

**MINNETONKA PLANNING COMMISSION**  
**Jan. 16, 2020**

**Brief Description**

Items concerning Shady Oak Road Redevelopment:

- 1) Master Development Plan,
- 2) Site and Building Plan review,
- 3) Preliminary Plat,
- 4) Detachment/Annexation and associated actions with the parcel:
  - Comprehensive guide plan amendment, and
  - Rezoning

**Recommendation**

Recommend the planning commission recommend city council approval.

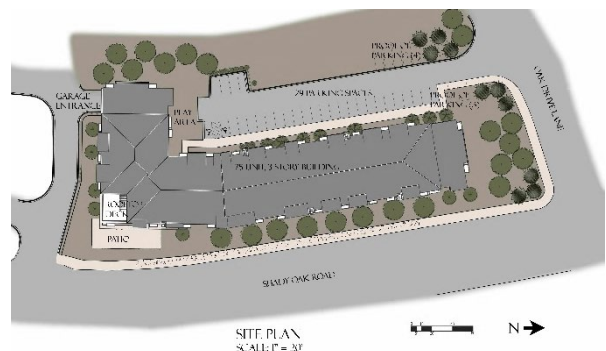
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**Proposal Summary**

Ron Clark Construction, the applicant, is proposing to revise the previously approved 2017 plans. The new plans propose a three-story, 75-unit apartment building on the property located at 4312 Shady Oak Road and on a portion of the property to the south that is located in the city of Hopkins. The applicant has a purchase agreement for the property in Hopkins. The proposed apartment building would have underground parking, resident community room, exercise room, on-site manager's office, and an outdoor play area. Apartment units would be a mix of alcove, studio, 1- and 2-bedroom apartments ranging between 450 and 1,200 sq. ft. with an average size of 847 sq. ft. Rents are anticipated to be between \$1,000 and \$1,300 per month for the affordable units and between \$1,100 and \$2,400 per month for the market-rate units. Thirty percent (23 units) of the 75 units are proposed to be affordable to those earning 60 percent of the area median income.



2017 Approved Plan



2019 Revised Plan

## Background

The City of Minnetonka purchased the property at 4312 Shady Oak Rd. in March 2015. In Nov. 2016, after several neighborhood meetings and a developer interview process, the city council selected Ron Clark Construction to begin negotiations and had the developer propose a development concept.

On Sept. 25, 2017, after an extensive community outreach, the city council approved the Shady Oak Crossings redevelopment project. The project, as approved, is a two and three-story, 49-unit apartment building with underground parking, resident community room, exercise room, on-site manager's office, and an outdoor play area. The building would have a mix of 1, 2, and 3 bedroom apartments with rents expected to be between \$800 and \$1200 per month. (On Dec. 16, 2019, the city council extended the approval to Dec. 31, 2020. The extension is customary for approvals when construction has not commenced.)

After the 2017 approval, the developer worked towards 2018 tax credit financing for the project (through the state). In Nov. 2018, the developer was notified that they were not awarded tax credits.

In May 2019, Ron Clark Construction announced it was proposing to make revisions to the approved plan, which included the acquisition of adjoining property in Hopkins. This 67-unit project was reviewed as a concept plan by the planning commission and city council. Both bodies appreciated the revised plan, generally stating it was an improvement on the approved plan. The city council also introduced an ordinance for the revised project on July 8, 2019. Also during that time, Ron Clark Construction had submitted an application for tax credits. Unfortunately, the credits were oversubscribed, and the project again did not receive credits. Ron Clark Construction decided not to pursue further public reviews of that plan.

Complete information on the project's history is posted on the city's website [here](#).

## Revised Proposal

Ron Clark Construction has submitted revised plans and is now proposing a three-story, 75-unit apartment building on the property located at 4312 Shady Oak Road and on a portion of the property to the south that is currently in the City of Hopkins. The proposed apartment building would have underground parking, resident community room, exercise room, onsite manager's office, and an outdoor play area. Apartment units would be a mix of alcove, studio, 1- and 2-bedroom apartments ranging between 450 and 1,200 sq. ft. with an average size of 847 sq. ft. Rents are anticipated to be between \$1,000 and \$1,300 per month for the affordable units and between \$1,100 and \$2,400 per month for the market-rate units. To be clear, 30% (22 units) of the 75 units are proposed to be affordable.

The revised proposal requires the approval of:

- 1) **Master Development Plan.** The proposal requires an amendment to the previously approved project for the increase in building size, housing units, and changes to the site plan.
- 2) **Final Site and Building Plans.** By city code, site and building plan review is required for the construction of any new building of the proposed size.

- 3) **Subdivision/Lot Line Adjustment.** The applicant is proposing to subdivide a portion of the adjacent and also city-owned residential property and allowance for stormwater management to occur on that property with an easement. The subdivision would also incorporate adjacent land currently located in the city of Hopkins.
- 4) **Annexation/Detachment.** The applicant has secured a purchase agreement with an adjacent property located in the City of Hopkins that would provide additional land to expand the project. A concurrent detachment/annexation process could be undertaken by both cities.
  - **Comprehensive Guide Plan Amendment.** With annexation, the Hopkins land parcel would need to be designated in the comprehensive plan as high-density residential, consistent with the guidance of the Minnetonka parcel. As both cities 2040 comprehensive plans are not yet in effect, any project approvals would be conditioned on the Metropolitan Council approving the plans.
  - **Rezoning.** With annexation, the Hopkins land parcel would need to be zoned consistent with the Minnetonka parcel and comprehensive plan. The applicant has requested Planned Unit Development.

### Proposal Summary

The following is intended to summarize the applicant's proposal. Additional information associated with the proposal can be found in the "Supporting Information" section of this report.

- **Existing Site Conditions**

The subject property is 1.59 acres in size and is occupied by a vacant 25,680 sq. ft. retail building. The southerly parcel, 0.36 acres in size, is vacant and located in the city of Hopkins. The site generally slopes from east to west with a grade change at the lower level building walkout. Existing hard surface covers approximately 73 percent of the property.

- **Existing Zoning and Guide Plan Designation**

The property is currently zoned PUD, planned unit development. It is designated as high-density residential in the comprehensive guide plan. The Hopkins parcel to be annexed is zoned and guided commercial.

- Proposed Use**

As proposed, the 3-story apartment building would include 75 units configured as follows:

Unit Type	Count
Alcove	9
Studio	14
1 BR	21
1 BR + Den	9
2 BR	22
<b>Total</b>	<b>75</b>

The three-story building is a flat roof design approximately 35 to 37 feet in height with shorter two-story areas at the north and south ends of the building, which are 26 feet in height. The building would be faced with brick and composite materials. Two building entry points are located along Shady Oak Road, a third on the south elevation and a fourth on the west elevation from the surface parking lot.



The building would be served by 106 parking stalls, which include 77 under-building garage spaces and 29 surface parking stalls on the west side of the building. Additionally, the surface parking area identifies 7 proof of parking spaces.

	<b>Shady Oak Crossings</b>
<b>Footprint</b>	27,926 sq.ft.
<b>Total Habitable Space</b>	78,375 sq.ft.
<b>Floor Area Ratio</b>	0.92
<b>Hardcover</b>	61 percent
<b>Number of Units</b>	75
<b>Stories</b>	3 stories
<b>Density</b>	38 units/acre



- **Surface Water Management**

The existing site has no surface water management system. As proposed, a stormwater treatment basin would be constructed on the adjacent city-owned property to accommodate the development. The stormwater basin would be located in the rear yard of the existing home and north of the existing trees along the wetland edge. The stormwater pond is sited to minimize impacts to the existing trees. The stormwater pond would be required to meet surface water treatment minimums.

### **Primary Questions and Analysis**

A land-use proposal is comprised of many details. In evaluating a proposal, staff first reviews these details and then aggregates them into a few primary questions or issues. The following outlines both the primary questions associated with the proposed Shady Oak Crossings project and staff's findings.

- **Is the proposed project a reasonable use for the property?**

Yes, the proposal is a reasonable use of the property as it is similar to the previously approved 2017 project and improves upon other aspects. The city approved a similar two and three-story, 49-unit apartment building. The proposal required changes to the comprehensive guide plan from commercial to high density residential and rezoning from B-2 commercial to planned unit development. The subject property is currently zoned B-2, commercial. In this case, staff's opinion is that the proposed 49-unit apartment building would have fewer negative impacts than the existing commercial building and tenants that may locate and operate within it would have on the surrounding neighborhood and commercial corridor. Those reduced impacts include the following:

- Surface water management and treatment,
- Garbage and refuse management,
- Increase in green space,
- Building and parking lot lighting spill and glare,
- Clean up of a contaminated property,
- Potential for a number of commercial businesses generating noise, odor, and other potential nuisance conditions.

- **Is the proposed project consistent with the vision for the area?**

There is a market demand for increased density and affordable housing. Since 2005, the city has studied a number of redevelopment options for the Shady Oak Road corridor and specifically for this property, including commercial and residential development. Those options have ranged from commercial to a number of mid- and high-density residential alternatives. Again in 2016, the city conducted a specific engagement process for the property with the full intention of exploring redevelopment alternatives. Similar to the 2005 study, the alternatives generated by the neighborhood participants

were mid- and high-density residential housing. In 2016, the city received four high-density residential concepts through its Request for Information solicitation to the development community. Three of the four concepts were apartment concepts ranging between 69 and 72 units. The fourth concept was the three-story 56 unit Ron Clark apartment proposal. The city ultimately entered into negotiations with Ron Clark.



*Redevelopment concept from the 2005 study*

Locating higher density housing along more highly traveled roadway corridors, and close to commercial services is good land-use practice. The subject site is located along Shady Oak Road, which is an arterial street. The location affords residents the ability of convenient roadway and bus route access. Its location also provides the ability to walk or bike to commercial businesses and services along Shady Oak Road and downtown Hopkins. There are several high-density residential housing units immediately adjacent to single-family residential neighborhoods. This project would be as well. However, its location on the Shady Oak Road provides screening, buffering and less impactful use characteristics to the residential neighborhood that otherwise would not be possible if the property remained as commercial use.

Planned unit development zoning is a commonly used zoning tool by the city to achieve specific objectives with a development project to further city goals. As defined in city code, "...PUD zoning may be considered by the city when it would result in one of the following public benefits:

- a) Greater preservation of existing natural resources, in number or quality, than would otherwise be provided under non-PUD development;
- b) Provision of affordable housing;
- c) Provision of a housing type or target housing price that is desirable to the city;
- d) A mix of land use types;
- e) Development that is compatible with existing, surrounding development type and intensity that is no longer allowed in other existing zoning districts; or

- f) Greater energy conservation through building and site design than would otherwise be achieved under non-PUD development;
- g) Other public benefits as recognized by the city.”

The code further states that, “...a PUD may be approved when the following general standards are met:

- a) The PUD results in at least one of the public benefits as outlined in section 2 of this ordinance;
- b) The PUD is consistent with and advances the community-wide goals of the comprehensive plan; and
- c) The PUD is appropriately integrated into existing and proposed surrounding development. This does not mean the PUD reflects the specific standards of the surrounding area, such as lot size, density, setbacks, or design. While integration may be achieved through such standards, it may also be achieved through the continuation of existing land use types, architectural transitions, landscape buffering, or other means.”

- **Is the proposed site and building design reasonable?**

Yes. The applicant has proposed a three-story building, with underground parking that is an architecturally attractive and fits in the context of the Shady Oak Road commercial corridor. The building would be faced with brick and composite materials. The building would have a flat roof with two-story features at the north and south ends to soften the edges at Main Street and Oak Drive Lane intersections with Shady Oak Road. Apartment units would have glass sliding doors with a deck attached to the unit. The applicant has also proposed a landscaping plan that would over-story trees along the street frontages to soften the building’s appearance. These features have improved the building aesthetics, impact, and site functionality since the initial concept plan submittal.

The proposed project would extend the sidewalk along Oak Drive Lane from Shady Oak Road to the driveway. The applicant has proposed internal walkways to connect the sidewalk to building access points. A tot lot play area is proposed for residents on the west side of the building.

### **City Council Introduction**

The city council introduced the ordinance at its Jan. 6, 2020, meeting. The council asked/made the following questions and statements:

- Disappointed that the project isn’t 100 percent affordable but encouraged that 30 percent of the units are affordable to those earning 60 percent of the area median income. Why the change of percent affordable?
- Concern about the increase in monthly rent and 3 bedroom units.
- Provide an analysis of project density and affordability.
- How is stormwater management being addressed?
- What is the alignment of the surface parking driveway to other driveways on Oak Drive Lane?

- Is there enough parking provided?
- Ability to provide a different look and feel to the first floor instead of balconies.

### **Neighborhood Meeting**

The developer will be hosting a neighborhood meeting prior to the planning commission meeting. The meeting will be held in the Purgatory Room at the Community Center on Jan. 16, 2020, from 5 – 6 p.m. Staff will provide a summary of the input received at the planning commission meeting.

### **Summary Comments**

Staff supports the proposal. The additional site area, access configurations, and housing units will improve the overall appearance and functionality of the site. The proposal also enhances the image of the Shady Oak Rd./Main St. intersection.

### **Staff Recommendation**

Recommend the city council adopt the following:

- 1) Ordinance adopting a Master Development Plan,
- 2) Resolution approving final site and building plans and preliminary plat,
- 3) Resolution approving a concurrent Detachment/Annexation and associated actions with the parcel:
  - Comprehensive guide plan amendment to high-density residential, and
  - Rezoning to planned unit development (PUD)

Originator: Loren Gordon, AICP, City Planner

**Supporting Information**

**Surrounding** Northerly: storm water ponding property; guided low density residential

**Land Uses** Easterly: Single family residential in the city of Hopkins  
 Westerly: Single family residential; guided low density residential  
 Southerly: commercial retail in the city of Hopkins

**Planning** Guide Plan designation: High Density Residential  
 Existing Zoning: PUD, Planned Unit Development

**Development Standards**

The proposed development standards are included as the regulatory standards under the planned unit development ordinance.

	<b>Proposed</b>
<b>Building Setbacks</b>	
North property line	50 ft
South property line	45 ft
East property line	17 ft
West property line	18 ft
<b>Other Setbacks</b>	
Floodplain	n/a
Wetland	n/a
<b>Floor Area Ratio</b>	0.92
<b>Impervious Surface</b>	61%
<b>Density</b>	38 units/acre
<b>Height</b>	36 ft.

**Previous Reviews And Public Engagement**

The city has been actively involved in promoting redevelopment of the site. The following is a summary of meetings and open houses for the Ron Clark project.

67-unit plan – City Council and Planning Commission Meetings

May 2, 2019 – Planning Commission Meeting

- Concept plan review

May 20, 2019 – City Council Meeting

- Concept plan review

July 8, 2019 – City Council Meeting

- Introduction of master development plan ordinance

Approved 49-unit plan - City Council, Economic Development Advisory Commission and Planning Commission Meetings

Aug. 14, 2017 - Joint EDAC/City Council Meeting

- Review of project financials
- Introduction of the rezoning ordinance

April 24, 2017 - City Council Meeting

- Concept plan review

Feb. 27, 2017 - City Council Meeting

- Concept plan review

Feb. 16, 2017 - Planning Commission Meeting

- Concept plan review

Nov. 14, 2016 - City Council Special Meeting

- Shady Oak Road Redevelopment Developer Interviews

Open House and Neighborhood Meetings

Aug. 2, 2017 – 7:30-9 a.m., 11 a.m.-1 p.m., 5-7 p.m.

- Updated concept plan. Feedback collected via Minnetonka Matters. Additional feedback was collected via Minnetonka Matters. Please note, City of Minnetonka staff documented feedback from the April 6 meeting on the discussion forum using the "Minnetonka Matters" admin account.

Feb. 15, 2017

- Presentation of concept plan

Shady Oak Road Redevelopment Study

April 7, 2016

- The third meeting discussed feedback we have received and more development option information.

Feb. 11, 2016

- The second meeting discussed, in more detail, the possible redevelopment of the property.

Jan. 13, 2016



- Discuss the future of the property including a general outline of a process that will take place over several months and potential changes to the property.

**Preliminary and Final Plat**

The preliminary and final plat reorients the existing property line between the two city-owned parcels. The property line readjustment provides room for the apartment parking lot and additional frontage for the single-family home. (See attached). Staff has reviewed the request and determined that the proposal meets the city ordinance.

**Stormwater**

As proposed, drainage from the site would be managed located in an open pond to the west of the building. As a condition of approval, a final stormwater management plan and specifications must be submitted prior to the issuance of a grading permit. The plans must meet the standards of the city's Water Resources Management Plan, incorporating rate control, volume control, and water quality treatment.

**Solar Energy**

Solar energy opportunities are not proposed but are always an option for the property.

**Utilities**

Public water, sanitary, and storm sewer facilities are available to the site from Oak Drive Lane. A 50-foot sanitary sewer easement extends along the west property line. The easement provides clearance for a 54-inch sanitary sewer pipe that services a large portion of the city.

**Sidewalks**

A sidewalk exists on Shady Oak Road, which was newly constructed along with the roadway improvements. An additional sidewalk is proposed along the north side of the site.

**Traffic and Parking Study**

The city commissioned a traffic study to:

1. Understand the existing traffic and parking conditions of the site;
2. Evaluate potential impacts of the proposed redevelopment; and
3. Address improvement options for any issues, if necessary.

In evaluating each of these items, the city's traffic engineering consultants drew on general engineering principles, as well as specific observations of the existing site.

The study generally concluded the project would have no negative impacts on the surrounding roadway system. A few of the study's key findings include:

- The proposed site will generate 36 trip ends in the am peak hour, and 46 trip ends in the pm peak hour with an estimated 90% of the traffic using the Main Street, and Shady Oak Road intersection through a new driveway that forms the west leg of

this intersection. The remaining 10% of the traffic is estimated to use Oak Drive Lane.

- With the development of the site, the level of service for traffic at Main Street and Shady Oak Road intersection is LOS “A” in the am peak hour and LOS “B” in the pm peak hour. The intersection operates at a very good level of service in both peaks and has ample spare capacity for future traffic growth.
- It is recommended the new west approach to Main Street and Shady Oak Road intersection have a separate left-turn lane with at least 75 feet of storage. The through and right-turn movements can be shared on one lane. It is also recommended that the westbound approach be restriped to allow a separate left-turn lane and a through right-lane. The through and right-turn movements can be shared on one lane on the westbound approach as well.
- With the removal of the existing businesses from the project site, and due to the primary access to the apartment being from the Main Street and Shady Oak Road intersection, the traffic levels at the intersection of Oak Drive Lane and Shady Oak Road are expected to be lower than current levels, and the intersection is anticipated to operate at an acceptable level.

## Parking

The city code requires 2 parking spaces per unit for multi-family developments, with one of those spaces located in an enclosed structure. As proposed, the Shady Oak Crossings project would provide 106 of the required 150 parking spaces or a ratio of 1.41 parking spaces per unit and 1.29 spaces per bedroom. Although the project has an additional 7 proof of parking spaces providing the opportunity for 113 parking spaces, it is short of the traditional code standard. The city has seen that 2 spaces per unit is more than is needed. A better approach for parking a building is by the number of spaces per bedroom. The approach provides a more realistic pairing of potential or would-be drivers and bedrooms. The more recently constructed buildings tend to have fewer bedrooms per unit. Shady Oak Crossings provides 1.29 parking spaces per bedroom, which is higher than other similar and more recently constructed apartment buildings.

<b>Shady Oak Crossings</b>		
	<b>Number of Spaces Required</b>	<b>Number of Spaces Provided</b>
<b>Underground</b>	75	77
<b>Surface</b>	75	29
<b>TOTAL</b>	150	106

	<b>Stalls per Bedroom</b>	<b>Stalls per Unit</b>
<b>Traditional Code Standard</b>	n/a	2
<b>ITE</b>	n/a	1.10-1.37
<b>Shady Oak Crossings Proposed</b>	<b>1.29</b>	<b>1.41</b>
<b>Tonka on the Creek</b>	1.15	1.49
<b>Carlson Island</b>	1.03	1.55
<b>The Ridge</b>	.93	2

**Transit**

The site at 4312 Shady Oak Road, is served by bus routes 12 and 670. The nearest bus stop is located directly across the street from the site at Main Street and 20th Avenue North in Hopkins. Bus route 12 connects Minnetonka riders to Hopkins, St. Louis Park, and Minneapolis with service every 12-30 minutes during the weekday between 6 - 7 p.m. Express route 670 provides riders weekday express access to downtown in the morning and evenings. In addition, the proposed Southwest LRT Shady Oak Road station is approximately a half-mile from the proposed project. When completed in 2021, riders will have service to downtown Minneapolis and St. Paul.

**Building Design**

The proposed building would have a three-story appearance but have two-story features to soften the north and south building ends. (See attached). The proposed building would have a code-defined height of 36 feet, as shown.

Building materials would include brick and composite materials. As a condition of approval, final materials and color palate board must be submitted for staff review and approval.

**Natural Resources**

Trees

There are 38 significant trees on the site. As proposed, the project would remove 1 high priority tree and 15 significant trees. The subject proposal would meet the tree protection ordinance.

There are a couple of opportunities to increase tree preservation by reconfiguring the stormwater basin to save high priority tree 4822 and tree 4828.

If tree 4822 cannot be saved through grading changes, mitigation will be required for it and two significant trees (4804 and 4805) for a total mitigation of 48-inches or 24, two-inch trees. The landscape plan indicates that they will plant 40 deciduous trees, so this will satisfy the mitigation requirement.

### Landscape Plan

Generally, the landscape plan meets ordinance requirements, and staff approves it with the following comments:

- Trees cannot be planted within the utility or sanitary easement line. These could be relocated to the adjoining city property.
- A final landscape plan be provided for review and approval by staff.
- Provide a final project and landscape value.
- Plantings surrounding the pond be of a native seed mix.
- Thirteen maples are specified to consider substituting 5 with a different genus for added diversity.

### Wetland Items

- A 16.5-foot buffer with conservation easement around the wetland is required.

### Erosion Control

- Meet city standard erosion control requirements, including compliance escrow and the installation and maintenance of erosion control devices during construction.

## **Affordable Housing**

The project proposes 23 units would be affordable to persons earning 60 percent of the area median income. Rents would also be required to meet certain affordability requirements. With 30 percent of the 75 units affordable, the proposal would exceed the required 20 percent city's affordable housing policy. The economic development advisory commission reviewed a request from Ron Clark Construction for city financial assistance to secure the affordability of those 23 units. The commission recommended the city provide assistance. The city council/economic development authority will consider the request from Ron Clark Construction and recommendation from the economic development advisory commission on Feb. 24, 2020, along with the project land use approvals.

## **Motion Options**

Typically, the planning commission has four options, as noted below. For this project at this time, and as noted previously in the report, the only option staff recommending is to table the item.

1. Concur with the staff recommendation. In this case, a motion should be made recommending the city council adopt the rezoning ordinance, preliminary and final plat resolution, and site and building plan resolution, with variances.

2. Disagree with staff's recommendation. In this case, a motion should be made recommending the city council deny the requested rezoning, preliminary and final plat, and final site and building plans, with variances. This motion must include a statement as to why denial is recommended.
3. Concur with some of staff's recommendations and disagree with the others. In this case, a motion should be made recommending approval of the some and denial of the others. This motion must include a statement as to why denial is recommended.
4. Table the requests. In this case, a motion should be made to table the item. The motion should include a statement as to why the request is being tabled with direction to staff, the applicant, or both.

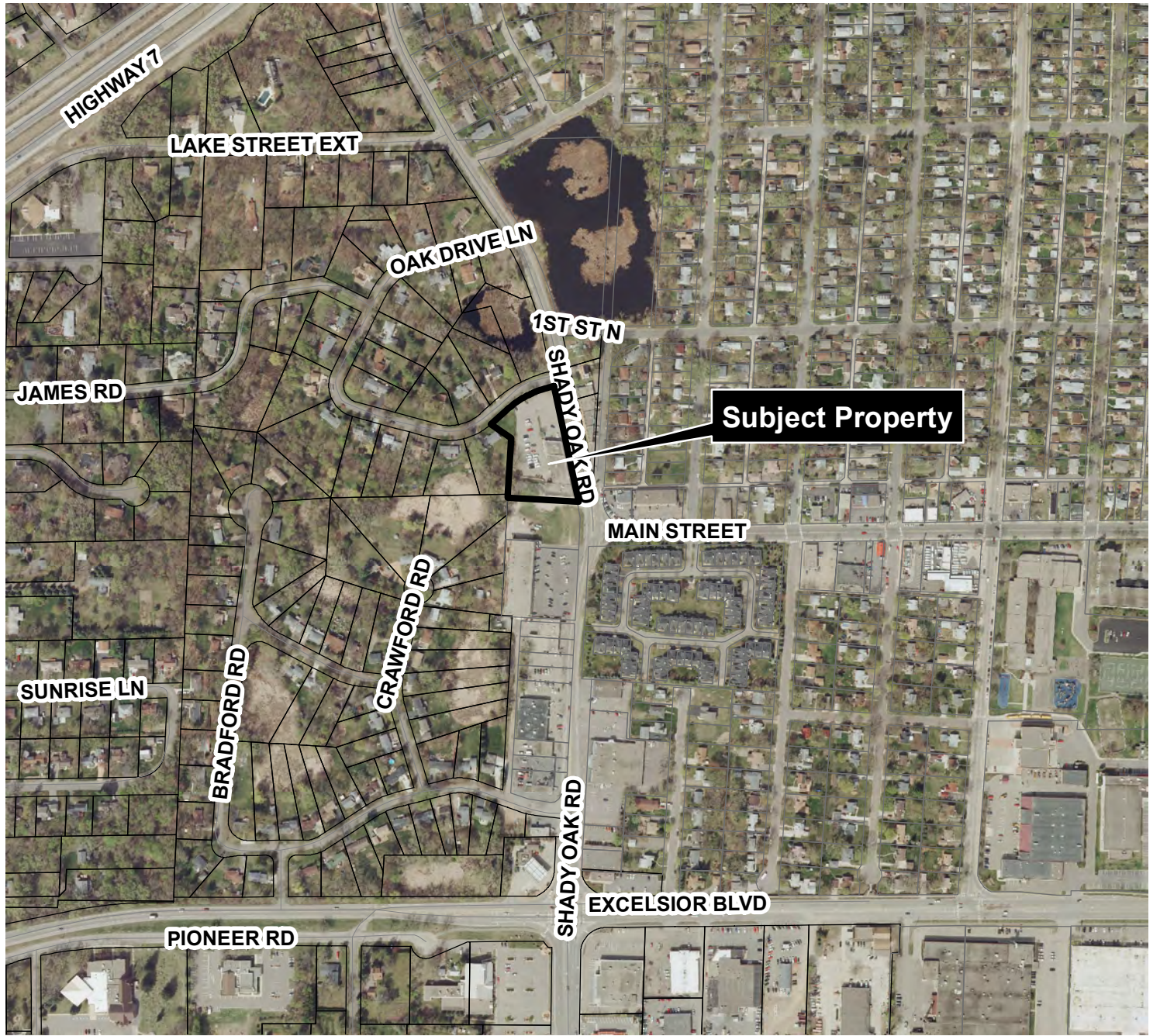
**Neighborhood  
Comments**

At the time of publication of this report, the city had received no written comments. Notices were sent to 365 property owners in Minnetonka and Hopkins.

**Deadline for Action**

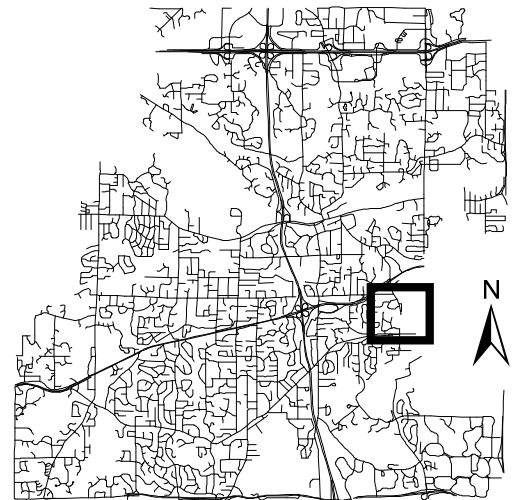
April 1, 2020





## LOCATION MAP

Project: Shady Oak Redevelopment  
Address: 4312 Shady Oak Rd.



This map is for illustrative purposes only.



Monday, December 02, 2019

Loren Gordon  
City of Minnetonka  
14600 Minnetonka Blvd  
Minnetonka, MN 55345

RE: Shady Oak Crossing Project Narrative

**Ron Clark Construction** is proposing a three-story, 75-unit apartment building on the property located at 4312 Shady Oak Road and on a portion of the property to the south that is currently in the city of Hopkins.

The proposed apartment building would have underground parking, resident community room, exercise room, onsite manager's office and an outdoor play area.

It is proposed to have a mix of Alcoves, Studios, 1- and 2-bedroom apartments and they currently expect the unit rents to be between \$1,000 and \$1,300 per month for the affordable units and between \$1,100 and \$2,400 per month for the Market Rate units. (See attachments).

Zoning for the property is currently B-2, limited business district. The city's comprehensive plan guides the property for commercial use.

**Change from Tax Credit to a Mixed Income Apartment.** We have submitted for tax credits the last 2 years and have been unsuccessful, as such we have worked with staff and decided to propose a mixed income project. We are disappointed that we were not able to obtain the tax credits, but the process has become more and more competitive over the last few years and without being within ½ mile of the Light Rail platform, we just don't get enough points to beat other projects. We have revised the project with more of a Market Rate unit mix and we have also adjusted the sizes of the units to be consistent with comparable projects, which allows for a smaller overall building. We will have 9 Alcove, 14 Studios, 21 One Bedroom, 9 One Bedroom + Dens and 22 Two Bedroom units.



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Edina, MN  
55439

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fax (952) 947-3030

**Rezoning and Comprehensive Plan:** The proposed residential use requires a rezoning and guide plan change.

The proposed housing component would qualify the project for public benefit under the planned unit development zoning district.

A complementary high density residential comprehensive plan re-guidance would align with the zoning density of 27.18 units/acre. (75 units/2.76 acre)

**Building Design:** The proposed 3 story building with a combination of sloped and flat roof and two-story components at each end represents significant first step in the redevelopment of the Shady Oak Road corridor between Highway 7 and Excelsior Boulevard.

This existing commercial building is dilapidated and unlikely to be a candidate for remodeling. The other residential redevelopment in the area includes The Oaks of Mainstreet townhome development (late 1990s) at the corner of Shady Oak Road and Mainstreet.

The proposed apartment building incorporates an attractive roof design and an articulated façade, underground parking and common building entry accesses.

