Locations and Groundwater Resources

Dominium EAW
Minnetonka, MN



0 500 Feet
1 inch = 500 feet





	Project Location
	1000 Ft Buffer
MPCA Site	
	Investigation and Cleanup
	Water
	Hazardous Waste
	Solid Waste
	Tanks and Leaks
	Multiple Activities



Figure 13 : Potential Contamination Areas
 Dominion EAW
 Minnetonka, MN



0 500 Feet
 1 inch = 500 feet



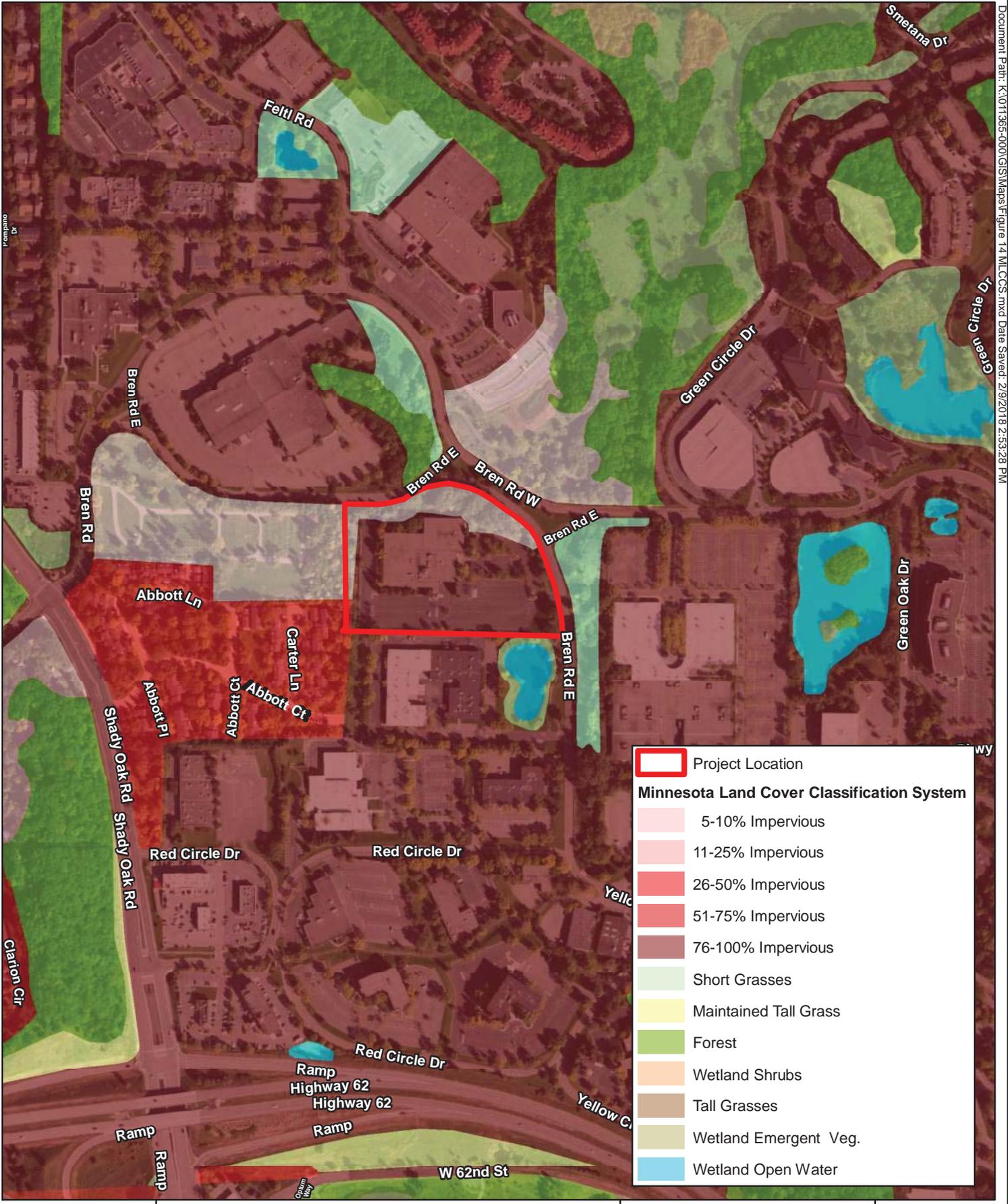
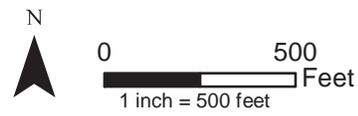


Figure 14 : MLCCS Land Cover
 Dominion EAW
 Minnetonka, MN



EAW COMMENTS

Record of Decision

Dominium EAW
City of Minnetonka

July 12, 2018

Prepared by:
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763-287-7196

I. ADMINISTRATIVE BACKGROUND

Pursuant to Minnesota Rule 4410.4300, the City of Minnetonka has prepared an Environmental Assessment Worksheet (EAW) for the proposed Dominion Development. This Record of Decision addresses the State of Minnesota environmental review requirements as established in Minnesota Rule 4410.1700. Dominion Management is the project proposer for the EAW. The City of Minnetonka is the Responsible Governmental Unit (RGU).

The EAW was filed with the Minnesota Environmental Quality Board (EQB) and circulated for review and comments to the required EAW distribution list. A Notice of Availability for the initial EAW was published in the EQB Monitor on May 7, 2018. Notices of Availability and Press Releases were published in the *Lakeshore Weekly News* on May 10, 2018.

The public comment period ended June 6, 2018. Comments were received from the Department of Transportation (MnDOT), Metropolitan Council, Minnesota Pollution Control Agency (MPCA), and State Historic Preservation Office (SHPO). All comments were considered in determining the potential for significant environmental impacts. Summaries of the comments received and the City of Minnetonka's responses to those comments are provided in Section III.

II. FINDINGS OF FACT AND CONCLUSIONS

As to the need for an Environmental Impact Statement (EIS) on this project and based on the record in this matter, including the EAW and comments received, the City of Minnetonka makes the following Findings of Fact and Conclusions:

A. PROJECT DESCRIPTION

Dominion proposes to redevelop an existing 9.4-acre commercial site in the City of Minnetonka to include 482 units of rental multi-family housing. The project is near the future Southwest Light Rail (SWLRT) and Opus Station and is located in the southwest corner of Bren Road East and Bren Road West

B. PROJECT HISTORY

- The project was subject to a mandatory EAW per Minnesota Rule 4410.4300 Subpt 19.D – Residential Development
- The EAW was distributed to the EQB and to the EQB mailing list on May 2, 2018.
- Public notices containing information about the availability of the EAW for public review were provided to the *Lakeshore Weekly News* on May 10, 2018.
- Hard copies of the EAW were provided for public review at Minnetonka City Hall and the Environmental Conservation Library. An electronic copy of the EAW was available on the City's website.
- A notice was published for the EAW in the May 7, 2018 EQB Monitor. The public comment period ended June 6, 2018. Comments were received from the Department of Transportation (MnDOT), Metropolitan Council, Minnesota Pollution Control Agency (MPCA), and State Historic Preservation Office (SHPO). Copies of these comment letters are hereby incorporated for reference and included in **Attachment A**.

C. CRITERIA FOR DETERMINING THE POTENTIAL FOR SIGNIFICANT ENVIRONMENTAL EFFECTS.

Minnesota Rule 4410.1700, subp. 1, states “An EIS [Environmental Impact Statement] shall be ordered for projects that have the potential for significant environmental effects.” In deciding whether a project has the potential for significant environmental effects, the City of Minnetonka must consider the four factors set out in Minnesota Rule 4410.1700, subp. 7. With respect to each of these factors, the City of Minnetonka finds the following:

1. MINNESOTA RULE 4410.1700, SUBP. 7.A - TYPE, EXTENT, AND REVERSIBILITY OF ENVIRONMENTAL EFFECTS

- a. The type of environmental impacts and mitigation efforts anticipated as part of this project include:

Land Use: The current Comprehensive Plan designates the future land use as Mixed Use. This land use is compatible with the project and with adjacent land uses.

Soil Disturbance: The project will involve soil disturbance. A National Pollutant Discharge Elimination System (NPDES) permit will be required and erosion control best management practices (BMPs) such as silt fence, inlet protection, and a stabilized construction entrance will be in place during construction to reduce sedimentation and prevent erosion from the site. A permit from the Nine Mile Creek Watershed District (NMCWD) will also be needed.

Wetlands: No wetland impacts are expected with the project. However, if as design progresses, wetland impacts are anticipated, wetland impacts will be minimized to the greatest extent possible and reviewed through the local and federal wetland permitting processes.

Wastewater – Additional sewer will be installed within portions of the site to serve the new development. The City is working with the developer to reconfigure the sewer system in the area to split flows between the Opus Lift Station and the gravity station. However, given the plant’s treatment capacity and the estimated wastewater that will be generated at the apartment complex, it is anticipated that the wastewater generated at the apartments will not have significant impacts on the plant’s ability to effectively treat wastewater. Additionally, given the nature of the wastewater flow, domestic wastewater, specific pretreatment measures will not be required.

Water Supply: Water to the Dominion development will be connecting to the City of Minnetonka’s existing distribution system. Similar to the wastewater flow calculations, the existing office building has daily water consumption that needs to be estimated. It was estimated that an average of 5,004 gallons of water per day are being used by the existing building. By taking this water usage estimation into consideration, the net average water usage increase on site once the apartment complex is constructed will be 112,198 gpd. Given the water usage estimated for the project site once the apartment complex is constructed, expansion of the local distribution system will not be required at this time.

The project area is located inside Edina’s Drinking Water Supply Management Area (DWSMA) (DWSMA ID #546). In terms of vulnerability, the area surrounding the project site is classified as low vulnerability. In addition to Edina’s DWSMA, the project site borders Minnetonka’s DWSMA #215. This area of the DWSMA is also classified as low vulnerability.

Stormwater: A stormwater facility will need to be designed at the site to retain 1-inch of runoff from the site and to provide water quality treatment to meet NMCWD and the City of Minnetonka requirements. The development plans to include underground infiltration and irrigation reuse to manage stormwater within the site. There will be no anticipated downstream environmental effects from the proposed project based on the project needing to meet state and local requirements.

Wildlife: The DNR Natural Heritage Database has indicated that there are no records or listings within the project area or a 1-mile buffer. Based on this review and a review of the site in its current developed conditions, the project is anticipated to have no impact on rare or threatened species.

Noise: The project site is located within a suburban area and is surrounded by both Trunk Highway (TH) 62 and TH169 freeways, office, industrial, and institutional development. Existing noise sources consist mainly of traffic on the area freeways and roadways. There are no sensitive receptors (such as hospitals) near the site which raise special concerns for further study; however construction noise will be limited consistent with the City of Minnetonka's noise ordinance.

Transportation: The city has performed a number of studies to manage traffic within the Opus business park and the interchanges of the adjacent regional freeway systems at TH62 and TH169. The baseline for these studies are managing the Bren Road and TH169 interchange capacity. Prior to 2006, the Bren Road and TH169 interchange was essentially operating at PM peak hour capacity. Without improvements to increase that capacity, the city could not approve development projects that would decrease the level of service.

In 2006, United Health Group (UHG), the largest employer in Opus, proposed an expansion of its campus located just south and west of the Bren Road and TH169 interchange. The expansion would include two 10-story buildings comprising 650,000 sf and an additional 5,400 daily trips. As proposed, the expansion would have exceeded the PM peak hour capacity of the Bren Road and TH169 interchange. In addition to the UHG expansion, the city was also evaluating other pending development proposals which would also increase regional system trips.

In anticipation of those developments, the city prepared a traffic study for both phases of the United Health Group Data Park Expansion as part of the site master planning. This study also satisfied the transportation related questions for the project's required EAW at the time. The following traffic studies helped inform this study:

- Minnetonka Mixed-Use Development Traffic Study - February 28, 2006 - 36,000 sf mixed use development
- American Medical Systems Traffic Study, March 22, 2006 - 50,000 sf office development
- Opus Traffic Study, May 1, 2006 - 238,000 sf office development
- UHG Expansion Traffic Study, October 16, 2006 - Phase I 360,000 sf office, Phase II 300,000 sf of office

To better plan for future redevelopment and growth in Opus, the city committed to improving the interchange in 2009. The improved interchange was designed to accommodate all future growth identified in the city's 2030 Comprehensive Plan. After accounting for all approved and proposed development, there was a reserve capacity of 789 PM peak hour trips which was allocated to the parcels in the Opus Overlay District.

As a long-term management tool, the city developed the Opus Area Overlay Ordinance to control the distribution of the reserve PM peak hour capacity by parcel. The ordinance allots the 789 PM peak hour trips to the parcels within the Opus Overlay area. The potential traffic growth in the travel-shed for the interchange is limited to the area of the Overlay Ordinance and therefore the ordinance ensures that the capacity of the interchange will not be exceeded.

Copies of the different traffic studies and the ordinance are available from at the city. From all the past traffic study and subsequent ordinance that the city put in place to address traffic and development in this area, this information satisfies and replaces the need for an additional traffic study for the Dominion EAW.

The Dominion project is a redevelopment project. Based on the Opus Overlay ordinance, the proposed development would create fewer peak direction trips than the site is allocated within the ordinance and would still fall within the threshold for capacity at the Bren Road and TH169 interchange.

- b. The extent and reversibility of environmental impacts for the proposed project are consistent with those of a typical residential development project. Impacts will be minimized to the extent practical, with mitigation provided for those impacts which cannot be avoided to resources.

2. MINNESOTA RULE 4410.1700, SUBP. 7.B - CUMULATIVE POTENTIAL EFFECTS OF RELATED OR ANTICIPATED FUTURE PROJECTS

Development is expected to begin in 2018 and be completed in 2019. Redevelopment is occurring in some areas of the city around the area. The proposed Southwest Line Light Rail (SWLRT) extension and light rail station will be located immediately east of the proposed Dominion development. The SWLRT is expected to be in operation by 2023. While redevelopment is likely to occur around the SWLRT, as identified in the SWLRT EIS, there are no specific known redevelopment occurring around the Dominion project that would interact with the project.