**Changes from the previously approved development plan:**

During our previous City approval process most of the concern from the neighbors was the impact of traffic on Oak Drive Lane. We had attempted during the previous application to approach our neighbor to the south on acquiring some additional property to allow for a change of access to the site, but we were unsuccessful. After our approval we re-kindled those discussions and now have a purchase agreement for the additional land needed to make the access off Shady Oak Road possible at the current stop light location.

Our current design includes 75 units. The previous design submitted in May earlier this year contained 67 units, but the building footprint was much larger which was a major concern of the Planning Commission and City Council. Our new design has

smaller units and the building is approximately 60' shorter in length along Shady Oak Road and much farther from Oak Drive Lane.

**Site Design:** Like our previous proposal, this proposal would site the apartment building toward Shady Oak Road while providing greenspace to separate the building from the sidewalk.

Surface parking and a play area are provided on the west side of the building and the underground parking is now accessed only from Shady Oak Road.

Site and building design consider the relationships of public and private spaces. A strong relationship of the sidewalk, front yard space and the building's first floor is essential for great spaces, including an outdoor patio and rooftop deck, both facing the main street intersection.

**Changes to Site Design:**

The previously approved site plan in 2017 had the entrance to the parking garage coming from Oak Drive Lane. The parking garage now enters from Shady Oak Road. The only traffic to Oak Drive Lane will come from our small surface parking lot of 29 parking stalls that will mainly be used by visitors.

The building now has shifted south to allow the garage entrance to come from Shady Oak Road.

The building exterior has changed to more blend and complement the existing residential neighborhood and the front of the building is faced toward and connected to the sidewalk along Shady Oak Road while providing greenspace to separate the building from the sidewalk.

Accenting landscaping will be placed at the north and south ends of the building to provide an attractive updated presence along Shady Oak Road. All efforts will be made to protect the existing trees as well as adding additional trees and landscaping to screen the existing neighbors from the surface parking.

**Stormwater Management:**

The current property is covered with 1.53 acres of impervious surface and primarily drains to the wetland. The new development stormwater management system for the site will convey all site runoff to a new basin installed on the adjacent property to the West. The impervious area for the new development (1.18 ac) provides a 23% reduction from the existing site condition. The development will meet all management standards required by the City of Minnetonka, the Nine Mile Creek Watershed District and the MPCA NPDES Permit.

**Traffic:** Prior to our previously approved proposal the city consultant prepared a traffic study of the area and it clearly shows that the new use will have less traffic than other currently allowed uses and the effect on the surrounding intersections was minimal. The impact of our current design will be dramatically reduced from our previously approved proposal due to most of our traffic will now enter directly onto Shady Oak Road vs Oak Drive Lane.

**Affordable Housing:** The project will include some units that are affordable based on 60% of area medium income (AMI).

**Professional Management:** Steven Scott Management will be our management company, they are a highly respected local company.

We will have an onsite resident caretaker as well as a building manager who is at the building a minimum of 30 hours per week, along with leasing agent and a Senior Manager who oversees the building management.

As part of the maintenance and management of the building we are in each unit, normally monthly or bi-monthly to maintain equipment and to do a quick inspection to confirm no lease violations or undo wear and tear is happening.

Monday, December 02, 2019

Loren Gordon  
City of Minnetonka  
14600 Minnetonka Blvd  
Minnetonka, MN 55345

RE: Shady Oak Crossing – Request for Annexation

**Ron Clark Construction** is under Purchase Agreement to acquire a portion of the property described on the attached Exhibit as Parcel A. This property will become the main access to the proposed apartment building and will also have a small portion of the apartment building on it, the remaining portion of the parcel will remain with the current owner and be used for additional parking and access for the retail building to the south.

We are requesting annexation of Parcel A per the attached exhibit into the city of Minnetonka as part of our development approvals.

Respectfully



J. Michael Waldo

CEO, Ron Clark Construction and Design.

Monday, January 06, 2020


Loren Gordon  
City of Minnetonka  
14600 Minnetonka Blvd  
Minnetonka, MN 55345

RE: Shady Oak Crossing – Request for Annexation

**Ron Clark Construction** is under Purchase Agreement to acquire a portion of the property described on the attached Exhibit as Parcel A. This property will become the main access to the proposed apartment building and will also have a small portion of the apartment building on it, the remaining portion of the parcel will remain with the Me and be used for additional parking and access for the retail building to the south.

I am requesting annexation of Parcel A per the attached exhibit into the city of Minnetonka as part of Ron Clark Construction city of Minnetonka development approvals. This request is contingent on the project receiving the required city approvals and the subsequent purchase of Parcel A by Ron Clark Construction.

Respectfully

  
Eric S. Johnson

Property Owner



## **Submitted Plans**





OAK DRIVE LANE

PROOF OF PARKING (4)

29 PARKING SPACES

PROOF OF PARKING (3)

75 UNIT, 3 STORY BUILDING

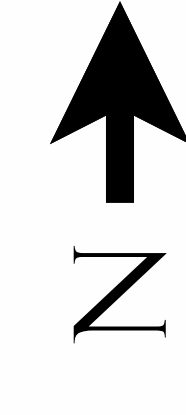
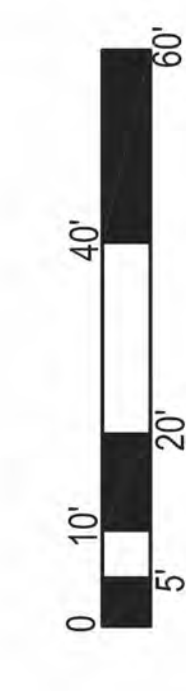
PLAY AREA

GARAGE ENTRANCE

ROOFTOP DECK

PATIO

SHADY OAK ROAD



SITE PLAN  
SCALE: 1" = 20'





















SOUTH ELEVATION SCALE: 3/32" = 1'-0"



WEST ELEVATION SCALE: 3/32" = 1'-0"



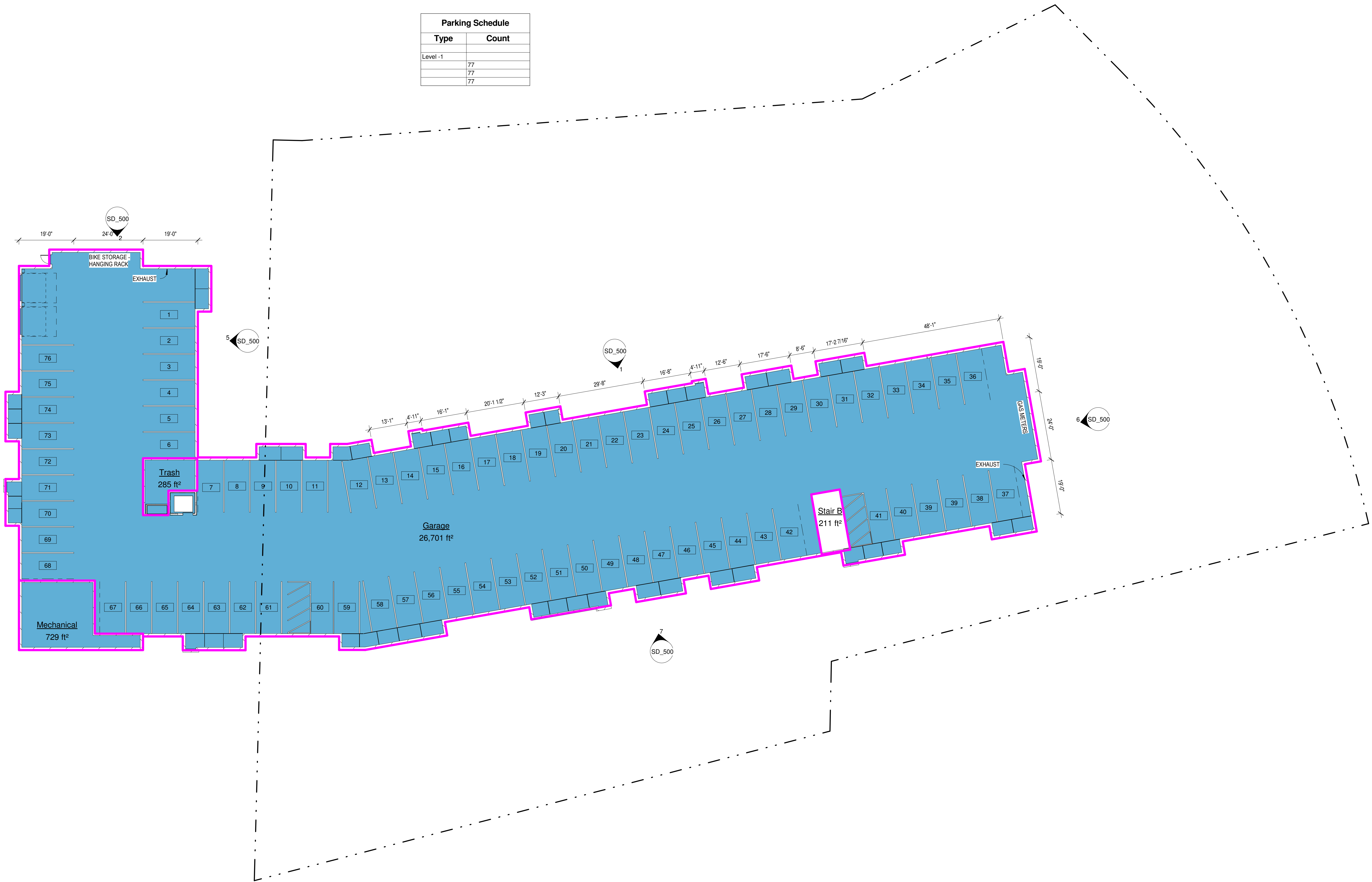
EAST ELEVATION SCALE: 3/32" = 1'-0"



NORTH ELEVATION SCALE: 3/32" = 1'-0"



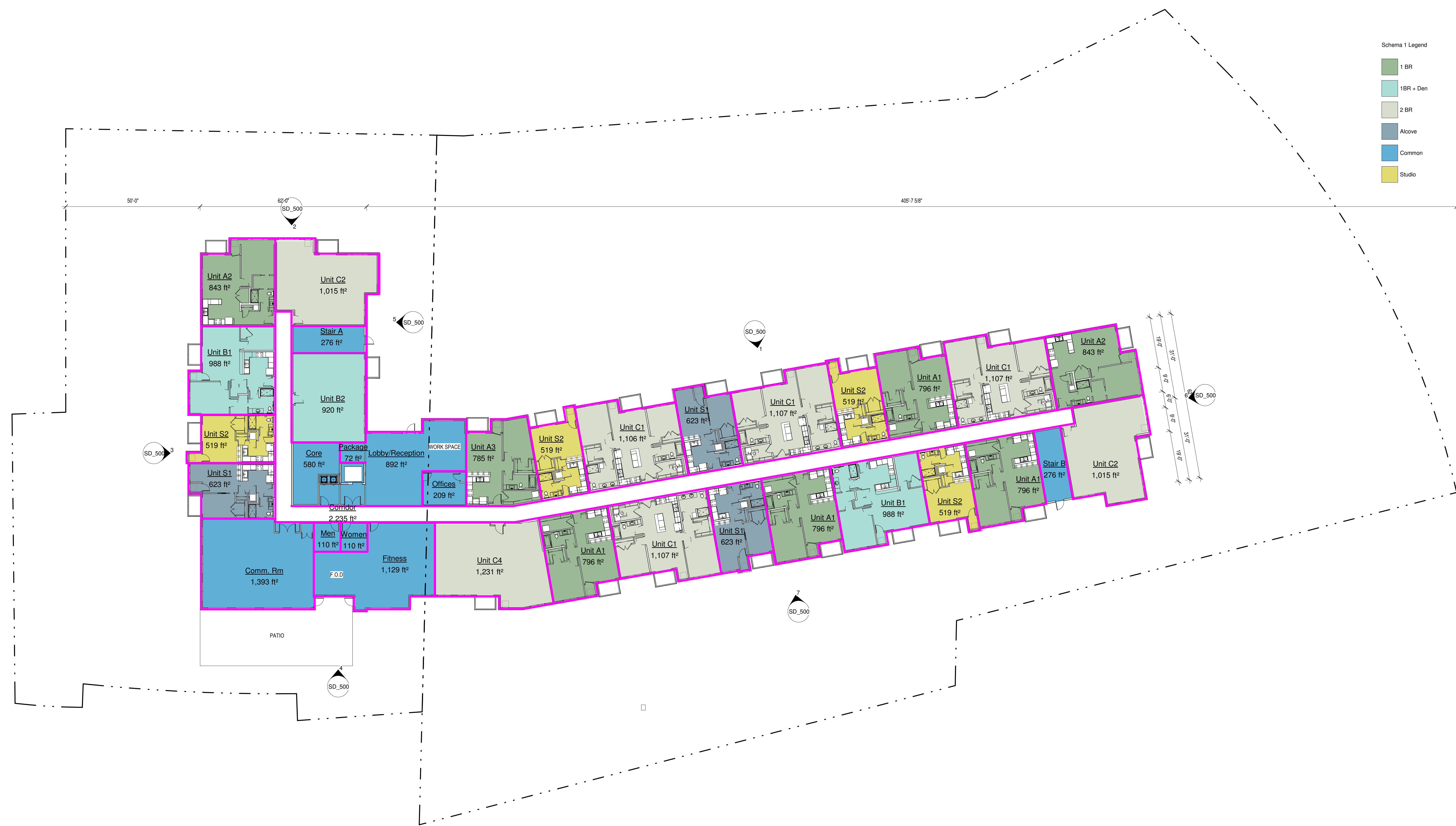
Parking Schedule	
Type	Count
Level -1	77
	77
	77



① Level -1  
1/16" = 1'-0"

Schema 1 Legend

- 1 BR
- 1BR + Den
- 2 BR
- Alcove
- Common
- Studio



① Level 1  
1/16" = 1'-0"



Schema 1 Legend

- 1 BR
- 1BR + Den
- 2 BR
- Alcove
- Common
- Studio



① Level 2  
1/16" = 1'-0"

Schema 1 Legend

- 1 BR
- 1BR + Den
- 2 BR
- Alcove
- Common
- Studio



1 Level 3  
1/16" = 1'-0"



### Unit Mix by Floor

Name	Count	Net Area	Gross Area	Level
Level 1				
Unit A1	4	704 ft <sup>2</sup>	796 ft <sup>2</sup>	Level 1
Unit A2	2	757 ft <sup>2</sup>	843 ft <sup>2</sup>	Level 1
Unit A3	1	721 ft <sup>2</sup>	785 ft <sup>2</sup>	Level 1
Unit B1	2	915 ft <sup>2</sup>	988 ft <sup>2</sup>	Level 1
Unit B2	1	857 ft <sup>2</sup>	920 ft <sup>2</sup>	Level 1
Unit C1	4	1,029 ft <sup>2</sup>		Level 1
Unit C2	2	939 ft <sup>2</sup>	1,015 ft <sup>2</sup>	Level 1
Unit C4	1	1,151 ft <sup>2</sup>	1,231 ft <sup>2</sup>	Level 1
Unit S1	3	549 ft <sup>2</sup>	623 ft <sup>2</sup>	Level 1
Unit S2	4	448 ft <sup>2</sup>	519 ft <sup>2</sup>	Level 1
	24			
Level 2				
Unit A1	5	704 ft <sup>2</sup>	796 ft <sup>2</sup>	Level 2
Unit A2	2	757 ft <sup>2</sup>	843 ft <sup>2</sup>	Level 2
Unit A3	1	721 ft <sup>2</sup>	785 ft <sup>2</sup>	Level 2
Unit B1	2	915 ft <sup>2</sup>	988 ft <sup>2</sup>	Level 2
Unit B2	1	857 ft <sup>2</sup>	920 ft <sup>2</sup>	Level 2
Unit C1	5	1,029 ft <sup>2</sup>	1,107 ft <sup>2</sup>	Level 2
Unit C2	2	939 ft <sup>2</sup>	1,030 ft <sup>2</sup>	Level 2
Unit C3	1	1,197 ft <sup>2</sup>	1,282 ft <sup>2</sup>	Level 2
Unit C4	1	1,151 ft <sup>2</sup>	1,231 ft <sup>2</sup>	Level 2
Unit S1	3	549 ft <sup>2</sup>	623 ft <sup>2</sup>	Level 2
Unit S2	5	448 ft <sup>2</sup>	519 ft <sup>2</sup>	Level 2
	28			
Level 3				
Unit A1	5	704 ft <sup>2</sup>	796 ft <sup>2</sup>	Level 3
Unit A3	1	721 ft <sup>2</sup>	785 ft <sup>2</sup>	Level 3
Unit B1	2	915 ft <sup>2</sup>	988 ft <sup>2</sup>	Level 3
Unit B2	1	857 ft <sup>2</sup>	920 ft <sup>2</sup>	Level 3
Unit C1	5	1,029 ft <sup>2</sup>	1,107 ft <sup>2</sup>	Level 3
Unit C4	1	1,151 ft <sup>2</sup>	1,231 ft <sup>2</sup>	Level 3
Unit S1	3	549 ft <sup>2</sup>	623 ft <sup>2</sup>	Level 3
Unit S2	5	448 ft <sup>2</sup>	519 ft <sup>2</sup>	Level 3
	23			
Grand total: 75	75			

### Unit Mix by Unit Type (Gross SF)

Name	Count	Unit Type
1 BR		
Unit A1	14	1 BR
Unit A2	4	1 BR
Unit A3	3	1 BR
	21	
1BR + Den		
Unit B1	6	1BR + Den
Unit B2	3	1BR + Den
	9	
2 BR		
Unit C1	14	2 BR
Unit C2	4	2 BR
Unit C3	1	2 BR
Unit C4	3	2 BR
	22	
Alcove		
Unit S1	9	Alcove
	9	
Studio		
Unit S2	14	Studio
	14	
Grand total: 75	75	

### Total Gross Area

Level	Area
Level 3	23,549 ft <sup>2</sup>
Level 2	27,360 ft <sup>2</sup>
Level 1	27,466 ft <sup>2</sup>
Level -1	27,926 ft <sup>2</sup>
Grand total	106,300 ft <sup>2</sup>

### Parking Schedule

Type	Count
Level -1	
	77
	77
	77



**WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**

TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

# SHADY OAK CROSSING

## MINNETONKA, MN

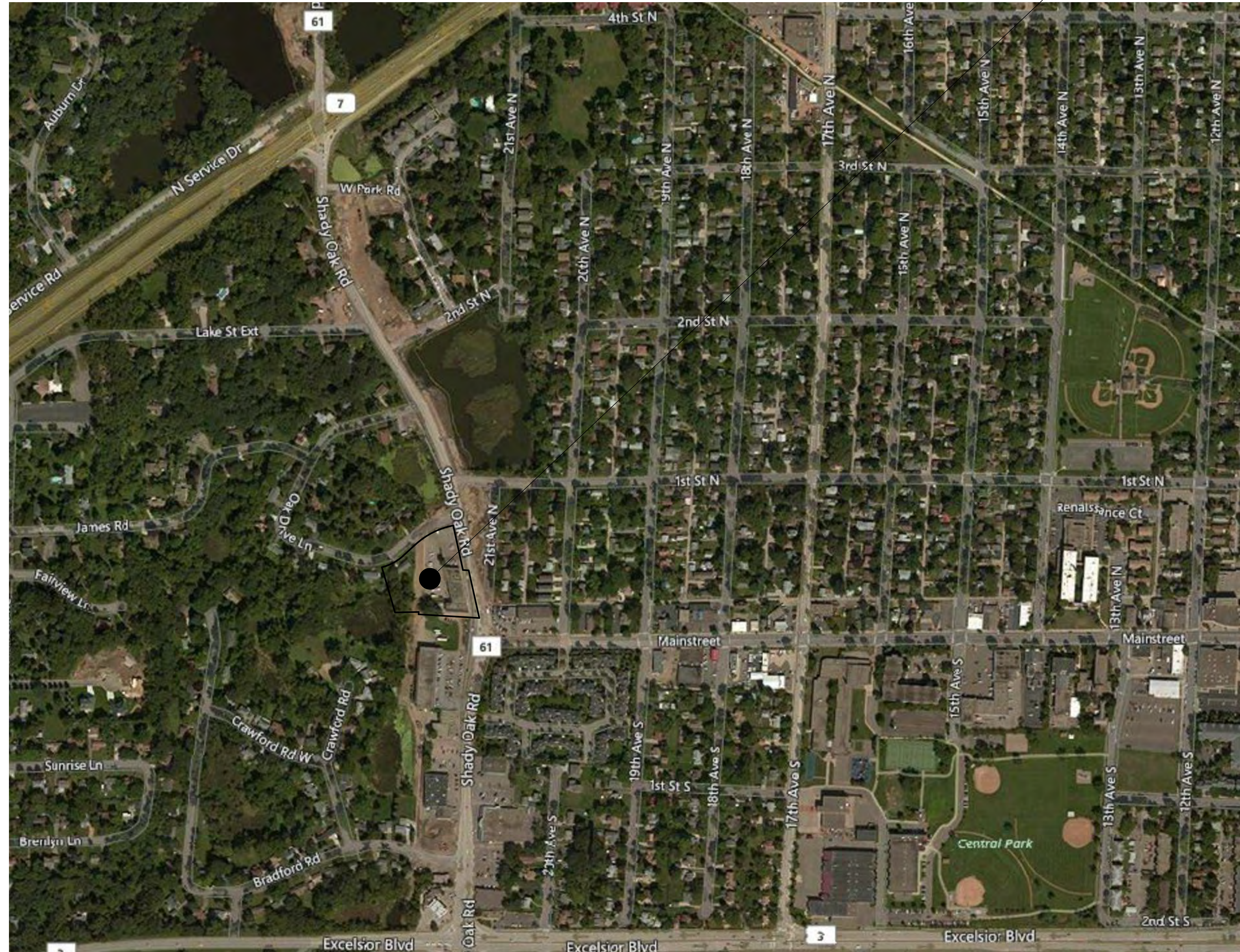
**PROJECT DIRECTORY**

**OWNER:**  
 RON CLARK CONSTRUCTION & DESIGN  
 MIKE ROEBUCK  
 7500 WEST 78TH STREET  
 EDINA, MN 55439  
 PH. 952.947.3022  
 EMAIL: MIKE@RONCLARK.COM

**ENGINEER:**  
 CAMPION ENGINEERING SERVICES, INC.  
 MARTY CAMPION  
 1800 PIONEER CREEK CENTER  
 MAPLE PLAIN, MN 55364  
 PH. 763.479.5172  
 EMAIL: MCAMPION@CAMPIONENG.COM

**SURVEYOR:**  
 WENCK ASSOCIATES  
 1800 PIONEER CREEK CENTER  
 MAPLE PLAIN, MN 55359  
 PH. 763.479.4200

PROJECT LOCATION



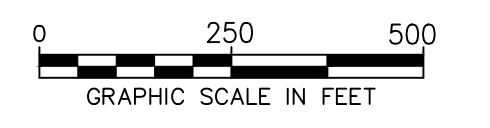
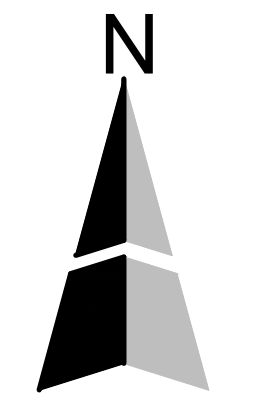
**NOTE:**  
 BOUNDARY AND TOPOGRAPHIC INFORMATION PER SURVEY PREPARED BY WENCK ASSOCIATES, DATED FEBRUARY 6, 2017.

**GOVERNING SPECIFICATIONS:**

1. THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" LATEST EDITION & SUPPLEMENTS.
2. CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD UTILITIES SPECIFICATIONS. (LATEST EDITION)
3. ALL APPLICABLE FEDERAL, STATE AND LOCAL LAWS AND ORDINANCE WILL BE COMPLIED WITH IN THE CONSTRUCTION OF THIS PROJECT.
4. CITY OF MINNETONKA STANDARD SPECIFICATIONS & DETAILS.

**INDEX**

SHEET NO.	DESCRIPTION
1.	COVER SHEET
2.	CERTIFICATE OF SURVEY
3.	PRELIMINARY PLAT—SHADY OAK CROSSING
4.	PRELIMINARY SITE PLAN
5.	PRELIMINARY UTILITY PLAN
6.	PRELIMINARY GRADING PLAN
7.	PRELIMINARY STORM WATER POLLUTION PREVENTION PLAN
8.	TREE INVENTORY
9.	TREE PRESERVATION PLAN
10.	DETAILS
11.	DETAILS



Plot Date & Time:

C:\Users\mkr-ca\OneDrive\2017\17-011 Shady Oak Crossing\CAD\CAD1 COVER SHEET.dwg

NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**

• Civil Engineering • Land Planning  
 1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.  
 Martin P. Campion -Lic. # 19901 Date:

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

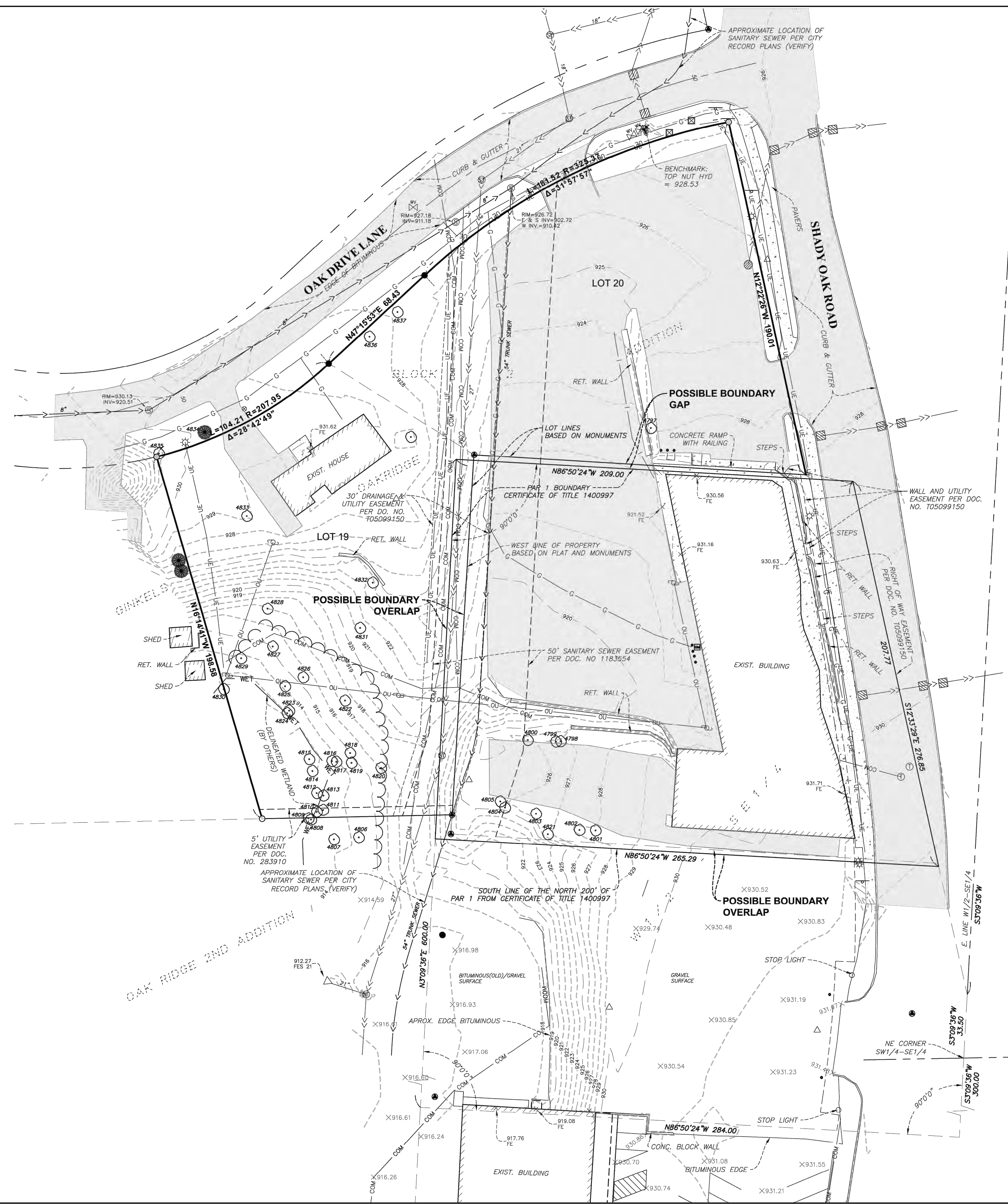
**COVER SHEET**  
 SHEET NO. 1 OF 11 SHEETS

PROJECT NO:  
**17-011**  
 DATE:  
**12/02/2019**



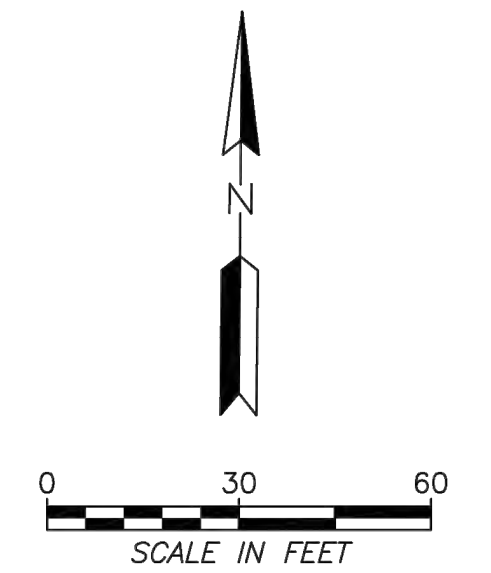
# CERTIFICATE OF SURVEY

MINNETONKA, MN



**LEGEND**

- SET 3/4"ODx14" IRON PIPE WITH PLASTIC CAP 43055 OR MAG NAIL W/WASHER
- PROPERTY MARKER FOUND BY HENNEPIN COUNTY SURVEYOR
- FOUND MONUMENT
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- ⊙ STORM SEWER INLET
- ⊙ STORM SEWER INLET
- ⊙ TELEPHONE MANHOLE
- ⊙ GAS METER
- ⊙ COMMUNICATIONS PEDESTAL
- ⊙ ELECTRIC MANHOLE
- ⊠ ELECTRIC TRANSFORMER
- ⊠ TRAFFIC SIGN
- BOLLARD/POST
- ⊙ UTILITY POLE
- ⊙ LIGHT POLE
- CONIFEROUS TREE
- DECIDUOUS TREE
- ⊙ WATER VALVE
- ⊙ HYDRANT
- > STORM SEWER
- > SANITARY SEWER
- > WATERMAIN
- > UNDERGROUND GAS LINE
- > COM UNDERGROUND COMMUNICATION LINE
- > OVERHEAD UTILITY LINE
- > TREE LINE
- ▨ BUILDING
- ▨ CONCRETE SURFACE
- ▨ ASPHALT SURFACE



**PROPERTY DESCRIPTION:**

Certificate of Title 1400998

Lot 19 Block 2, Ginkels Oakridge Addition AND

Certificate of Title 1400997

Par 1:

All that portion of the tract or parcel of land described at paragraph "A" below, which lies Northerly of a line drawn parallel to and 200 feet Southerly of the North line thereof and the same extended, to-wit:

Paragraph "A". That portion of the West Half of the Southeast Quarter of Section 23, Township 117, Range 22, described as follows: Starting at the Northeast corner of the Southwest Quarter of the Southeast Quarter of said Section; thence South along the East line of the West Half of the Southeast Quarter of said Section, a distance of 300 feet; thence Westerly at right angles to said East line for a distance of 284 feet; thence Northerly along a line parallel to said East line a distance of 600 feet; thence Easterly along a line at right angles to said East line 209 feet to the center line of McGinty Road; thence Southeasterly along the center line of McGinty Road to the East line of the West Half of the Southeast Quarter of said Section 23; thence Southerly along said East line 33.5 feet to the point of beginning.

Par 2:

Lot 20, Block 2, Ginkels Oakridge Addition

**SURVEYORS NOTES:**

1. Utility lines shown hereon are based on field markings and maps provided to us as a result of Gopher State One Call private utility locate (Ticket Numbers 170260089, 170260090). the surveyor cannot guarantee that all utilities were marked or that the markings/maps are accurate.
2. Horizontal Datum: Hennepin County Coordinate System NAD83(11)
3. Vertical Datum: NAVD88
4. Date of fieldwork: 2/3/2017
5. Gross area = 2.38 acres.
6. Portions of the subject property were covered by snow and ice at the time of survey, the surveyor does not guarantee that all improvements are shown hereon.
7. This survey was prepared based on a cursory title review, the surveyor does not guarantee that all or any adverse interests, easements or other encumbrances are shown or that the owner listed has fee title to the property.

**SURVEYORS CERTIFICATION:**

I hereby certify that this survey was completed by me or under my direct supervision and that I am a duly licensed land surveyor under the laws of the State of Minnesota.

Chris Ambourn Date  
LS 43055

 <b>WENCK ASSOCIATES</b> Responsive partner. Exceptional outcomes. 1802 WOODDALE DRIVE WOODBURY, MN 55125 Ph: 651-395-5212	CLIENT NAME	PROJECT TITLE	
	RON CLARK CONSTRUCTION & DESIGN		CERTIFICATE SURVEY
#	XXX XXX XX/XX/XX	DWN BY	MLH
REV	REVISION DESCRIPTION	DWN	APP
PROJECT NO. 1531-0009		SHEET NO. 2 OF 11	
DWG DATE 6-FEB-2017		SCALE 1" = 30'	



**WARNING:**

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CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

**LEGEND**

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- ⊙ SANITARY SEWER MANHOLE
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- ▨ CONCRETE SURFACE
- ▨ ASPHALT SURFACE
- - - EASEMENT LINE
- · - · - STRUCTURE SETBACK LINE

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Lot 19 Block 2, Ginkels Oakridge Addition  
 AND

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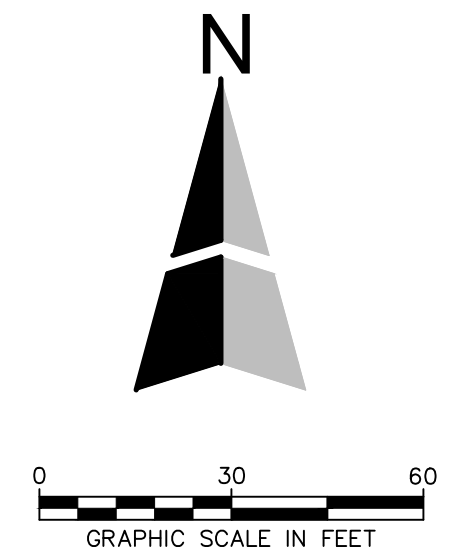
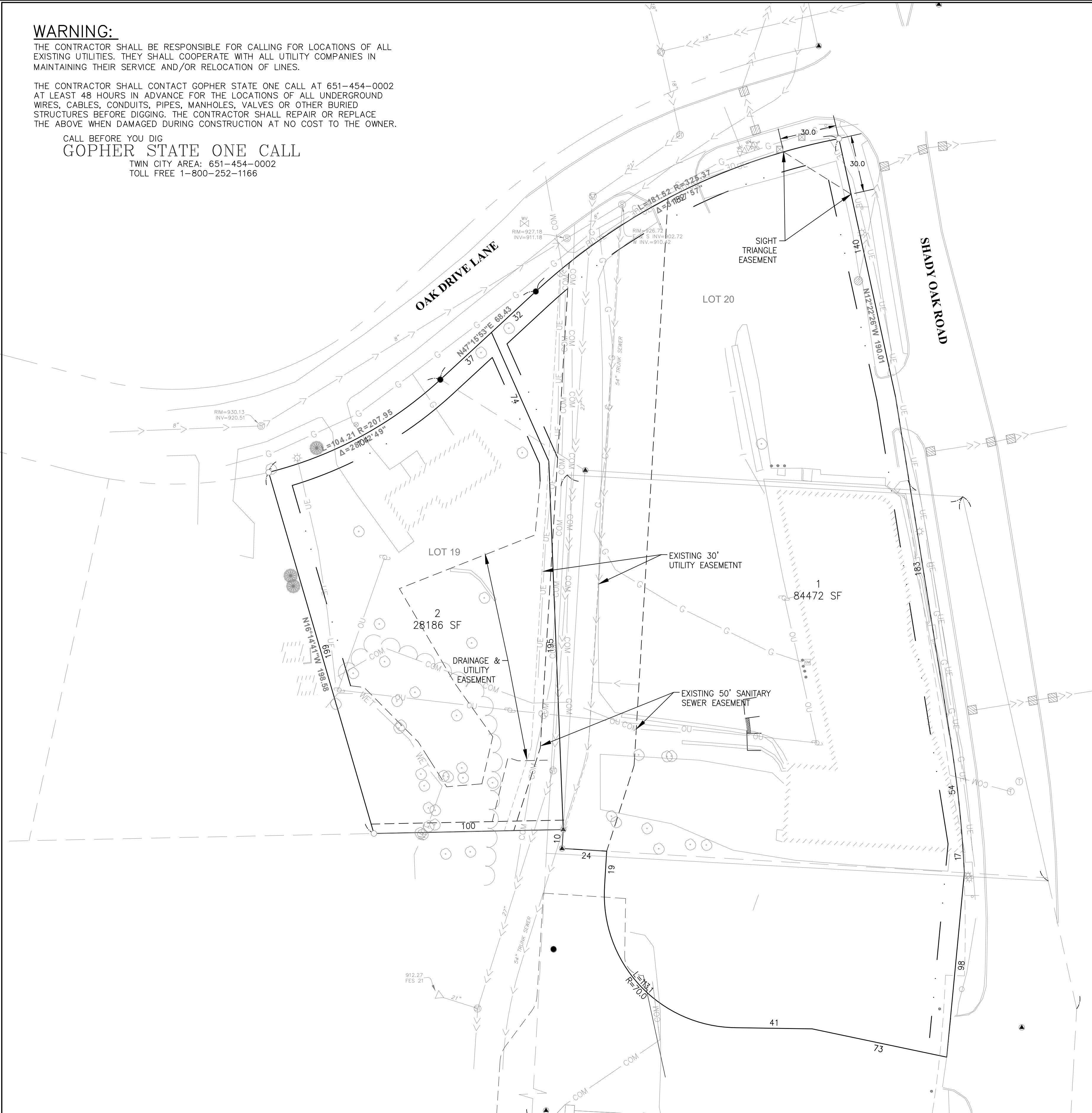
Par 2:  
 Lot 20, Block 2, Ginkels Oakridge Addition

EXISTING ZONING = B-2, R-1  
 PROPOSED ZONING = PUD

LOT 1 = 1.96 AC  
 LOT 2 = 0.66 AC  
 TOTAL AREA = 2.76 AC  
 SHADY OAK RIGHT-OF-WAY AREA = 0.16 AC  
 WETLAND AREA = 0.63 AC

Plot Date & Time:

C:\Users\kik-ca\OneDrive\2017\17-011 Shady Oak Crossing\CAD\CAD\_3 preliminary.plt.dwg



NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**

● Civil Engineering ● Land Planning  
 1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.

Martin P. Campion -Lic. # 19901 Date:

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY PLAT**

SHEET NO. 3 OF 11 SHEETS

PROJECT NO:  
**17-011**

DATE:  
**12/02/2019**

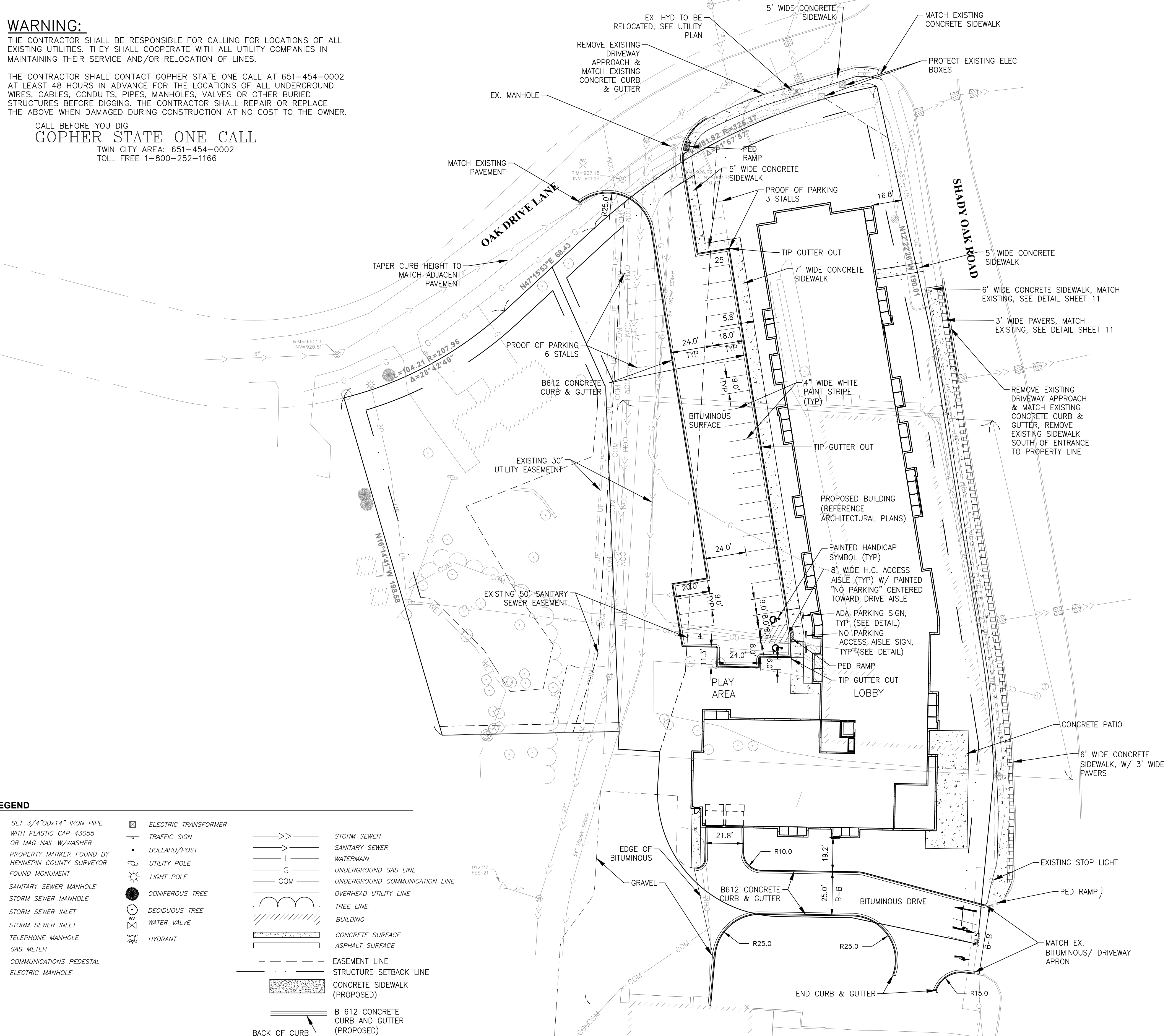


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CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166



**NOTES:**

- BOUNDARY AND EXISTING CONDITION INFORMATION PER SURVEY PREPARED BY WENCK AND CITY RECORD PLANS.
- ALL EXISTING CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR.
- REFER TO ARCHITECTURAL PLANS FOR BUILDING DIMENSIONS AND LOCATIONS OF EXITS, RAMPS, CONCRETE APRONS AND STOOPS.
- ALL PARKING LOT STRIPING SHALL BE 4" WIDE LINES, STRIPED USING HIGH VISIBILITY TRAFFIC AND HIGHWAY APPROVED WHITE PAINT. PROVIDE APPROPRIATE STRIPING AND PAVEMENT MARKINGS FOR ALL HANDICAP PARKING AND ACCESS AISLES.
- GUTTER SHALL BE CONSTRUCTED TO TIP OUT ALONG THE FRONT OF ALL BUILDINGS.
- DIMENSIONS TAKE PRECEDENCE OVER SCALE. ALL DIMENSIONS ARE TO BACK OF CURB, EDGE OF SIDEWALK OR EXTERIOR BUILDING UNLESS OTHERWISE NOTED.
- UNLESS OTHERWISE INDICATED ON THE PLAN, CONTRACTOR SHALL PROVIDE CONTROL JOINTS, CONSTRUCTION JOINTS, AND EXPANSION JOINTS IN SLAB ON GRADE, SIDEWALKS AND CONCRETE DRIVES. CONTROL JOINT MAXIMUM SPACING: WALKS - 8' O.C., ALL OTHERS - 10' O.C. SAW CUT CONTROL JOINTS MINIMUM ONE-QUARTER CONCRETE THICKNESS. EXPANSION JOINT MAXIMUM SPACING: WALKS - 24' O.C., ALL OTHERS - 40' O.C. DOWEL ALL EXPANSION JOINTS - MAXIMUM 24" O.C.
- WHEN DOING ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, THE CONTRACTOR SHALL COORDINATE THE REMOVAL LIMITS WITH THE CITY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL PEDESTRIAN RAMPS MEET ADA AND MNDOT GUIDELINES FOR ACCESSIBILITY.
- CONTRACTOR SHALL VERIFY CONDUIT REQUIREMENTS FOR POWER AND IRRIGATION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE FIRE MARSHAL THE LOCATION OF FIRE LANE SIGNAGE AND CURB MARKINGS (IF NEEDED).
- CONTRACTOR SHALL BE RESPONSIBLE FOR DEMOLITION AND REMOVAL OF ALL EXISTING STRUCTURES THAT INTERFERE WITH NEW WORK AS SHOWN.
- ALL LANDSCAPE AREAS ARE TO BE IRRIGATED.
- ALL NEW UTILITIES SHALL BE UNDERGROUND. COORDINATE LOCATIONS WITH UTILITY PROVIDERS.
- SIDEWALKS SHALL BE CONSTRUCTED WITH A MAXIMUM CROSS SLOPE OF 2%. ANY SIDEWALK EXCEEDING 2% CROSS SLOPE MUST BE REMOVED AND REPLACED AT CONTRACTOR'S EXPENSE.

**GENERAL NOTES:**

- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THAT ALL PERMITS AND APPROVALS HAVE BEEN OBTAINED. NO CONSTRUCTION SHALL BEGIN UNTIL THE CONTRACTOR HAS REVIEWED ALL PLANS AND OTHER DOCUMENTS APPROVED BY THE PERMITTING AUTHORITIES.
- WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AND THE REQUIREMENTS AND STANDARDS OF THE LOCAL GOVERNING AUTHORITY. THE SOILS REPORT AND RECOMMENDATION SET FORTH THEREIN ARE A PART OF THE REQUIRED CONSTRUCTION DOCUMENTS AND IN CASE OF CONFLICT SHALL TAKE PRECEDENCE UNLESS SPECIFICALLY NOTED OTHERWISE ON THE PLANS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCY BETWEEN THE SOILS REPORT AND PLANS.

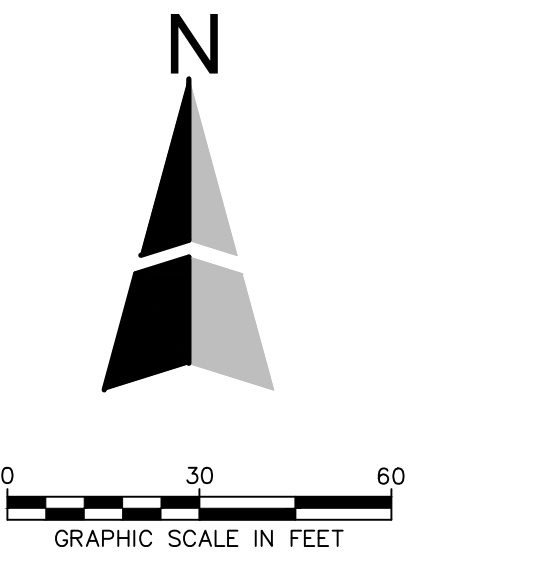
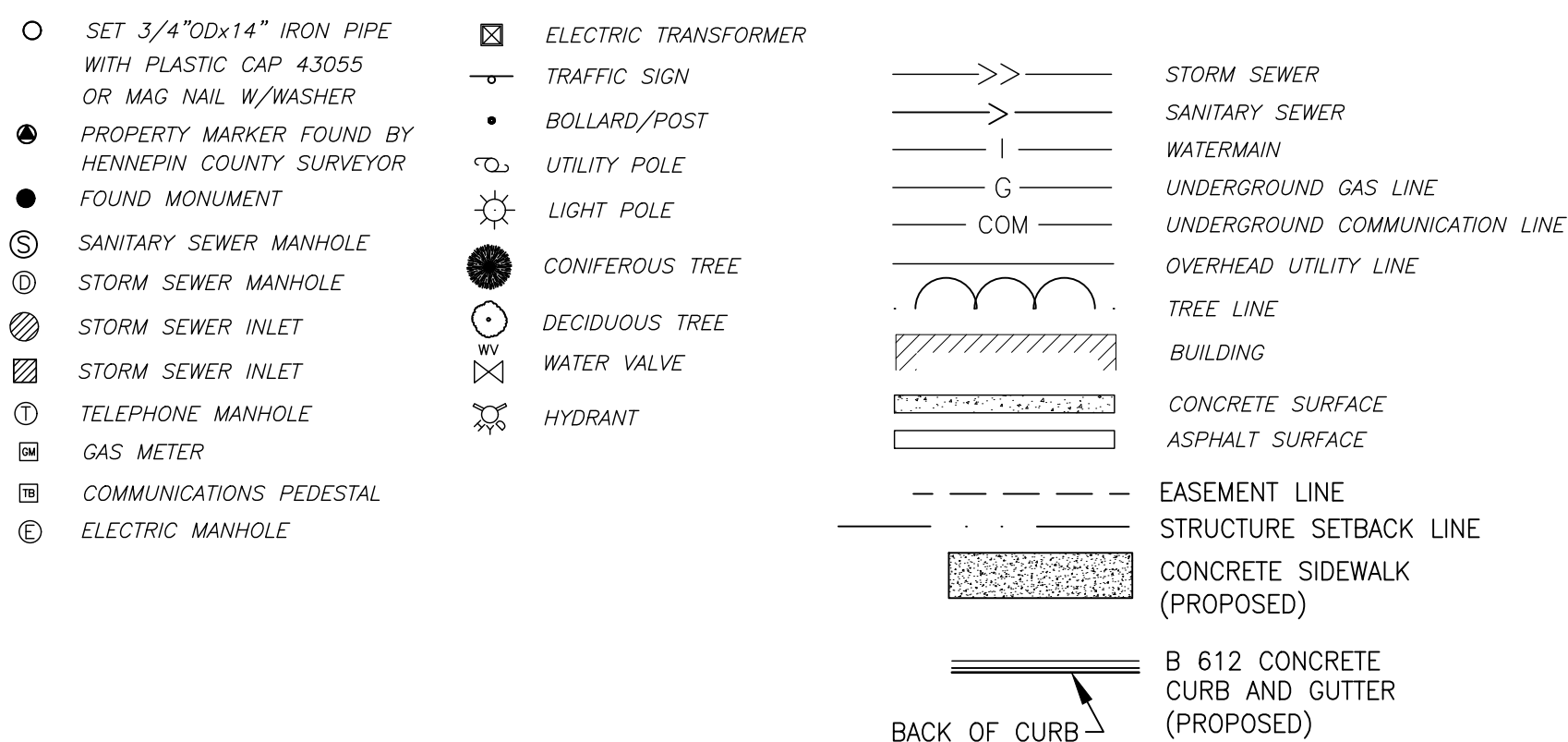
**PROPOSED PARKING STALLS:**  
 STANDARD & H.C. (2) ABOVE GROUND = 29  
 STANDARD & H.C.(2) UNDER GROUND = 78  
 TOTAL = 107

**STRUCTURE SETBACKS:**  
 10' FROM RIGHT OF WAY AND SOUTH PROPERTY LINE

**ADA REQUIREMENTS:**  
 CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING ALL PEDESTRIAN RAMPS MEET ADA AND MNDOT GUIDELINES FOR ACCESSIBILITY

**PARKING LOT STRIPING REQUIREMENTS:**  
 ALL PARKING LOT STRIPING SHALL BE 4" WIDE WHITE TRAFFIC AND HIGHWAY APPROVED MARKING PAINT AS APPROVED BY OWNER. H.C. PARKING AND ACCESS AISLES SHALL BE STRIPED AS INDICATED IN BLUE PAINT AND EACH H.C. STALL SHALL INCLUDE INTERNATIONAL SYMBOL OF ACCESSIBILITY CENTERED ON EACH STALL AS INDICATED ON PLANS.

**LEGEND**



NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**  
 Civil Engineering • Land Planning  
 1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.  
 Martin P. Campion -Lic. # 19901 Date: \_\_\_\_\_

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY SITE PLAN**  
 SHEET NO. 4 OF 11 SHEETS  
 PROJECT NO: 17-011  
 DATE: 12/02/2019



**WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

**NOTES:**

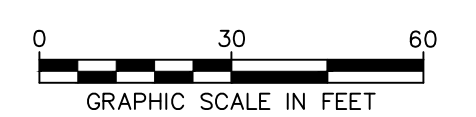
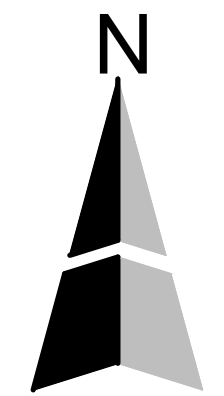
- EXISTING CONDITION INFORMATION PER SURVEY PREPARED BY WENCK ASSOCIATES AND CITY RECORD PLANS.
- THE LOCATION AND ELEVATION OF ALL EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY OF ANY CONFLICTS.
- THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND FACILITIES TO ALLOW PROPER FUNCTIONING DURING AND AFTER CONSTRUCTION. SUPPORTING STRUCTURES, IF REQUIRED, SHALL BE SUPPLIED BY THE CONTRACTOR AS WORK INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY CONFLICTS BETWEEN EXISTING UTILITIES AND THE PROPOSED CONSTRUCTION. THE ENGINEER WILL COORDINATE WITH UTILITY COMPANY IN QUESTION TO DETERMINE THE NEED FOR RELOCATION OF THE EXISTING UTILITY.
- EXISTING CONDITIONS SUCH AS SAND IN MANHOLES OR VALVE BOXES SHALL BE IDENTIFIED BY THE CONTRACTOR AND THESE SHALL BE REPORTED TO THE ENGINEER PRIOR TO EXCAVATION BY THE CONTRACTOR. ONCE CONSTRUCTION HAS BEGUN, ALL DAMAGE TO UNDERGROUND UTILITIES WILL BE ASSUMED TO HAVE BEEN CAUSED BY THE CONTRACTOR, AND REPAIRS NECESSARY SHALL BE PERFORMED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
- FINAL PLAT SHALL GOVERN FOR EASEMENTS AND LOT LINES.
- THE CONTRACTOR SHALL COORDINATE WITH THE LOCAL JURISDICTION TO OBTAIN PERMITS AND METER FOR WATER SOURCE. ALL ASSOCIATED COSTS SHALL BE INCIDENTAL TO THE CONTRACTOR, INCLUDING DISPOSAL OF TEST WATER INTO CITY'S SANITARY SEWER SYSTEM. THE CONTRACTOR SHALL NOT OPERATE GATE VALVES OR HYDRANTS ON THE CITY WATER SYSTEM WITHOUT FIRST RECEIVING CITY APPROVAL.
- THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER AND THE PROJECT ENGINEER 48 HOURS PRIOR TO STARTING WORK OR AS REQUIRE BY THE CITY.
- THE CONTRACTOR SHALL KEEP ACCESS ROADS CLEAR OF SOIL OR OTHER DEBRIS, AND PERFORM DAILY STREET CLEANING AS REQUIRED. POSITIVE DRAINAGE, CONTROLLED WITH EROSION CONTROL AND EROSION PREVENTION MEASURES AS REQUIRED SHALL BE PERFORMED. INLET PROTECTION SHALL BE INSTALLED WITHIN 48 HOURS AFTER INLET CONSTRUCTION. UNLESS SPECIFIED ON THE PLANS OR AS A BID ITEM ON THE BID FORM, ANY TEMPORARY CULVERTS, DITCHES, FILTER FABRIC, ETC. NECESSARY TO ACCOMPLISH THIS SHALL BE PERFORMED AS INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL PRESERVE AND PROTECT THE MARKERS AND MONUMENTS SET FOR THE SUBDIVISION OF LAND.
- THE CONTRACTOR SHALL SCHEDULE THE SOILS ENGINEER TO FACILITATE CERTIFICATION OF ALL CONTROLLED FILLS IN A TIMELY FASHION. DENSITY TESTS SHALL MEET THE FOLLOWING:
  - DENSITY TESTS SHALL BE TAKEN ON ALL TRENCHES AT LOCATIONS AS DETERMINED BY THE ENGINEER OF HIS REPRESENTATIVE.
  - WITHIN 3 FEET OF PAVEMENT SUBGRADE, CONTRACTOR SHALL UTILIZE IMPROVED SOILS THAT ARE WITHIN 1% OPTIMUM MOISTURE CONTENT. COMPACTION SHALL MEET 100% STANDARD PROCTOR. BELOW THE UPPER 3FEET, COMPACTION SHALL BE 95%. GRADING TOLERANCE SHALL BE 0.1".
- THE OWNER SHALL PAY FOR ALL COMPACTION TESTING. ANY AREAS WHICH FAIL TO MEET THE ABOVE STANDARDS SHALL RE-CORRECTED AND RE-TESTED BY THE OWNER'S TESTING AGENT AT THE CONTRACTOR'S EXPENSE.
- SANITARY SEWER AND WATERMAIN SHALL BE INSTALLED AT ELEVATIONS SHOWN ON THE PLAN.
- WHERE WATERMAIN MUST BE LOWERED TO AVOID CONFLICT WITH ANOTHER UTILITY, THE COST OF BENDS, FITTINGS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL PROVIDE TEMPORARY TRAFFIC CONTROL IN COMPLIANCE WITH THE CURRENT MNDOT "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS FIELD MANUAL" FOR CONSTRUCTION ADJACENT TO TRAVEL WAYS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF THE DEPTH OF EXISTING UTILITIES LISTED ON THIS PLAN PRIOR TO ORDERING OF ANY FITTINGS, STRUCTURES, CASTINGS, ETC. ENGINEER AND THE OWNER SHALL NOT BE RESPONSIBLE FOR ANY DISCREPANCIES FOUND AS DEPTHS ARE ESTIMATED.
- CONTACT CITY FOR INSPECTION PRIOR TO CONNECTING TO EXISTING CITY UTILITIES.
- CONTRACTOR TO COMPLY WITH ALL REGULATORY AGENCY PERMIT CONDITIONS FOR PERMITS OBTAINED BY OWNER AND FOR PERMITS OBTAINED BY GENERAL CONTRACTOR.
- ALL WORK PERFORMED AND MATERIALS USED FOR CONSTRUCTION OF UTILITIES MUST CONFORM TO THE CITY OF MINNETONKA STANDARD SPECIFICATIONS AND DETAILS.
- SANITARY SEWER SERVICE SHALL BE EITHER 6" PVC SDR 35.
- WATER SERVICE SHALL BE 6" DIP CLASS 52 WITH 4 MIL POLY WRAP.
- WATER SERVICE SHALL BE INSTALLED WITH 7.5' COVER MINIMUM.
- VERIFY LOCATION OF FIRE DEPARTMENT CONNECTION (FDC) WITH MECHANICAL ENGINEER.
- INSTALL SERVICES TO WITHIN 5' OF THE BUILDING. VERIFY LOCATION WITH MECHANICAL ENGINEER.
- ALL 12" - 15" RCP STORM SEWER SHALL BE CLASS V.
- ALL STORM SEWER SHALL HAVE CLASS C BEDDING.
- HDPE STORM SEWER SHALL BE DUAL WALL CORRUGATED WITH SMOOTH INTERIOR.
- ALL STRUCTURE ADJUSTMENT SHALL BE CONSIDERED INCIDENTAL TO PAVING.

**BENCHMARK:**

- TOP NUT HYDRANT NE CORNER OF SITE.  
ELEVATION 928.53

**GOVERNING SPECIFICATIONS:**

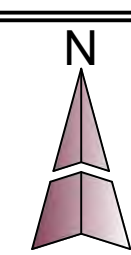
- MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" LATEST EDITION AND SUPPLEMENTS.
- CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD UTILITIES SPECIFICATION. LATEST EDITION.
- CITY OF MINNETONKA STANDARD SPECIFICATIONS AND DETAILS
- ALL APPLICABLE FEDERAL, STATE AND LOCAL LAWS AND ORDINANCE WILL BE COMPLIED WITH IN THE CONSTRUCTION OF THIS PROJECT.