3. MINNESOTA RULE 4410.1700, SUBP. 7.C - THE EXTENT TO WHICH ENVIRONMENTAL AFFECTS ARE SUBJECT TO MITIGATION BY ONGOING PUBLIC REGULATORY AUTHORITY

- a) The following permits or approvals will be required for the project:

Unit of Government	Type of Application	Status
Federal		
US Army Corps of Engineers	Section 404 Permit	To Be Obtained, if needed
State		
Department of Natural Resources	Water Appropriation Permit	To Be Obtained, if needed
Pollution Control Agency	NPDES Construction Permit	To Be Obtained

Pollution Control Agency	Sanitary Sewer Extension	To Be Obtained, if needed
Pollution Control Agency	Section 401 Permit	To Be Obtained, if needed
Department of Health	Watermain Extension	To Be Obtained, if needed
Department of Health	Permit to abandon and seal private wells	To Be Obtained, if needed
Department of Transportation	Work within or affecting MnDOT ROW	To Be Obtained, if needed
Local		
City of Minnetonka	Development Application/Land Disturbance Permit	To Be Obtained
City of Minnetonka	Building Permits	To Be Obtained
City of Minnetonka	Wetland Conservation Act Approval	To Be Obtained, if needed
City of Minnetonka	Preliminary and Final Plat Approvals	To Be Obtained
Nine Mile Creek Watershed District	Grading Permit	To Be Obtained
Nine Mile Creek Watershed District	Erosion and Sediment Control	To Be Obtained
Nine Mile Creek Watershed District	Stormwater Management	To Be Obtained
Metropolitan Council	Sanitary Sewer Connection Permit	To Be Obtained

b) The City of Minnetonka finds that the potential impacts identified as part of the proposed Dominion project are minimal and can be addressed through the regulatory agencies as part of the permitting process. As a result, additional analysis of these impacts is not required.

4. MINNESOTA RULE 4410.1700, SUBP. 7.D - THE EXTENT TO WHICH ENVIRONMENTAL EFFECTS CAN BE ANTICIPATED AND CONTROLLED AS A RESULT OF OTHER AVAILABLE ENVIRONMENTAL STUDIES UNDERTAKEN BY PUBLIC AGENCIES OR THE PROJECT PROPOSER, INCLUDING OTHER EISs.

The city finds:

1. The proposed project is reasonably similar to other mixed-use development projects.
2. The city is planning for development with its Comprehensive Plan process. Development is required to meet the city's local planning and permitting requirements.
3. Development impacts within the city are subject to local, regional, state, and federal requirements.
4. Considering the results of environmental review and permitting processes for similar projects, the City of Minnetonka finds that the environmental effects of the project can be adequately anticipated, controlled, and mitigated.

D. CONCLUSIONS

The Dominion EAW and comments received have generated information adequate to

determine that the proposed project does not have the potential for significant environmental effects.

The EAW has identified areas where the potential for environmental effects exist, but appropriate mitigation measures can be incorporated into the project plans and the required approvals and permits to mitigate these effects will be obtained. The project will comply with all local, county, and federal review agency requirements.

Based on the criteria established in Minnesota Rule 4410.1700, the project does not have the potential for significant environmental effects.

Based on the Findings of Fact and Conclusions, the project does not have the potential for significant environmental impacts.

Therefore, an EIS is not required for the Dominion Area Development.

III. AGENCY COMMENTS AND CITY OF MINNETONKA'S RESPONSES

A 30-day comment period for the Dominion EAW ended on June 6, 2018. Comments were received from Department of Transportation (MnDOT), Metropolitan Council, Minnesota Pollution Control Agency (MPCA), and State Historic Preservation Office (SHPO). Responses are provided below. These letters are included in **Attachment A**.

Minnesota Department of Transportation

Comment 1: A traffic study is required, as the development is expected to generate more than 2,500 trips per day. The study should include the interchanges of US 169/Bren Rd and MN 62/Shady Oak Rd. MnDOT guidance on traffic impact studies can be found in Chapter 5 of the Access Management Manual.

Response 1: The city met with MnDOT and Met Council on June 11, 2018 to discuss the comments that a traffic study was needed. The City has completed numerous traffic studies for the larger regional area and has adopted an ordinance that allocates trips to development sites to stay within the range of the previous traffic studies. The city has provided a summary of the past traffic studies that were completed in the area to MnDOT and Met Council on July 12, 2018. Based on these studies, the city does not think that additional traffic studies will provide meaningful information to the city or to the agencies. The Dominion site meets the requirement of the overlay ordinance for the area and therefore a traffic study is not needed. The city will continue to work with and coordination with these agencies to address traffic concerns.

Comment 2: Permits: Any use of or work within or affecting MnDOT right of way requires a permit. Permit forms are available from MnDOT's utility website.

Response 2: Work in MnDOT Right-of-Way is not expected for this project. If it is needed, a permit will be obtained.

Metropolitan Council

Comment 1: Item 18 – Transportation. The Met Council understands that the City is aware of the need to complete a traffic impact study as part of the EAW. EQB rules require such a study to be complete expected to generate more than 2,500 trips. In the

case of the Dominion project, the estimated total daily trips is 2,622. In particular, the traffic impact study should analyze impacts on the interchanges of Trunk Highway 62 and Shady Oak Road as well as U.S. 169 and Bren Road W.

Response 1: The city met with MnDOT and Met Council on June 11, 2018 to discuss the comments that a traffic study was needed. The City has completed numerous traffic studies for the larger regional area and has adopted an ordinance that allocates trips to development sites to stay within the range of the previous traffic studies. The city has provided a summary of the past traffic studies that were completed in the area to MnDOT and Met Council on July 12, 2018. Based on these studies, the city does not think that additional traffic studies will provide meaningful information to the city or to the agencies. The Dominion site meets the requirement of the overlay ordinance for the area and therefore a traffic study is not needed. The city will continue to work with and coordination with these agencies to address traffic concerns.

Comment 2: Item 9. Land Use - Forecasts (Todd Graham, 651-602-1322)
The project site is located in transportation analysis zone (TAZ) #1022. The Metropolitan Council's preliminary set of forecasts includes an increase of just 51 1 households from 2014 to 2040. Council staff expect that other housing redevelopment will occur in this zone. We recommend that the City increase the forecast allocation for TAZ #1022 by 400 additional households and 900 population. This can be balanced with redistribution from other TAZs. This can be reflected in your comprehensive plan update.

Response 2: The city will review this as part of the Comprehensive Plan update.

Comment 3: Item 9 - Land Use (Michael Larson, 651-602-1407). The project site is located immediately adjacent to the future Opus Station on the METRO Green Line. The 1/2-mile station area will be subject to density and activity level policies of the 2040 Transportation Policy Plan (TPP) during the Council's review of the City's comprehensive plan update. Among all areas that are planned for residential redevelopment in the Opus Station Area, the TPP requires an average minimum guiding density of 20 dwelling units per acre (du/acre). Evaluated on its own, the proposed project meets this minimum standard at approximately 51 du/acre. Please refer to the fact sheet in the Local Planning Handbook, Density and Activity Near Transit, for additional guidance.

The EAW correctly identifies the current guiding land use in Minnetonka's comprehensive plan as Mixed Use. The Opus area is identified in the City's comprehensive plan as a Major Change Site with a minimum density of 20 du/acre. No upper limit is identified. Please note that any new guiding land uses proposed in the forthcoming comprehensive plan amendment should include an upper limit. We encourage the City to adopt a density range that goes beyond the minimum, one which implements the target densities identified in the TPP (40-75+ units/acre).

Response 3: The city will take this comment under advisement and review this within its Comprehensive Plan process.

Comment 4: Item 18 – Transportation (Russ Owen, 651-602-1724). The traffic information provided in the table suggests that the development will only generate 14 inbound trips in the AM peak hour and 26 trips in the PM outbound peak hour. This likely underestimates the trip generation for those periods.

Response 4: See response to Comment #1.

Minnesota Pollution Control Agency (MPCA)

Comment 1: Water Resources (Item 11). The EAW does not contain specifics on the best management practices (BMPs) to be implemented. Due to an impaired water located within 1 mile of the Project, the Project proposer will need to implement Appendix A. Part C. 1 and 2 of the National Pollutant Discharge Elimination System/State Disposal System (NPDES/SDS) Construction Stormwater Permit (CSW permit), including stabilizing soils within 7 days of temporarily or permanently ceasing soil disturbing activity on any portion of the site and utilizing temporary sediment basins to control runoff during construction if 5 acres of disturbed soils will drain to one location. The Project proposer will also need to ensure adequate BMPs are used to protect the wetland on the site from sedimentation

Response 1: The developer will need to apply and meet all permit requirements, including the NPDES permit. The developer will also ensure adequate BMPs and wetland protection measures are in place at the site.

Comment 2: Low Impact Design. The MPCA advocates the use of Low Impact Design (LID) practices to aid in the minimization of stormwater impacts. LID is a stormwater management approach and site-design technique that emphasizes water infiltration, values water as a resource, and promotes the use of natural systems to treat water runoff. Examples include: special ditches arranged in a series that soak up more water, vegetated filter strips at the edges of paved surfaces, trees or swales between rows of cars, residential or commercial rain gardens designed to capture and soak in stormwater, porous pavers, concrete and asphalt for sidewalks and parking lots, narrower streets, rain barrels and cisterns, and green roofs.

LID concepts may be found in the State of Minnesota Stormwater Manual dated November 2005 located on the MPCA website at:
<http://www.pcastate.mn.us/water/stormwater/stormwater-manual.html>.
In addition, the MPCA LID webpage provides a description and examples of LID features such as permeable pavement, rain gardens, and green roofs. Links to other resources on LID are available as well. The website is located at:
<https://www.oca.state.mn.us/water/stormwater-management-lowimpact-development-and-green-infrastructure>.

Response 2: The developer will consider the use of Low Impact Design practices to minimize stormwater impacts.

Comment 3: Contamination/Hazardous Materials/Wastes (Item 12). This section of the EAW should provide information regarding the historical uses of the actual Project site, including the potential for soil or groundwater contamination as a result of those uses. This information is usually gathered during Phase I and Phase II investigations of projects prior to site development. State law requires that persons properly manage contaminated soil and water they uncover or disturb - even if they are not the party responsible for the contamination. Developers considering construction on or near contaminated properties should begin working early in their planning process with the MPCA's Brownfields Program to receive necessary technical assistance in managing contamination. For some properties, special construction might be needed to prevent the further

spreading of the contamination and/or prevent vapors from entering buildings or utility corridors. Information regarding the Brownfields Program can be found at: <https://www.pca.state.mn.us/waste/brownfields>. If contamination is found, it must be reported immediately to the state duty officer at 651-649-5451 or 800-422-0798.

Response 3: The city is not aware of contamination in the area, but will take this comment under advisement as development occurs in the area. The city may require review of a Phase I Environmental Site Assessment from the developer.

Comment 4: Noise (Item 17) The MPCA appreciates that the Project proposer plans to use mufflers on all construction equipment, and that noise will be limited to the daytime hours of 7 a.m. to 10 p.m. The MPCA does, however, have some additional comments.

The MPCA treats the state noise standards as a total standard. Considering the number of residential units being planned and the development's relative proximity to major roadways (Highway 62, Highway 169, and Shady Oak Road) and local office/business uses, there is some concern about post-construction noise in the residential development. Residences are considered to be under the strictest state noise standards, under Noise Area Classification (NAC) 1. State noise standards are receptor-based and are applied independently of municipal zoning.

The MPCA would like the City to keep in mind Minn. R. ch. 7030.0030, "Noise Control Requirement," which states "[a]ny municipality having authority to regulate land use shall take all reasonable measures within its jurisdiction to prevent the establishment of land use activities listed in noise area classification (NAC) 1, 2, or 3 in any location where the standards established in part 7030.0040 will be violated immediately upon establishment of the land use." Thus, the MPCA encourages the City to conduct a noise monitoring study (instead of or in addition to modeling) in the proposed Project area prior to any final planning activities. We suggest doing so to allow for the avoidance or mitigation of any potential noise exceedances prior to final Project planning, as traffic and residential-related noise may increase after development occurs. For noise related questions, please contact Christine Steinwand at 651-757-2327 or Christine.Steinwand@state.mn.us.

Response 4: The City will consider a noise analysis to determine if any mitigation measures may be warranted.

State Historic Preservation Office

Comment 1: Based on our review of the project information, we conclude that there are no properties listed in the National or State Registers of Historic Places, and no known or suspected archaeological properties in the area that will be affected by this project.

Response 1: No response is needed.

Attachment A – Agency Comments

June 5, 2018

Loren Gordon, City Planner
City of Minnetonka
14600 Minnetonka Boulevard
Minnetonka, MN 55343

RE: City of Minnetonka Dominion Environmental Assessment Worksheet (EAW)
Metropolitan Council Review No. 21940-1
Metropolitan Council District No. 3

Dear Mr. Gordon:

The Metropolitan Council received the EAW for the Dominion project in Minnetonka on May 4, 2018. The proposed project is located 11001 Bren Road E. The proposed development consists of 9.4 acres with 482 units of multi-family rental housing.

Staff finds this EAW to be incomplete. Consequently, we are unable to determine at this time whether the project fully conforms with regional system plans and policies. We encourage you to issue a supplemental EAW to address the missing data.

Item 18 – Transportation (Russ Owen, 651-602-1724)

We understand that the City is aware of the need to complete a traffic impact study as part of the EAW. EQB rules require such a study to be completed when a development is expected to generate more than 2,500 trips. In the case of the Dominion project, the estimated total daily trips is 2,622. In particular, the traffic impact study should analyze impacts on the interchanges of Trunk Highway 62 and Shady Oak Road as well as U.S. 169 and Bren Road W.

We also offer the following comments for your consideration.

Item 9. Land Use - Forecasts (Todd Graham, 651-602-1322)

The project site is located in transportation analysis zone (TAZ) #1022. The Metropolitan Council's preliminary set of forecasts includes an increase of just 511 households from 2014 to 2040. Council staff expect that other housing redevelopment will occur in this zone. We recommend that the City increase the forecast allocation for TAZ #1022 by 400 additional households and 900 population. This can be balanced with redistribution from other TAZs. This can be reflected in your comprehensive plan update.

Item 9. Land Use - Plans (Michael Larson, 651-602-1407)

The project site is located immediately adjacent to the future Opus Station on the METRO Green Line. The ½-mile station area will be subject to density and activity level policies of the *2040 Transportation Policy Plan (TPP)* during the Council's review of the City's comprehensive plan update. Among all areas that are planned for residential redevelopment in the Opus Station Area, the TPP requires an average minimum guiding density of 20 dwelling units per acre (du/acre). Evaluated on its own, the proposed project meets this minimum standard at approximately 51 du/acre. Please refer to the

fact sheet in the Local Planning Handbook, Density and Activity Near Transit, for additional guidance.

The EAW correctly identifies the current guiding land use in Minnetonka's comprehensive plan as Mixed Use. The Opus area is identified in the City's comprehensive plan as a Major Change Site with a minimum density of 20 du/acre. No upper limit is identified. Please note that any new guiding land uses proposed in the forthcoming comprehensive plan amendment should include an upper limit. We encourage the City to adopt a density range that goes beyond the minimum, one which implements the target densities identified in the TPP (40-75+ units/acre).

Item 18 – Transportation (Russ Owen, 651-602-1724)

The traffic information provided in the table suggests that the development will only generate 14 inbound trips in the AM peak hour and 26 trips in the PM outbound peak hour. This likely underestimates the trip generation for those periods.

This concludes the Council's review of the EAW. The Council will not take formal action on the EAW. If you have any questions or need further information, please contact Michael Larson, Principal Reviewer, at 651-602-1407.

Sincerely,



LisaBeth Barajas, Manager
Local Planning Assistance

CC. Tod Sherman, Development Reviews Coordinator, MnDOT - Metro Division
Jennifer Munt, Metropolitan Council District 3
Michael Larson, AICP, Sector Representative / Principal Reviewer
Raya Esmaeili, Reviews Coordinator

March 23rd, 2018

Loren Gordon
City of Minnetonka
14600 Minnetonka Boulevard
Minnetonka, MN 55435

SUBJECT: EAW18-009
Dominium Management
WB Exit Ramp to Shady Oaks Rd CSAH 61
Minnetonka, Hennepin County
Control Section 2773

Dear Mr. Gordon:

Thank you for the opportunity to review the Dominium Management Environmental Assessment. Please note that MnDOT's review of this EAW does not constitute approval of a regional traffic analysis and is not a specific approval for access or new roadway improvements. As plans are refined, we would like the opportunity to meet with our partners and to review the updated information. MnDOT's staff has reviewed the document and has the following comments:

Traffic:

A traffic impact study is required, as the development is expected to generate more than 2,500 trips per day. The study should include the interchanges of US 169/Bren Rd and MN 62/Shady Oak Rd. MnDOT guidance on traffic impact studies can be found in Chapter 5 of the Access Management Manual, located here: <http://www.dot.state.mn.us/accessmanagement/docs/pdf/manualchapters/chapter5.pdf>

For questions regarding these comments please contact Jason Junge at 651-234-7875 or jason.junge@state.mn.us.

Permits:

Any use of or work within or affecting MnDOT right of way requires a permit. Permit forms are available from MnDOT's utility website: <http://www.dot.state.mn.us/utility/forms.html>. Please include one 11x17 plan set and one full size plan set with each permit application.

Please direct any questions regarding permit requirements to Buck Craig (651-234-7911) of MnDOT's Metro Permits Section.

Review Submittal Options:

MnDOT's goal is to complete the review of plans within 30 days. Submittals sent in electronically can usually be turned around faster. There are four submittal options. Please submit either:

1. An electronic .pdf version of the plans. MnDOT can accept the plans via e-mail at metrodevreviews.dot@state.mn.us provided that each separate e-mail is less than 20 megabytes.
2. A compact disc with the plans in .pdf format. The disc can be sent to:

MnDOT – Metro District Planning Section
Development Reviews Coordinator
1500 West County Road B-2

Roseville, MN 55113

3. A .pdf version of the plans sent to MnDOT's external shared workspace site located at: <https://mft.dot.state.mn.us> Please contact MnDOT development review staff gain access to the shared workspace site. Also, please send a note to metrodevreviews.dot@state.mn.us indicating the file name and stating that the plans have been submitted on the shared workspace site.
4. If you are unable to send the plans electronically, please submit a set of full size plans to the above address.

If you have any questions concerning this review, please contact me at 651-234-7788.

Sincerely,

Jennifer Wiltgen
MnDOT Principal Planner

Copy sent via E-Mail:
Buck Craig, Permits
Nancy Jacobson, Design
Shane Rowbotham, Design
Hailu Shekur, Water Resources
Cameron Muhic, Bike-Ped
Doug Nelson, Right-of-Way
Russ Owen, Metropolitan Council
Jason Junge, Traffic
Natalie Ries, Noise
Andrew Lutaya, Area Engineer

June 6, 2018

Loren Gordon
City of Minnetonka
14600 Minnetonka Boulevard
Minnetonka, MN 55345

Re: Dominion Environmental Assessment Worksheet

Dear Loren Gordon:

Thank you for the opportunity to review and comment on the Environmental Assessment Worksheet (EAW) for the Dominion project (Project) in the city of Minnetonka, Hennepin County, Minnesota. The Project consists of construction of a multi-family housing development. Regarding matters for which the Minnesota Pollution Control Agency (MPCA) has regulatory responsibility or other interests, the MPCA staff has the following comments for your consideration.

Water Resources (Item 11)

Stormwater

- The EAW does not contain specifics on the best management practices (BMPs) to be implemented. Due to an impaired water located within 1 mile of the Project, the Project proposer will need to implement Appendix A. Part C. 1 and 2 of the National Pollutant Discharge Elimination System/State Disposal System (NPDES/SDS) Construction Stormwater Permit (CSW permit), including stabilizing soils within 7 days of temporarily or permanently ceasing soil disturbing activity on any portion of the site and utilizing temporary sediment basins to control runoff during construction if 5 acres of disturbed soils will drain to one location.
- The Project proposer will also need to ensure adequate BMPs are used to protect the wetland on the site from sedimentation. Questions regarding CSW permit requirements should be directed to Roberta Getman at 507-206-2629 or Roberta.Getman@state.mn.us.

Low Impact Design

The MPCA advocates the use of Low Impact Design (LID) practices to aid in the minimization of stormwater impacts. LID is a stormwater management approach and site-design technique that emphasizes water infiltration, values water as a resource, and promotes the use of natural systems to treat water runoff. Examples include:

- Special ditches, arranged in a series, that soak up more water
- Vegetated filter strips at the edges of paved surfaces
- Trees or swales between rows of cars
- Residential or commercial rain gardens designed to capture and soak in stormwater
- Porous pavers, concrete and asphalt for sidewalks and parking lots
- Narrower streets
- Rain barrels and cisterns
- Green roofs

LID concepts may be found in the *State of Minnesota Stormwater Manual* dated November 2005 located on the MPCA website at: <http://www.pca.state.mn.us/water/stormwater/stormwater-manual.html>.

In addition, the MPCA LID webpage provides a description and examples of LID features such as permeable pavement, rain gardens, and green roofs. Links to other resources on LID are available as well. The website is located at: <https://www.pca.state.mn.us/water/stormwater-management-low-impact-development-and-green-infrastructure>.

Contamination/Hazardous Materials/Wastes (Item 12)

- This section of the EAW should provide information regarding the historical uses of the actual Project site, including the potential for soil or groundwater contamination as a result of those uses. This information is usually gathered during Phase I and Phase II investigations of projects prior to site development. State law requires that persons properly manage contaminated soil and water they uncover or disturb - even if they are not the party responsible for the contamination. Developers considering construction on or near contaminated properties should begin working early in their planning process with the MPCA's Brownfields Program to receive necessary technical assistance in managing contamination. For some properties, special construction might be needed to prevent the further spreading of the contamination and/or prevent vapors from entering buildings or utility corridors. Information regarding the Brownfields Program can be found at: <https://www.pca.state.mn.us/waste/brownfields>. If contamination is found, it must be reported immediately to the state duty officer at 651-649-5451 or 800-422-0798.

Noise (Item 17)

The MPCA appreciates that the Project proposer plans to use mufflers on all construction equipment, and that noise will be limited to the daytime hours of 7 a.m. to 10 p.m. The MPCA does, however, have some additional comments:

- The MPCA treats the state noise standards as a total standard. Considering the number of residential units being planned and the development's relative proximity to major roadways (Highway 62, Highway 169, and Shady Oak Road) and local office/business uses, there is some concern about post-construction noise in the residential development. Residences are considered to be under the strictest state noise standards, under Noise Area Classification (NAC) 1. State noise standards are receptor-based and are applied independently of municipal zoning.
- The MPCA would like the City to keep in mind Minn. R. ch. 7030.0030, "Noise Control Requirement," which states "[a]ny municipality having authority to regulate land use shall take all reasonable measures within its jurisdiction to prevent the establishment of land use activities listed in noise area classification (NAC) 1, 2, or 3 in any location where the standards established in part 7030.0040 will be violated immediately upon establishment of the land use." Thus, the MPCA encourages the City to conduct a noise monitoring study (instead of or in addition to modeling) in the proposed Project area prior to any final planning activities. We suggest doing so to allow for the avoidance or mitigation of any potential noise exceedances prior to final Project planning, as traffic and residential-related noise may increase after development occurs. For noise related questions, please contact Christine Steinwand at 651-757-2327 or Christine.Steinwand@state.mn.us.

Loren Gordon
Page 3
June 6, 2018

We appreciate the opportunity to review this Project. Please provide your specific responses to our comments and notice of decision on the need for an Environmental Impact Statement. Please be aware that this letter does not constitute approval by the MPCA of any or all elements of the Project for the purpose of pending or future permit action(s) by the MPCA. Ultimately, it is the responsibility of the Project proposer to secure any required permits and to comply with any requisite permit conditions. If you have any questions concerning our review of this EAW, please contact me by email at Karen.kromar@state.mn.us or by telephone at 651-757-2508.

Sincerely,



Karen Kromar
Project Manager
Environmental Review Unit
Resource Management and Assistance Division

KK:bt

cc: Dan Card, MPCA, St. Paul
Roberta Getman, MPCA, Rochester
Christine Steinwand, MPCA, St. Paul
Teresa McDill, MPCA, St. Paul

June 5, 2018

Mr. Loren Gordon
City of Minnetonka
14600 Minnetonka Blvd
Minnetonka, MN 55345

RE: EAW - Dominion Redevelopment at SW corner of Bren Road E and Bren Road W
T117 R22 S36
Minnetonka, Hennepin County
SHPO Number: 2018-1921

Dear Mr. Gordon:

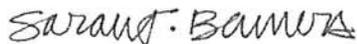
Thank you for providing this office with a copy of the Environmental Assessment Worksheet (EAW) for the above-referenced project.

Based on our review of the project information, we conclude that there are no properties listed in the National or State Registers of Historic Places, and no known or suspected archaeological properties in the area that will be affected by this project.

Please note that this comment letter does not address the requirements of Section 106 of the National Historic Preservation Act of 1966 and 36 CFR § 800. If this project is considered for federal financial assistance, or requires a federal permit or license, then review and consultation with our office will need to be initiated by the lead federal agency. Be advised that comments and recommendations provided by our office for this state-level review may differ from findings and determinations made by the federal agency as part of review and consultation under Section 106.

Please contact our Environmental Review Program at (651) 201-3285 if you have any questions regarding our review of this project.

Sincerely,



Sarah J. Beimers
Environmental Review Program Manager

cc: Andi Moffatt, WSB & Associates

History/Architecture Inventory

PROPERTY NAME	ADDRESS	Twp	Range	Sec	Quarters	USGS	Report	NRHP	CEF	DOE	Inventory Number
COUNTY	Hennepin										
CITY/TOWNSHIP:	Minnetonka										
Feltl Farmstead	5435 Feltl Rd.	117	22	36	NW-NW-NW	Hopkins					HE-MKC-014
Bridge 27545	Shady Oak Road over TH 62 WB	117	22	36	SW-SW	Hokpins					HE-MKC-081
Bridge 27546	Shady Oak Road over TH 62 EB	117	22	36	SW-SW	Hokpins					HE-MKC-082



Minnesota Department of Natural Resources
Division of Ecological & Water Resources
500 Lafayette Road, Box 25
St. Paul, MN 55155-4025

February 27, 2018

Correspondence # ERDB 20180308

Mr. Matt Unmacht
WSB & Associates, Inc.
701 Xenia Avenue South, Suite 300
Minneapolis, MN 55416

RE: Natural Heritage Review of the proposed Dominion Development,
T117N R22W Section 36; Hennepin County

Dear Mr. Unmacht,

As requested, the above project has been reviewed for potential effects to known occurrences of rare features. Given the project details provided with the data request form, I do not believe the proposed project will negatively affect any known occurrences of rare features.

The Natural Heritage Information System (NHIS), a collection of databases that contains information about Minnesota's rare natural features, is maintained by the Division of Ecological and Water Resources, Department of Natural Resources. The NHIS is continually updated as new information becomes available, and is the most complete source of data on Minnesota's rare or otherwise significant species, native plant communities, and other natural features. However, the NHIS is not an exhaustive inventory and thus does not represent all of the occurrences of rare features within the state. Therefore, ecologically significant features for which we have no records may exist within the project area. **If additional information becomes available regarding rare features in the vicinity of the project, further review may be necessary.**

For environmental review purposes, the results of this Natural Heritage Review are valid for one year; the results are only valid for the project location (noted above) and the project description provided on the NHIS Data Request Form. Please contact me if project details change or for an updated review if construction has not occurred within one year.

The Natural Heritage Review does not constitute review or approval by the Department of Natural Resources as a whole. Instead, it identifies issues regarding known occurrences of rare features and potential effects to these rare features. If needed, please contact your [DNR Regional Environmental Assessment Ecologist](#) to determine whether there are other natural resource concerns associated with the proposed project. Please be aware that additional site assessments or review may be required.

Thank you for consulting us on this matter, and for your interest in preserving Minnesota's rare natural resources. Please include a copy of this letter in any state or local license or permit application. An invoice will be mailed to you under separate cover.

Sincerely,

A handwritten signature in black ink that reads "Samantha Bump". The signature is written in a cursive style with a large, prominent "S" and "B".