**LEGEND**

○ SET 3/4"ODx14" IRON PIPE WITH PLASTIC CAP 43055 OR MAG NAIL W/WASHER	⊠ ELECTRIC TRANSFORMER	→→→ STORM SEWER
● PROPERTY MARKER FOUND BY HENNEPIN COUNTY SURVEYOR	⊠ TRAFFIC SIGN	→ SANITARY SEWER
● FOUND MONUMENT	• BOLLARD/POST	→ WATERMAIN
⊙ SANITARY SEWER MANHOLE	⊙ UTILITY POLE	→ UNDERGROUND GAS LINE
⊙ STORM SEWER MANHOLE	⊙ LIGHT POLE	→ UNDERGROUND COMMUNICATION LINE
⊙ STORM SEWER INLET	● CONIFEROUS TREE	→ OVERHEAD UTILITY LINE
⊙ TELEPHONE MANHOLE	● DECIDUOUS TREE	→ TREE LINE
⊙ GAS METER	● WATER VALVE	▭ BUILDING
⊙ COMMUNICATIONS PEDESTAL	● HYDRANT	▭ CONCRETE SURFACE
⊙ ELECTRIC MANHOLE		▭ ASPHALT SURFACE

NO.	DATE	DESCRIPTION



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I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.  
 Martin P. Campion -Lic. # 19901 Date:

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

<b>PRELIMINARY UTILITY PLAN</b>		PROJECT NO: <b>17-011</b>
SHEET NO. 5 OF 11 SHEETS		DATE: <b>12/02/2019</b>

Plot Date & Time: C:\Users\kik-ca\OneDrive\2017\12-011 Shady Oak Crossing\CAD\CIVIL UTILITY PLAN.dwg



**WARNING:**

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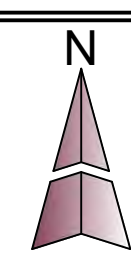
**LEGEND**

- SET 3/4"Øx14" IRON PIPE WITH PLASTIC CAP 43055 OR MAG NAIL W/WASHER
- PROPERTY MARKER FOUND BY HENNEPIN COUNTY SURVEYOR
- FOUND MONUMENT
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- ⊙ STORM SEWER MANHOLE
- ⊙ STORM SEWER INLET
- ⊙ STORM SEWER INLET
- ⊙ TELEPHONE MANHOLE
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- ⊙ UTILITY POLE
- ⊙ LIGHT POLE
- CONIFEROUS TREE
- DECIDUOUS TREE
- ⊙ WATER VALVE
- ⊙ HYDRANT
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED CONTOURS
- X 896.55 PROPOSED SPOT ELEVATION
- STORM SEWER
- SANITARY SEWER
- WATERMAIN
- UNDERGROUND GAS LINE
- UNDERGROUND COMMUNICATION LINE
- OVERHEAD UTILITY LINE
- TREE LINE
- BUILDING
- CONCRETE SURFACE
- ASPHALT SURFACE

Plot Date & Time:

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NO.	DATE	DESCRIPTION



**CAMPION ENGINEERING SERVICES, INC.**

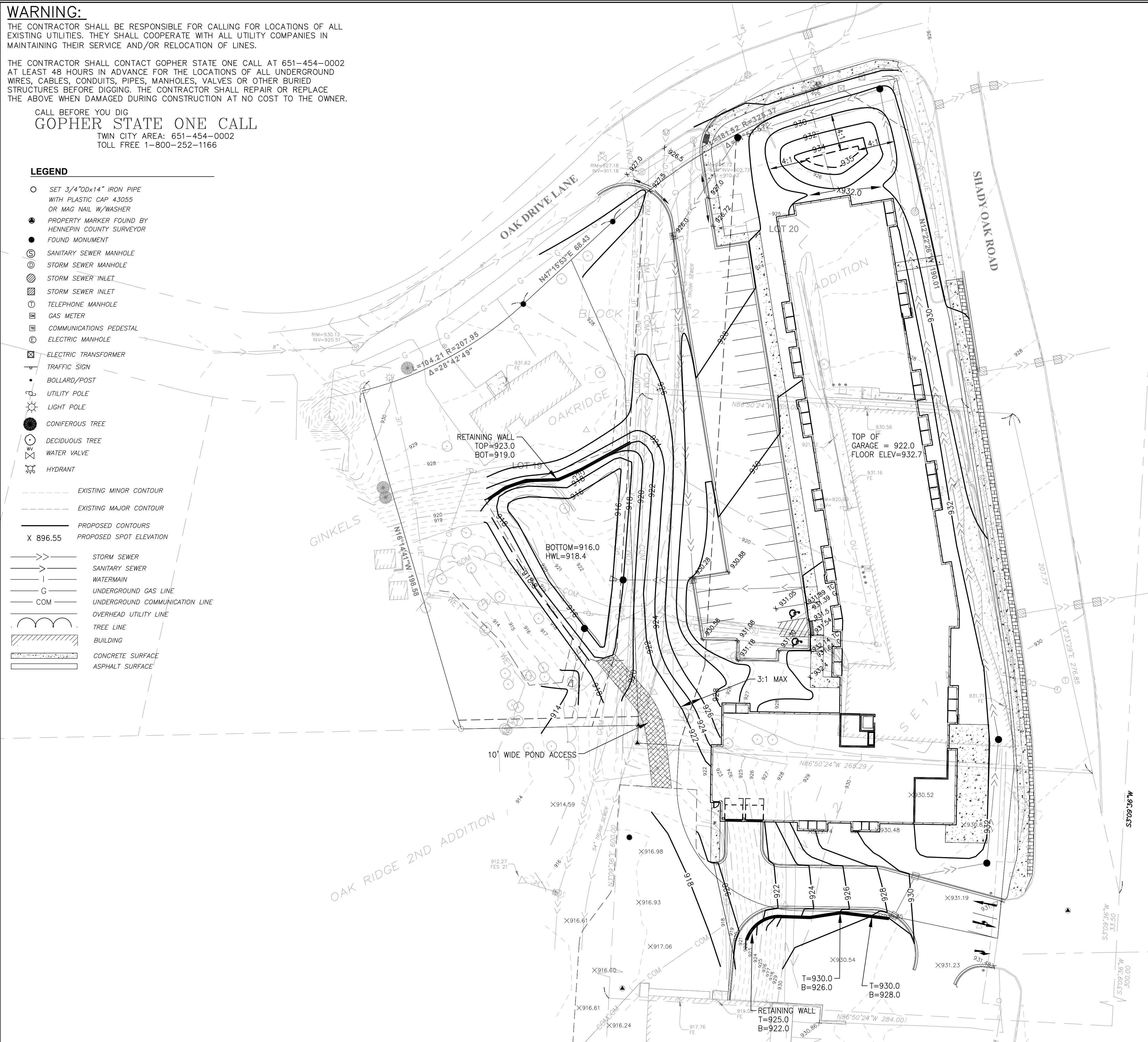
• Civil Engineering • Land Planning  
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 Martin P. Campion -Lic. # 19901 Date:

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY GRADING PLAN**  
 SHEET NO. 6 OF 11 SHEETS

PROJECT NO:  
**17-011**  
 DATE:  
**12/02/2019**



**GRADING NOTES**

1. CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES.
2. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
3. THE CONTRACTOR MUST CONTACT ALL APPROPRIATE UTILITY COMPANIES AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF EXISTING UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE LOCATIONS OF SMALL UTILITIES SHALL BE OBTAINED BY THE CONTRACTOR BY CALLING GOPHER STATE ONE CALL (1-800-252-1166).
4. SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
6. THE CONTRACTOR SHALL RESTRICT ALL GRADING AND CONSTRUCTION ACTIVITIES TO AREAS DESIGNATED ON THE PLANS. ACTIVITIES PROHIBITED OUTSIDE THE CONSTRUCTION BOUNDARIES INCLUDE, BUT ARE NOT LIMITED TO: STOCKPILING SOILS AND OTHER MATERIAL, STORING EQUIPMENT OR OTHER MACHINERY, DRIVING VEHICLES, LEAKING OR SPILLING OF ANY "WASHOUT" OR OTHER TOXIC MATERIALS.
7. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTES AND INSPECTIONS WITH THE SOILS ENGINEER.
8. PRIOR TO PLACEMENT OF ANY STRUCTURE OR PAVEMENT, A TEST ROLL WILL BE REQUIRED ON THE PARKING LOT SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER AND AS SPECIFIED.
9. THE SITE HAS NOT NECESSARILY BEEN DESIGNED TO BALANCE THE ON-SITE MATERIALS. AFTER THE SITE GRADING IS COMPLETE, IF EXCESS SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS SOIL MATERIAL OFF-SITE IN A MANNER ACCEPTABLE TO THE OWNER AND THE REGULATING AGENCIES.
10. THE EXISTING TOPSOIL ON THIS SITE VARIES IN DEPTH. IT IS THE CONTRACTOR'S RESPONSIBILITY THAT ALL SURFACE VEGETATION AND ANY TOPSOIL OR OTHER LOOSE, SOFT OR OTHERWISE UNSUITABLE MATERIAL BE REMOVED FROM THE STREET AND BUILDING PAD AREAS PRIOR TO PLACEMENT OF ANY EMBANKMENT IN ACCORDANCE WITH THE SOILS REPORT.
11. EMBANKMENT MATERIAL NOT PLACED IN THE PARKING LOT OR BUILDING PAD AREAS SHALL BE COMPACTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE QUALITY COMPACTION METHOD AS OUTLINED IN MN/DOT 2105.3F2 OR AS DIRECTED BY THE SOILS ENGINEER.
12. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE SOILS ENGINEER. EMBANKMENT MATERIAL PLACED IN THE PARKING LOT SHALL BE COMPACTED IN ACCORDANCE WITH THE SPECIFIED DENSITY METHOD AS OUTLINED IN MN/DOT 2105.3F1. EMBANKMENT MATERIAL PLACED IN THE BUILDING PAD AREA SHALL BE COMPACTED IN ACCORDANCE WITH THE SOILS REPORT.
13. TOLERANCES:
  - a. THE PARKING LOT SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
  - b. AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED BY THE ENGINEER.
  - c. TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/8 INCH OF THE SPECIFIED THICKNESS.
14. ALL DISTURBED UNSURFACED AREAS ARE TO IMMEDIATELY RECEIVE FOUR INCHES OF TOPSOIL, SEED AND MULCH AND BE WATERED UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.
15. SPOT ELEVATIONS SHOWN INDICATE FINISHED GRADE ELEVATION UNLESS OTHERWISE NOTED.
16. PROPOSED CONTOURS ARE TO FINISHED SURFACE GRADE OR GUTTER LINE IF CURB. PLAN GRADE IS TO FINISHED GARAGE FLOOR ELEVATION.
17. CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE ALONG MATCHING PAVEMENT AREAS AND/OR CURBS.
18. THE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM THE BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
19. THE CONTRACTOR SHALL PROVIDE DEWATERING AS REQUIRED TO COMPLETE THE SITE GRADING CONSTRUCTION OR AS DIRECTED BY THE SOILS ENGINEER.

**CAUTION NOTES:**

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE.

THE CONTRACTOR MUST CONTACT ALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE LOCATIONS OF SMALL UTILITIES SHALL BE OBTAINED BY THE CONTRACTOR BY CALLING GOPHER STATE ONE CALL AT 800-252-1166 OR 651-454-0002.

**POND GRADING**

1. THE CONTRACTOR SHALL AVOID COMPACTING THE POND BOTTOM. RUBBER Tired EQUIPMENT SHALL BE PROHIBITED WHEN WORKING IN THE POND AREA.
2. THE BOTTOM AND SIDE SLOPES SHALL BE STABILIZED WITH THE SPECIFIED PLANT MATERIALS WITHIN 7 DAYS OF COMPLETING THE POND GRADING.
3. POND SHALL BE GRADED 12" - 18" ABOVE FINISHED GRADE UNTIL SITE GRADING IS SUBSTANTIALLY COMPLETE.
4. AS PART OF FINISH GRADING, NATIVE SOILS IN THE POND SHALL BE DE-COMPACTED TO A DEPTH OF 12" - 18".

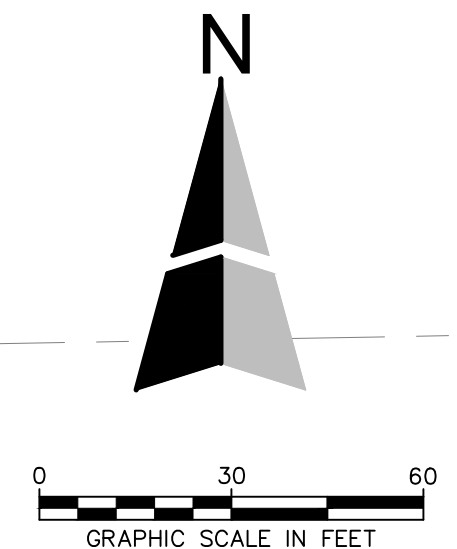
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**NOTES:**

1. ALL RETAINING WALLS 4' AND HIGHER SHALL BE DESIGNED BY A STRUCTURAL ENGINEER





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CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

**PROJECT DIRECTORY**

**OWNER:**  
 RON CLARK CONSTRUCTION & DESIGN  
 MIKE ROEBUCK  
 7500 WEST 78TH STREET  
 EDINA, MN 55439  
 PH. 952.947.3022  
 EMAIL: MIKE@RONCLARK.COM



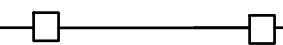
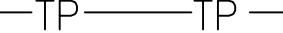
**SWPPP DESIGNER:**  
 CAMPION ENGINEERING SERVICES, INC.  
 MARTY CAMPION  
 1800 PIONEER CREEK CENTER  
 MAPLE PLAIN, MN 55364  
 PH. 763.479.5172  
 EMAIL: MCAMPION@CAMPIONENG.COM

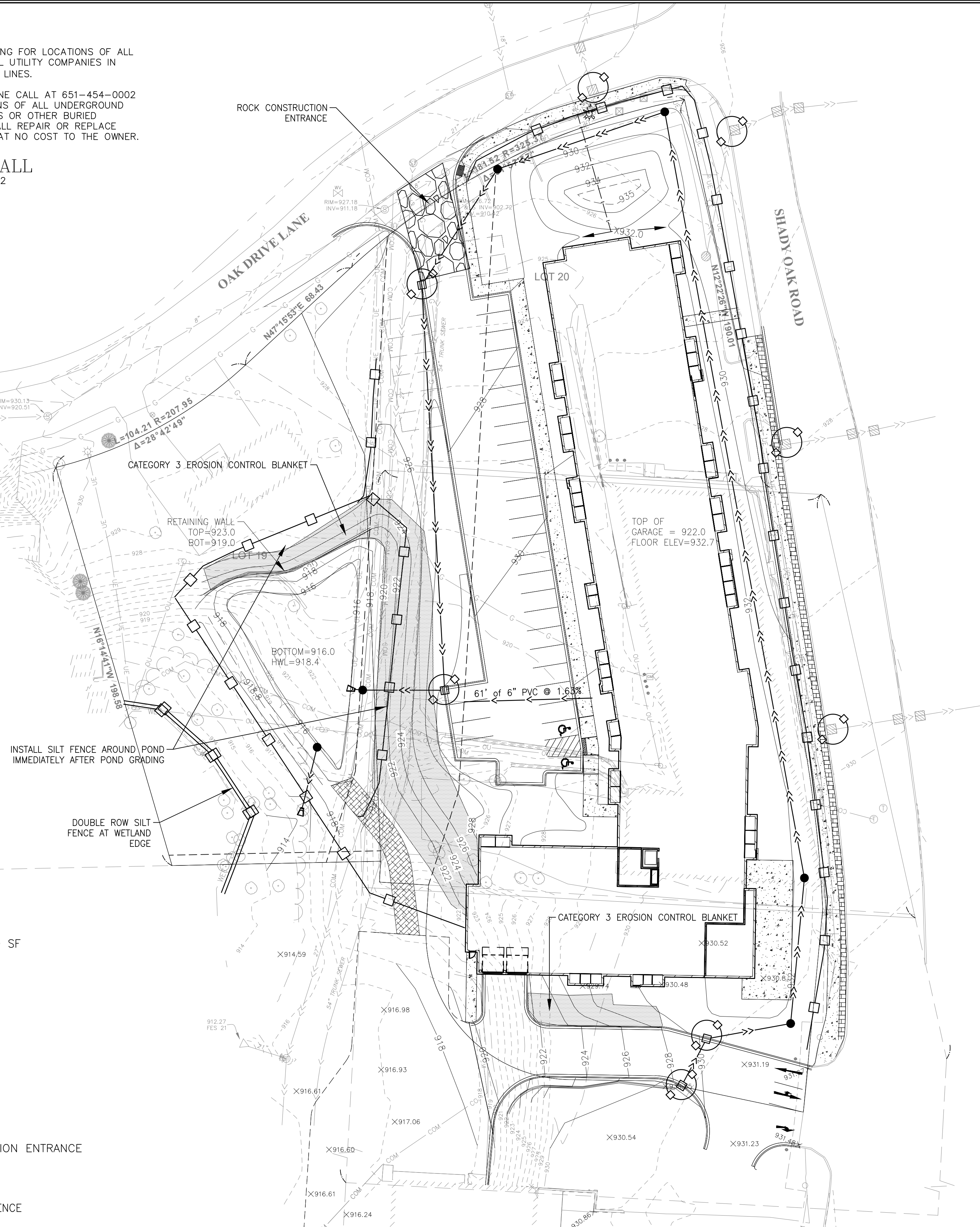
**EROSION CONTROL INSTALLER**  
 NAME: \_\_\_\_\_  
 CONTACT: \_\_\_\_\_  
 ADDRESS: \_\_\_\_\_  
 PHONE: \_\_\_\_\_

**CONTRACTOR**  
 NAME: \_\_\_\_\_  
 CONTACT: \_\_\_\_\_  
 ADDRESS: \_\_\_\_\_  
 PHONE: \_\_\_\_\_

**EROSION CONTROL QUANTITIES:**

SITE RESTORATION = 1.0 AC  
 SILT FENCE = 1325 LF  
 INLET PROTECTION = 8 EA  
 ROCK CONSTRUCTION ENTRANCE = 1 EA  
 CATEGORY 3 EROSION CONTROL BLANKET = 7,770 SF

-  DENOTES INLET PROTECTION
-  DENOTES ROCK CONSTRUCTION ENTRANCE
-  DENOTES SILT FENCE
-  DENOTES TREE PROTECTION FENCE



**EROSION/SILTATION CONTROL**

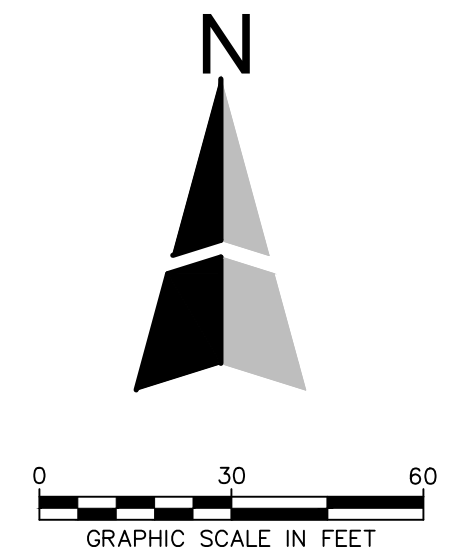
1. ALL EROSION CONTROL AND SILTATION CONTROL WILL COMPLY WITH MINNESOTA'S BEST MANAGEMENT PRACTICES MANUAL AND REGULATIONS OF THE CITY.
2. THE CONTRACTOR SHALL BE FAMILIAR WITH AND FOLLOW ALL REQUIREMENTS OF THE MPCA NPDES PHASE II PERMIT FOR CONSTRUCTION ACTIVITIES INCLUDING BUT NOT LIMITED TO: WEEKLY EROSION CONTROL INSPECTIONS, INSPECTION AFTER 0.5" RAINFALL OR MORE AND DOCUMENTATION OF ALL CORRECTIVE MEASURES. BY BEGINNING CONSTRUCTION, THE CONTRACTOR ACKNOWLEDGES THE TERMS OF THIS PERMIT AND AGREES TO ABIDE BY THEM.
3. THE CONTRACTOR SHALL PERFORM ANY CORRECTIVE MEASURES ORDERED BY THE CITY OR THE MPCA WITHIN 24 HOURS OF NOTIFICATION. ALSO, ADDITIONAL EROSION CONTROL MEASURES DEEMED NECESSARY BY EITHER THE CITY OR THE MPCA SHALL BE INSTALLED WITHIN 24 HOURS OF NOTIFICATION.
4. ANY DEPOSITING OF SILT OR MUD ON NEW OR EXISTING PAVEMENT, IN TEMPORARY SEDIMENTATION BASINS, OR IN EXISTING STORM SEWERS OR SWALES SHALL BE REMOVED AFTER EACH RAIN AND AFFECTED AREAS CLEANED.
5. THE CONTRACTOR SHALL ASSUME COMPLETE RESPONSIBILITY FOR CONTROLLING ALL SILTATION INCLUDING BUT NOT LIMITED TO ROCK ENTRANCES AND/OR SILT FENCES. CONTROL SHALL COMMENCE WITH GRADING AND CONTINUE THROUGHOUT THE PROJECT UNTIL ACCEPTANCE OF THE WORK BY THE OWNER. THE CONTRACTOR'S RESPONSIBILITY INCLUDES ALL DESIGN AND IMPLEMENTATION AS REQUIRED TO PREVENT EROSION AND THE DEPOSITING OF SILT. THE OWNER MAY, AT HIS/HER OPTION DIRECT THE CONTRACTOR IN HIS/HER METHODS AS DEEMED FIT TO PROTECT PROPERTY AND IMPROVEMENTS.
6. ANY DEPOSITING OF SILT IN SWALES SHALL BE REMOVED AFTER EACH RAIN AND AFFECTED AREAS CLEANED TO THE SATISFACTION OF THE OWNER, ALL AT THE EXPENSE OF THE CONTRACTOR. THE SILT FENCES SHALL BE REMOVED AND THE SILT REMOVED FROM THE PONDING AREAS BY THE CONTRACTOR AFTER THE TURF IS ESTABLISHED.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH AND MONITORING THE REQUIREMENT OF THE MPCA PERMIT.
8. ALL DISTURBED AREAS, EXCEPT ROADWAYS, BUILDING AREAS, PARKING AREAS, ISLANDS AND SIDEWALK, SHALL BE RESTORED WITH A MINIMUM 4 INCHES TOPSOIL, SEEDED AND MULCHED (TYPE I) WITHIN 72 HOURS OF COMPLETION OF SITE GRADING, OR WITHIN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY CEASED. SEEDING SHALL BE IN ACCORDANCE WITH MNDOT SPECIFICATION 2575, SEED MIX 150 @ 40 LBS/ACRE (OR APPROVED EQUAL). DORMANT SEEDING AREAS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH MNDOT SPECIFICATIONS, STRAW MULCHING QUANTITY SHALL BE TWO TONS PER ACRE, FERTILIZER (10-10-20) SHALL BE APPLIED AT A RATE OF 200 POUNDS PER ACRE (CAN BE OMITTED IN LANDSCAPED AREAS IF LANDSCAPED SEEDING IS DONE CONCURRENTLY).
9. CONSTRUCTION SHALL PROCEED IN THE FOLLOWING SEQUENCE:
  - a. CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY.
  - b. INSTALL EROSION CONTROL MEASURES AND ROCK CONSTRUCTION ENTRANCE.
  - c. CONTACT CITY FOR APPROVAL OF EROSION CONTROL INSTALLATION.
  - d. CONSTRUCT TEMPORARY SEDIMENTATION PONDS AND INSTALL TEMPORARY OUTFALL WITH STANDPIPE.
  - e. CONSTRUCT TEMPORARY DRAINAGE DITCHES/SWALES/STORM SEWER TO DIVERT SURFACE RUNOFF TO TEMPORARY SEDIMENTATION PONDS.
  - f. MAINTAIN EROSION MEASURE, I.E. SILT FENCE, ROCK CONSTRUCTION ENTRANCE.
  - g. MAINTAIN ALL TEMPORARY SEDIMENTATION PONDS. COMPLETE SITE GRADING TOLERANCING.
  - h. INSTALL SEED AND MULCH ON AREAS THAT ARE NOT TO BE HARD SURFACES.
10. SLOPE PROTECTION AND EROSION CONTROL SHALL BE DONE IN ACCORDANCE WITH SECTION 02370 SLOPE PROTECTION, EROSION CONTROL AND STORM WATER POLLUTION PREVENTION PLAN OF THE PROJECT MANUAL.
11. ALL STORM SEWER INLETS AND FLARED END SECTIONS SHALL BE ADEQUATELY PROTECTED BEFORE AND AFTER PAVEMENT CONSTRUCTION UNTIL ALL DISTURBED AREAS ARE STABILIZED. CONTRACTOR SHALL PLACE MIRAFI FABRIC AND GRAVEL OVER ALL CATCH BASIN GRATE INLETS UNTIL PAVING SURFACES ARE PAVED AND THE LANDSCAPING IS COMPLETED.
12. STOCKPILE AREAS WHICH REMAIN ON THE SITE FOR MORE THAN SEVEN DAYS SHALL BE SEEDED, MULCHED, AND SURROUNDED BY SILT FENCE.
13. TEMPORARY AND PERMANENT SEDIMENTATION PONDS, AT LOCATION SELECTED BY OWNER, SHALL BE CONSTRUCTED WITH THE INITIAL GRADING. THE SEDIMENT MUST BE REMOVED FROM THESE PONDS, AS NECESSARY, PRIOR TO COMPLETION OF THE PROJECT.
14. BIO LOGS SHALL BE INSTALLED AT PIPE INLETS AND OUTLETS UNTIL RIPRAP IS INSTALLED. PERMANENT ENERGY DISSIPATORS SHALL BE INSTALLED WITHIN 24 HOURS OF CONNECTION TO A SURFACE WATER.
15. INSTALL SILT FENCE AROUND POND UPON COMPLETION OF GRADING.
16. EROSION AND SEDIMENT CONTROL PRACTICES MUST REMAIN IN PLACE UNTIL THE OWNER HAS DETERMINED THAT THE SITE SOILS HAVE BEEN PERMANENTLY STABILIZED AND SHALL BE REMOVED WITHIN 30 DAYS THEREAFTER.
17. ALL STOCKPILES OF SOIL OR OTHER MATERIALS SUBJECT TO EROSION BY WIND OR WATER SHALL BE COVERED, VEGETATED, ENCLOSED, FENCED ON THE DOWN GRADIENT SIDE OR OTHERWISE EFFECTIVELY PROTECTED FROM EROSION IN ACCORDANCE WITH THE AMOUNT OF TIME THE MATERIAL WILL BE ON SITE AND THE MANNER OF ITS PROPOSED USE.

**WETLAND BUFFER RE-VEGETATION**

1. EROSION/SEDIMENT CONTROL PRACTICES SHALL BE USED DURING BUFFER VEGETATION ESTABLISHMENT.
2. DISTURBED BUFFER AREAS SHALL BE PLANTED WITH STATE SEED MIX 35-641 (BWSR U5) OR EQUIVALENT.

**EROSION MAT REQUIREMENTS**

1. TEMPORARY EROSION MATS SHALL BE BIODEGRADABLE DOULBE-NET STRAW (NORTH AMERICAN GREEN 5150 OR APPROVED EQUAL) AND EROSION MATS SHALL HAVE A MINIMUM FUNCTIONAL LONGEVITY OF 10 MONTHS.
2. PERMANENT EROSION CONTROL MAT SHALL BE ENKAMAT 7010/7016 OR APPROVED EQUAL AND SHALL BE COVERED WITH TEMPORARY EROSION CONTROL MAT AFTER TOPSOIL AND SEEDING.
3. EROSION MATS PLACED ON SLOPES 3:1 OR GREATER SHALL BE ROLLED DOWN SLOPE AND WITH 4" MINIMUM LAP AND STAPLED AT 1' O.C.
4. CONSTRUCT A 6"x12" ANCHOR TRENCH 3' BEYOND CREST AND TOE OF SLOPE. STAPLE EROSION MAT IN TRENCH AT 1' O.C.



NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**

• Civil Engineering • Land Planning  
 1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.