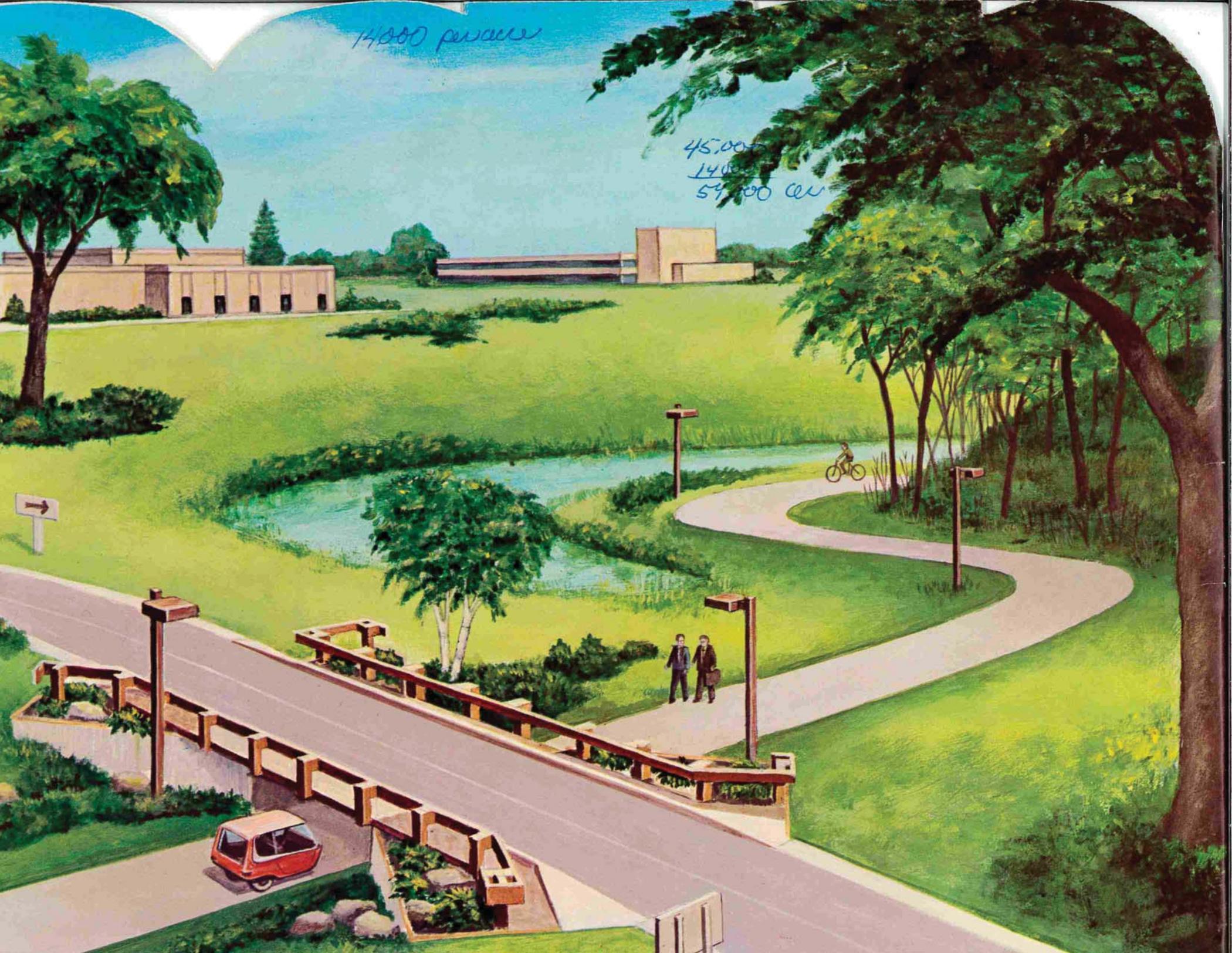
Samantha Bump
Natural Heritage Review Specialist
Samantha.Bump@state.mn.us

Links: DNR Regional Environmental Assessment Ecologist Contact Info
http://www.dnr.state.mn.us/eco/ereview/erp_regioncontacts.html

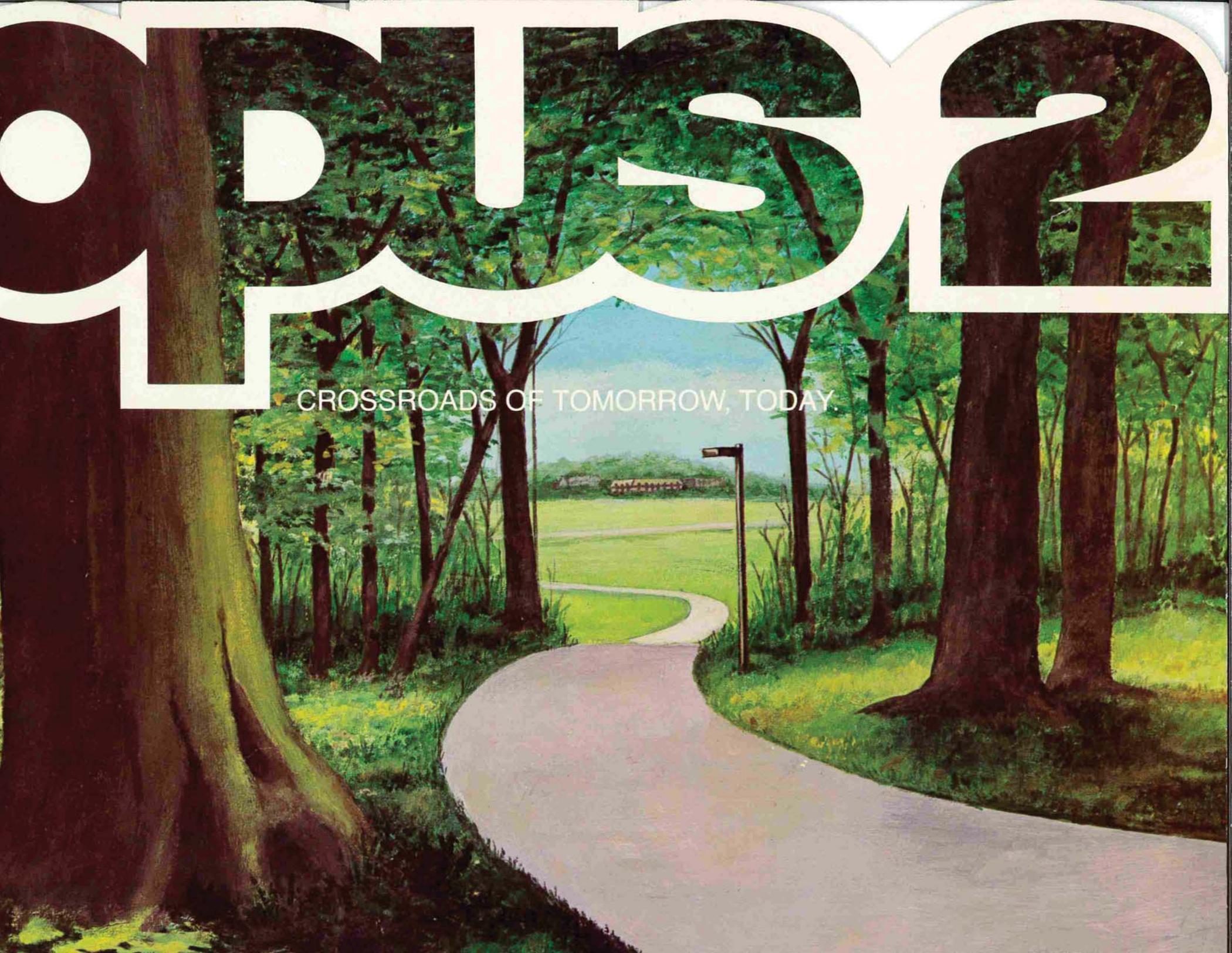
OPUS DEVELOPMENT
ORIGINAL CONCEPT

14,000 pavers

45,000
14,000
59,000 c/w



OPTUS



CROSSROADS OF TOMORROW, TODAY.

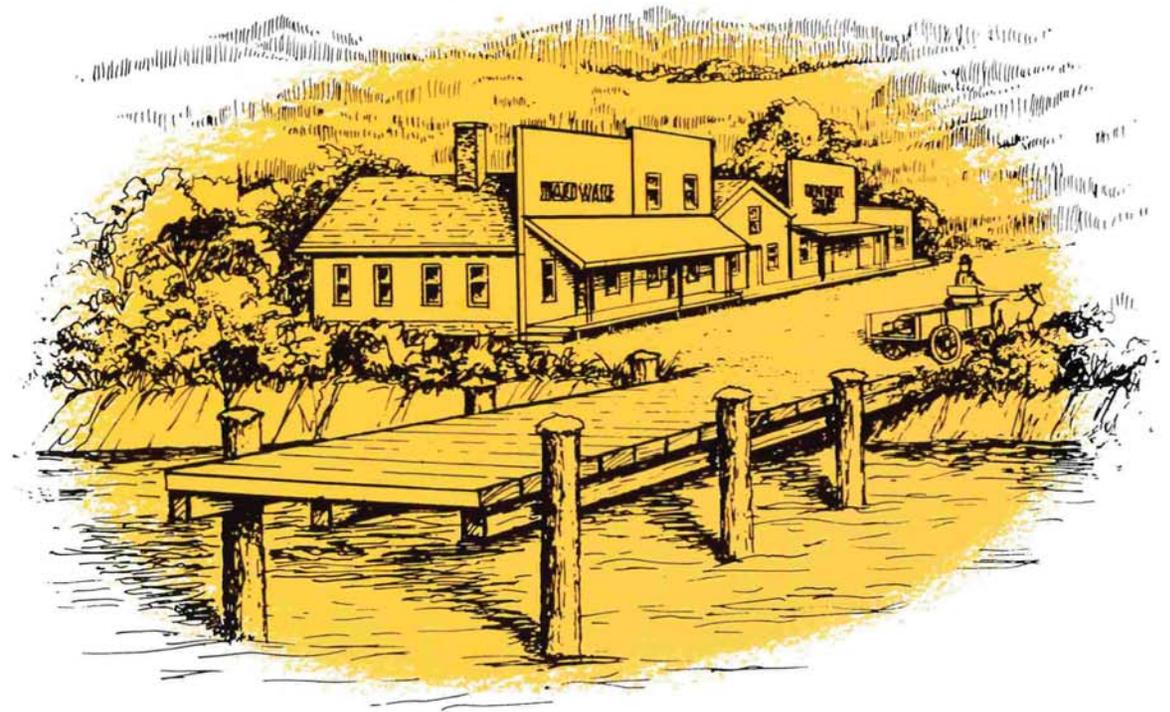
NEW LIFE IN THE RAW FRONTIER

In the early 1800's, Minnesota was a vast tract of land inhabited only by various bands of Chippewa and Sioux Indians. Around the middle of the century things started to change. Settlers arrived in increasing numbers at St. Paul Landing, the recently designated political capitol for the large expanse of land between the St. Croix and Missouri Rivers.

After a short stay in St. Paul, many of the settlers moved further up river to the smaller village of St. Anthony, the sawmill town by the falls. St. Paul and St. Anthony, both raw frontier communities, offered the excitement, hustle and bustle characteristic of newly created boom towns.

The trail to points west led from these fledgling cities past Lake Calhoun, Lake Harriet, paralleled Minnehaha Creek and eventually ended in the rich farm land surrounding Lake Minnetonka. Those here for the purpose of homesteading or farming followed this trail westward in search of fertile land.

The area comprising Hopkins, Minnetonka, Edina and Eden Prairie soon was settled with families. Civilization had come to this newly instituted Territory of Minnesota. The areas that were populated by these pioneers eventually became towns and villages that still exist today.



oppls 2

The Township of Eden Prairie and Minnetonka came into existence in 1858. Eden Prairie's name was bestowed on it by Elizabeth Ellet, an author of national fame. She was impressed with the beautiful rolling prairies and likened them to her conception of the Garden of Eden. Others must have agreed with her as the township was officially chartered under the name of Eden Prairie in 1858.

About the same time, the Township of Minnetonka was officially chartered, taking its name from the large lake close by. The lake was originally named Peninsula Lake by Calvin Tuttle and Simon Stevens, earlier pioneers. Governor Alexander Ramsey later renamed it Minnetonka, a Sioux word meaning big water.

Hopkins, then a part of Minnetonka Township, had its beginning roughly around 1870. The Minneapolis and St. Louis Railway purchased right-of-ways across farmers' land for their line to St. Louis, Missouri. Once the line was completed, a station was constructed opposite the home of Harley Hopkins and was given the name of Hopkins. With the added growth brought by the railroad, Hopkins became an entity in its own right and in 1887 the village was formally incorporated and separated from Minnetonka Township.

In 1888, Edina followed suit, electing to make their settlement a separate village from that of Richfield. Andrew and John Craik, immigrants from the Old World and pioneers in the new Territory, had come to Minnesota from Edinburgh, Scotland. They opened a flour mill and named it Edina in honor of their homeland. It is from the Craik brothers' Edina flour mill that the village of Edina took its name.

From their first perilous foothold, these four cities grew and prospered. Today, they offer Minnesota a heritage rich in determination, vision and progress, a history as much a part of the present and future as it is of the past.

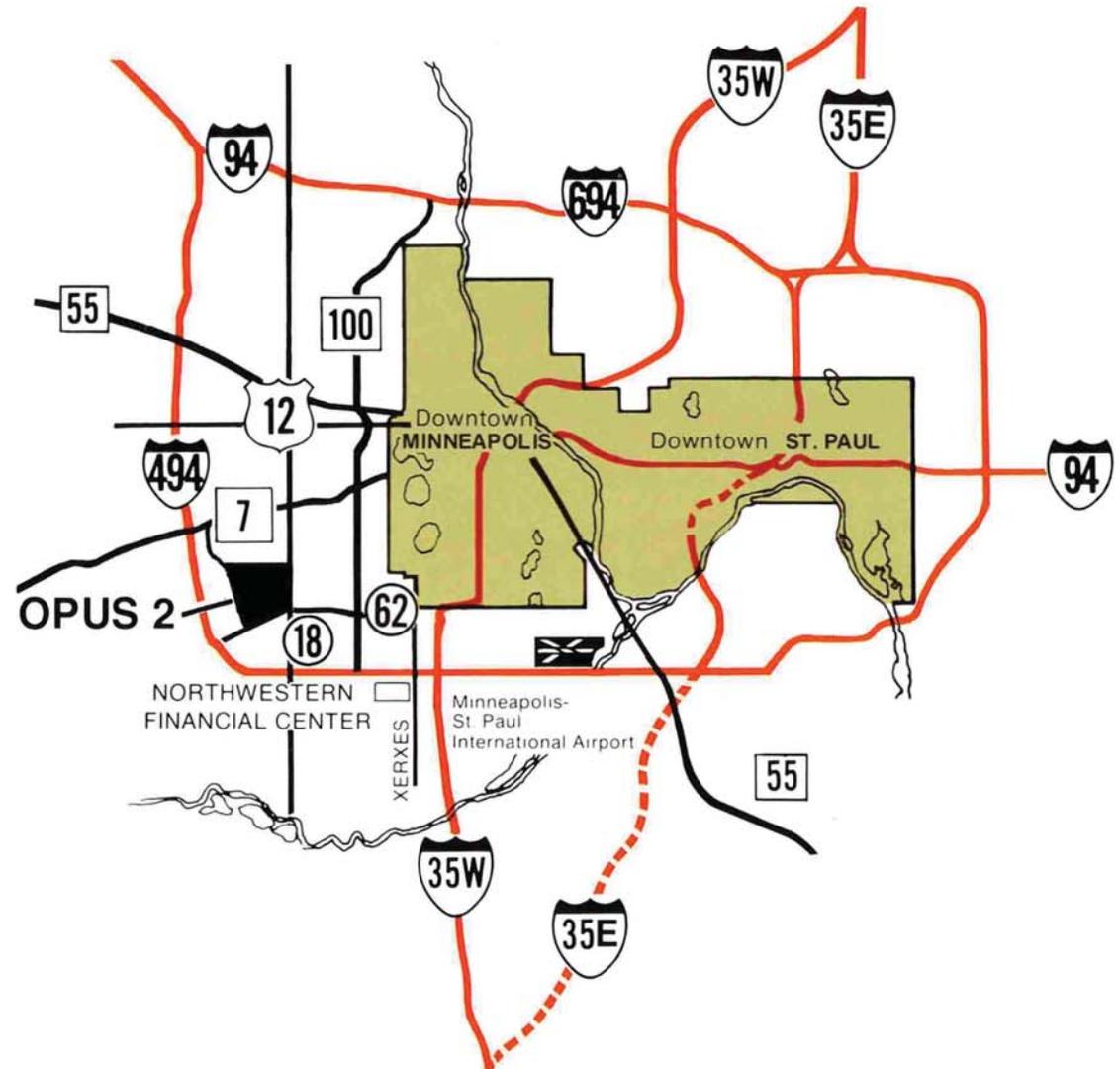
At the convergence of these four progressive communities, a new pioneering effort has begun. 410 acres of small truck farms and private estates that once belonged to the Minnesota Pioneers has been acquired by Rauenhorst Corporation. The land, located in Minnetonka, Edina, and Eden Prairie, and bordered by Hopkins, will be the site of a new innovative community geared to our modern way of working and living.