Martin P. Campion -Lic. # 19901 Date: \_\_\_\_\_

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY STORM WATER POLLUTION PREVENTION PLAN**

SHEET NO. 7 OF 11 SHEETS

PROJECT NO: 17-011  
 DATE: 12/02/2019



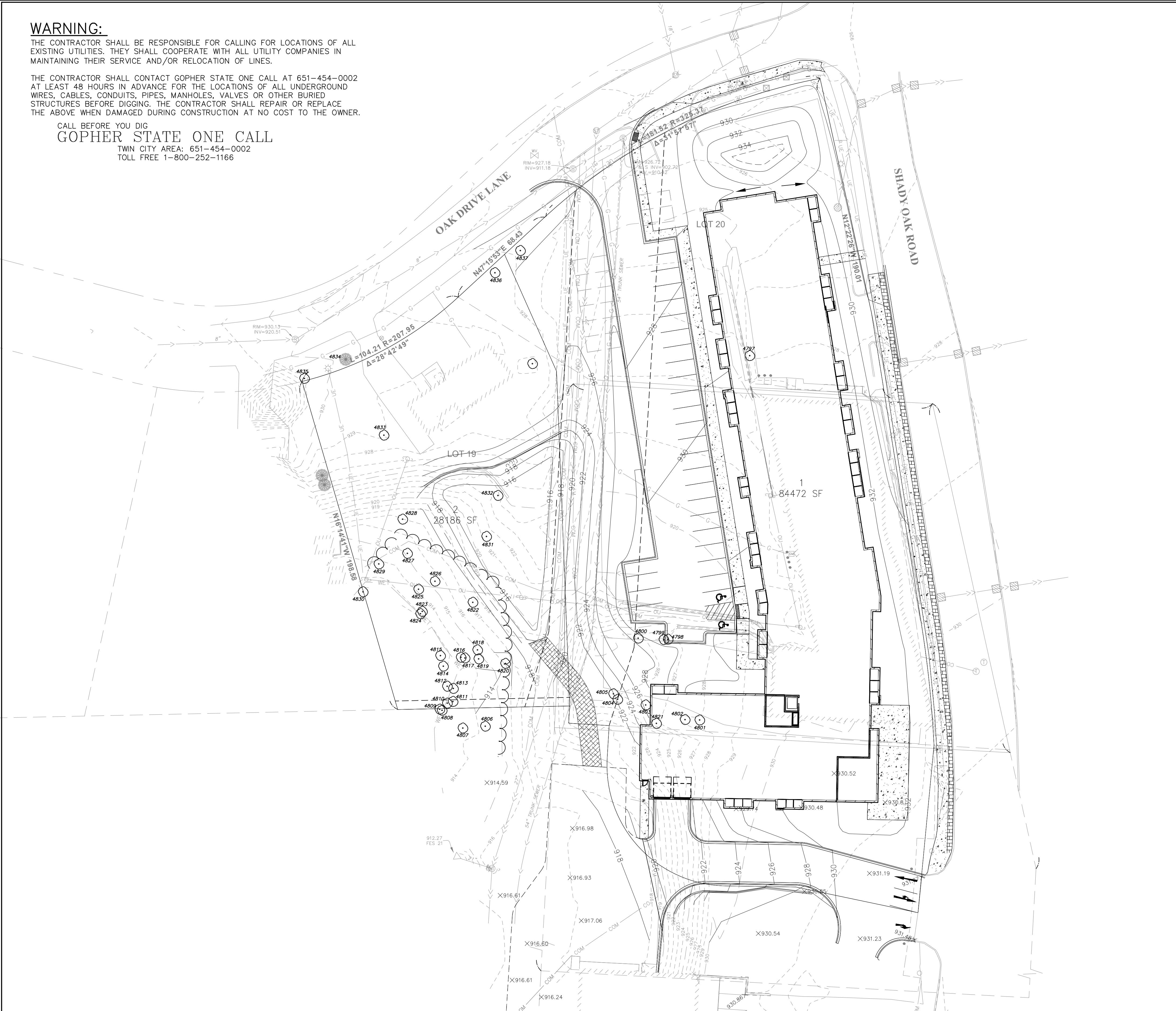
**WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

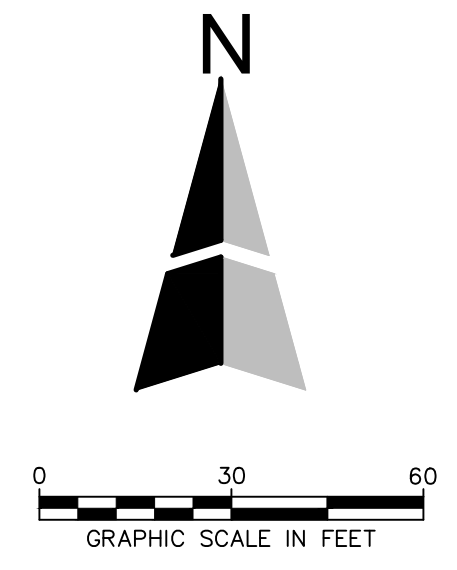
CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

Plot Date & Time:



#	Tag number	Size	Common Name	Scientific Name	Notes
1	4797	15	Siberian Elm	<i>Ulmus pumila</i>	
2	4798	11	White Mulberry	<i>Morus alba</i>	
3	4799	10	White Mulberry	<i>Morus alba</i>	
4	4800	8	White Mulberry	<i>Morus alba</i>	
5	4801	19,11	Siberian Elm	<i>Ulmus pumila</i>	
6	4802	22,12	Siberian Elm	<i>Ulmus pumila</i>	
7	4803	16	Siberian Elm	<i>Ulmus pumila</i>	
8	4804	11,11,10,9,8	Siberian Elm	<i>Ulmus pumila</i>	
9	4805	12,11,10	Siberian Elm	<i>Ulmus pumila</i>	
10	4806	24	Siberian Elm	<i>Ulmus pumila</i>	major internal decay along trunk
11	4807	10	Green Ash	<i>Fraxinus pennsylvanica</i>	
12	4808	12	Green Ash	<i>Fraxinus pennsylvanica</i>	
13	4809	15	Cottonwood	<i>Populus deltoides</i>	
14	4810	14	Cottonwood	<i>Populus deltoides</i>	
15	4811	25	Cottonwood	<i>Populus deltoides</i>	
16	4812	20	Silver Maple	<i>Acer saccharinum</i>	
17	4813	18	Cottonwood	<i>Populus deltoides</i>	
18	4814	11,11	Green Ash	<i>Fraxinus pennsylvanica</i>	
19	4815	11	Green Ash	<i>Fraxinus pennsylvanica</i>	
20	4816	16	Green Ash	<i>Fraxinus pennsylvanica</i>	
21	4817	8	Box Elder	<i>Acer negundo</i>	
22	4818	17	Box Elder	<i>Acer negundo</i>	
23	4819	9	Box Elder	<i>Acer negundo</i>	
24	4820	8	American Elm	<i>Ulmus americana</i>	
25	4821	12	Siberian Elm	<i>Ulmus pumila</i>	
26	4822	22,22	Northern Pin Oak	<i>Quercus ellipsoidalis</i>	
27	4823	16	Green Ash	<i>Fraxinus pennsylvanica</i>	
28	4824	14	Siberian Elm	<i>Ulmus pumila</i>	
29	4825	40	Cottonwood	<i>Populus deltoides</i>	
30	4826	8	Green Ash	<i>Fraxinus pennsylvanica</i>	
31	4827	17,16,12	White Mulberry	<i>Morus alba</i>	internal decay in 16" stem along trunk
32	4828	25	Siberian Elm	<i>Ulmus pumila</i>	
33	4829	8	Green Ash	<i>Fraxinus pennsylvanica</i>	
34	4830	9,8	Green Ash	<i>Fraxinus pennsylvanica</i>	
35	4831	18,11	Silver Maple	<i>Acer saccharinum</i>	
36	4832	13	White Mulberry	<i>Morus alba</i>	
37	4833	25	Siberian Elm	<i>Ulmus pumila</i>	
38	4834	14	Colorado Blue Spruce	<i>Picea pungens</i>	25' in height
39	4835	42	Silver Maple	<i>Acer saccharinum</i>	
40	4836	10	Russian Olive	<i>Elaeagnus angustifolia</i>	
41	4837	11,10	Russian Olive	<i>Elaeagnus angustifolia</i>	

TREE INVENTORY COMPLETED BY KEN ARNDT, SR. FOREST ECOLOGIST, MIDWEST NATURAL RESOURCES



NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**

- Civil Engineering • Land Planning

1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.

Martin P. Campion -Lic. # 19901 Date:

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY TREE INVENTORY PLAN**

SHEET NO. 8 OF 11 SHEETS

PROJECT NO:  
**17-011**

DATE:  
**12/02/2019**



**WARNING:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.

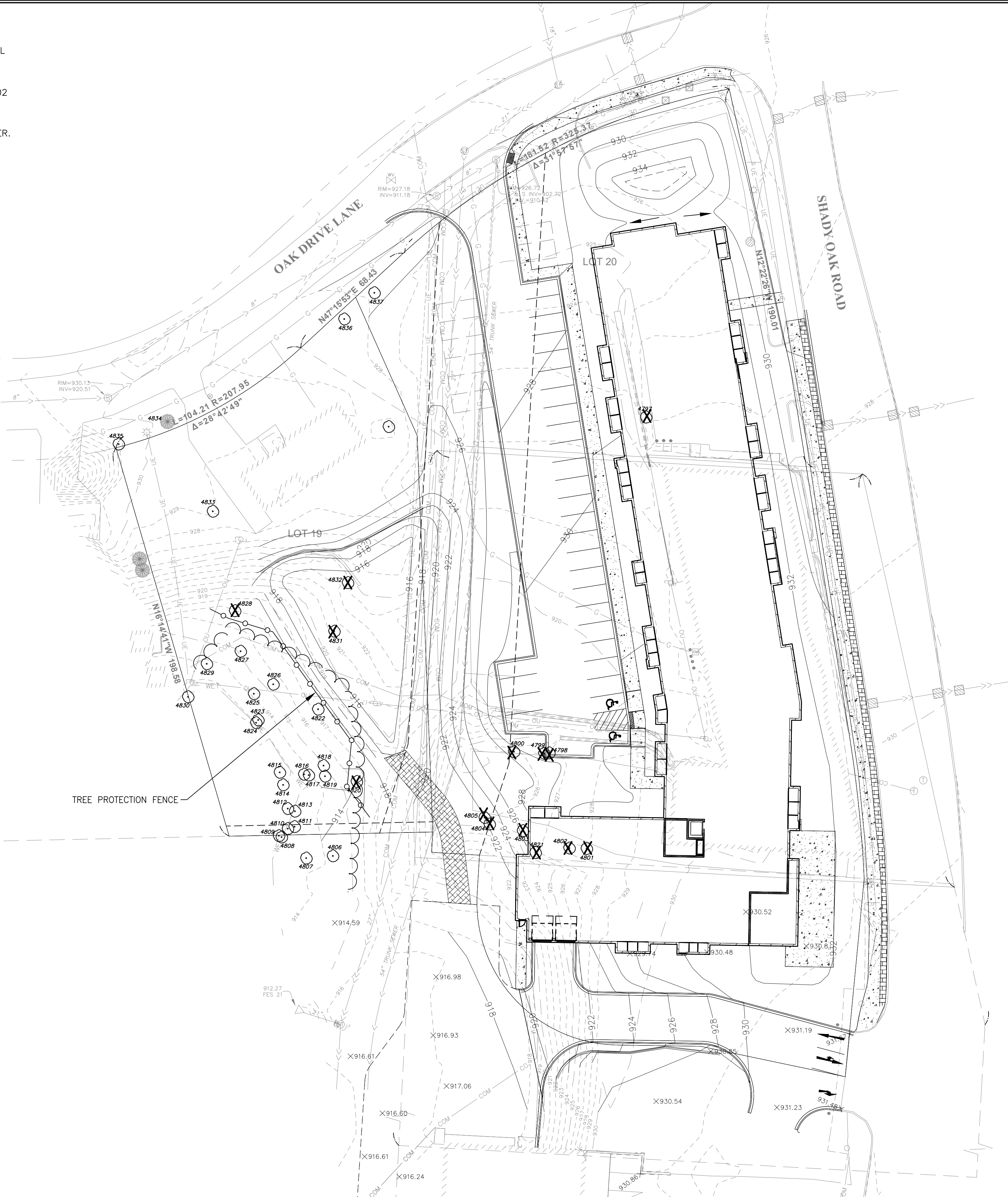
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CALL BEFORE YOU DIG  
**GOPHER STATE ONE CALL**

TWIN CITY AREA: 651-454-0002  
 TOLL FREE 1-800-252-1166

**TREE PRESERVATION NOTES**

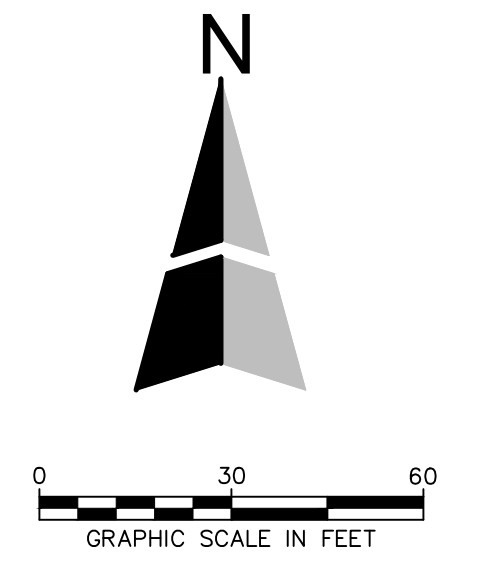
- BEFORE LAND CLEARING BEGINS, CONTRACTOR SHOULD MEET WITH THE CONSULTANT ON SITE TO REVIEW ALL WORK PROCEDURES, ACCESS ROUTES, STORAGE AREAS AND TREE PROTECTION MEASURES.
- TREE PROTECTION FENCE SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION ACTIVITIES AND IS TO BE PLACED OUTSIDE OF THE DRIP LINE OF ALL SIGNIFICANT TREES TO BE SAVED. FENCE TO REMAIN UNTIL ALL SITE WORK IS COMPLETED. FENCE SHOULD NOT BE MOVED OR RELOCATED WITHOUT VERBAL OR WRITTEN COMMUNICATION WITH THE CONSULTANT. HEAVY DUTY SILT FENCE SHALL BE USED FOR TREE PROTECTION FENCE.
- NO FILL SHOULD BE PLACED AGAINST THE TRUNK, ON THE ROOT CROWN, OR WITHIN THE DRIP LINE AREA OF ANY TREES THAT ARE TO BE SAVED.
- CARE MUST BE TAKEN TO PREVENT CHANGE IN THE SOIL CHEMISTRY DUE TO CONCRETE WASHOUT AND LEAKAGE OR SPILLAGE OF TOXIC MATERIALS SUCH AS PAINTS OR FUELS.
- ALL CONSTRUCTION EQUIPMENT, VEHICLE TRAFFIC AND STORAGE AREAS MUST BE LOCATED OUTSIDE OF ANY TREE PROTECTION AREA.
- PRUNING OF OAK TREES MUST NOT TAKE PLACE FROM APRIL 15 TO JULY 1.
- IF WOUNDING OF OAK TREES OCCUR, A NON-TOXIC WOUND DRESSING MUST BE APPLIED IMMEDIATELY. (EXCAVATORS MUST HAVE A NON-TOXIC TREE WOUND DRESSING WITH THEM ON DEVELOPMENT SITES.)
- HEAVY-DUTY SILT FENCE SHALL BE STAKED AT THE DRIP LINE OF EXISTING TREES TO BE SAVED.
- NO BURNING PERMITS WILL BE ISSUED FOR TREES TO BE REMOVED. TREES & SHRUBS MUST EITHER BE REMOVED FROM SITE OR CHIPPED.
- ANY TREES DAMAGED OR REMOVED NOT LISTED ON THIS TREE PRESERVATION PLAN MUST BE REPLACED AT A RATE OF 2:1 DIAMETER INCHES.



Tag Number	Size	Common Name	High Priority	Save/Remove	Notes
4797	15	Siberian Elm	No	Remove (1)	
4798	11	White Mulberry	No	Remove (1)	
4799	10	White Mulberry	No	Remove (1)	
4800	8	White Mulberry	No	Remove (1)	
4801	19,11	Siberian Elm	No	Remove (1)	
4802	22,12	Siberian Elm	No	Remove (1)	
4803	16	Siberian Elm	No	Remove (1)	
4804	11,11,10,9,8	Siberian Elm	No	Remove	
4805	12,11,10	Siberian Elm	No	Remove	
4806	24	Siberian Elm	No	Save	major internal decay along trunk
4807	10	Green Ash	No	Save	
4808	12	Green Ash	No	Save	
4809	15	Cottonwood	No	Save	
4810	14	Cottonwood	No	Save	
4811	25	Cottonwood	No	Save	
4812	20	Silver Maple	No	Save	
4813	18	Cottonwood	No	Save	
4814	11,11	Green Ash	No	Save	
4815	11	Green Ash	No	Save	
4816	16	Green Ash	No	Save	
4817	8	Box Elder	No	Save	
4818	17	Box Elder	No	Save	
4819	9	Box Elder	No	Save	
4820	8	American Elm	No	Remove (2)	
4821	12	Siberian Elm	No	Remove (1)	
4822	22,22	Northern Pin Oak	Yes	Save	
4823	16	Green Ash	No	Save	
4824	14	Siberian Elm	No	Save	
4825	40	Cottonwood	No	Save	
4826	8	Green Ash	No	Save	
4827	17,16,12	White Mulberry	No	Save	internal decay in 16" stem
4828	25	Siberian Elm	No	Remove (2)	
4829	8	Green Ash	No	Save	
4830	9,8	Green Ash	No	Save	
4831	18,11	Silver Maple	No	Remove (2)	
4832	13	White Mulberry	No	Remove (2)	
4833	25	Siberian Elm	No	Save	
4834	14	Colorado Blue Spruce	No	Save	25' in height
4835	42	Silver Maple	No	Save	
4836	10	Russian Olive	No	Save	
4837	11,10	Russian Olive	No	save	

Total trees	55
Significant trees	53
High priority trees	1
Significant trees removed outside of Basic Tree Removal and Pond area	8
High Priority trees removed	0
Required Mitigation (8 x 2")	= 16"
Remove (1)	Indicates trees removed within the "basic tree removal area"
Remove (2)	Indicates trees removed within the ponding area

X DENOTES TREE TO BE REMOVED



Plot Date & Time:

C:\Users\kik-ca\OneDrive\2017\17-011 Shady Oak Crossing\CAD\CAD\9 TREE PRESERVATION PLAN.dwg

NO.	DATE	DESCRIPTION

**CAMPION ENGINEERING SERVICES, INC.**

• Civil Engineering • Land Planning  
 1800 Pioneer Creek Center,  
 P.O. Box 249  
 Maple Plain, MN 55359  
 Phone: 763-479-5172  
 Fax: 763-479-4242  
 E-Mail: mcampion@campioneng.com

I hereby certify that this plan, specification or report has been prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota.

Martin P. Campion -Lic. # 19901 Date: \_\_\_\_\_

**SHADY OAK CROSSING**  
**RON CLARK CONSTRUCTION**  
 MINNETONKA, MN

**PRELIMINARY TREE PRESERVATION PLAN**

PROJECT NO:  
**17-011**

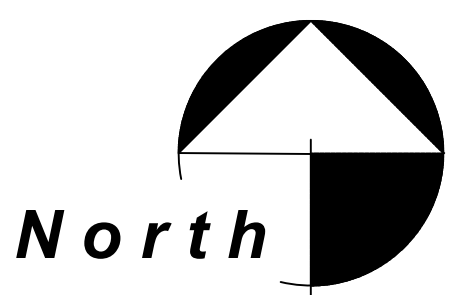
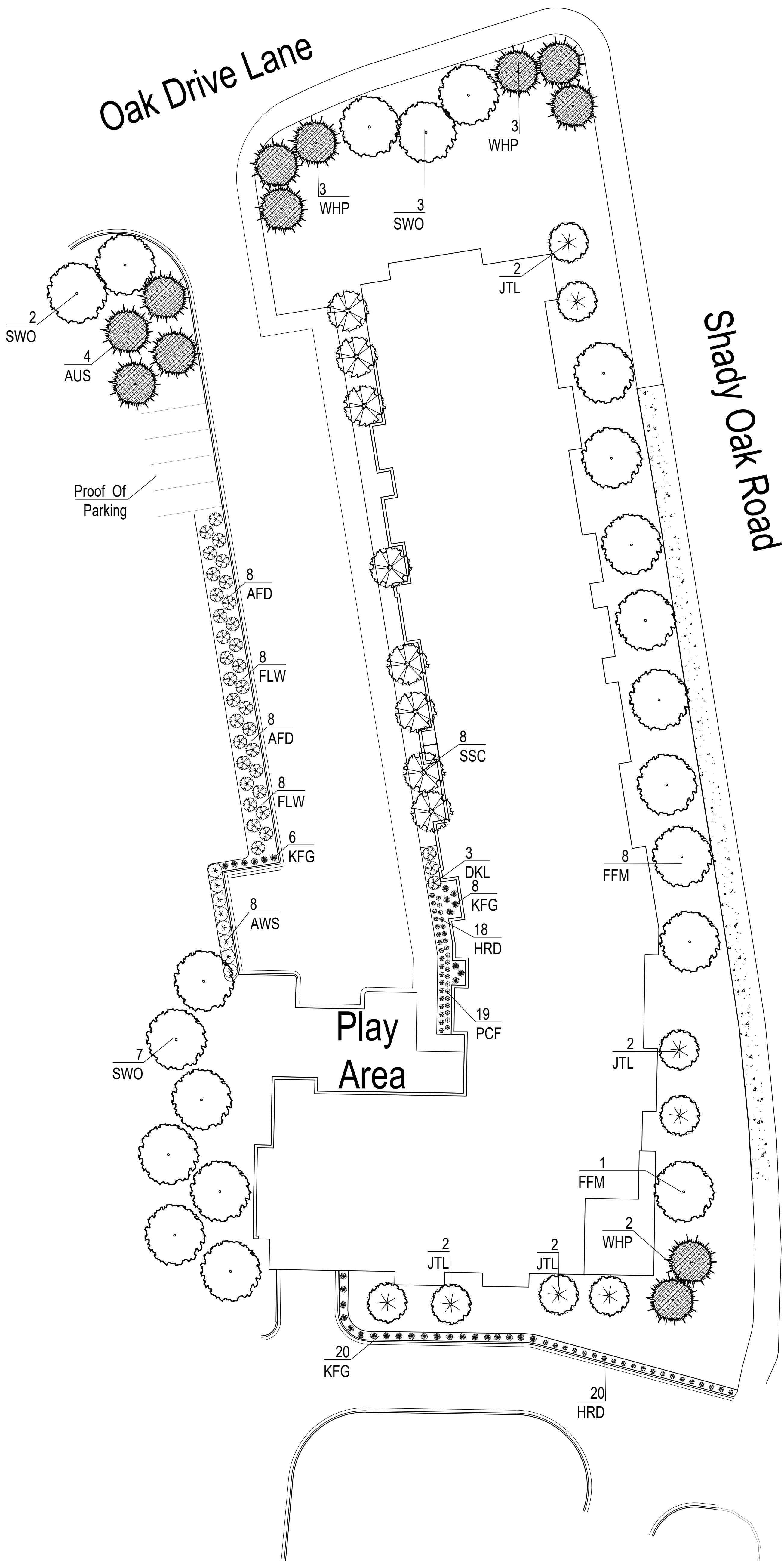
DATE:  
**12/02/2019**

SHEET NO. **9** OF **11** SHEETS



Oak Drive Lane

Shady Oak Road



# Shady Oak Crossing Minnetonka, MN

Markell Labree Design Group  
895 Park Knoll Drive  
Eagan, MN 55123  
651-468-8714  
[leemarkell@comcast.net](mailto:leemarkell@comcast.net)

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly registered LANDSCAPE ARCHITECT under the laws of the State of Minnesota.  
1/20/2019 10:45:00 AM  
Lee Markell  
Date: 06/19/19  
License No. 19313

NO.	REVISION / ISSUE	DATE

PREPARED FOR:

DESIGNED: AD SHEET: L1

DRAWN: Ad

CHECKED: LM

DATE: 06/19/19



QTY.	CODE	Common Name	Size	Notes
------	------	-------------	------	-------

**Overstory/ Ornamental Trees**

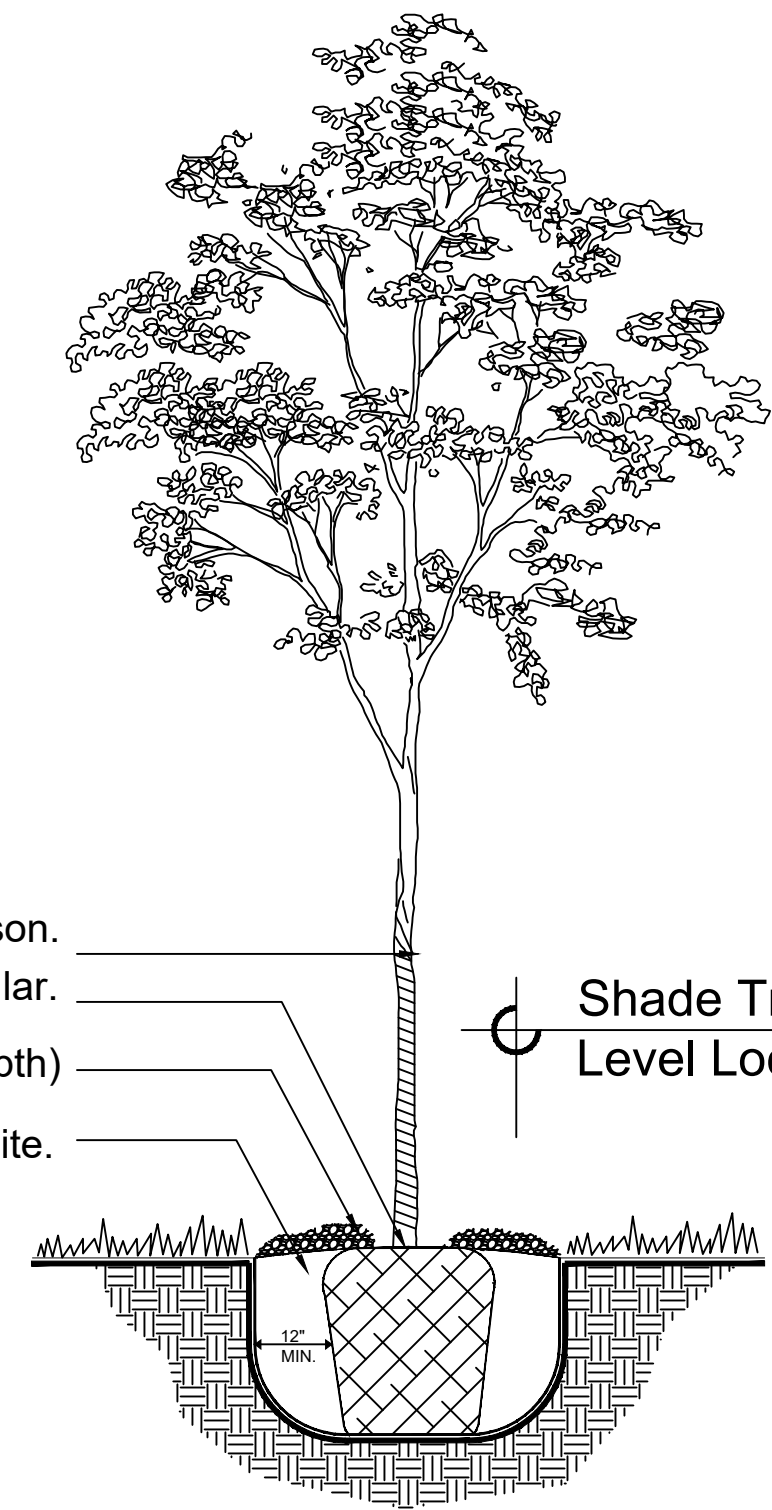
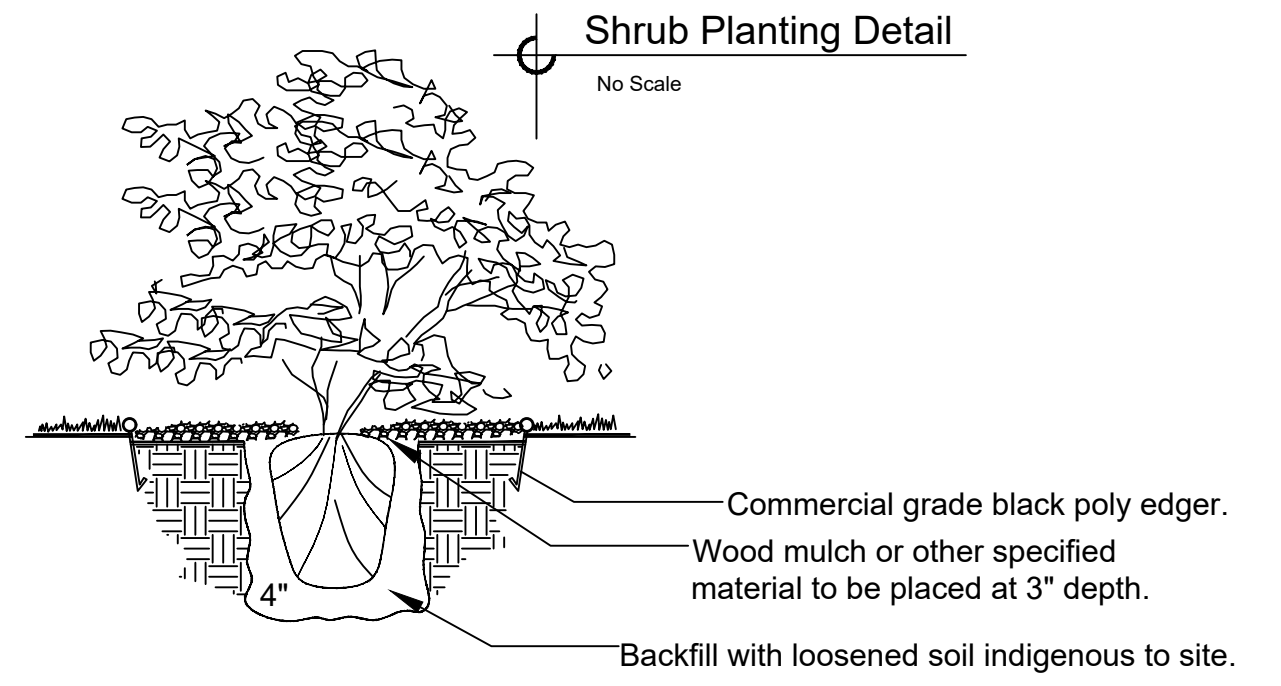
9	FFM	Fall Fiesta Maple	2.5" BB	
12	SWO	Swamp White Oak	2.5" BB	
8	JTL	Japanese Tree Lilac	2.5" BB	
8	SSC	Spring Snow Crabapple	2.5" BB	

**Evergreen Trees**

8	WHP	White Pine	6' BB	
4	AUS	Austrian Pine	6' BB	

**Shrubs and Perennials**

8	AWS	Anthony Waterer Spirea	#3	
3	DKL	Dwarf Korean Lilac	#3	
16	AFD	Arctic Fire Dogwood	#3	
16	FLW	Flame Willow	#3	
19	PCF	Purple Coneflower	#1	
28	HRD	Happy Returns Daylily	#1	
34	KFG	Karl Foerster Grass	#1	



Approved tree wrap to be applied prior to winter season.  
Do not allow backfill or mulch material to come into contact with root collar.  
Wood mulch or other specified material. (3" Depth)  
Backfill with loosened soil indigenous to site.

\*NOTE: Tree to be staked & guyed only on an as needed basis.

**GENERAL NOTES PLAN SPECIFIC:**

All plantings shall be true to name and size in accordance with American Nurseryman's Standards.

All plantings shall be guaranteed for one year (365 days) from date of acceptance. Landscape Contractor shall replace any dead or damaged plants at no additional cost to Owner during the guarantee period. Landscape Contractor shall make monthly site maintenance inspections and notify owner of maintenance deficiencies.

All trees shall be guyed at the discretion of the landscape contractor. Landscape contractor shall warrant plants to be plumb at the end of the warranty period. All trees shall be wrapped at the end of November of installation year.

All shrub beds and areas indicated as receiving rock mulch shall receive a 5" deep layer of 2-4" size Washed River Rock over 3 ounce landscape fabric.

Landscape maintenance bed around the building is 3 feet wide with 1 1/2 inch river rock over 3 ounce landscape fabric.

All single trees shall receive a 4" layer of shredded bark mulch free of leaves, twigs, and other extraneous debris over weed barrier fabric.