OPUS 2

There has long been a need for a new approach to community planning, especially on a large scale. Major cities, unlike smaller communities, are decades behind in responding to our present needs, work habits and life styles. Traffic systems and patterns can no longer handle the growing number of commuters. Present day transportation is producing a pollution problem that was undreamed of back in the 1950's and early 60's. Today's major cities are no longer people oriented.

Mr. Rauenhorst, aware of the direction community planning has taken in the last twenty years and of the problems that have resulted, devised an entirely new approach, one that was people oriented. He called it Opus 2.

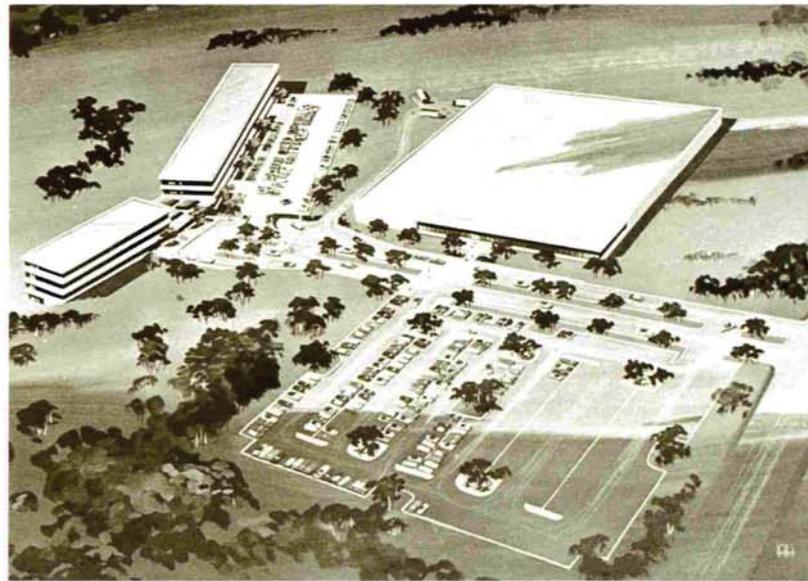
Opus 2 combines the history of the past, needs of the present and the projected requirements of the future into a self-contained working / living center offering 95% of what is essential to life. It coordinates office, industrial, commercial and residential areas into an integral working / living environment able to provide jobs, recreation, housing, shopping, medical and cultural facilities. It is self-sustaining, making it profitable for companies to locate there, and it is convenient for commuting. Opus, which means creation, is an appropriate name to apply to this unusual approach to community planning.

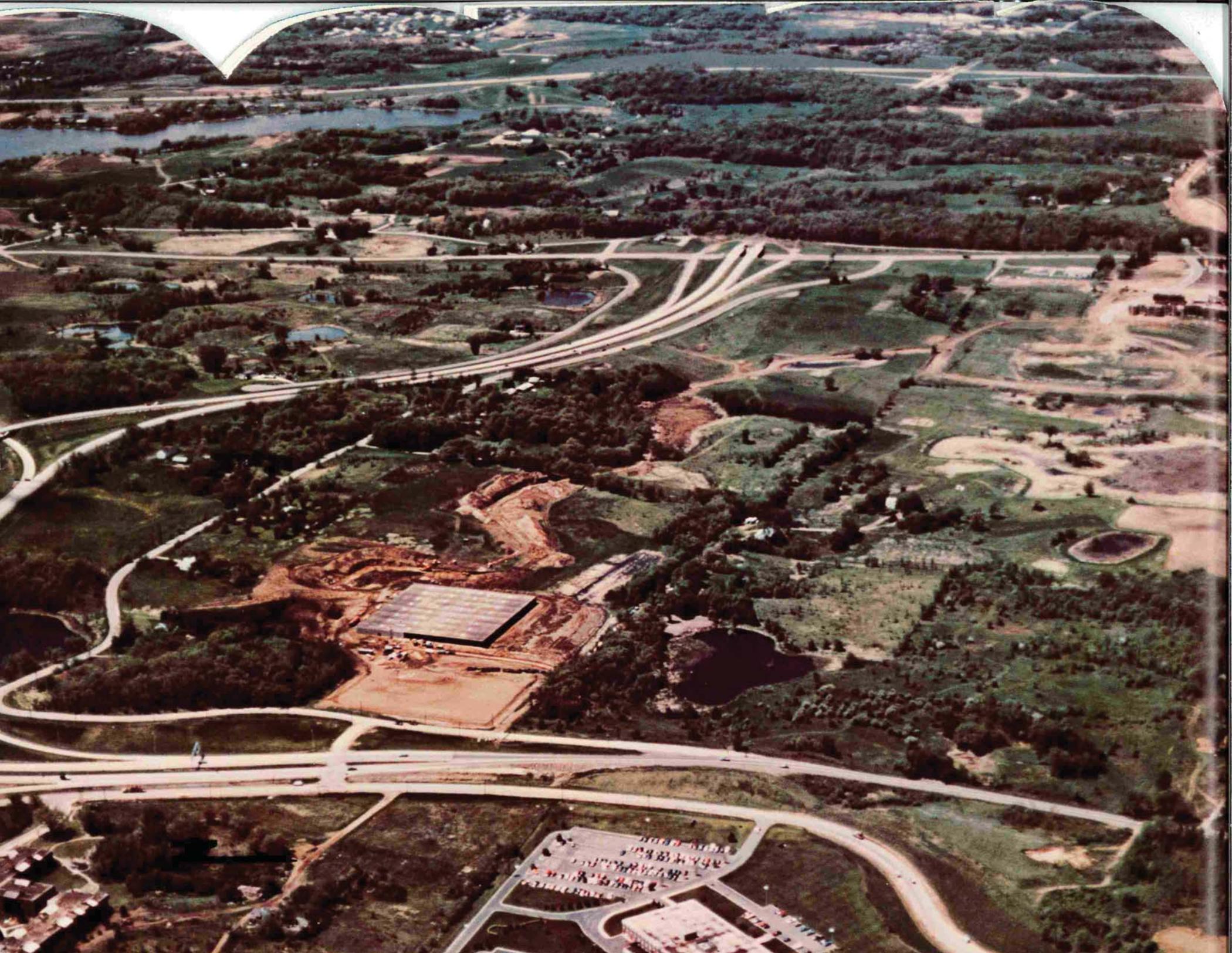


Opus 2

A SEARCH SPARKS THE BEGINNING

Opus 2 went from concept to reality when Data 100 approached Rauhenhorst Corporation to build new Corporate Headquarters, an office and plant facility. Rauhenhorst was asked to find a suitable site within a designated area. Twenty-five acres were eventually located west of the Twin Cities bordered by Shady Oak Road, County Road 18 and Crosstown Highway 62. While in the process of acquiring the land for Data 100, Rauhenhorst noticed that the adjoining acreage was also available. The area was ideal for the Opus 2 concept. The most important criteria were there: Proximity to the surrounding communities, existing access through roads and freeways and over 400 acres of undeveloped land in a suburban location. Rauhenhorst Corporation decided to use this opportunity to implement Opus 2.





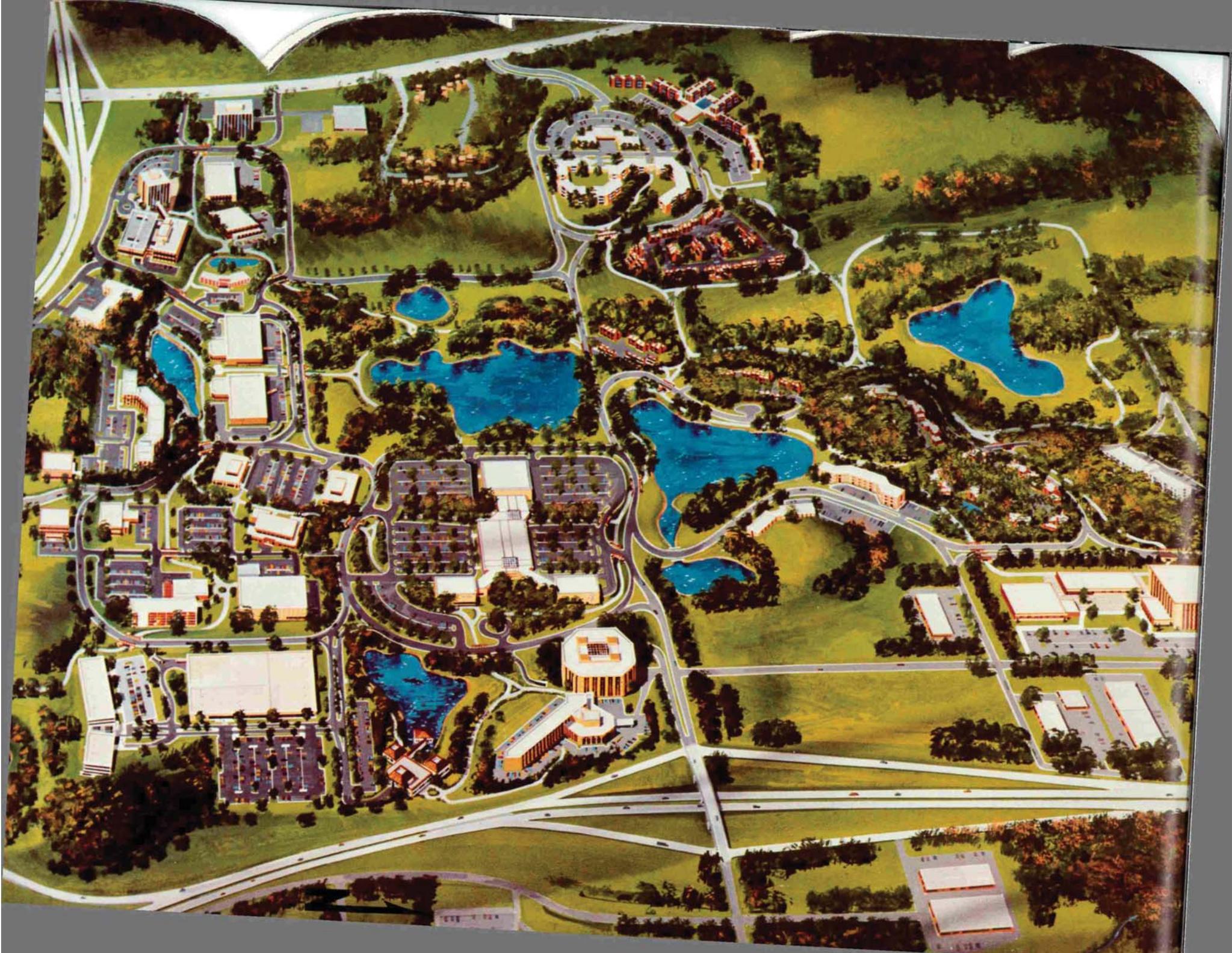
Opus 2

PREPARATION AND CONSERVATION

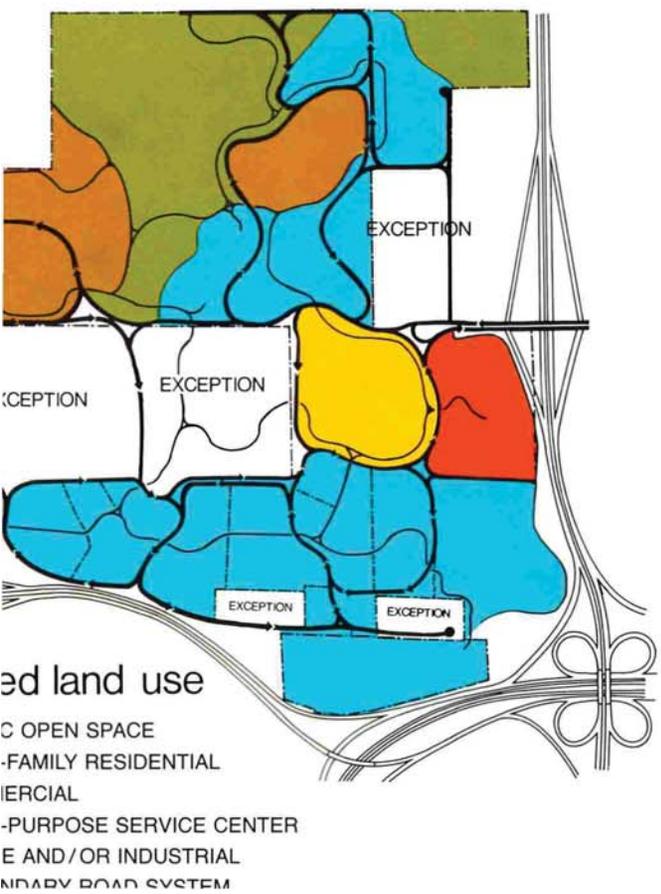
Much in-depth research was required concerning the environmental aspects of Opus 2's impact on the area. Independent studies were initiated to determine the feasibility of the automobile primary road system and the pedestrian traffic secondary road system concept as it related to the land use pattern. An environmental assessment was performed. Informal meetings were held with several different agencies, councils, commissions, and governments at the staff level including: the Nine Mile Creek Watershed District, the Environmental Quality Control Council, the Metropolitan Council, the Hennepin County Highway Department, the planning and engineering staffs of Edina, Eden Prairie, Hopkins, and Minnetonka, the Hennepin County Conservation Department and the Metropolitan Transit Commission.

Input received from these groups helped to determine the strength of each element of the Opus 2 concept and how well it would work with other elements of the plan.

One of the main elements Rauenhorst Corporation considered during planning was preservation of the area's natural amenities. Rather than redesign the topography to fit the needs of Opus 2, Opus 2 has been designed to coordinate with the environment that already exists. Great care is being taken to preserve ecosystems such as wooded sections, marshes, knolls, valleys and natural water retention areas that enrich and enhance the environmental setting. It is this care and concern for the unspoiled beauty of the land that makes Opus 2 unique.



Opus 2



WORKING IN OPUS 2

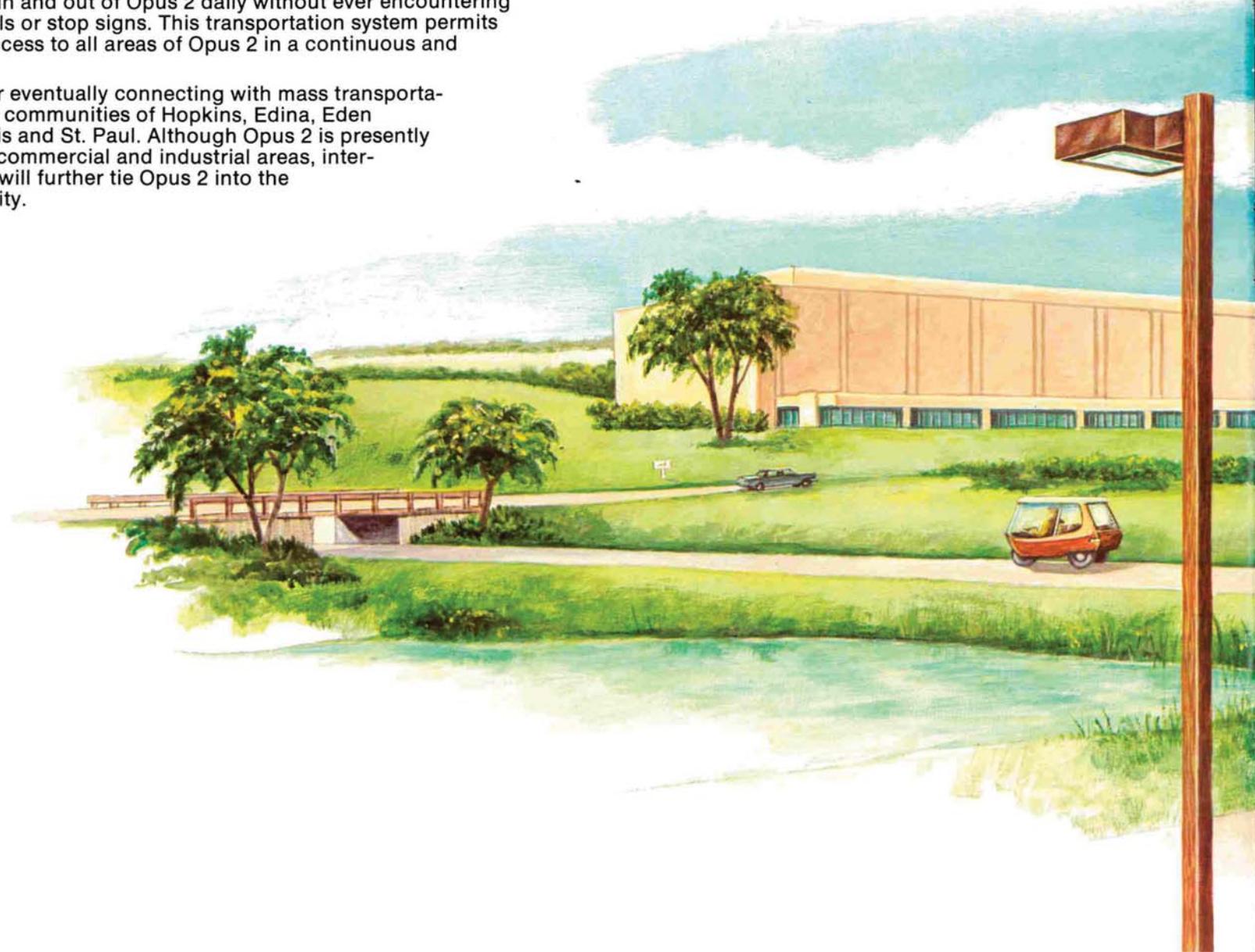
Opus 2 is a staged development, taking an estimated ten years for completion. The industrial and office portions of Opus 2, consisting of 2 million square feet, are presently being developed. These will be coordinated with the 55,000 square foot neighborhood convenience shopping center, some residential housing and the 300,000 square foot multi-purpose service center.

Designed to fit today's working/living needs, Opus 2 offers many advantages not found elsewhere. Opus 2's location is nearly perfect for the businessman. Services such as restaurants, hotels, shopping centers, and some of the Twin Cities' greatest recreational facilities are located either in Opus 2 or are just moments away. Opus 2 is serviced by one of the metropolitan area's major arteries, Crosstown Highway 62, which puts the office, commercial and industrial areas of downtown Minneapolis, downtown St. Paul, the suburbs that circle the metropolitan area and the Minneapolis/St. Paul International Airport within minutes of the busy executive. Opus 2 is situated in the heart of the blue and white collar labor markets and is surrounded by four executive residential communities. Added to this, buildings constructed in Opus 2 are architecturally designed to meld with the environment, avoiding visual congestion and enhancing the natural scenic amenities.

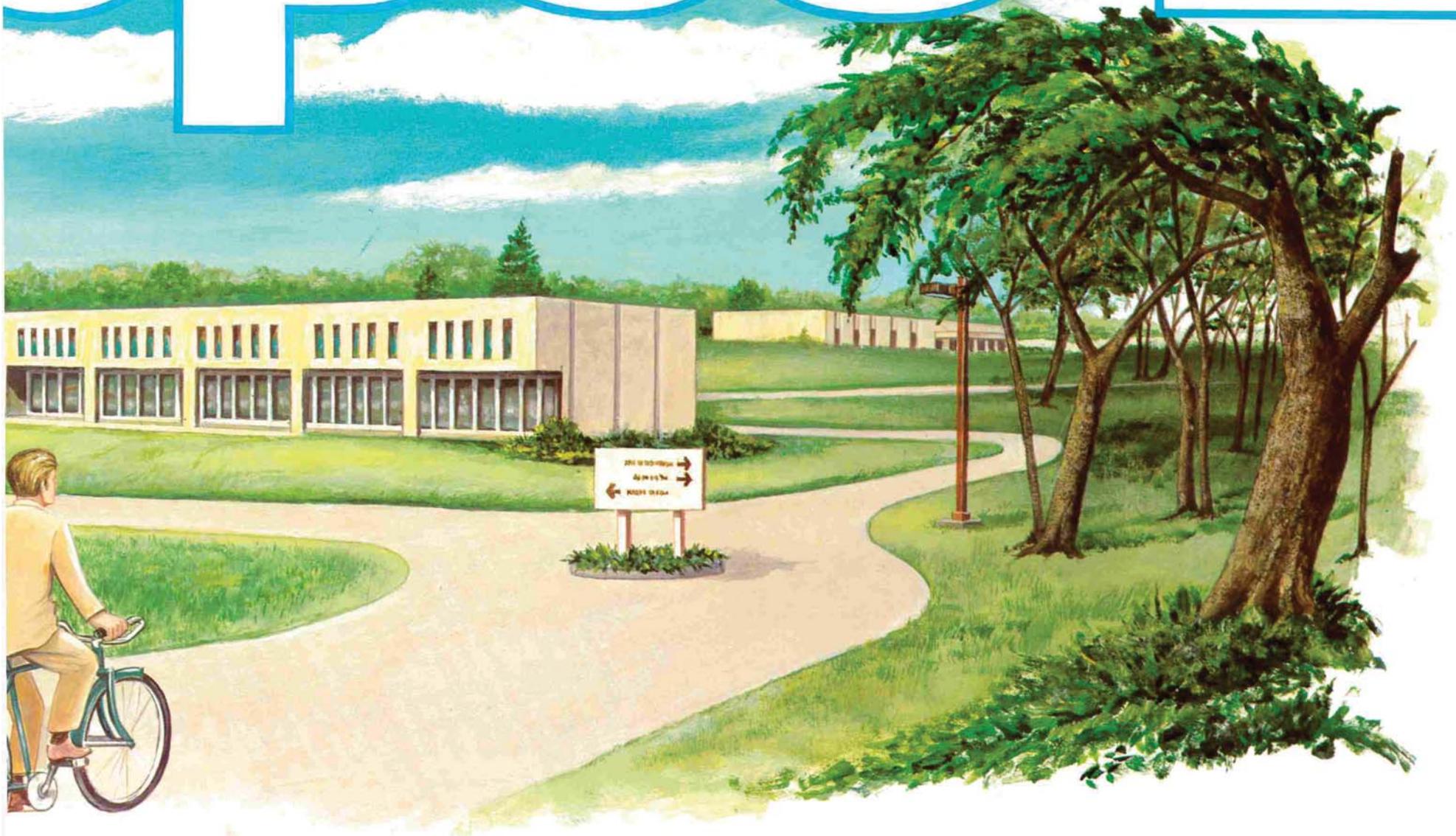
As Opus 2 was being engineered, much thought was given to controlling traffic peaks, thus avoiding rush hour traffic jams. The result is a dual roadway system

that intra-connects Opus 2. It consists of a one-way primary roadway for standard automobiles and a two-way secondary roadway for pedestrian, bicycle traffic and electric vehicles. The two systems are totally separate and are bridged wherever they intersect. Traffic from the primary system can't cross over or interfere with traffic on the secondary system. Counter-rotating traffic circles and the use of one-way streets in the primary system enable 50,000 vehicles to move in and out of Opus 2 daily without ever encountering oncoming vehicles, traffic signals or stop signs. This transportation system permits easy employee and customer access to all areas of Opus 2 in a continuous and uninterrupted fashion.

Opus 2 is in an ideal location for eventually connecting with mass transportation systems of the surrounding communities of Hopkins, Edina, Eden Prairie, Minnetonka, Minneapolis and St. Paul. Although Opus 2 is presently only minutes from these office, commercial and industrial areas, interconnected mass transportation will further tie Opus 2 into the Metropolitan business community.



OPTUS 2



LIVING IN OPUS 2

The central feature that blends the office, commercial and industrial portion of Opus 2 with the residential areas is the focal point of the working / living community, the multi-purpose service center. This structure, intended to serve Opus 2 and the surrounding area, will combine a number of uses on the same site. Proposed are high-value specialty shops, cultural facilities such as a community theater, an ecumenical chapel, dining establishments, police, fire and medical auxiliary services, all combined and located in a uniquely designed building.

Situated within casual walking distance of the multi-purpose service center will be a number of neighborhoods, each with its own architectural style and individual characteristics. They will be serviced by the same primary and secondary roadway system that intra-connects the office, commercial and industrial portions of Opus 2.

The treatment of the residential areas will reflect the Rauenhurst Corporation commitment to preserve and enhance the natural environment. Exquisitely manicured grounds will accent the aesthetically designed buildings. Each neighborhood will vary in density and will be convenient to the office, commercial and industrial portions of Opus 2, as well as to the surrounding services, communities, mass transportation systems, parks and recreational areas. The housing will range from rental units to condominiums, providing a way of life that is both distinctive and elegant. Residents will enjoy comfort, beauty, quality and peace of mind living.

Opus 2 living is designed for the discriminating. The over one thousand units planned will provide the ultimate in modern living, offering a new vista in housing experience.



opus 2



EXCITING CHALLENGES

Numerous challenges are presented by the Opus 2 project; corporate headquarters with adjacent housing, mixed professional, commercial, office, and research facilities, industrial condominiums, preservation of natural amenities, aesthetically designed buildings, new techniques of crime prevention through internal security systems, experimentation with energy supply, the primary/secondary roadway concept, mass transit systems and people movers. These are just a few of the exciting developments planned for Opus 2.

Intense research is presently underway concerning the last category, people movers. Proposed are electric vehicles. They would be advantageous to residents as they would adapt to the dual roadway concept, would cost a fraction of the present sub-compact car, both to purchase and to operate, and they would be non-polluting.

RAUENHORST CORPORATION AND OPUS 2

Opus 2 originated from Mr. Rauenhorst's deep-seated conviction that he and his firm have a responsibility to society to research and create new methods and ideas for living and working. These new ideas are then implemented through the Rauenhorst Corporation concept of Total Responsibility which includes: site selection, architectural design, financing, development, engineering, construction, leasing, management and maintenance—all under one unified contract. As applied to Opus 2, the Total Responsibility concept has played a major role in helping to create a compatible working/living environment, developed and maintained along stringent standards, that will provide 95% of what is essential to life as well as ensure steadily increasing property values for your firm's investment. Therefore, Opus 2 isn't just another development. It's a singular working/living experience at the crossroad of what was, and what ought to be.

OPUS 2—CROSSROADS OF TOMORROW, TODAY.

OPUS 2

RAUENHORST CORPORATION



SUITE 2200
NORTHWESTERN FINANCIAL CENTER
7900 XERXES AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55431
TELEPHONE (612) 830-4444

THE CITY OF MINNETONKA

Critical to the development of an enterprise such as Opus 2 is the understanding, and support of local governmental bodies during the planning, programming, and construction phases. We have been fortunate indeed to have had a cooperative endeavor emerge with a number of such governmental groups, but especially with the City of Minnetonka. Even as we wrote our Opus 2 brochure, events were moving forward with gratifying rapidity. Zoning of our industrial park and commercial areas was obtained, concept plan approval for the housing area of the plan was granted, and an industrial revenue bond issue providing for the timely completion of the industrial/commercial areas was authorized by the City Council.

We at Rauenhorst Corporation extend special thanks and appreciation to the Council, Planning Commission, and Staff of Minnetonka for their assistance in making the promise of Opus 2 a reality today.

A handwritten signature in cursive script, reading "Gerald Rauenhorst". The signature is written in dark ink and is positioned below the main body of text.