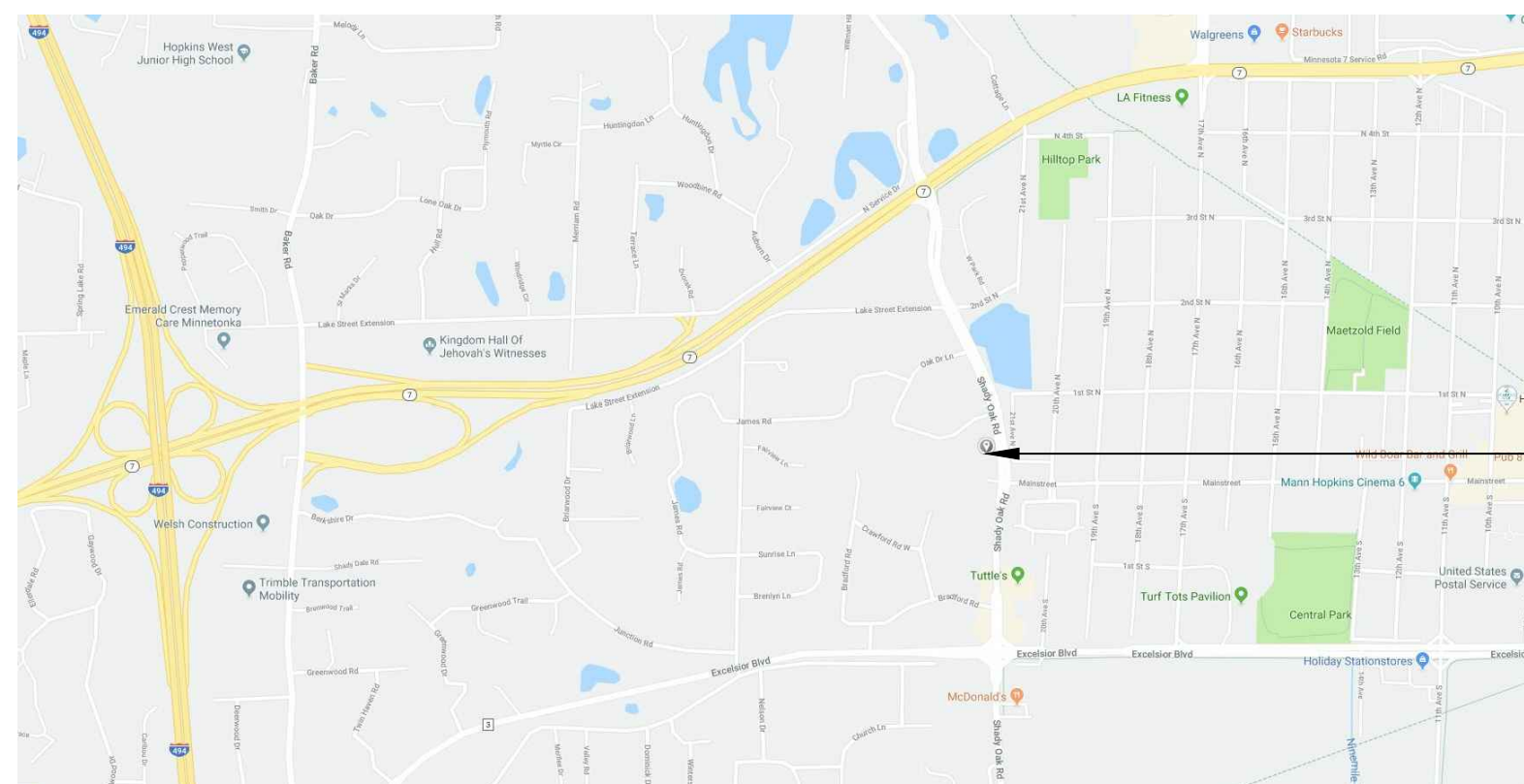
Sod shall be cultured Kentucky bluegrass, free of weeds and clumps. All area within the irrigations limits shall be sodded. Landscape Contractor will water at time of installation and roll all sod as needed to assure a smooth turf. All slopes greater than 3 to 1 shall be staked.

Any sliding of sod shall be replaced by Landscape Contractor at no cost to the owner.

All areas outside the irrigation limits shall be seeded with MNDOT 25-131 and mulch with straw disc anchored. Any slopes greater than 3:1 shall be blanketed with 2 side straw blanket.

A performance base irrigation system shall be installed by the Landscape Contractor including sleeve as needed. RPZ shall be supplied to the general contractor for installation. Coordinate with the general contractor for the size of the irrigation stub. IRRIGATION INSTALLED ONLY IN THE SODDED TURF AREAS.

Landscape Contractor shall be responsible for locating all utilities by actual location in the field prior to any planting operation.



**Shady Oak Crossing**

NO.	REVISION / ISSUE	DATE

PREPARED FOR:

# Shady Oak Crossing Minnetonka, MN

Lee Markell  
Date: 06/19/19  
License No. 19313

Markell Laberee Design Group  
895 Park Knoll Drive  
Eagan, MN 55123  
651-468-8714  
[leemarkell@comcast.net](mailto:leemarkell@comcast.net)

DESIGNED: AD

DRAWN: Ad

CHECKED: LM

DATE: 06/19/19

SHEET:

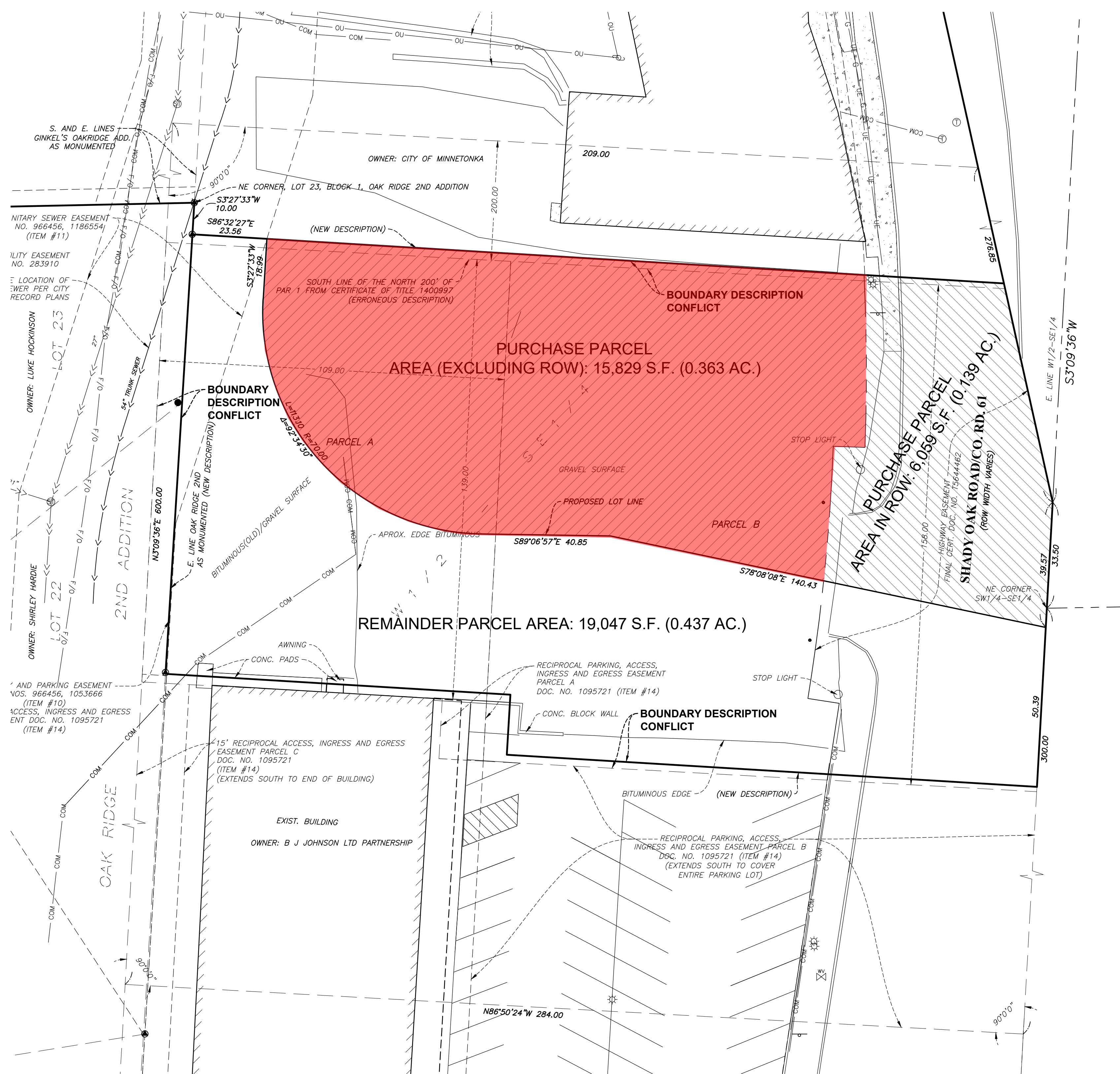
**L2**



# Detachment/Annexation Property

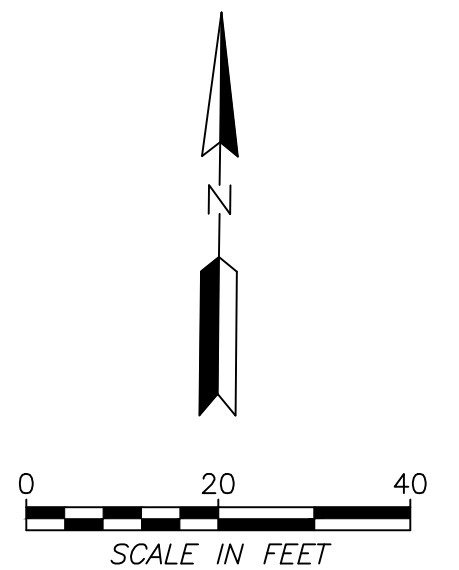
# PARCEL SPLIT EXHIBIT

Plot Date & Time: 6 January 2020, 12:48 PM  
 M:\1531\0014 - Shady Oak Johnson, 41A\SURVEY\CAD\1531-0014 - boundary corrections.dwg



### LEGEND

- SET 3/4"ODx14" IRON PIPE WITH PLASTIC CAP 43055 OR MAG NAIL W/WASHER
- PROPERTY MARKER FOUND BY HENNEPIN COUNTY SURVEYOR
- FOUND MONUMENT
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- ⊙ STORM SEWER INLET
- ⊙ STORM SEWER INLET
- ⊙ TELEPHONE MANHOLE
- ⊙ GAS METER
- ⊙ COMMUNICATIONS PEDESTAL
- ⊙ ELECTRIC MANHOLE
- ⊠ ELECTRIC TRANSFORMER
- ⊠ TRAFFIC SIGN
- BOLLARD/POST
- ⊙ UTILITY POLE
- ⊙ LIGHT POLE
- ⊙ CONIFEROUS TREE
- ⊙ DECIDUOUS TREE
- ⊙ WATER VALVE
- ⊙ HYDRANT
- STORM SEWER
- SANITARY SEWER
- — — — WATERMAIN
- — — — UNDERGROUND GAS LINE
- — — — UNDERGROUND COMMUNICATION LINE
- — — — OVERHEAD UTILITY LINE
- — — — TREE LINE
- ▨ BUILDING
- ▨ CONCRETE SURFACE
- ▨ ASPHALT SURFACE



### EXISTING PROPERTY DESCRIPTION:

Certificate of Title 1492019

All that portion of the tract or parcel of land described as Paragraph "A" below, which lies Southerly of a line drawn parallel to and 200 feet Southerly of the North line thereof and the same extended, to wit:

Paragraph "A"; That portion of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West of the 5th Principal Meridian, described as follows: Starting at the Northeast corner of the Southwest Quarter of said Section, thence South along the East line of the West half of the Southeast Quarter of said Section, a distance of 300 feet, thence Westery at right angles to said East line for a distance of 284 feet; thence Northerly along a line parallel to said East line a distance of 600 feet; thence Easterly along a line at right angles to said East line 209 feet to the center line of McGinty Road; thence Southeasterly along the center line of McGinty Road to the East line of the West Half of the Southeast Quarter of said Section 23; thence Southerly along said East line 33.5 feet to the point of beginning.

Which are described as follows:

- Parcel A: The West 109.00 feet of the North 139.00 feet.
- Parcel B: That part of the North 158.00 feet thereof lying East of the West 109.00 feet thereof.

Together with an easement for driveway and parking purposes over, under and cross that part of Lot 22, Block 1, Oak Ridge 2nd Addition, lying Northeasterly of a line, and its extensions, drawn from the Southeast corner of said lot to a point on the Northwesterly line of said lot distant 120 feet Southwesterly, measured along said Northwesterly line, from the most Northerly corner of said lot, as contained in deed Doc. No. 966456; (See Order Doc. no. 1053666)

### PROPOSED PROPERTY DESCRIPTION:

That part of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West, Hennepin County, Minnesota being described as follows:

All that portion of the tract or parcel of land described as Paragraph "A" below, which lies Southerly of a line drawn parallel to and 200 feet Southerly of the North line thereof and the same extended, to wit:

Paragraph "A"; That portion of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West of the 5th Principal Meridian, described as follows: Starting at the Northeast corner of the Southwest Quarter of said Section, thence South along the East line of the West half of the Southeast Quarter of said Section, a distance of 300 feet, thence Westery at right angles to said East line for a distance of 284 feet; thence Northerly along a line parallel to said East line a distance of 600 feet; thence Easterly along a line at right angles to said East line 209 feet to the center line of McGinty Road; thence Southeasterly along the center line of McGinty Road to the East line of the West Half of the Southeast Quarter of said Section 23; thence Southerly along said East line 33.5 feet to the point of beginning.

Which are described as follows:

- Parcel A: The West 109.00 feet of the North 139.00 feet.
- Parcel B: That part of the North 158.00 feet thereof lying East of the West 109.00 feet thereof.

Together with an easement for driveway and parking purposes over, under and cross that part of Lot 22, Block 1, Oak Ridge 2nd Addition, lying Northeasterly of a line, and its extensions, drawn from the Southeast corner of said lot to a point on the Northwesterly line of said lot distant 120 feet Southwesterly, measured along said Northwesterly line, from the most Northerly corner of said lot, as contained in deed Doc. No. 966456; (See Order Doc. No. 1053666)

Which lies northeasterly of the following described line:

Commencing at the northeast corner of Lot 23, Block 1, OAK RIDGE 2ND ADDITION, according to the recorded plat thereof, Hennepin County, Minnesota; thence South 03 degrees 27 minutes 33 seconds West, along the east line of said Lot 23, a distance of 10.00 feet to the point of beginning of the line to be described; thence South 86 degrees 32 minutes 27 seconds East a distance of 23.56 feet to the point of beginning of the line to be described; thence South 03 degrees 27 minutes 33 seconds West a distance of 18.99 feet; thence southeasterly 113.10 feet along a tangential curve concave to the northeast having a radius of 70.00 feet and central angle of 92 degrees 34 minutes 30 seconds; thence South 89 degrees 06 minutes 57 seconds East a distance of 40.85 feet; thence South 78 degrees 08 minutes 08 seconds East a distance of 140.43 feet, more or less, to the east line of said West Half of the Southeast Quarter and there terminating.

### SURVEYORS CERTIFICATION:

I hereby certify that this survey was completed by me or under my direct supervision and that I am a duly licensed land surveyor under the laws of the State of Minnesota.

*Chris Ambour* 1/6/2020  
Chris Ambour Date  
LS 43055

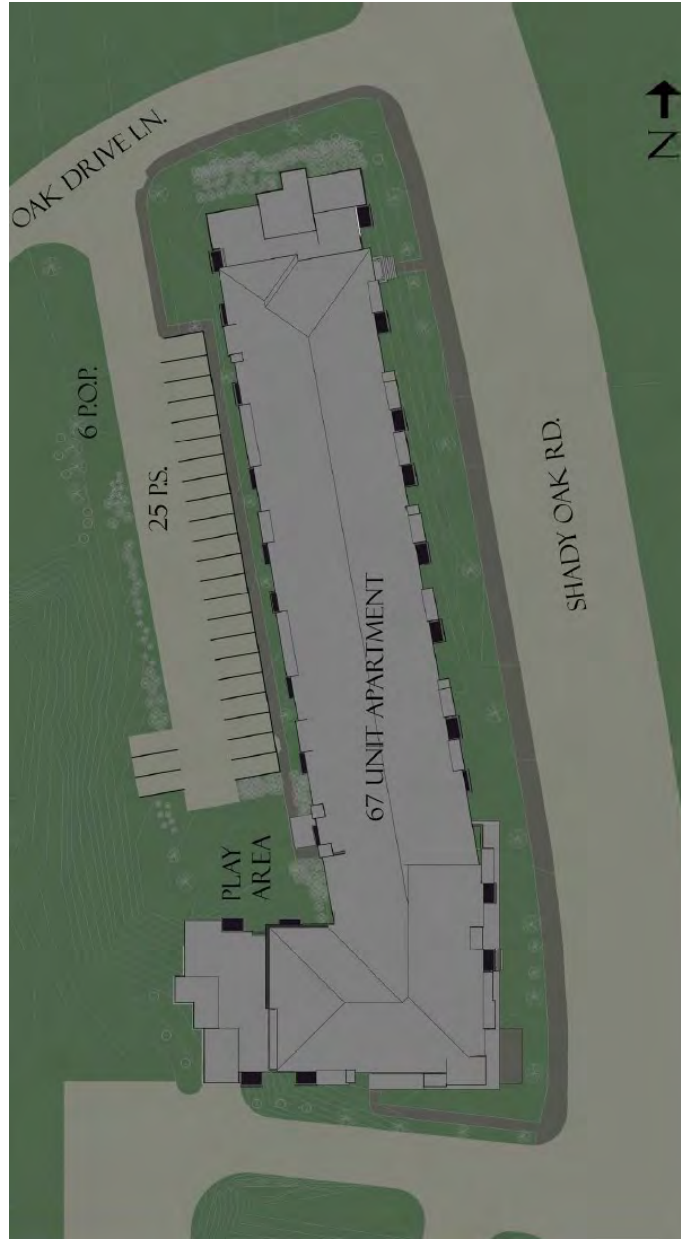
<p>Responsive partner. Exceptional outcomes.</p> <p>1802 WOODDALE DRIVE WOODBURY, MN 55125</p> <p>Ph: 651-395-5212</p>	CLIENT NAME		PROJECT TITLE					
	RON CLARK CONSTRUCTION & DESIGN		PARCEL SPLIT EXHIBIT					
#	XXX	XXX	XX/XX/XX	DWN BY CNA	CHK'D CNA	APP'D CNA	DWG DATE SCALE	SEE DRAWING 1" = 20'
REV	REVISION DESCRIPTION	DWN	APP	REV DATE	PROJECT NO.	SHEET NO.		
					1531-0014	1 OF 1		

## **Comparisons of plans**





**Approved 2017 Plan**  
**49 units**  
**300' length**



**June 2019 Proposal**  
**67 units**  
**415' length**



**Current Proposal**  
**75 units**  
**355' length**









**Approved 2017 Plan**



**June 2019 Proposal**



**Current Proposal**



# **Traffic Study**



## Memorandum

**To:** Loren Gordon, City Planner  
City of Minnetonka

**From:** Sudheer Dhulipala, PE, PTOE  
**cc:** Tony Heppelmann, PE

**Date:** January 10, 2020

**Re:** Shady Oak Redevelopment  
WSB Project No. 014518

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## Introduction

This traffic impact study addresses a proposed redevelopment of the property at 4312 Shady Oak Road with a 75-unit apartment building. The project location is shown on **Figure 1**. The development would replace the existing retail building on this site. The proposed site layout is shown on **Figure 2**. The proposed development would have two accesses. The primary access is to an underground parking garage from Shady Oak Road via a new driveway that will be constructed at the west leg of the intersection of Mainstreet and Shady Oak Road. The secondary access will be to Oak Drive Lane from a 29-space surface parking lot. There are a few businesses in the southwest quadrant of the intersection of Mainstreet and Shady Oak Road which currently have access to Shady Oak Road and it is proposed that they will also be able to access the new driveway to Shady Oak Road at Main Street. The primary issues for this project are the potential queues and delays on the west leg of the intersection of Shady Oak Road and Mainstreet and the preferred configuration for this approach.

## Existing Conditions

Shady Oak Road is a four-lane divided roadway, while Mainstreet is a two-lane undivided roadway; both have a posted speed limit of 30 mph. The intersection of these two roads is signalized with turn lanes as represented by turn arrows in **Figure 1**. WSB obtained traffic counts from the intersection of Shady Oak Road and Mainstreet from a prior study done in April 2016 shown in **Figure 3**. Average Annual Daily Traffic (AADT) data was obtained from MnDOT for Shady Oak Road and Mainstreet for recent years up to 2018. It was observed that AADT on Shady Oak Road went up by 17% from 11,500 in 2016 to 13,400 in 2018 but the AADT on Mainstreet has been gradually decreasing over the years – 5700 in 2005, 5200 in 2008, 4400 in 2012 and 4200 in 2016.

WSB set a video traffic counter at the south access to the businesses in the southwest quadrant of the intersection in front of Tonka Cycle and Ski in July 2019 to determine the amount of traffic using the businesses in the southwest quadrant during peak hours. Approximately 56 vehicles enter the business driveway during the AM peak hour and 51 vehicles enter during the PM peak hour. 39 vehicles exit the business driveway during the AM peak hour and 55 vehicles exit during the PM peak hour. It was observed that vehicles had difficulty making left turns out of the driveway during the evening peak hour.

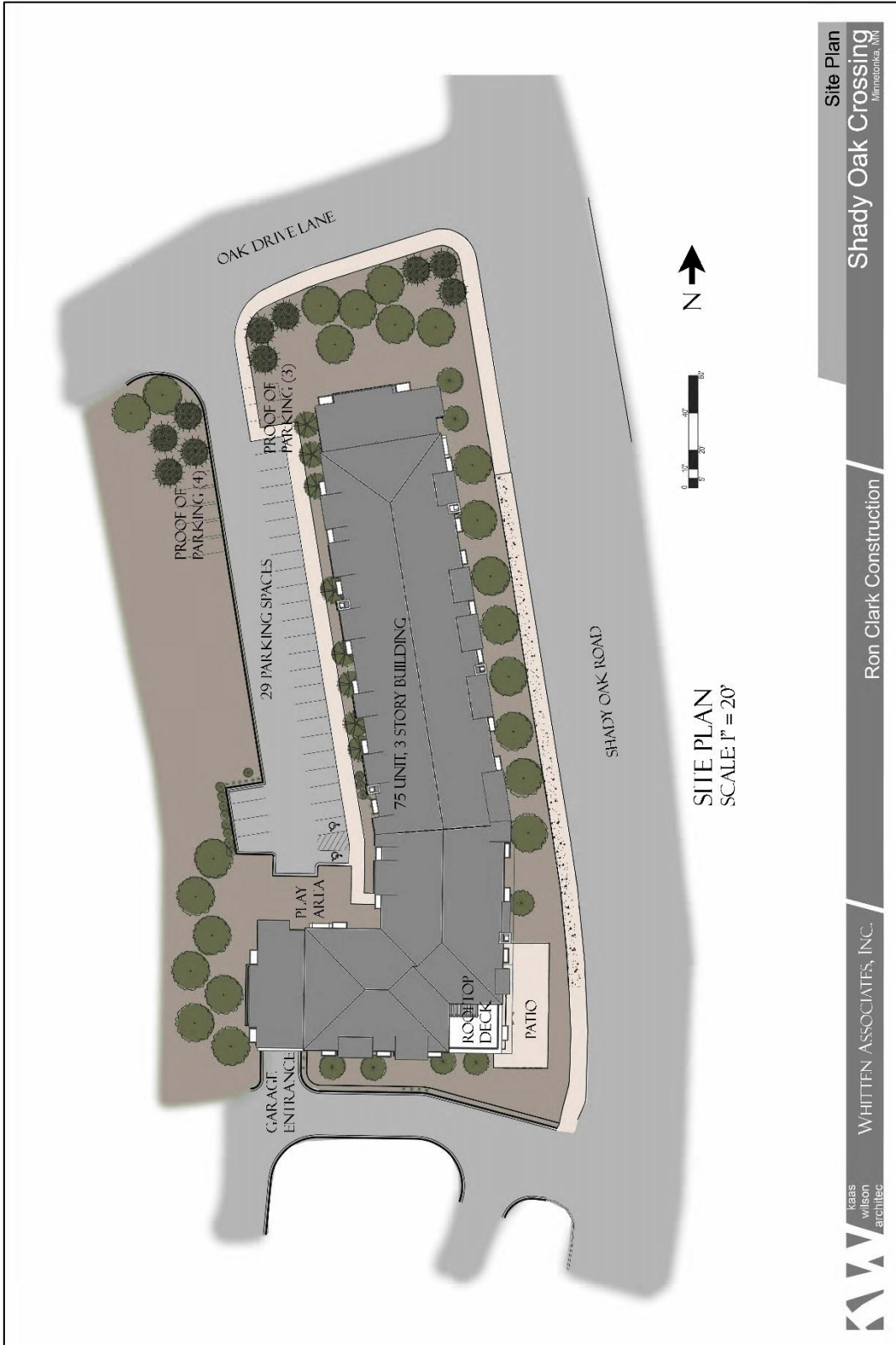


Figure 1  
Project Location





Figure 2  
Proposed Site Plan



Site Plan  
Shady Oak Crossing  
Minnetonka, MN

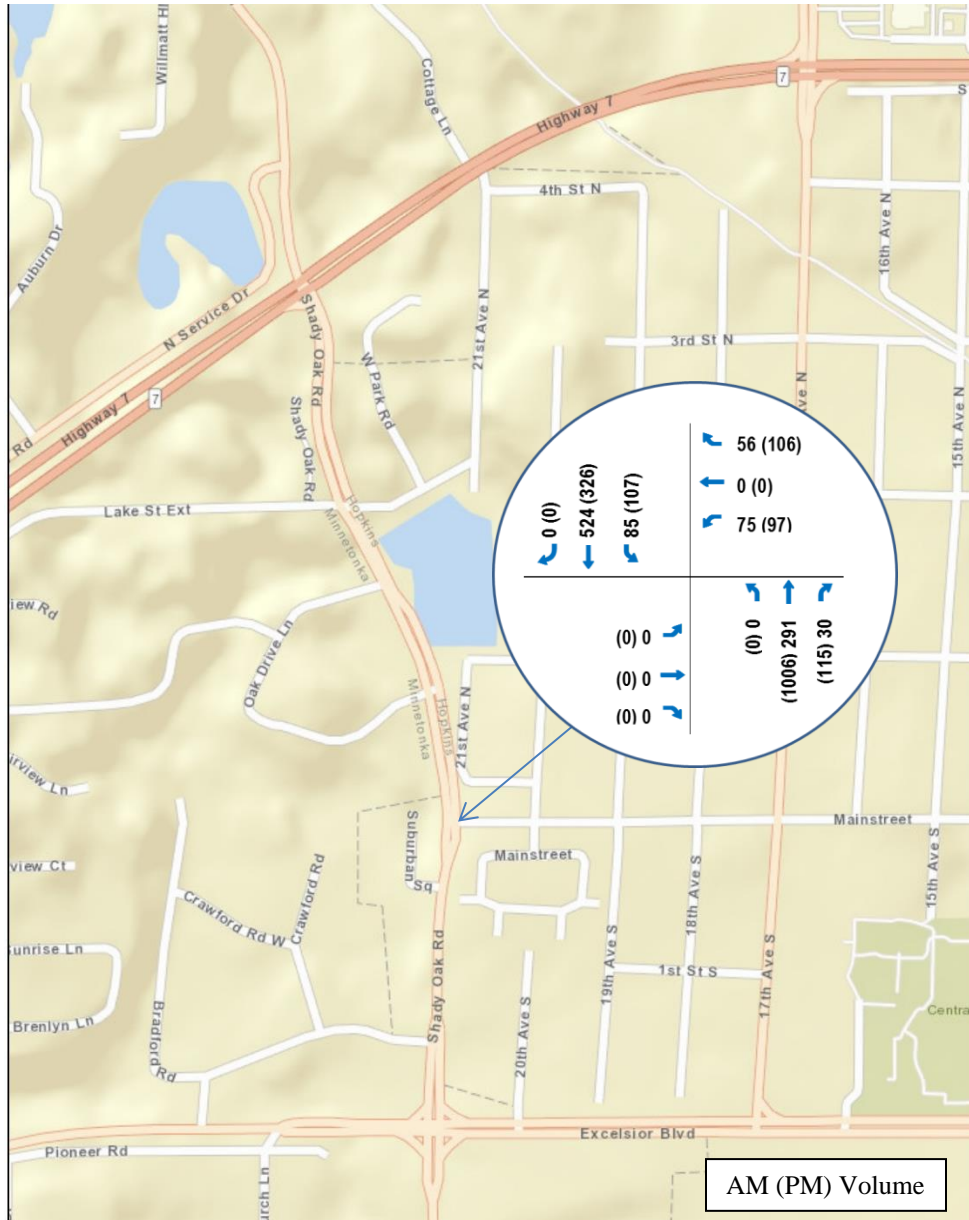
Ron Clark Construction

WHITTEN ASSOCIATES, INC.

kaas  
wilson  
architect



Figure 3  
2016 Traffic Volumes

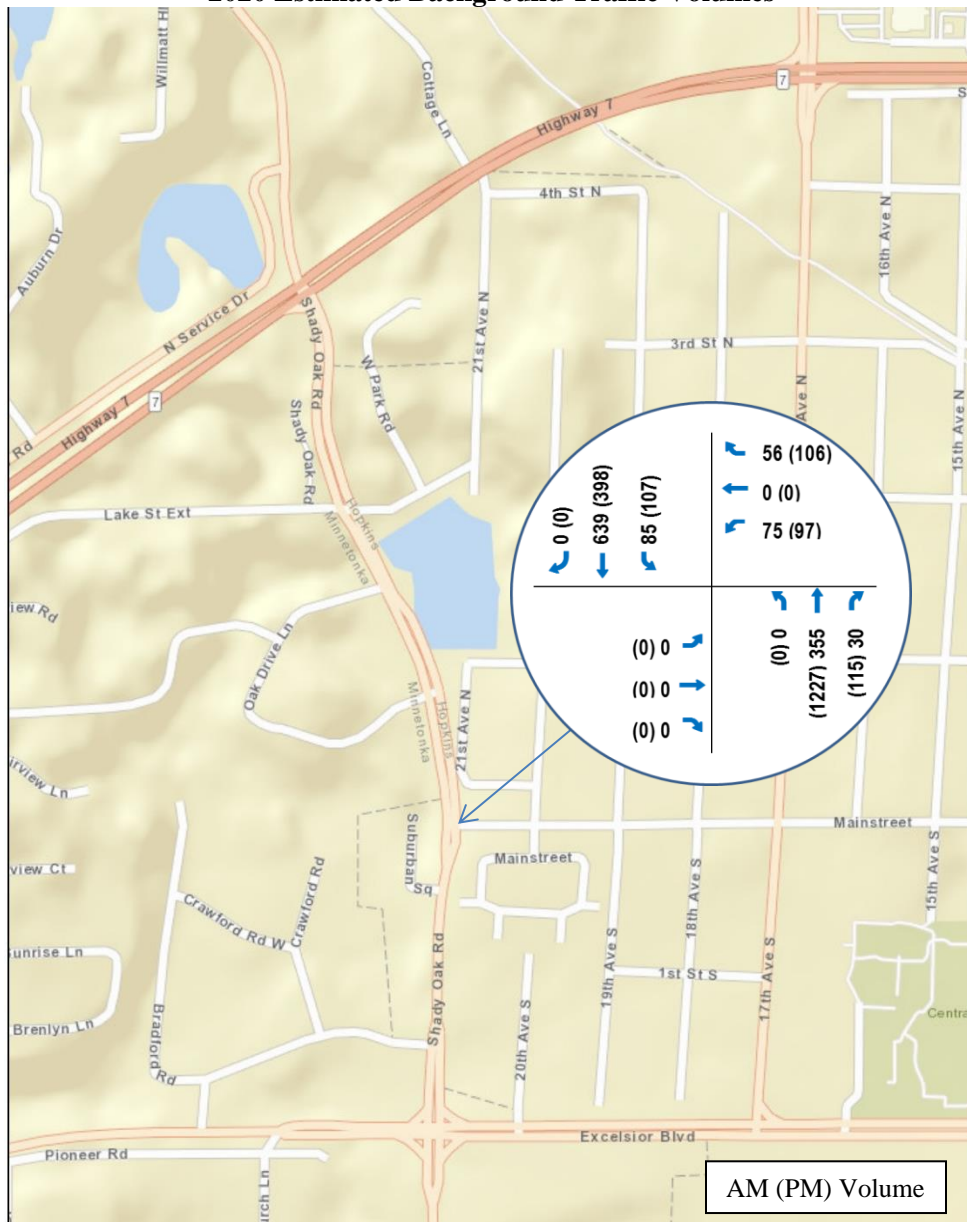




### Traffic Forecasts

Based on the 2016 turning movement counts and recent AADT counts, the am and pm peak hour turning movement volumes in year 2020 were estimated. The growth from 2016 to 2020 was estimated to be 22%, which is 17% to 2018 plus 5% from 2018 to 2020. This estimate is the background traffic level at the site not including the proposed development. It should be noted that the traffic levels on Mainstreet were not reduced based on the AADT trend and were kept at the 2016 level. The forecasted 2020 turning movement counts without the development are shown in **Figure 4**.