Opus Housing

November 2017



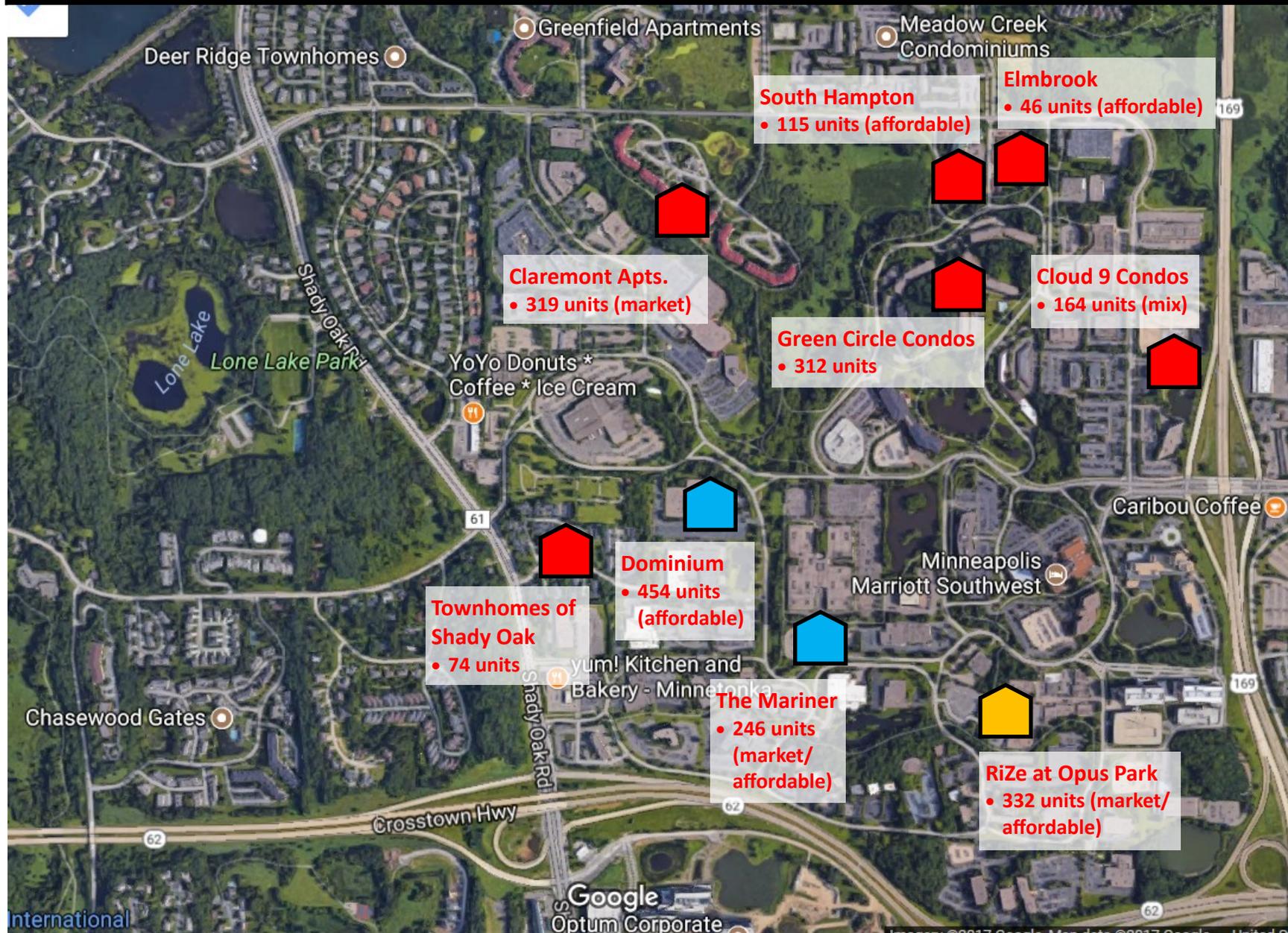
1,030 existing units



332 units under construction



700 units proposed



PC MINUTES

Powers appreciated the developer making revisions because it shows intent. Requiring a plan to be harmonious is too much of a burden on the applicant.

Sewall moved, second by Hanson, to recommend that the city council adopt the attached resolution with revisions provided in the change memo dated May 24, 2018 denying rezoning, master development plan, and building plans for the Ridgedale Executive Apartments.

Sewall, Hanson, and Kirk voted yes. Knight, O'Connell, and Powers voted no. Schack was absent. Motion carried.

This item is scheduled to be reviewed by the city council at its meeting on June 4, 2018.

C. Items concerning Bren Road Development, a multi-family residential development by Dominion, at 11001 Bren Road East.

Chair Kirk introduced the proposal and called for the staff report.

Gordon reported. He recommended approval of the application based on the findings and subject to the conditions listed in the staff report.

Sewall confirmed with Wischnack that the SWLRT is proceeding.

Ryan Lunderby, representing Dominion, the applicant, stated that he appreciated the commission's consideration and he was available for questions.

In response to O'Connell's question, Mr. Lunderby stated that the credits would have a 15-year compliance period and an additional 15-year-extended use. The whole property would be managed by Dominion, but owned by two entities because of the financing for the two buildings. Closing on the properties and construction would happen at the same time.

Wischnack provided that \$1,000 is the typical rent for a one-bedroom affordable unit, for a qualifying tenant earning less than 60 percent of area median income. The rent is set annually by HUD.

The public hearing was opened. No testimony was submitted and the hearing was closed.

In response to Hanson's question, Gordon reviewed a graphic illustrating proposed trails and walkways. Wischnack added that an underpass would not be possible due to engineering constraints. Chair Kirk noted that the SWLRT station would have planned controlled crossing areas.

Chair Kirk favored developers providing funding in addition to park dedication fees for improvements linked to the proposal. A crossing at a road would benefit the development and SWLRT.

Chair Kirk asked if the St. Margaret's cemetery could become a cut-through area. Gordon answered that that could happen. There is a fence, wooded area, and steep slope. It would not be an easy path to Shady Oak Road. Chair Kirk supports having a larger park in the area because of the potential pets.

Chair Kirk confirmed with Gordon that the Opus Station would have a park-and-ride area. Chair Kirk did not foresee a problem. The controlled entrance to the complex and its current design would be easier to modify into a controlled parking area if that would become necessary.

Powers moved, second by Hanson, to recommend that the city council adopt the following related to the Bren Road Development, a multi-family residential development by Dominium, at 11001 Bren Road East:

- 1) *Ordinance rezoning the property from I-1, industrial, to PUD, planned unit development, and adopting a master development plan.*
- 2) *The resolution approving final site and building plans.*
- 3) *A motion making a negative declaration on the need for an Environmental Impact Statement.*
- 4) *The resolution declaring the proposal consistent with the comprehensive plan.*

Sewall, Hanson, Knight, O'Connell, Powers, and Kirk voted yes. Schack was absent. Motion carried.

This item is scheduled to be reviewed by the city council at its meeting on June 18, 2018.

9. Adjournment

Sewall moved, second by Knight, to adjourn the meeting at 9:20 p.m. Motion carried unanimously.

By: _____

Lois T. Mason
Planning Secretary

PLAT AND VACATION

DOMINIUM 2ND ADDITION

R.T. DOC. NO. _____

KNOW ALL PERSONS BY THESE PRESENTS: That Minnetonka Leased Housing Associates II, LLLP, a Minnesota limited liability limited partnership, fee owner of the following described property situated in the County of Hennepin, State of Minnesota, to wit:

Lots 1 and 2, Block 1, OPUS 2 EIGHTH ADDITION.

Has caused the same to be surveyed and platted as DOMINIUM 2ND ADDITION, and does hereby dedicate to the public for public use the drainage and utility easements and utility easements as created by this plat.

In witness whereof said Minnetonka Leased Housing Associates II, LLLP, a Minnesota limited liability limited partnership, has caused these presents to be signed by its General Partner this _____ day of _____, 20____.

Minnetonka Leased Housing Associates II, LLLP
By: Minnetonka Leased Housing Associates II, LLC, its General Partner

By: _____
Ryan J. Lunerby, Vice President

STATE OF MINNESOTA
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 20____, by Ryan J. Lunerby, Vice President of Minnetonka Leased Housing Associates II, LLC, General Partner of Minnetonka Leased Housing Associates II, LLLP, a Minnesota limited liability limited partnership, on behalf of the partnership.

(Signature)

(Printed Name, Title)
Notary Public, _____ County, Minnesota
My Commission Expires _____

SURVEYORS CERTIFICATION

I, Paul J. McGinley, do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this Plat; and all public ways are shown and labeled on this plat.

Dated this _____ day of _____, 20____.

Paul J. McGinley, Licensed Land Surveyor,
Minnesota License No. 16099

STATE OF MINNESOTA
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 20____, by Paul J. McGinley, a Licensed Land Surveyor.

(Signature)

(Printed Name)
Notary Public, _____ County, Minnesota
My Commission Expires January 31, 2020

MINNETONKA, MINNESOTA

This plat of DOMINIUM 2ND ADDITION was approved and accepted by the City Council of Minnetonka, Minnesota, at a regular meeting thereof held this _____ day of _____, 20____. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the City or the prescribed 30-day period has elapsed without receipt of such comments and recommendations, as provided by Minnesota Statutes, Section 505.03, Subdivision 2.

City Council, Minnetonka, Minnesota

Mayor
By: _____
Clerk

RESIDENT AND REAL ESTATE DEPARTMENT, Hennepin County, Minnesota

I hereby certify that taxes payable in 20____ and prior years have been paid for land described on this plat, dated this _____ day of _____, 20____.

Mark V. Chapin, County Auditor
By: _____
Deputy

SURVEY DIVISION, Hennepin County, Minnesota

Pursuant to Minnesota Statutes Section 383B.565 (1969), this plat has been approved this _____ day of _____, 20____.

Chris F. Mavis, County Surveyor

REGISTRAR OF TITLES, Hennepin County, Minnesota

I hereby certify that the within plat of DOMINIUM 2ND ADDITION was filed in this office this _____ day of _____, 20____, at _____ o'clock _____ M.

Martin McCormick, Registrar of Titles
By: _____
Deputy

DOMINIUM 2ND ADDITION

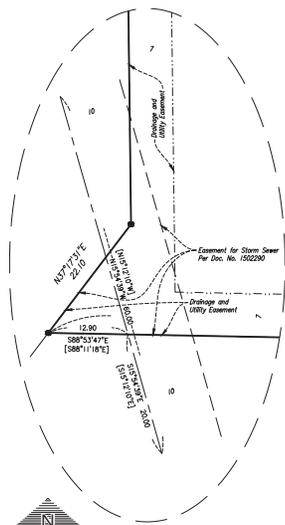
R.T. DOC. NO. _____

OPUS 2 FOURTH ADDITION

Bren Road East

Bren Road West

Detail

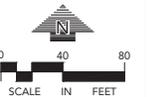


THE TOWNHOUSES OF SHADY OAK

THE TOWNHOUSES OF SHADY OAK

- BEARINGS ARE BASED ON THE WEST LINE OF LOT 1, BLOCK 1, OPUS 2 EIGHTH ADDITION HAVING A BEARING OF N0°42'29"W.
- DENOTES 1/2 INCH IRON MONUMENT FOUND, CAPPED "15 48988", UNLESS OTHERWISE SHOWN
 - ▲ DENOTES "PK NAIL" FOUND
 - DENOTES 1/2 INCH X 14 INCH IRON MONUMENT SET, MARKED "15 16099"
 - △ DENOTES "PK NAIL" SET
 - (INFO) DENOTES BEARING IN DOCUMENT NUMBER 1502290

A D D I T I O N



LOUCKS

Ordinance No. 2018-

**An ordinance rezoning the property at 11001 Bren Road East
From I-1, industrial, to PUD, planned unit development**

The City Of Minnetonka Ordains:

Section 1.

1.01 The subject property at 11001 Bren Road East is hereby rezoned from I-1, industrial, to PUD, planned unit development.

1.02 The property is legally described as: Lots 1 and 2, Block 1, Opus 2 Eighth Addition.

Section 2.

2.01 This ordinance is based on the following findings:

1. The rezoning to PUD would result in the provision of workforce rental housing, which is a living option desirable to the city.
2. The rezoning would be consistent with the intent of the zoning ordinance and of the comprehensive guide plan.
3. The rezoning would be consistent with the public health, safety, and welfare.

2.02 This ordinance is subject to the following conditions:

1. The site must be developed and maintained in substantial conformance with the following plans:
 - Site Plan, dated April 6, 2018
 - Temporary Site and Future Entrance Plans, dated April 6, 2018
 - Grading Plan, dated April 6, 2018
 - Sanitary Sewer and Watermain Plan, dated April 6, 2018
 - Stormsewer Plan, dated April 6, 2018
 - Landscape Plan, dated April 6, 2018

- Building Elevations, dated April 6, 2018
2. The development must further comply with all conditions outlined in City Council Resolution No. 2018-xx, adopted by the Minnetonka City Council on July 23, 2018.

Section 3. This ordinance is effective immediately.

Adopted by the city council of the City of Minnetonka, Minnesota, on July 23, 2018.

Brad Wiersum, Mayor

Attest:

David E. Maeda, City Clerk

Action on this ordinance:

Date of introduction: April 30, 2018

Date of adoption: July 23, 2018

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Ordinance adopted.

Date of publication:

I certify that the foregoing is a true and correct copy of an ordinance adopted by the city council of the City of Minnetonka, Minnesota at a regular meeting held on July 23, 2018.

David E. Maeda, City Clerk

Resolution No. 2018-

Resolution approving final site and building plans for a multi-family residential development at 11001 Bren Road East

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

- 1.01 Dominion Acquisition, LLC. has requested approval of final site and building plans for a three building, 482-unit rental housing development at 11001 Bren Road East.
- 1.02 The subject property is legally described as Lots 1 and 2, Block 1, Opus 2 Eighth Addition.
- 1.03 On May 24, 2018 the planning commission held a hearing on the proposal. The applicant was provided the opportunity to present information to the commission. The commission considered all of the comments received and the staff report, which are incorporated by reference into this resolution. The commission recommended the city council approve the final site and building plans.

Section 2. Site and Building Plan Standards and Findings.

- 2.01 City Code §300.27, Subd.5 outlines several items that must be considered in evaluation of site and building plans. Those items are incorporated by reference into this resolution.
- 2.02 The proposal would meet site and building plan standards outlined in the City Code §300.27, Subd.5.
 - 1. The proposed high-density residential development is consistent with the general housing goals of the 2030 Comprehensive Guide Plan and the Plan's specific goal to provide additional housing in the OPUS area. Further, the proposal has been reviewed by city planning, engineering, and natural resources staff and found to be generally consistent with the city's development guides, including the water resources management plan.
 - 2. The proposal is consistent with the zoning ordinance.

3. The subject property is a developed site. The only “natural” area is a Type-1 wetland on the northeast corner. While the proposal would result in tree and soil removal, the wetland area would not be disturbed.
4. The proposal would result in a harmonious relationship of buildings, with open space generally located at the perimeter of the site.
5. The proposal would result in a unique and attractively-designed neighborhood.
6. The proposal includes installation of rooftop solar energy systems, capable of producing more than 562,000 kilowatts of energy per year. As new construction, the building code would require use of additional energy saving features within the buildings themselves.
7. The proposal would visually and physically alter the property and the immediate area. However, this change would occur with any redevelopment of the site, which the city has long anticipated.

Section 3. Steep Slope Standard and Findings.

3.01 City Code §300.28, Subd.20, outlines several standards for construction within code-defined steep slopes. Those standards are incorporated by reference into this resolution.

3.02 The proposal would meet the standards outlined in the City Code §300.28, Subd.20.

1. The property is physically suitable for the design and siting of the proposed development. The proposal would preserve significant natural features by minimizing disturbance to existing topographical forms.
 - a) Retaining walls would be used as an alternative to banks of cut-and-fill. Cuts would be less than 25 feet in depth.
 - b) Off-site views of the proposed retaining walls would generally be minimized.
2. The development would not result in soil erosion, flooding, severe scarring, reduced water quality, inadequate drainage control, or other problems.
 - a) Construction would occur at the base of the slope.
 - b) The proposal would not be located on an existing slope that has an average grade of 30 percent or more.
3. The proposed development provides adequate measures to protect

public safety.

- a) As a condition of this resolution a construction phasing plan is required. The plan must detail the construction process for the proposed retaining walls.
- b) The slope of driveway access would be less than 10 percent.

Section 4. City Council Action.

4.01 The above described site and building plans are hereby approved based on the findings outlined in Sections 2 and 3 of this resolution. Approval is subject to the following conditions:

1. Subject to staff approval, the site must be developed and maintained in substantial conformance with the following plans, except as modified by the conditions below:
 - Site Plan, dated April 6, 2018
 - Temporary Site and Future Entrance Plans, dated April 6, 2018
 - Grading Plan, dated April 6, 2018
 - Sanitary Sewer and Watermain Plan, dated April 6, 2018
 - Stormsewer Plan, dated April 6, 2018
 - Landscape Plan, dated April 6, 2018
 - Building Elevations, dated April 6, 2018
2. A grading permit is required. This permit will cover grading and installation of sewer, water, stormwater facilities and construction of retaining walls. Unless authorized by appropriate staff, no site work may begin until a complete grading permit application has been submitted, reviewed by staff, and approved.
 - a) The following must be submitted for the grading permit to be considered complete.
 - 1) An electronic PDF copy of all required plans and specifications.
 - 2) Final site, grading, utility, stormwater management, landscape, and tree mitigation plans, and a stormwater pollution prevention plan (SWPPP) for staff approval.
 - a. Final site plan. The plan must:
 - Illustrate all existing and proposed easements. No structural improvements are allowed within the easements. This includes pool, pool deck,

fencing, ramps, stairs, playgrounds, or other elements as outlined in city policy.

- Illustrate city approved public trail improvements on the east and north side of the property as outlined in the development agreement.
- Note the required wetland setbacks, as follows:

	Required Setback
Structures	35 feet
Patios	25 feet
Trails	20 feet

b. Final grading plan must:

- Illustrate that trails and sidewalks meet Americans with Disability Act (ADA) standards.
- Reevaluate grading near the east driveway. Runoff must be captured onsite to the greatest extent possible, rather than draining, untreated, to the city storm sewer system.

c. Final utility plan. The plans must:

- Note unused services removed back to the main.
- Illustrate watermain looping.
- Consider the existing fire hydrant in the southeast corner of the site. If the retaining wall in that area remains as per grading plan dated April 4, 2018, the existing fire hydrant must be shown as removed and replaced with a new hydrant in a new location.
- Verify watermain crossings with other utilities. Provide insulation as necessary and maintain a minimum 18 inches of vertical separation.
- Indicate replacement of existing sanitary MH 1.
- Verify that Building A requires separate north and south sewer services. The city would

prefer that the service be combined, resulting in just one connection to the public sewer.

- Verify that Building C requires separate east and west sewer services. The city would prefer that the service be combined, resulting in just one connection to the public sewer on the west side of the building.
- d. Final stormwater management plan is required for the entire site's impervious surface. The plan must demonstrate conformance with the following criteria:
- Rate. Limit peak runoff flow rates to that of existing conditions from the 2-, 10-, and 100-year events at all points where stormwater leaves the site.
 - Volume. Provide for onsite retention of 1-inch of runoff from the entire site's impervious surface.
 - Quality. Provide for all runoff to be treated to at least 60 percent total phosphorus annual removal efficiency and 90 percent total suspended solid annual removal efficiency.

In addition:

- The plan must include confirmed Southwest Light Rail Transit plans for storm sewer and other improvements along the east side of the subject property.
- Review drainage atop the west retaining wall. Confirm with a structural engineer whether additional drainage considerations need to be implemented to protect the wall's integrity given the large drainage area that flows toward the wall.
- Provide evidence that the underground system will be able to support 83,000 pounds and 10,800 pounds per square foot outrigger load.
- The underground facilities must be inspected by a qualified third party during installation and that

party must verify that the pressure requirements are adequately met.

- e. Final landscaping plan must:
 1. Illustrate restoration of the wetland area and include final plant specifications, including any seed mixes.
 2. Not include Colorado spruce. Substitute with another species of evergreen.
 3. Include notation of project value and demonstrating the value of the proposed landscaping will meet code requirements.
 4. Illustrate all deciduous trees planted no closer than 15 feet behind the curb of Bren Road East or 10 feet from the edge of public trails and sidewalks. Evergreen trees may be no closer than 20 feet behind the curb of Bren Road or 15 feet from the edge of public trails and sidewalks.

3) Revised truck turning exhibits. The plans must include the truck dimensions and wheelbase used.

4) A utility exhibit. The exhibit must show only property lines, buildings, sewer, water, storm sewer and underground stormwater facilities. The exhibit must clearly note:

North-South Sanitary Main	PUBLIC
Existing East-West Watermain	PUBLIC
New North-South Watermain	PRIVATE
Storm sewer	PRIVATE
Fire Hydrants	PRIVATE

5) A sanitary sewer bypass plan to be implemented during relocation of the public sanitary sewer.

b) Prior to issuance of a grading permit:

- 1) This resolution must be recorded at Hennepin County.
- 2) Obsolete utility easements must be vacated.
- 3) One of the following must occur:

- a. The following easements must be dedicated:
 - A utility easement over the relocated sanitary sewer line must be dedicated. Prior to recording, the easement document must be submitted for review and approval of the city attorney. The easement must be 40-foot wide and centered over the line.
 - Secondary roadway easements over perimeter trails and underpass. Prior to recording, the easement document must be submitted for review and approval of the city attorney.
 - b. Preliminary and final plats must be approved and the final plat must be released for recording.
- 4) Park dedication in the amount of \$2,410,000 must be submitted. The city may credit verified costs for construction of the north underpass and connection trail. The details for cost verification and crediting will be outlined in the contract for private development.
- 5) Submit the following:
- a. Execute the contract for private development
 - b. A stormwater maintenance agreement in a city approved format for review and approval of city staff.
 - c. A private hydrant maintenance agreement in a city approved format for review and approval of city staff.
 - d. A construction phasing plan for staff review and approval. The plan must include details regarding construction of proposed retaining walls.
 - e. A MPCA Sanitary Sewer Extension permit or documentation that a permit is not required.
 - f. A MDH permit for the proposed water main construction.
 - g. A construction management plan. The plan must be in a city approved format and must outline minimum

site management practices and penalties for non-compliance.

- h. Financial guarantees in the amount of 125% of a bid cost or 150% of an estimated cost to comply with grading permit and landscaping requirements and to restore the site. Staff is authorized to negotiate the manner in which site work and landscaping guarantees will be provided. The city will not fully release guarantee until: (1) as-built drawings and tie-cards have been submitted; (2) a letter certifying that the underground facility has been completed according to the plans approved by the city; (3) vegetated ground cover has been established; and (4) required landscaping or vegetation has survived one full growing season.
 - i. Evidence that an erosion control inspector has been hired to monitor the site through the course of construction. This inspector must provide weekly reports to natural resource staff in a format acceptable to the city. At its sole discretion, the city may accept escrow dollars, in an amount to be determined by natural resources staff, to contract with an erosion control inspector to monitor the site throughout the course of construction.
 - j. Cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document the builder and property owner will acknowledge:
 - The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.
- 6) Install erosion control, and tree protection fencing and any other measures identified on the SWPPP for staff inspection. These items must be maintained throughout the course of construction.

- 7) Hold a preconstruction meeting with site contractors and city planning, engineering, public works, and natural resources staff. The meeting may not be held until all items required under 2(a) and 2(b)(5) of this resolution have been submitted, reviewed by staff, and approved.
 - 8) Permits may be required from other outside agencies including, Hennepin County, the Nine-Mile Creek Watershed District, and the MPCA. It is the applicant's responsibility to obtain any necessary permits.
3. Prior to issuance of any building permit, submit the following documents:
- a) A snow removal and chloride management plan.
 - b) A construction management plan. This plan must be in a city approved format and outline minimum site management practices and penalties for noncompliance. If the builder is the same entity doing grading work on the site, the construction management plan submitted at the time of grading permit may fulfill this requirement.
 - 1) Cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document the builder and property owner will acknowledge:
 - The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
 - If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion and/or grading problems.

If the builder is the same entity doing grading work on the site, the escrow submitted at the time of grading permit may fulfill this requirement.
4. Prior to issuance of a certificate of occupancy, the existing at grade crossing must be enhanced with either a rectangular rapid flashing beacon or a high-intensity activated crosswalk beacon, as required by the city engineer.
5. The property owner is responsible for replacing any required landscaping that dies.

6. Construction must begin by December 31, 2019, unless the planning commission grants a time extension.

Adopted by the City Council of the City of Minnetonka, Minnesota, on July 23, 2018.

Brad Wiersum, Mayor

Attest:

David E. Maeda, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held July 23, 2018.

David E. Maeda, City Clerk

SEAL

Resolution No. 2018-

**Resolution approving the preliminary and final plat of
DOMINIUM 2nd ADDITION**

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 Dominium Acquisition, LLC. has requested approval of preliminary and final plats of DOMINIUM 2nd ADDITION at 11001 Bren Road East.

1.02 The property is legally described as:

Lots 1 and 2, Block 1, OPUS 2 EIGHTH ADDITION

Section 2. General Standards.

2.01 City Code §400.030 outlines general design requirements for residential subdivisions. These standards are incorporated by reference into this resolution.

Section 3. Findings.

3.01 The proposed plats would meet the design standards as outlined in City Code §400.030.

Section 4. Council Action.

4.01 The above-described preliminary and final plats are hereby approved, subject to the following conditions:

1. Prior to release of the final plat for recording, submit the following:
 - a) Title evidence that is current within thirty days before release of the final plat.
 - b) An electronic CAD file of the plat in microstation or DXF.

- c) Three sets of mylars for city signatures.
- 2. Additional easements may be required by the city following the installation of public utilities.
- 3. This approval will be void on July 23, 2019, if: (1) a final plat is not recorded; and (2) the city council has not received and approved a written application for a time extension.

Adopted by the City Council of the City of Minnetonka, Minnesota, on July 23, 2018.

Brad Wiersum, Mayor

Attest:

David E. Maeda, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on July 23, 2018.

David E. Maeda, City Clerk

Resolution No. 2018-

**Resolution vacating public right-of-way and utility easements
at 11001 Bren Road East**

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 Dominion Acquisition, LLC. has requested vacation of public right-of-way and utility easements at 11001 Bren Road East.

1.02 The easements are legally described as follows, to wit:

That part of Outlot D, The Townhouses of Shady Oak, according to the plat thereof on file or of record in the office of the Registrar of Titles, Hennepin County, Minnesota which lies northeasterly of a line described as follows:

Commencing at the northeast corner of said Outlot D; thence on an assumed bearing of North 88 degrees 11 minutes 18 seconds West, along the north line of said Outlot D, a distance of 55.86 feet to the beginning of the line to be described; thence South 55 degrees 11 minutes 18 seconds East a distance of 11.95 feet; thence southeasterly, along a tangential curve concave to the southwest having a radius of 271.76 feet, to the easterly line of said Outlot D and said line there terminating and which lies within Lot 1, Block 1, Opus 2 Eighth Addition, according to the plat thereof on file or of record in the office of the Registrar of Titles, Hennepin County, Minnesota.

and

That part of the 30 foot wide utility easement over Outlot C, as dedicated in the plat of The Townhouses of Shady Oak, embraced within Lots 1 and 2, Block 1, Opus 2 Eighth Addition, and the 35 foot wide Utility Easement over said Lots 1 and 2, as dedicated in the plat of Opus 2 Eighth Addition, according to the recorded plats thereof on file in the office of the Registrar of Titles, Hennepin County, Minnesota, except those parts of said utility easements lying within the south 10 feet of said Lot 2 and the northerly 15 feet of said Lot 1.

1.03 As required by law, a hearing notice on this request was published in the City of Minnetonka's official newspaper.

- 1.04 On July 23, 2018, the city council held a hearing on the request, at which time all persons for and against the granting of the request were heard.
- Section 2. Standards.
- 2.01 Section 12.06 of the City Charter states that “No vacation shall be made unless it appears in the interest of the public to do so...”
- Section 3. Findings.
- 3.01 The Minnetonka City Council finds that the vacations are not counter to the public interest.
- Section 4. Council Action.
- 4.01 The city council vacates the easements as above-described.
- 4.02 These vacations are effective only upon the proper filing of DOMINIUM 2nd ADDITION and the dedication of additional easements over public utilities, as necessary.

Adopted by the City Council of the City of Minnetonka, Minnesota, on July 23, 2018.

Brad Wiersum, Mayor

Attest:

David E. Maeda, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on July 23, 2018.

David E. Maeda, City Clerk

Resolution No. 2018-

**Resolution issuing a negative declaration of need for EIS
for the Dominion development at 11001 Bren Road East**

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

- 1.01 The City of Minnetonka prepared an Environmental Assessment Worksheet (EAW) for the Dominion development pursuant to Minnesota Rules 4410.
- 1.02 The EAW was distributed to the agencies and public for the required 30-day comment period on May 7, 2018.
- 1.03 The 30-day comment period ended on June 6, 2018.
- 1.04 The preparation of the Dominion EAW and comments received on the EAW have generated information adequate to determine whether the proposed project has the potential for significant environmental impacts.
- 1.05 The EAW has identified areas where the potential for significant environmental effects exist, but appropriate measures have or will be incorporated into the project plan and/or permits to reasonably mitigate these impacts.
- 1.06 The Dominion development is expected to comply with all the City of Minnetonka and review agency standards.
- 1.07 Based on the criteria established in Minnesota Rules 4410.1700, the project does not have the potential for significant environmental effects.
- 1.08 Based on the Findings of Fact and Conclusions, the project does not have the potential for significant environmental impacts.

Section 2. City Council Action

- 2.01 The City Council of the City of Minnetonka hereby determines that an Environmental Impact Statement (EIS) for the Dominion development at 11001 Bren Road East is not required.

Adopted by the City Council of the City of Minnetonka, Minnesota, on July 23, 2018.

Brad Wiersum, Mayor

Attest:

David E. Maeda, City Clerk

Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held June 23, 2018.

David E. Maeda, City Clerk

SEAL