**Figure 4**  
**2020 Estimated Background Traffic Volumes**





The estimated trip generation for the site is shown in **Table 1** below. The estimated trips are based on trip generation rates from the Institute of Transportation Engineers Trip Generation Manual 10<sup>th</sup> Edition. The site is expected to generate 36 trips in the am peak hour and 46 trips in the pm peak hour.

**Table 1  
Site Trip Generation**

Description/ITE Code	Units	Expected Units (independent variable)	Calculated Daily Trips	AM Peak Trips - Total	AM In	AM Out	PM Peak Trips - Total	PM In	PM Out
Multi-family housing – low rise (Code:220) (adjacent street)	Dwelling Unit	75	526	36	8	28	46	29	17

It was assumed that 10% of the site trips would use the surface parking lot with access from Oak Drive Lane. In order to develop the traffic forecasts at the intersection of Mainstreet and Shady Oak Road with the proposed development, the above site generated trips were added to the 2020 background traffic. The background traffic volumes were not reduced to reflect the removal of the existing retail development on this site so the forecast is a conservative estimate of the future traffic volumes with this development.

The business driveway in the southwest quadrant of the intersection of Shady Oak Road and Mainstreet was assumed to remain open. An additional access from the business to the new driveway was assumed. This enables drivers who wish to go north on Shady Oak Road or east on Mainstreet to use the signal which is a safer option than the existing driveway. It was assumed that the new signal access will allow 50% more traffic than current levels to turn left from the businesses due to the added convenience of making the turn. It was also assumed that all the traffic entering the business area from the south will continue to use the existing driveway until the median is closed off preventing this movement. Half of the traffic entering the businesses from north was assumed to use the signal and the other half the existing driveway. All the traffic exiting the businesses to the south were assumed to use the existing driveway.

With the removal of the existing businesses from the project site, and due to the primary access to the apartment being from the Mainstreet and Shady Oak Road intersection, the traffic levels at the intersection of Oak Drive Lane and Shady Oak Road are expected to be lower than current levels.

The trips from the site were distributed according to the traffic patterns observed from existing counts.

Three different configurations were analyzed for the new eastbound approach.

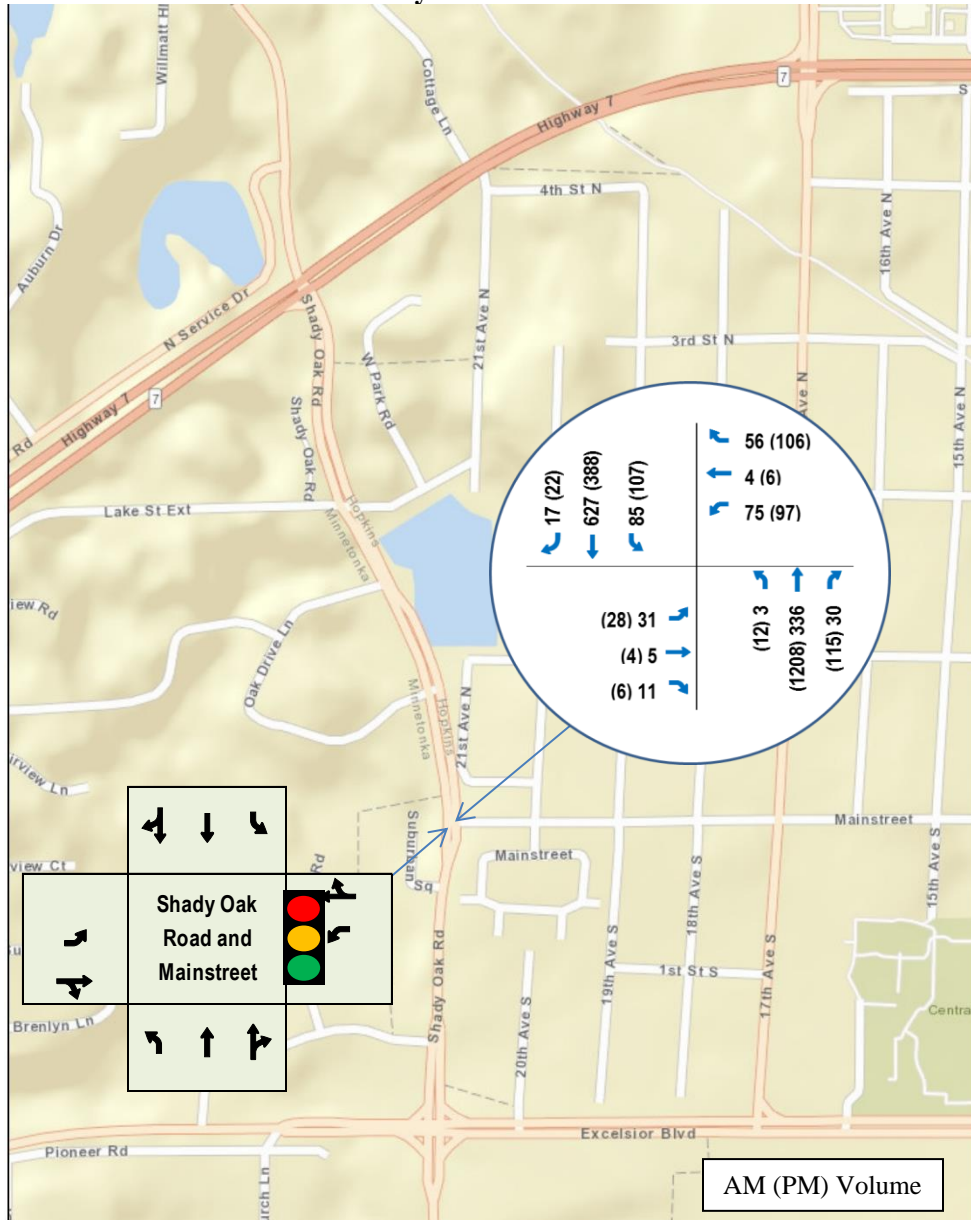
- A one-lane approach with no turn lanes.
- A two-lane approach with shared left and through movements and a separate right-turn lane.
- A two-lane approach with an exclusive left-turn lane and a shared through-right lane.

The level of service for all three alternatives were found to be acceptable as discussed in the next section. The recommended geometry is to have left-turn lanes on the eastbound and westbound approaches. The through movements would be shared with right-turns on these approaches. The recommended storage on eastbound left-turn lane is 75 feet.

**Figure 5** shows the recommended lane geometry and forecast traffic volumes with the proposed project.



**Figure 5**  
**2020 Lane Geometry and Forecast Traffic Volumes**



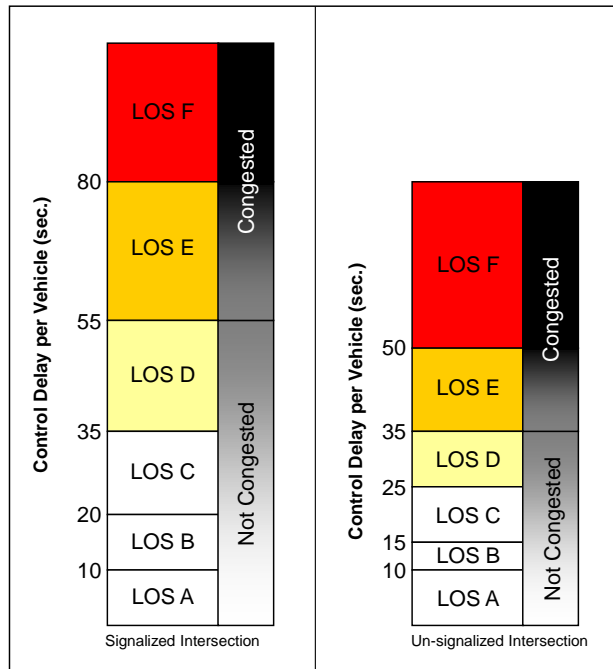


## Future Traffic Operations

WSB conducted traffic operations analysis of the forecast traffic volumes with the development to determine the level of delays, level of service and vehicle queues at the intersection of Mainstreet and Shady Oak Road.

Intersection operations are evaluated in terms of average seconds of delay per vehicle for the intersection, and for each approach and turning movement. The average number of seconds of delay is broken into six ranges assigned letter grades A through F defining each level of service (LOS) as shown in **Figure 6**. The ranges for unsignalized intersections are narrower than the ranges for signalized intersections. This is because many factors including the intangible factors of driver discomfort and frustration are considered. A one-minute delay at a red light is perceived as being more tolerable than one minute waiting for a gap in traffic at a stop sign, especially when there are vehicles queued behind. It is generally recognized that LOS D is the lowest acceptable LOS for urban intersections. The analysis also quantifies the length of queues of stopped vehicles. A 100-foot queue is approximately equal to four cars.

**Figure 6**  
Level of Service Definitions



SOURCE: Level of Service thresholds from the Highway Capacity Manual.  
K:\Traffic\Level of Service (LOS)\LOS Delay Graphic.ppt

**Tables 2-4** shows the results for year 2020 for various alternatives for the eastbound approach. The analysis shows that the intersection operates at an acceptable LOS in both AM and PM peak hours with the proposed development in all alternatives. There is capacity at the intersection for additional traffic growth. In the left-turn lane scenario, the eastbound approach operates at LOS “C” in the am peak hour and LOS “D” in the pm peak hour with an average delay of 32 seconds/vehicle in the am peak hour and 46 seconds/vehicle in the pm peak hour. The analysis indicates that the potential maximum queue length on the eastbound left turn lane is 56 feet or approximately 3 vehicles long at 20 feet per vehicle and the average is about 22 feet or approximately 1 vehicle long. It is recommended that the access from the businesses south of the new access be located as far west as possible to allow these vehicles to get into the correct lane at the intersection. As the traffic levels at the intersection of Oak Drive Lane and Shady Oak Road are expected to be lower than current levels and the intersection is anticipated to operate at an acceptable level.







**Table 3**  
**Delay and Level of Service at Mainstreet and Shady Oak Road with Development – Right-Turn lane on Eastbound approach and Shared Through-left lane**

AM Peak Hour

Intersection		Appr	Total Delay by Movement			LOS by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Appr	Average & Maximum Traffic Queueing (feet)								
Control	Location		L	T	R	L	T	R	Delay	LOS	Delay	LOS		Left-Turn			Through			Right-Turn		
														Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage
Signalized	1: CSAH 61 & Mainstreet	NB	9	5	3	A	A	A	5	A	7	A	NB	1	7	150	20	101				
		WB	41	45	5	D	D	A	26	C			WB				61	137		26	58	100
		SB	7	3	1	A	A	A	3	A			SB	27	82	300	47	114				
		EB	44	42	6	D	D	A	35	D			EB				31	98		8	37	50

Note: For movements that are shared on one lane, queue lengths are only shown one of the shared movements.

PM Peak Hour

Intersection		Appr	Total Delay by Movement			LOS by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Appr	Average & Maximum Traffic Queueing (feet)								
Control	Location		L	T	R	L	T	R	Delay	LOS	Delay	LOS		Left-Turn			Through			Right-Turn		
														Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage
Signalized	1: CSAH 61 & Mainstreet	NB	9	11	10	A	B	B	11	B	12	B	NB	2	19	150	129	350				
		WB	48	49	13	D	D	B	30	C			WB				77	152		45	107	100
		SB	18	3	2	B	A	A	6	A			SB	48	122	300	29	116				
		EB	44	61	5	D	E	A	40	D			EB				24	84		6	30	50

Note: For movements that are shared on one lane, queue lengths are only shown one of the shared movements.



**Table 4**  
**Delay and Level of Service at Mainstreet and Shady Oak Road with Development – Exclusive Left-Turn Lane and Shared Through-Right Lane**

AM Peak Hour

Intersection		Appr	Total Delay by Movement			LOS by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queueing (feet)								
Control	Location												Left-Turn			Through			Right-Turn		
			L	T	R	L	T	R	Delay	LOS	Delay	LOS	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage
Signalized	1: CSAH 61 & Mainstreet	NB	6	4	3	A	A	A	4	A	7	A	NB	1	19	150	26	105			
		WB	44	31	6	D	C	A	28	C			WB	54	120	100	32	106			
		SB	6	3	2	A	A	A	3	A			SB	21	56	300	46	123			
		EB	40	41	7	D	D	A	32	C			EB	22	56	75	12	49			

Note: For movements that are shared on one lane, queue lengths are only shown one of the shared movements.

PM Peak Hour

Intersection		Appr	Total Delay by Movement			LOS by Movement			LOS by Approach (Sec/Veh)		LOS by Intersection (Sec/Veh)		Average & Maximum Traffic Queueing (feet)									
Control	Location												Left-Turn			Through			Right-Turn			
			L	T	R	L	T	R	Delay	LOS	Delay	LOS	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	Ave Queue	Max Queue	Storage	
Signalized	1: CSAH 61 & Mainstreet	NB	9	10	9	A	B	A	10	B	12	B	NB	5	64	150	127	353				
		WB	48	54	13	D	D	B	30	C			WB				74	172		49	135	100
		SB	20	4	2	C	A	A	7	A			SB	50	129	300	34	132				
		EB	52	49	21	D	D	C	46	D			EB	2	19	75	28	74				

Note: For movements that are shared on one lane, queue lengths are only shown one of the shared movements.



## Conclusions and Recommendations

The following conclusions and recommendations were reached from the analysis that was conducted for this traffic study of the proposed 75-unit apartment at 4312 Shady Oak Road.

- The proposed site will generate 36 trip ends in the am peak hour and 46 trip ends in the pm peak hour with an estimated 90% of the traffic using the Mainstreet and Shady Oak Road intersection through a new driveway that forms the west leg of this intersection. The remaining 10% of the traffic is estimated to use Oak Drive Lane from the surface parking lot.
- The existing businesses in the southwest quadrant of the intersection of Mainstreet and Shady Oak Road will be given an additional access that connects to the new access to Shady Oak Road. This gives vehicles going north and east from the businesses a safer alternative to exit compared to the existing driveway from these businesses.
- With the development of the site, the level of service for traffic at Mainstreet and Shady Oak Road intersection is LOS “A” in the am peak hour and LOS “B” in the pm peak hour. The intersection operates at an acceptable level of service in both peaks and has ample capacity for future traffic growth.
- It is recommended the new eastbound approach to Mainstreet and Shady Oak Road intersection have a separate left-turn lane with at least 75 feet of storage. The through and right turn movements can be shared on one lane. It is also recommended that the westbound approach be restriped to allow a separate left-turn lane and a through right lane.
- The left-turn lane on the eastbound approach to the intersection of Mainstreet is anticipated to have a short queue of one car length on the average and occasionally this might grow to three cars long. The through-right movement is anticipated to have a queue of 1-2 cars.
- It is recommended that the access from the businesses to the new access be constructed as far away from the intersection as feasible to allow vehicles turning in and out of the access to enter and exit efficiently without being blocked by the eastbound queues on the approach.
- With the removal of the existing businesses from the project site, and due to the primary access to the apartment being from the Mainstreet and Shady Oak Road intersection, the traffic levels at the intersection of Oak Drive Lane and Shady Oak Road are expected to be lower than current levels and the intersection is anticipated to operate at an acceptable level.



**67-unit plan Concept Plan and Ordinance  
Intro. Meeting Minutes**

## PLANNING COMMISSION MAY 2, 2019 MEETING MINUTES

### A. Concept plan review for amendments to Shady Oak Crossing at 4312 Shady Oak Road.

Chair Kirk introduced the concept plan and called for the staff report.

Gordon reported. Staff recommends that planning commissioners provide comments and feedback on the identified key issues and other issues commissioners deem appropriate. The discussion is intended to assist the applicant with future direction that may lead to the preparation of more detailed development plans.

Luke asked if the setbacks on the north side would meet ordinance requirements. Gordon explained that the property is zoned as a planned unit development (PUD). The current north setback is approximately 37.5 feet. The proposal would reduce the north setback.

Chair Kirk noted that the houses in the area were built before setback ordinances were adopted. Gordon stated that the standard front setback for houses in the area is 35 feet.

In response to Henry's question, Wischnack explained that the city of Hopkins has not provided comments on the concept plan and would do so when a formal application would be submitted.

In response to Henry's question, Wischnack explained tax-increment financing.

Powers asked who would pay for the annexation and detachment. Wischnack answered that the developer would be required to provide all necessary documents.

Gordon reviewed the four areas staff would appreciate comments regarding: density, building design, site design, and traffic and circulation.

Mike Waldo, of Ron Clark Construction, applicant, reviewed the concept plan and stated that:

- He still likes the original project proposal.
- Comments were received regarding the traffic on Oak Drive Lane.
- He spent time with the land owner on the south and purchased part of the property. The property owner lost a lot of parking space from the street improvement project. The current purchase agreement is for 17,000 square feet.
- The proposal should reduce the traffic on Oak Drive Lane by 80 percent to 90 percent. There would be 90 stalls down below.
- The current proposal is a better project for the applicant, the neighbors, the city, and would provide 18 workforce-housing residences.
- There is no way to have no parking off of Oak Drive Lane. There is an elevation change of 11 feet and would not work for trucks to access and exit the site.
- The entrance to the parking lot was moved farther west to provide more



- room for stacking on Oak Drive Lane.
- He spoke with adjacent neighbors about providing landscaping for screening.
- There would be more area for the tot lot and play area.
- The applicant would agree to reduce the parking area if staff would be comfortable with that.
- The 37-foot setback was reduced to a 20-foot setback.
- The applicant thought that a soft-pitch roof would look more residential than a flat roof. He requested commissioners provide their comments.

Tim Whitten, architect with Whitten and Associates, on behalf of the applicant, gave a presentation on the concept plan:

- He described the drive area and traffic flow.
- He provided slides of the concept plan with and without landscaping.
- He described the roof provided in the concept plan.
- The building would be closer to the sidewalk than the previous proposal to allow for the turning radius of trucks.
- Grading could occur up to the first floor and would be able to have landscaping.
- On the northwest corner, there would be a two-story component that would move into the tot lot area and main entrance for visitors.
- The southwest corner would preserve a lot of trees.
- The exterior materials would be brick and made up of a cement board panel system. He explained the horizontal and vertical components.
- The elevation on the north end is one of his favorites. There would be a flat, bungalow-like feel.
- He is proud of how all of the pieces have been brought together and is excited about the opportunity.

In response to Henry's question, Mr. Whitten explained the evolution of the changes to the roof.

Luke thought the traffic pattern would be better with a signaled intersection for the entrance. She asked about walkability and where sidewalks and entrances to the building would be located. Mr. Waldo stated that there would be a sidewalk around the entire building. Mr. Whitten pointed out the patio area and entrances.

Henry asked if having the tot lot closer to the building had been discussed. Mr. Waldo said that it was determined that having the connection to the main access on Shady Oak Road was more of a priority. The play area would be fenced in. He could see positives and negatives with both scenarios.

Powers applauded the concept. It is an improvement from the 49-unit apartment building. He was amazed how the applicant tried to please everyone by making the south end look more urban and the north end residential. He asked if he understood correctly that there would be more room for buffering on the Oak Drive Lane side. Mr. Waldo answered affirmatively. He would work with staff. The applicant is committed to doing better projects. The applicant knew that was an important factor.

Knight stated that he likes the balconies. Mr. Whitten stated that the balconies would be solid aluminum and last as long as the apartment building.

Henry discussed buffering options for neighbors on Oak Drive Lane with Mr. Waldo. Mr. Waldo stated that the applicant would be happy to meet with the neighbor to see what the neighbor would prefer for buffering.

Chair Kirk invited those present to provide comments.

Chris Aanestad, 4255 Oak Drive Lane, stated that:

- He questioned how long the project would take.
- He appreciated the proposal moving the access off of Oak Drive Lane.
- The building would be too large. He did not understand how the building was shown on the plan.

Cynthia Jung, 18505 Spring Crest Drive, stated that:

- She represented the Minnetonka Housing Team. The Minnetonka Housing Team supports adding more units of affordable housing.
- They support the proposal.

Chair Kirk concluded receiving public comments.

Chair Kirk discussed with Mr. Waldo and Mr. Whitten how it is difficult to show the different elevations and scale of the building on a screen.

Chair Kirk stated that the building is too long to keep it to scale on the screen and it is difficult to represent a building that has an elevation change because it is distorted.

Wischnack explained that the city has a contract with the applicant. If tax credits would be awarded this year, then construction would begin in 2020. Relocation of businesses could occur in 2019.

Gordon provided that the 2017 plan proposed 31 units per acre and this concept plan has 33 units per acre.

Knight likes the concept plan. The building looks nice. He likes the change in the roofline. That improves the look significantly. He remembered residents on the south concerned with motorists cutting through their neighborhood. He asked if the proposal would create a cut through. Gordon recalled that a resident was concerned with motorists using Bradford Street to cross southbound traffic to make a left-hand turn onto Excelsior Blvd. to travel east or north on Shady Oak Road. Gordon did not see the concept plan creating that type of circumstance. Mr. Waldo described the traffic pattern for semis with trailers.

Powers liked everything about the new concept plan. He liked the roof lines. He liked how the developer worked so diligently to make this work. Work force housing is needed now. The sooner this is approved and started the happier he will be. The area would be



wonderful for kids. He liked the architect thinking of the change in exterior colors and materials.

Luke liked the concept plan more than the previous proposal. The building would be attractive. She liked how traffic would be managed on the south side of the building instead of accessing Oak Drive Lane. She thought the 31 parking stalls looked like a lot of parking.

Henry felt that the type of housing is needed in the city. He liked how there would be more of a buffer between the proposed apartment building and residence on the west side. The building would be massive. Lowering the building to two levels sooner on the south side like it is on the north would make it more visually appealing. He would like to see how much sun would be blocked by the building.

Chair Kirk supports affordable housing. He voted no for the previous proposal because he wanted to see it improved. Moving the access to the south was key. He was happy to see that happen. He suggested aligning the road to prevent headlight wash. Mr. Waldo explained that the parking lot would be sloped down so a vehicle would travel with headlights pointed down.

Chair Kirk asked if a playground could be located over a sewer pipe. Gordon stated that would be looked at. The concept plan provides more flexibility to move the playground around. Chair Kirk suggested moving the playground further south and aligned with the backyard of 4292 Oak Drive Lane. He would like proof of parking. He looks forward to seeing a clear landscaping plan with trees, sidewalks, and snow storage. The building is still too close to the road. He was fine with the roofline. He hoped the number of units could be condensed a little to help condense the setback from Oak Drive Lane.

Powers stated that he visited residents of Oak Drive Lane and most of them did not care about the size of the building, except for the closest neighbors. It is 2020 and density is going to increase. He sees buildings in suburbs twice this size. The density is appropriate. The building is appropriately sized and takes into account that the city needs affordable housing now. This type of opportunity, for this type of parcel, along a major street, that has already been developed, is rare. The site's proximity to amenities makes it even rarer. He favored keeping the density as it is.

Luke felt that it would be nice to have an apartment building near single-family houses instead of an industrial area. That would make the apartments more attractive and keep the workforce housing near other workforce housing. The site would be accessible to amenities including the grocery store. It would be a very good location.

Chair Kirk noted that the owners of surrounding businesses would welcome the residents. The site is ready for redevelopment.

Henry would like the playground area moved closer to the main entrance if it could be done without ending up in a completely shaded area.

## Unapproved City Council May 20, 2019 Meeting Minutes

### 14. Other Business:

#### A. Concept plan review for the Shady Oak Redevelopment located at 4312 Shady Oak Road.

Gordon gave the staff report.

Happe thought the structure would be massive for the area. Three stories seems high. He asked how many units would be in the third story. Gordon estimated 14 units in the third story. The proposed building on the concept plan has a total of 67 units.

Schack noted that using the adjacent lot for drainage was discussed in 2017. She asked if that is part of the concept plan. Gordon answered affirmatively. The adjacent lot on the west is owned by the city and would be used for stormwater treatment.

In response to Wiersum's question, Wischnack explained that once the city receives a formal application, the next steps would include detachment and annexation of the Hopkins property, land use approvals, and establishment of TIF funding. The approval of the previous application would remain in place if a new application would not move forward.

Mike Waldo, of Ron Clark Construction, who submitted the concept plan, stated that:

- The previously approved project for the site is very good, but there was concern with increasing traffic on Oak Drive Lane.
- The previous project did not meet the threshold for median cost after the removal of five units, addition of a flat roof, and other changes were made that increased cost-containment issues.
- The concept plan is pretty good. The previous plan could be modified to save costs, but he thinks the current concept plan would be better.
- The concept plan would improve the traffic on Oak Drive Lane by 80 percent to 90 percent compared to the previously approved plan.
- There would be 90 underground parking stalls.
- He wants to keep 31 outdoor parking stalls. A few outdoor stalls could be made into green space now and kept as proof of parking.
- He was confident the proposal would have 18 affordable units and provide a more efficient use of resources.
- Hopkins staff are supportive of the concept plan.

Tim Whitten, of Whitten Associates, architects for the concept plan, stated that:

- The building was made larger in the concept plan than the previously-approved plan. He described the floor plans and shape of the building.
- The access to structured parking would connect to Main Street.
- He explained the loading dock area and traffic pattern.
- The access being removed from Oak Drive Lane would allow more landscaping and a raised grade.



## Unapproved City Council Minutes

Meeting of May 20, 2019

- There would be three access points to the trail on Shady Oak Road.
- He provided renderings of the building and elevations from each direction.
- The need for stacking was reduced with the guest-housing component.
- He could see how either project would work.

Mr. Waldo clarified that the top floor of the concept plan contains 20 units. Two-story apartments do not work, so that could not be done. The building would provide a transition from commercial to residential areas.

Ellingson asked how steep the grade would be on the access from Main Street down to the structured parking. Mr. Whitten estimated nine percent. The city has a slope maximum of 10 percent. The concept plan building length from north to south is 412 feet.

In response to Carter's comment, Mr. Waldo answered that the play area would be fenced in.

Happe asked if there are other three-story buildings in the area. Gordon answered that most of the buildings in the area on Shady Oak Road are single-story commercial or two-story residential.

Wiersum asked for the estimated height of the building. Mr. Waldo stated that the eave height would be a little higher, but the roof would be the same height as the building in the project previously approved.

Wiersum would appreciate an example of a building of comparable size be provided with an application. Wischnack explained that there is a chart that shows the size of every building in the city. The Ridge has four stories instead of three stories, but, otherwise, is similar in size to the building in the concept plan.

Calvert asked for the number of units in Applewood Pointe. Wischnack responded 84 units. She noted that Applewood Pointe is a cooperative so the square foot per unit is much larger than an apartment.

Wiersum invited those present to comment. No one chose to speak.

Wiersum likes the change to the access location, but the change increased the cost of the project and, in turn, caused an increase in the size of the building. That is the tradeoff.

Schack stated that:

- The concept plan is better than the previously approved project.
- The access being located on Oak Drive Lane was her biggest reservation when she reviewed the application in 2017 as a planning commissioner. Now the traffic concern on Oak Drive Lane has been resolved.
- Another huge benefit of the current concept plan compared to the project from 2017 is that councilmembers know that lite rail is coming and the concept plan would provide access to the lite rail.

## Unapproved City Council Minutes

Meeting of May 20, 2019

- She did not think the proposed amount of parking would be needed. The public transportation options for the area will be increasing. She was comfortable with the proof of parking proposed in the second option.
- The second option also places the playground in a better location.

Calvert agreed with Schack that the concept plan is a dramatic improvement. She stated that:

- The hip roof is a wonderful architectural detail that does not add a great deal of height.
- She was flexible with the playground's location knowing that it would be fenced in. She suggested placing it in an area that would receive sunlight.
- She was excited for the access to be located on Main Street, the parking area being moved away from neighbors and that there would be more room for landscaping.
- She did not have a problem with the mass of the building.
- She supports additional workforce housing, especially units with one, two, and three bedrooms. That is very rare.
- She likes the architectural details that break up the mass.
- The building in the concept plan would be at least one story shorter, have a flatter roof than Applewood Pointe, and would be more compact since it would have fewer amenities. The number of units does not bother her in this location.

Bergstedt stated that:

- He appreciated the project having an excellent developer. The developer did a great job making the building in the concept plan look more urban, similar to row houses, and stepped down in height on both ends as well as moving the access off of Oak Drive Lane.
- Lining up the access at a signaled intersection on Main Street would make the concept plan a thousand times better and safer than the previously approved project.
- He preferred proof of parking.
- He liked the play area moved to the east. It would be closer to the units rather than being on the outside.
- The hip roof is nice and provides a softer feel.
- The concept plan provides more workforce housing which the city needs.
- He saw a lot of positives.
- The building would be very long, but, with the applicant having to purchase the lot to change the access, he acknowledged that the concept plan would not work without the additional units.

Happe stated that:

- The concept plan is very good.
- He agreed with councilmembers.



## Unapproved City Council Minutes

Meeting of May 20, 2019

- The building would be too big. The building would be close to Shady Oak Road and three stories high. He thought it would tower over motorists on Shady Oak Road and be too large for the area.
- The layout, design, and concept plan look great, but the setback would not be far enough from the street in relation to the size of the building.

Ellingson stated that:

- He agrees with the observations.
- The access change is a necessary improvement.
- He was concerned with the size of the building and lack of setback from the street. It would not fit in the area very well. The building would be very large compared to the others on the street.

Calvert supports making sure motorists would be able to safely turn left onto Oak Drive Lane and that headlights would not shine on the residence across the street.

Wiersum stated that:

- The concept plan is better than the previously-approved plan due to the change in the access location to underground parking. That is a big and expensive deal.
- The concept plan is much better. It works.
- He was concerned with the mass of the building. The building would be 412 feet long. The design works well. He understood the concern with the height and mass. There are three-story and four-story buildings located nearby on Main Street in Hopkins. The area will become more urban as time goes on.
- Shady Oak Road is a busy street. The proposed building would act as a sound wall to muffle traffic noise. He acknowledged that it would be a big building.
- The concept plan would improve the access and provide workforce housing in a location near future lite rail. The benefits of the concept plan make a lot of sense. He supports the concept plan.
- He likes the design elements. He supports breaking up the building as much as possible. The hip roof works.
- He prefers to go with proof of parking.
- He prefers locating the play area more internally on the site.
- He likes limiting the amount of hard surface on the exterior as much as possible.

Gordon thanked councilmembers for their comments.

The mayor and councilmembers reviewed and discussed the concept plan. No formal action required.

## CITY COUNCIL JULY 8, 2019 MEETING MINUTES

### A. Items concerning Shady Oak Road Redevelopment:

- 1) Master Development Plan,
- 2) Site and Building Plan review,
- 3) Preliminary Plat,
- 4) Detachment/Annexation,
- 5) Comprehensive guide plan amendment, and
- 6) Rezoning

City Planner Loren Gordon gave the staff report.

Calvert asked what the setbacks were on Shady Oak Road. Gordon reviewed the setbacks with the council.

Happe questioned if there had been any material changes since the concept plan was reviewed on May 20. Gordon stated he would know more about this by Friday. He commented on the surface it appears the plans were very similar.

Wiersum inquired if the applicant had anything to add. The applicant had nothing to add at this time.

Schack moved. Carter seconded a motion to introduce the ordinance amending the master development plan, rezoning the property to Planned Unit Development and refer it to the planning commission. Schack, Carter, Bergstedt, Ellingson, Calvert and Wiersum voted "yes". Happe voted "no". Motion carried.



## **Misc. Development Information**

## Project Comparison

Name	Details	Stories	Height	Residential Density (Gross)	Building Size (sq. ft)	FAR	Hard surface
<b><i>Ridgedale Area Developments</i></b>							
Ridgedale Active Adult Apartments 12610 Ridgedale Dr.	168 units of age restricted (50+)	6	60 ft.	71 units/acre	187,862	1.81	80%
Ridgedale Executive Apts. 12501 Ridgedale Dr.	77-unit market rate	4	54 ft.	17 units/acre	174,060	0.78	51%
Highland Bank 1730 Plymouth Rd	Mixed use building with 16,000 sq. ft. of retail and 120 units of market rate apartments	6	76 ft.	59 units/acre	155,000	1.7	85%
Cherrywood Pointe 2004 Plymouth Rd.	100-unit senior apartments	4	55 ft.	34 units/acre	117,000	0.93	45%
Woodbine Condos 12700 Sherwood Pl	45-unit residential condominium building	3	40 ft.*	6 units/acre	90,000	0.3	
The Ridge 12708 Wayzata Blvd	64-unit affordable apartment building	4	40 ft.	37 units/acre	113,000	1.5	64%
Ridgepoint 12600 and 12800 Marion Ln W	274-unit senior apartments	8	76 ft.*	32 units/acre	283,000	0.8	
Ridgeway 1919 YMCA La	60-unit market rate apartments	3	42 ft.*	15 units/acre	68,000	0.4	
<b><i>Glen Lake Developments</i></b>							
The Exchange 14403 Excelsior Blvd	Mixed use building with 22,000 sq. ft. of retail and 52 units of market rate apartments	4	54 ft.	24 units/acre	82,000	0.9	87%
St Therese 5300 Woodhill Rd	150-unit senior housing building	5	55 ft.	56 units/acre	225,000	1.9	75%
Beacon Hill 5300 & 5330 Beacon Hill Rd	152-unit senior apartments/assisted	4		22 units/acre			
The Landing	97-unit senior apartments/assisted	5		44 units/acre			
The Atrium	83-unit condominium	3		15 units/acre			
Zvago (One Two One)	54-unit senior cooperative	4	45 ft.	22 units/acre	117,079	1.0	49%
<b><i>Recently Approved Projects</i></b>							



### Project Comparison

Dominium	482-unit affordable apartments (220 general/262 senior occupancy)	4-6	43-81 ft.	51 units/acre			60%
Carlson Island Apartments 501 Carlson Pkwy	174-unit market rate apartments	5	55 ft.	15 units/acre	188,000	0.4	21%
Crest Ridge Senior Living (Orchards at Minnetonka) 10955 Wayzata Blvd	147-units market rate senior	4	53.5 ft	21 units/acre	200,000	.65	38%
Havenwood of Minnetonka 17710 Old Excelsior	100 units (independent, assisted and memory care)	3	46-ft	40 units/acre	104,000	.95	
Applewood Pointe 12201 Minnetonka Blvd	87-unit senior cooperative building	4	50 ft.	22 units/acre	179,000	1.1	45%
Minnetonka Hills Apartment 2814 Jordan Avenue	78-unit market rate apartments	5	52.5 ft	32 units/acre (27 units/acre overall)	80,000	.99	46%
Rize at Opus 10101 Bren Rd E	322-unit (10% units at 80% ami)	6	83 ft	42 units/acre	400,000	1.15	50%
Tonka on the Creek 9731 Minnetonka Blvd	100-unit apartment building	4	54 ft.	49 units/acre	107,000	1.2	42%
The Chase (At Home Apts.)	106-unit apartment building	4	50 ft.	32 units/acre	105,040	0.74	39%

\*approximate

Name	Address	No. of Stories
Marriott Southwest	5801 Opus Pkwy	16
601 Tower	601 Carlson Pkwy	14
701 Tower	701 Carlson Pkwy	14
Cloud 9	5601 Smetana Dr	10
UHG	9900 Bren Rd W	10
UHG	9800 Health Care Ln	10

### Project Comparison

UHG	9700 Healthcare Ln	10
Crescent Ridge	10900 Wayzata Blvd	9
Crescent Ridge	11100 Wayzata Blvd	9
Ridgepoint	12600 Marion Ln W	8
Ridgepoint	12800 Marion Ln W	8
UHG	12700 Whitewater Dr	6



## **Ordinance and Resolutions**

## Ordinance No. 2020

### An ordinance amending the master development plan for a 3-story, 75-unit apartment building the property located at 4312 Shady Oak Rd.

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The City of Minnetonka Ordains:

#### Section 1.

- 1.01 On Sept. 25, 2017, the city council adopted Ordinance No. 2017-15 that rezoned the property located at 4312 Shady Oak Rd. from B-2 Commercial to PUD/Planned Unit Development.
- 1.02 On Feb. 24, 2020, the city council considered a major amendment to the master development plan for the property. The master development plan contemplated a 75 unit, 3-story apartment building.
- 1.03 City Code Section 300.22 Subd. 9 states that major amendments to an approved master development plan "...may be approved by the city council after review by the planning commission."
- 1.04 City Code Section 300.22 Subd. 1 allows for planned unit development zoning "...to provide flexibility from certain zoning and subdivision regulations in order to realize public benefits that may not otherwise be achieved through non-PUD development."
- 1.05 City Code Section 300.22 Subd. 2 states that, "...PUD zoning may be considered by the city when it would result in one of the following public benefits:
- a) Greater preservation of existing natural resources, in number or quality, than would otherwise be provided under non-PUD development;
  - b) Provision of affordable housing;
  - c) Provision of a housing type or target housing price that is desirable to the city;
  - d) A mix of land use types;



- e) Development that is compatible with existing, surrounding development type and intensity that is no longer allowed in other existing zoning districts; or
- f) Greater energy conservation through building and site design than would otherwise be achieved under non-PUD development;
- g) Other public benefits as recognized by the city.

## Section 2.

2.01 This action is based on the following findings:

- a. The rezoning to PUD would provide the following public benefits:
  - (1) A greater protection of natural resources.
    - a. The project would remove existing site contamination.
    - b. The project would incorporate stormwater management to treat surface water run-off.
    - c. The project would increase green space and landscaping on site.
  - (2) It would result in the provision of affordable housing and a housing type and price that is desirable to the city.
  - (3) As a transition property, the proposed development is compatible with the surrounding development type south along the Shady Oak Road corridor, which is represented by a mix of commercial and multi-family residential uses.
- b. The rezoning is consistent with the comprehensive guide plan, the Shady Oak Road Study, and the intent of the zoning ordinance.
- c. The rezoning is consistent with the public health, safety, and welfare.

## Section 3.

3.01 Approval is subject to the following conditions:

- a. The property must be developed and maintained in substantial conformance with the following plans:
  - 1. Subject to staff approval, Shady Oak Crossings must be developed and maintained in substantial conformance with the following plans, except as modified by the conditions below:

- Preliminary Plat, dated Dec. 2, 2019
- Site Plan, dated Dec. 2, 2019
- Grading Plan, dated Dec. 2, 2019
- Utility Plan, dated Dec. 2, 2019
- Tree Preservation Plan, Dec. 2, 2019
- Landscape Plan, dated June 19, 2019
- SWPPP, Dec. 2, 2019
- Architectural Plans, received Dec. 2, 2019

The plans outlined above constitute the master development plan for the subject properties.

2. Development must further comply with all conditions outlined in City Council Resolution No. 2020-xx, approving the final site and building plans.
3. This ordinance is contingent on detachment/annexation approval.

Section 4. A violation of this ordinance is subject to the penalties and provisions of Chapter XIII of the city code.

Section 5. This ordinance is effective immediately.

Adopted by the city council of the City of Minnetonka, Minnesota, on Feb. 24, 2020.

---

Brad Wiersum, Mayor

Attest:

---

Becky Koosman, City Clerk

**Action on this Ordinance:**

Date of introduction: Jan. 6, 2020

Date of adoption:

Motion for adoption:

Seconded by:



Voted in favor of:  
Voted against:  
Abstained:  
Absent:  
Ordinance adopted.

Date of publication:

**Certified Copy:**

I certify that the foregoing is a true and correct copy of an ordinance adopted by the city council of the City of Minnetonka, Minnesota, at a meeting held on Feb. 24, 2020.

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Becky Koosman, City Clerk

## EXHIBIT A

That part of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West, Hennepin County, Minnesota being described as follows:

All that portion of the tract or parcel of land described as Paragraph "A" below, which lies Southerly of a line drawn parallel to and 200 feet Southerly of the North line thereof and the same extended, to wit:

Paragraph "A"; That portion of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West of the 5th Principal Meridian, described as follows: Starting at the Northeast corner of the Southwest Quarter of the Southeast Quarter of said Section, thence South along the East line of the West half of the Southeast Quarter of said Section, a distance of 300 feet, thence Westerly at right angles to said East line for a distance of 284 feet; thence Northerly along a line parallel to said East line a distance of 600 feet; thence Easterly along a line at right angles to said East line 209 feet to the center line of McGinty Road; thence Southeasterly along the center line of McGinty Road to the East line of the West Half of the Southeast Quarter of said Section 23; thence Southerly along said East line 33.5 feet to the point of beginning.

Which are described as follows:

Parcel A: The West 109.00 feet of the North 139.00 feet.

Parcel B: That part of the North 158.00 feet thereof lying East of the West 109.00 feet thereof.

Together with an easement for driveway and parking purposes over, under and cross that part of Lot 22, Block 1, Oak Ridge 2nd Addition, lying Northeasterly of a line, and its extensions, drawn from the Southeast corner of said lot to a point on the Northwesternly line of said lot distant 120 feet Southwesterly, measured along said Northwesternly line, from the most Northerly corner of said lot, as contained in deed Doc. No. 966456; (See Order Doc. No. 1053666)

Which lies northeasterly of the following described line:

Commencing at the northeast corner of Lot 23, Block 1, OAK RIDGE 2ND ADDITION, according to the recorded plat thereof, Hennepin County, Minnesota; thence South 03 degrees 27 minutes 33 seconds West, along the east line of said Lot 23, a distance of 10.00 feet to the point of beginning of the line to be described; thence South 86 degrees 32 minutes 27 seconds East a distance of 23.56 feet to the point of beginning of the line to be described; thence South 03 degrees 27 minutes 33 seconds West a distance of 18.99 feet; thence southeasterly 113.10 feet along a tangential curve concave to the northeast having a radius of 70.00 feet and central angle of 92



degrees 34 minutes 30 seconds; thence South 89 degrees 06 minutes 57 seconds East a distance of 40.85 feet; thence South 78 degrees 08 minutes 08 seconds East a distance of 140.43 feet, more or less, to the east line of said West Half of the Southeast Quarter and there terminating.

**Resolution No. 2020-**

**Resolution approving a preliminary plat and final site and building plans for Shady Oak Crossings located at 4292 Oak Drive Lane, 4312 Shady Oak Road and a portion the property located at 2 Shady Oak Rd.**

---

Be it resolved by the City Council of the City of Minnetonka, Minnesota, as follows:

Section 1. Background.

1.01 Ron Clark Construction and Design has requested approval of preliminary plats and final site and building plans for a rental apartment building.

1.02 The properties are located at 4292 Oak Drive Lane, 4312 Shady Oak Road and a portion the property located at 2 Shady Oak Rd. They are legally described on EXHIBIT A of this resolution.

1.03 On Jan. 16, 2020, the planning commission held a hearing on the proposal. The applicant was provided the opportunity to present information to the commission. The commission considered all of the comments received and the staff report, which are incorporated by reference into this resolution. The commission recommended that the city council approve the preliminary plat and final site and building plans.

Section 2. Preliminary Plat Standards and Findings.

2.01 City Code §400.030 outlines design standards for residential subdivisions. These requirements are incorporated by reference into this resolution.

2.02 The proposed preliminary plats meet the design requirements as outlined in City Code §400.030.

Section 3. Site Plan Standards and Findings.

3.01 City Code §300.27, Subd. 5, outlines several items that must be considered in the evaluation of site and building plans. Those items are incorporated by reference into this resolution.

3.02 The proposal would meet site and building plan standards outlined in the City



## Code §300.27, Subd.5.

1. The proposal would result in a high-density residential development consistent with the site's rezoning and comprehensive guide plan. Further, the proposal has been reviewed by city planning, public works, engineering, and natural resources staff and found to be generally consistent with the city's development guides, including the water resources management plan.
2. The proposed building and parking lot would be appropriately located with reference to both existing constructed and natural features. As proposed a three-story building, with underground parking, is architecturally attractive and fits in the context of the Shady Oak Road commercial corridor. The building would be faced with brick and composite materials.
3. The proposal would visually and physically alter the site and corridor by removing the current one-story commercial building for a 3-story residential building. However, redevelopment of the subject property would clean up a blighted and contaminated property, improve surface water management and treatment, increase green space and landscaping and reduce the potential for other nuisance issues such as lighting, noise and odor that may be generated by a commercial property.

## Section 4. City Council Action.

4.01 The above-described preliminary plat and site and building plans are hereby approved subject to the following conditions:

1. Subject to staff approval, Shady Oak Crossings must be developed and maintained in substantial conformance with the following plans, except as modified by the conditions below:
  - Preliminary Plat, dated Dec. 2, 2019
  - Site Plan, dated Dec. 2, 2019
  - Grading Plan, dated Dec. 2, 2019
  - Utility Plan, dated Dec. 2, 2019
  - Tree Preservation Plan, Dec. 2, 2019
  - Landscape Plan, dated June 19, 2019
  - SWPPP, Dec. 2, 2019
  - Architectural Plans, received Dec. 2, 2019
2. Prior to issuance of a building permit:
  - a) Submit a final plat for approval and recording,
  - b) Submit the following for staff review and approval:
    - 1) An electronic PDF copy of all required plans and specifications.

- 2) One full size set of construction drawings and project specifications.
- 3) Items associated with site work:
  - a. Final site, grading, stormwater management, utility, landscape, tree mitigation, and natural resource protection plans, and a stormwater pollution prevention plan (SWPPP) for staff approval.
    1. Final site plan:
      - Confirm sidewalk connection on north side of the building to Shady Oak Road meets ADA.
    2. Final stormwater management plan must meet the requirements of the city's Water Resources Management Plan, Appendix A. Design. The plan must include a narrative, impervious surface information, soil boring data, and modeling demonstrating rate control and water quality treatment.
    3. Final utility plan must include:
      - Water Service.
        - Provide water service separate from hydrant lead. Cut in new tee with 3 valves (1 on each leg).
        - Salvage and deliver hydrant and valve to public works. Remove tee and add pipe to extend beyond sidewalk, add valve and hydrant.
      - Sanitary Sewer.
        - Confirm adequacy of 6-inch sewer lateral. If insufficient, relocate service lateral to manhole in driveway entrance at north side of the site, core drill into manhole and install inside drop. The old sewer lateral would need to be removed back to the DIP section and plugged.



- Keep trees out of easement. Smaller shrubs permissible.
- Show sanitary sewer service to 4292 Oak Drive Lane to avoid potential conflict with storm sewer installation.
- Developer to provide maintenance agreement that states property owner is responsible for replacing parking lot section if the city or county needs to perform maintenance on the underlying utilities.
- All sanitary sewer within the property must maintain a minimum of 2 percent grade. All PVC piping material must be schedule 40 minimum.
- Coordinate with public works during sewer lateral work for confirming trunk sewer line.
- Stormwater.
  - A MPCA NPDES permit is required.
  - Submit a stormwater maintenance agreement in the city approved format for review and approval of city staff.
  - Add note to access road: 12' wide infiltration basin access (compacted to 95% density constructed with structural material, finished with 4" topsoil and seeded)
  - Locate the access road within the project parcel or the portion of land from Hopkins being developed with the project. Do not cross 4401 Crawford Road as currently depicted.
  - Add note to access road: 12' wide infiltration basin access (compacted

- to 95% density constructed with structural material, finished with 4" topsoil and seeded)
- Label EOF on grading plan.
  - Relocate CBMH 3 to mid-point or northwest corner of 4 stall parking area so that the structure is not atop the county storm sewer line. Confirm clearance over 27" storm pipe. Adjust D&U easement as needed to accommodate.
  - Note that unused sewer services will be removed back to the main and the wye will be cut out and sleeved.
  - Final stormwater management plan is required for the entire site's impervious surface. The plan must demonstrate conformance with the following criteria:
    - Rate: limit peak runoff to that of the existing conditions from the 2-, 10-, and 100-year events at all points where stormwater leaves the site.
    - Volume: provide for onsite retention of 1-inch of runoff from the entire site's impervious surface.
    - Quality: provide for runoff to be treated to at least 60 percent total phosphorus annual removal efficiency and 90 percent total suspended solid annual removal efficiency.
  - If the proposed 373 ft. of HDPE pipe is within 10 ft. of the building, the pipe must be tested per 2015 MN plumbing code.
  - A Nine Mile Creek permit will be required for this project.



- Streets.
  - Confirm with Hennepin County if ROW permits will be required.
- 4. Final landscaping and tree mitigation plans must:
  - Overstory and evergreen trees cannot be planted within the utility or sanitary easement line.
  - Landscaping plan must meet minimum landscaping and mitigation requirements as outlined in ordinance. Only small shrubs, perennials, and grasses may be located in public easements.
  - Include information relating to species, sizes, quantities, locations and landscape values.
  - Include pollinator-friendly species.
  - Stormwater pond be planted with a native seed mix.
  - Thirteen maples are specified, substitute 5 of the maples with a different genus. Code is satisfied but a little more diversity is desirable.
  - The proposed grading will still result in the loss of tree 4822. Adjust the grading and the outlet pipe to preserve the tree. The grading associated with the pond construction can be no closer than 15' to the tree; the plan shows 14-feet which equates to about 35% impact to the critical root zone. The grading associated with the storm pipe outlet cannot be located within the critical root zone of the tree (CRZ is a 46' radius).
- 5. Wetlands.
  - Provide a 16.5-foot buffer with conservation easement.

- Meet the watershed district rules.
- b. A sequencing plan for review and approval of the city engineer. The plan must notate the series of construction events that will occur involving driveway construction and sanitary sewer and water main connections and disconnections. The number of events in which disturbances to the street and utilities occur must be minimized. For example, multiple crews may be required to disconnect water services simultaneously.
  - c. Title evidence must include all parcels included in the plat.
  - d. Provide cross access easement with the commercial property to the south.
  - e. The following documents for the review and approval of the city attorney:
    1. Development agreement.
    2. Stormwater maintenance agreements over all stormwater facilities. This agreement must state that the city will not maintain private structures within public easements.
    3. A private fire hydrant maintenance agreement.
  - f. Proof of subdivision registration and transfer of NPDES permit.
  - g. Evidence of closure/capping of any existing wells, septic systems, and removal of any existing fuel oil tanks.
  - h. A construction management plan. The plan must be in a city approved format and must outline minimum site management practices and penalties for non-compliance.
  - i. Individual letters of credit or cash escrow for 125% of a bid cost or 150% of an estimated cost to construct parking lot and utility improvements, comply with grading permit, tree mitigation requirements, landscaping requirements, and to



restore the site. One itemized letter of credit is permissible, if approved by staff. The city will not fully release the letters of credit or cash escrow until:

1. A final as-built survey has been submitted;
  2. An electronic CAD file or certified as-built drawings for public infrastructure in microstation or DXF and PDF format have been submitted;
  3. Vegetated ground cover has been established; and
  4. Required landscaping or vegetation has survived one full growing season.
- j. Cash escrow in an amount to be determined by city staff. This escrow must be accompanied by a document prepared by the city attorney and signed by the builder and property owner. Through this document the builder and property owner will acknowledge:
1. The property will be brought into compliance within 48 hours of notification of a violation of the construction management plan, other conditions of approval, or city code standards; and
  2. If compliance is not achieved, the city will use any or all of the escrow dollars to correct any erosion or grading problems.
- k. Any required administration and engineering fees.
- l. Park dedication fees in the amount of \$375,000. City staff is authorized to reduce this amount commensurate with the cost of qualified public improvements.
- 4) Items associated with building work:
- a. A final material and color palate board for staff review and approval.
  - b. All required hook-up fees.

- c) Obtain and submit a permit from the Minnesota Department of Health.
  - d) Obtain and submit a sanitary sewer extension permit from the Minnesota Pollution Control Agency.
  - e) Install a temporary rock driveway, erosion control, tree and wetland protection fencing and any other measures identified on the SWPPP for staff inspection. These items must be maintained throughout the course of construction.
  - f) Schedule and hold a preconstruction meeting with engineering, planning, and natural resources staff as determined by city staff.
4. The applicant may choose to submit a separate grading permit application to facilitate site work prior to issuance of a building permit. In such case, prior to issuance of a grading permit, the items outlined in preceding condition 3(a)(3) – “Items associated with site work” – must be submitted for staff review and approval and required erosion control must be installed for inspection.
  5. Retaining walls over four feet in height must be engineered.
  6. During construction the street must be kept free of debris and sediment.
  7. The property owner is responsible for replacing any required landscaping that dies.
  8. The applicant must work with the city for identification of acceptable street light fixtures.
  9. Provide a snow removal, salt and chloride management plan for staff review and approval.
  10. This resolution is contingent on detachment/annexation approval.

Adopted by the City Council of the City of Minnetonka, Minnesota, on Feb. 24, 2020.

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Brad Wiersum, Mayor

Attest:

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Becky Koosman, City Clerk



Action on this resolution:

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka, Minnesota, at a duly authorized meeting held on Feb. 24, 2020.

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Beck Koosman, City Clerk

**EXHIBIT A**

That part of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West, Hennepin County, Minnesota being described as follows:

All that portion of the tract or parcel of land described as Paragraph "A" below, which lies Southerly of a line drawn parallel to and 200 feet Southerly of the North line thereof and the same extended, to wit:

Paragraph "A"; That portion of the West Half of the Southeast Quarter of Section 23, Township 117 North, Range 22 West of the 5th Principal Meridian, described as follows: Starting at the Northeast corner of the Southwest Quarter of the Southeast Quarter of said Section, thence South along the East line of the West half of the Southeast Quarter of said Section, a distance of 300 feet, thence Westerly at right angles to said East line for a distance of 284 feet; thence Northerly along a line parallel to said East line a distance of 600 feet; thence Easterly along a line at right angles to said East line 209 feet to the center line of McGinty Road; thence Southeasterly along the center line of McGinty Road to the East line of the West Half of the Southeast Quarter of said Section 23; thence Southerly along said East line 33.5 feet to the point of beginning.

Which are described as follows:

Parcel A: The West 109.00 feet of the North 139.00 feet.

Parcel B: That part of the North 158.00 feet thereof lying East of the West 109.00 feet thereof.

Together with an easement for driveway and parking purposes over, under and cross that part of Lot 22, Block 1, Oak Ridge 2nd Addition, lying Northeasterly of a line, and its extensions, drawn from the Southeast corner of said lot to a point on the Northwesterly line of said lot distant 120 feet Southwesterly, measured along said Northwesterly line, from the most Northerly corner of said lot, as contained in deed Doc. No. 966456; (See Order Doc. No. 1053666)

Which lies northeasterly of the following described line:

Commencing at the northeast corner of Lot 23, Block 1, OAK RIDGE 2ND ADDITION, according to the recorded plat thereof, Hennepin County, Minnesota; thence South 03 degrees 27 minutes 33 seconds West, along the east line of said Lot 23, a distance of 10.00 feet to the point of beginning of the line to be described; thence South 86 degrees 32 minutes 27 seconds East a distance of 23.56 feet to the point of beginning of the line to be described; thence South 03 degrees 27 minutes 33 seconds West a distance of 18.99 feet; thence southeasterly 113.10 feet along a tangential curve concave to the northeast having a radius of 70.00 feet and central angle of 92 degrees 34 minutes 30 seconds; thence South 89 degrees 06 minutes 57 seconds East a distance of 40.85 feet; thence South 78 degrees 08 minutes 08 seconds East a distance of 140.43 feet, more or less, to the east line of said West Half of the Southeast Quarter and there terminating.



**City of Hopkins  
Resolution No.**

**City of Minnetonka  
Resolution No. 2020-**

**JOINT RESOLUTION REQUESTING CONCURRENT DETACHMENT FROM HOPKINS AND  
ANNEXATION TO MINNETONKA OF CERTAIN LANDS PURSUANT TO MINNESOTA  
STATUTES SECTION 414.061**

**WHEREAS**, certain real property, approximately 0.36 acres in size, legally described on Exhibit A attached hereto and incorporated herein and depicted on the survey thereof attached hereto and incorporated herein as Exhibit B (the “Annexation Property”) is located in the City of Minnetonka; and

**WHEREAS**, the Annexation Property abuts real estate along its southerly border that is located in the City of Minnetonka; and

**WHEREAS**, Ron Clark (“Developer”) desires to use the Annexation Property to facilitate the platting and development of a residential development; and

**WHEREAS**, the City of Hopkins, the City of Minnetonka and the Developer desire that the entire SHADY OAK CROSSINGS plat be located within the boundaries of one municipality; and

**WHEREAS**, the City of Hopkins and the City of Minnetonka desire to modify their municipal boundary in order to allow the platting and development of SHADY OAK CROSSINGS; and

**WHEREAS**, to modify the boundary the City of Hopkins desires to detach and the City of Minnetonka desires to attach the Annexation Property pursuant to Minnesota Statutes Section 414.061.

**NOW, THEREFORE, BE IT RESOLVED:**

1. The City of Hopkins and the City of Minnetonka jointly request that the Office of Administrative Hearings – Municipal Boundary Adjustment concurrently detach the Annexation Property from the City of Hopkins and annex the same to the City of Minnetonka at the earliest possible date.
2. The city clerk for each city is directed to submit this Joint Resolution to the Office of Administrative Hearings – Municipal Boundary Adjustments for the purposes set forth herein.
3. The City of Hopkins and the City of Minnetonka agree that, within 30 days of receipt of this Resolution, passed and adopted by each party, the Office of Administrative Hearings – Municipal Boundary Adjustments, may review and comment, but shall order the concurrent detachment and annexation consistent with the terms of this Resolution.

4. The City of Hopkins has designated a Comprehensive Plan Land Use Guiding of Commercial and a Zoning District designation of Commercial for the Annexation Property. Upon annexation into the City of Minnetonka, the Comprehensive Plan Land Use Guiding shall be High Density Residential and the Zoning District designation shall be planned unit development. These designations reflect the comprehensive plan land use guidance of high density residential and zoning designation of planned unit development of the adjacent property in the City of Minnetonka.

Adopted by the City Council of the City of Minnetonka on Feb. 24, 2020.

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Brad Wiersum, Mayor

**ATTEST:**

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Becky Koosman, City Clerk

**Action on this resolution:**

Motion for adoption:

Seconded by:

Voted in favor of:

Voted against:

Abstained:

Absent:

Resolution adopted.

I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Minnetonka at a meeting held on Feb. 24, 2020.

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Becky Koosman, City Clerk

(Seal)



Passed and adopted by the City Council of the City of Hopkins on the \_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Jason Gadd, Mayor

**ATTEST:**

\_\_\_\_\_  
Amy Domeier, City Clerk

**CERTIFICATION**

The undersigned, being the duly qualified Clerk of the City of Hopkins, hereby certifies the foregoing Resolution No. \_\_\_\_\_ is a true and correct copy of a resolution presented to and adopted by the Council of the City of Hopkins at a duly authorized meeting thereof held on the \_\_\_\_ day of \_\_\_\_\_, 2020, as shown by the minutes of the aforesaid meeting in possession of the undersigned.

\_\_\_\_\_  
Amy Domeier, City Clerk

(Seal)

## EXHIBIT A

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